



# RED HILL AVENUE SPECIFIC PLAN

Adopted November 6, 2018  
Ordinance 1498





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November 6, 2018  
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Prepared for:  
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# 1 EXECUTIVE SUMMARY AND INTRODUCTION

## Executive Summary and Introduction

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## 1. Executive Summary and Introduction

### 1.1 Executive Summary

The Red Hill Avenue Specific Plan (RHASP) focuses on the commercial area north and south of the existing Interstate 5 freeway in the City of Tustin. The Specific Plan area is comprised of approximately 36 acres of land (approximately 52 acres including road right-of way) adjacent to Red Hill Avenue, a northeast-to southwest-trending arterial roadway located near the geographical center of the City. Red Hill Avenue is primarily a vehicular corridor developed with a mixture of retail stores and restaurants within traditional shopping centers that are not meeting their full potential to serve the community.

The RHASP calls for a greater mix of uses within the Specific Plan area. In keeping with the aspirations of the community, greater emphasis will be placed on the pedestrian and bicyclist, in addition to the needs of the automobile and public transit, in allocating the use of space within the public rights-of-way. Adding residential uses and increasing pedestrian-oriented retail and commercial uses will help create a more dynamic, eclectic, and attractive place for both residents and visitors. A greater variety of activities along the street is encouraged through the development of either horizontal or vertical mixed-uses that reinforce pedestrian orientation. The RHASP provides for an additional 500 dwelling units and an additional 325,000 square feet of non-residential uses as a threshold of development intensity. The total development in the Specific Plan area anticipated with the buildout potential of the Specific Plan *in addition* to the existing development is 521 dwelling units and 621,446 of non-residential development.

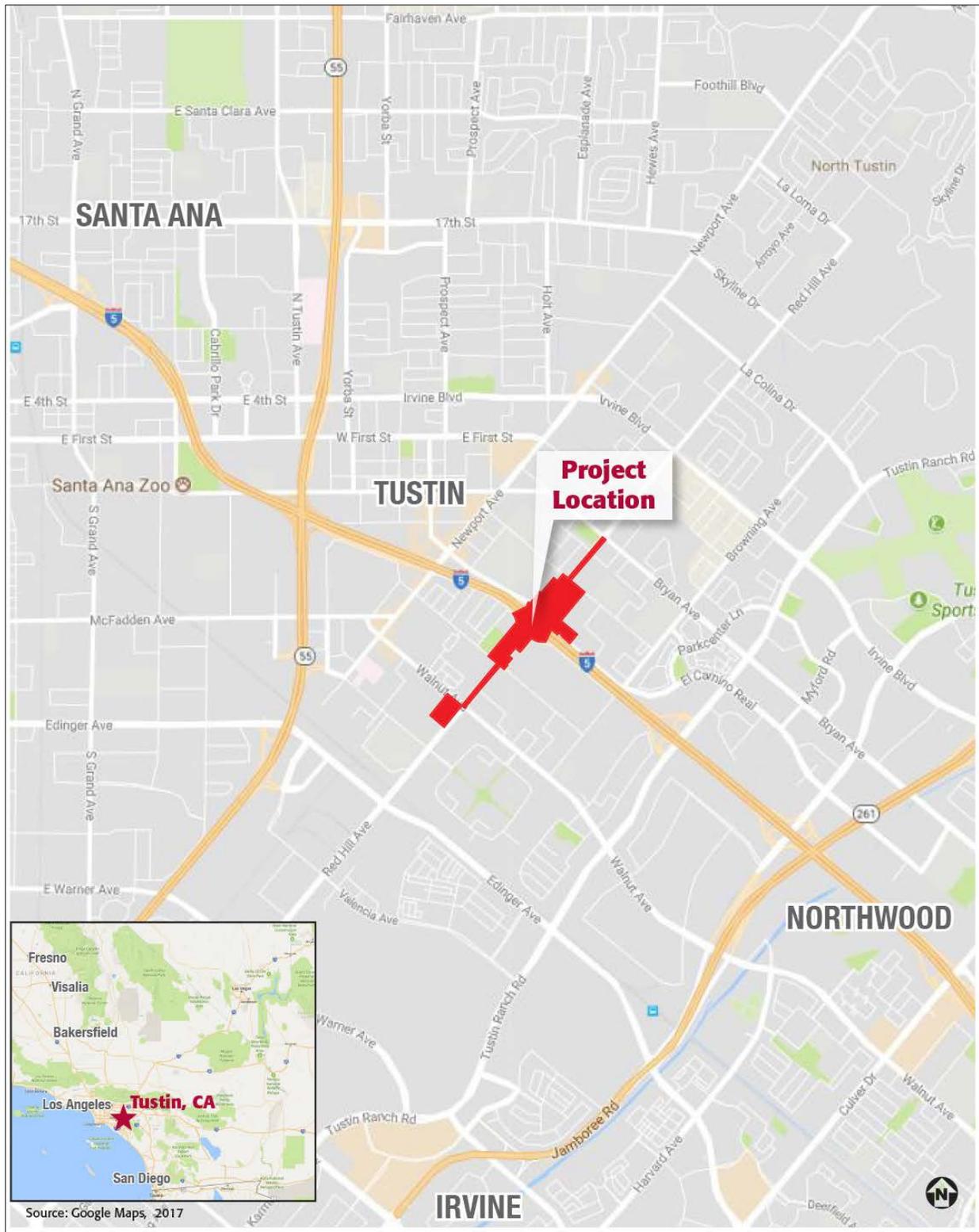
This regulatory planning document sets forth a vision with goals and objectives to achieve that vision, as well as development standards and design criteria that will become the basis for future development.

### 1.2 Overview

#### 1.2.1 Location and Setting

The City of Tustin is in central Orange County, bordered by the Cities of Irvine and Santa Ana to the south and west respectively; the City of Orange and unincorporated County of Orange areas to the north; and on the east by the City of Irvine and unincorporated County of Orange territory as illustrated in Exhibit 1-1, *Regional Vicinity Map*. The City of Tustin is located approximately two miles north of Orange County's John Wayne Airport and is transected by two major regional freeways: Interstate 5 (I-5) and State Route 55 (SR-55).

The RHASP area is located in central Tustin, east of Old Town, and southwest of the Tustin Ranch development. Exhibit 1-2, *Specific Plan Area*, illustrates the Specific Plan area, which is comprised of approximately 36 acres of land (approximately 52 acres including road right-of way). The Specific Plan area includes properties on both sides of Red Hill Avenue, a northeast- to southwest-trending arterial roadway, and is bisected by the I-5 Freeway. It extends along Red Hill Avenue to Bryan Avenue to the northeast, and generally Walnut Avenue to the southwest.



**Exhibit 1-1 Regional Vicinity Map**

# RED HILL AVENUE SPECIFIC PLAN

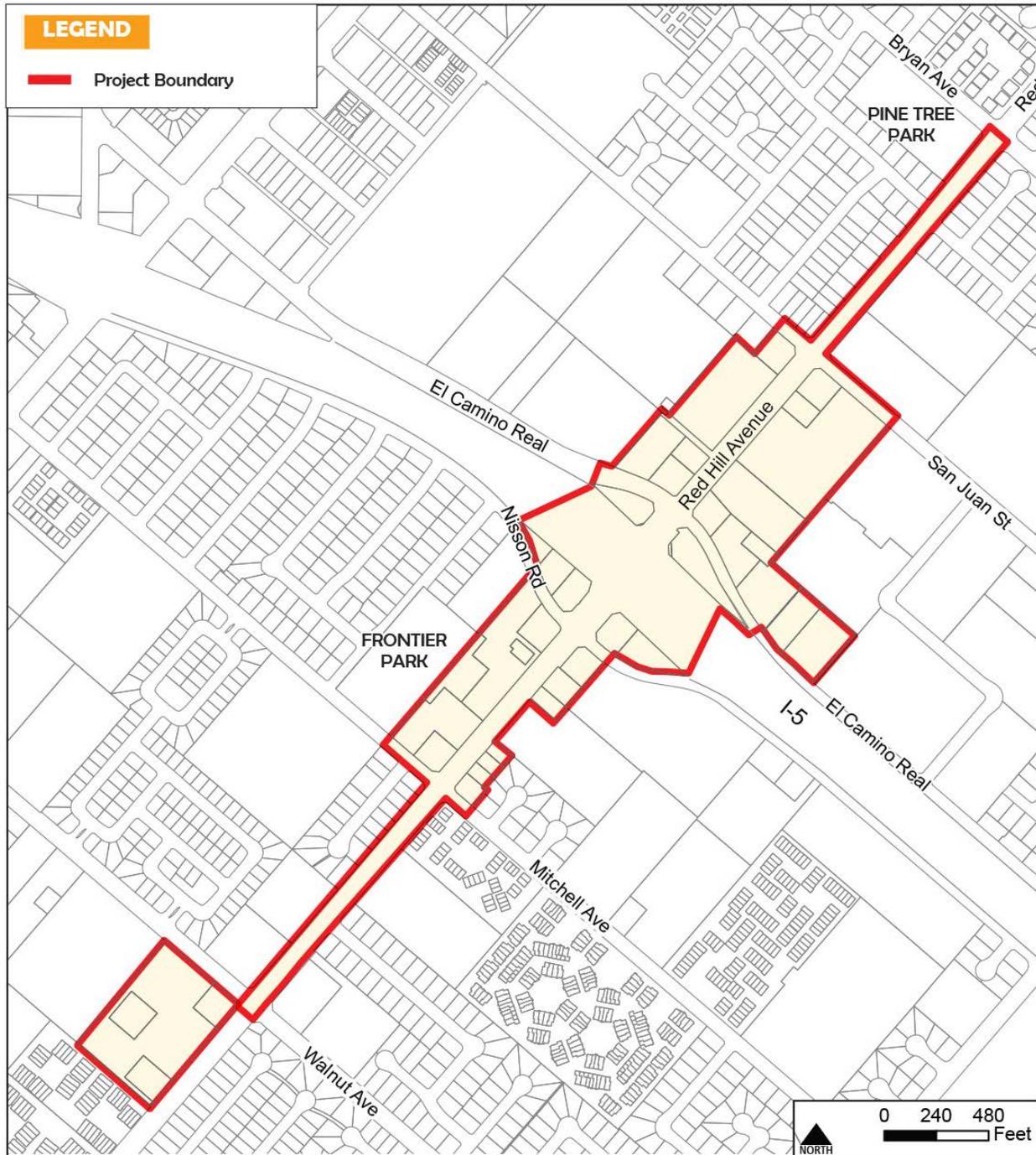


Exhibit 1-2 Specific Plan Area

## 1.3 Historical Context

Red Hill Avenue is named after a relatively well-known Tustin historical landmark—a hill only 347 feet high and 1,000 feet long with rust-colored outcroppings. American Indians called it “katuktu,” meaning “signifying hill of prominence or place of refuge” because of a legend that told of people gathering at the hill to weather an epic flood. By the time European settlement began, Mexicans had given the hill its present name, Cerro Colorado, or Red Hill. The new settlers recognized the reason for the hill’s reddish hue: cinnabar, the ore that yields mercury. Several attempts were made to mine the ore, but the hill produced too little profit to continue.

The surrounding area took its name from the hill, as did Red Hill Avenue itself. Agricultural uses also defined the character of this area of Tustin, with citrus and avocado groves in the area providing produce to large citrus association packing houses within the City.

## 1.4 Planning Context

### 1.4.1 Existing Conditions

Today, Red Hill Avenue is a major arterial roadway in Tustin, oriented northeast to southwest through the City.

The Specific Plan area, which generally extends from Bryan Avenue to the northeast and Walnut Avenue to the southwest, generally contains a mix of low intensity commercial, retail, and office uses in an auto-oriented setting. See more detail on existing land uses in the paragraphs below.

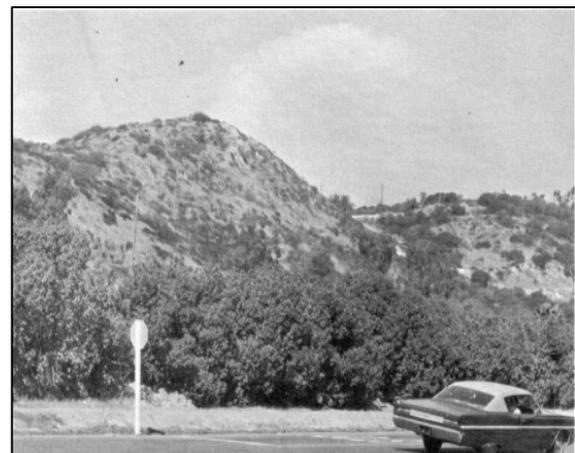
Detailed existing conditions for the Specific Plan area can be found in Appendix A: *Existing Conditions Report*.



*Tustin’s Agricultural Past*



*Historic Agricultural Uses*



*“Red Hill”*

# RED HILL AVENUE SPECIFIC PLAN

Within the Specific Plan area, Red Hill Avenue is a six-lane divided roadway with three travel lanes in each direction and a center two-way left-turn lane. A raised, landscaped median is present in the segment south of Bryan Avenue. The existing street parkways include sidewalks, with some portions containing landscaping and trees. Interstate 5 bisects the Specific Plan area creating a natural divide between the northern and southern portions of the Specific Plan area.

Existing zoning within the Specific Plan area consists of commercial and office zones: Retail Commercial (C1), Central Commercial (C2), General Commercial (CG) and Professional (PR). Existing uses include commercial retail, shopping centers, professional offices, residential, motel, and an institutional use. Many buildings are outdated, experiencing deferred maintenance, and are in need of renovation. There are also vacant parcels within the Specific Plan area.

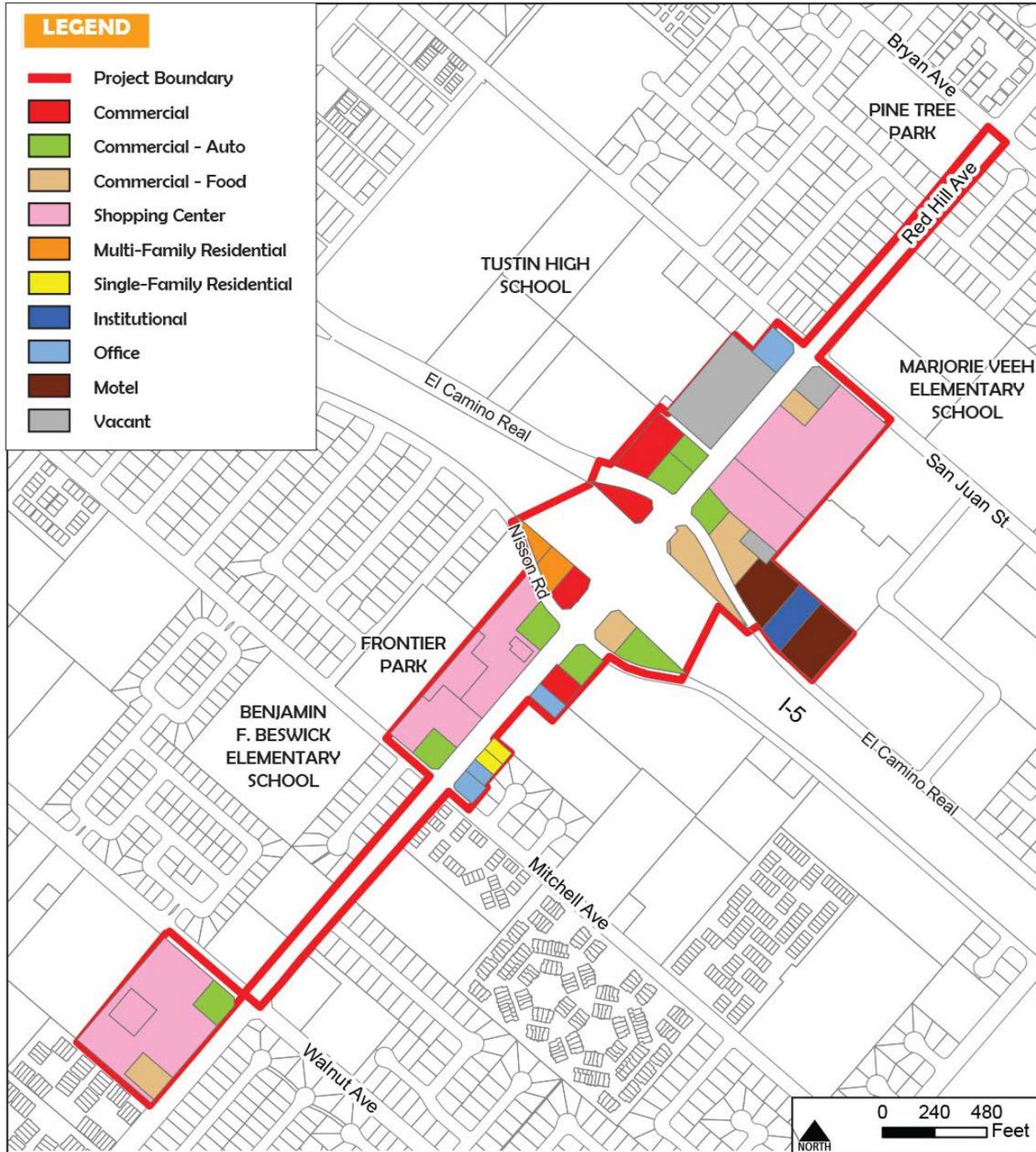
Exhibit 1-3, *Existing Land Use*, illustrates the on-the-ground land uses. Commercial and retail uses are spread throughout the Specific Plan area. There are currently non-conforming uses along Nisson Road with multi-family residential uses located on parcels zoned for commercial uses, two single-family homes north of Mitchell Avenue on parcels zoned for professional office uses, and self-storage uses on El Camino Real. Detailed information on existing zoning in the Specific Plan area can be found in *Appendix A: Existing Conditions Report*.

Land uses adjacent to the study area north of Interstate 5 are characterized by high density residential uses, Pine Tree Park at the intersection of Red Hill and Bryan Avenues, and Tustin High School to the west of the Specific Plan area. Land uses adjacent to the study area south of Interstate 5 are characterized by a mixture of low-, medium-, and high-density residential uses and Frontier Park to the west of the Specific Plan area. The Specific Plan area presently contains approximately 270,618 square feet of commercial uses, 12,633 square feet of office uses, 11,946 square feet of an institutional use, 3.65 acres of vacant property, and 21 dwelling units.



*Public transportation and pedestrian/ADA facilities in the Specific Plan area*

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 1-3 Existing Land Use**

## A. Demographics and Employment

In 2015, there was a population of 80,796 residents and approximately 26,400 households living in the City of Tustin. From 2010 to 2015, the City's population increased by 8.2 percent. By comparison, the population of Orange County increased by 3.8 percent from 2010 to 2015. Average household size in Tustin was 3.04 persons per household in 2015 (Kosmont, 2017).

Per the California Employment Development Department, the unemployment rate in Tustin for April 2016 was 3.6 percent, which is lower than the County's unemployment rate of 3.9 percent. The top job categories within the City include retail trade; professional, scientific, and technical services; and hotel accommodation and food services. According to the most recent U.S. Census Economic Study published in 2013, most City of Tustin residents (94%) work outside of the City. Approximately six percent (6%) of the working population who live in Tustin also work in the City. Detailed demographic and employment data for Tustin can be found in Appendix B: Kosmont, *Tustin Red Hill Corridor Retail Plan*.

## B. Market Demand

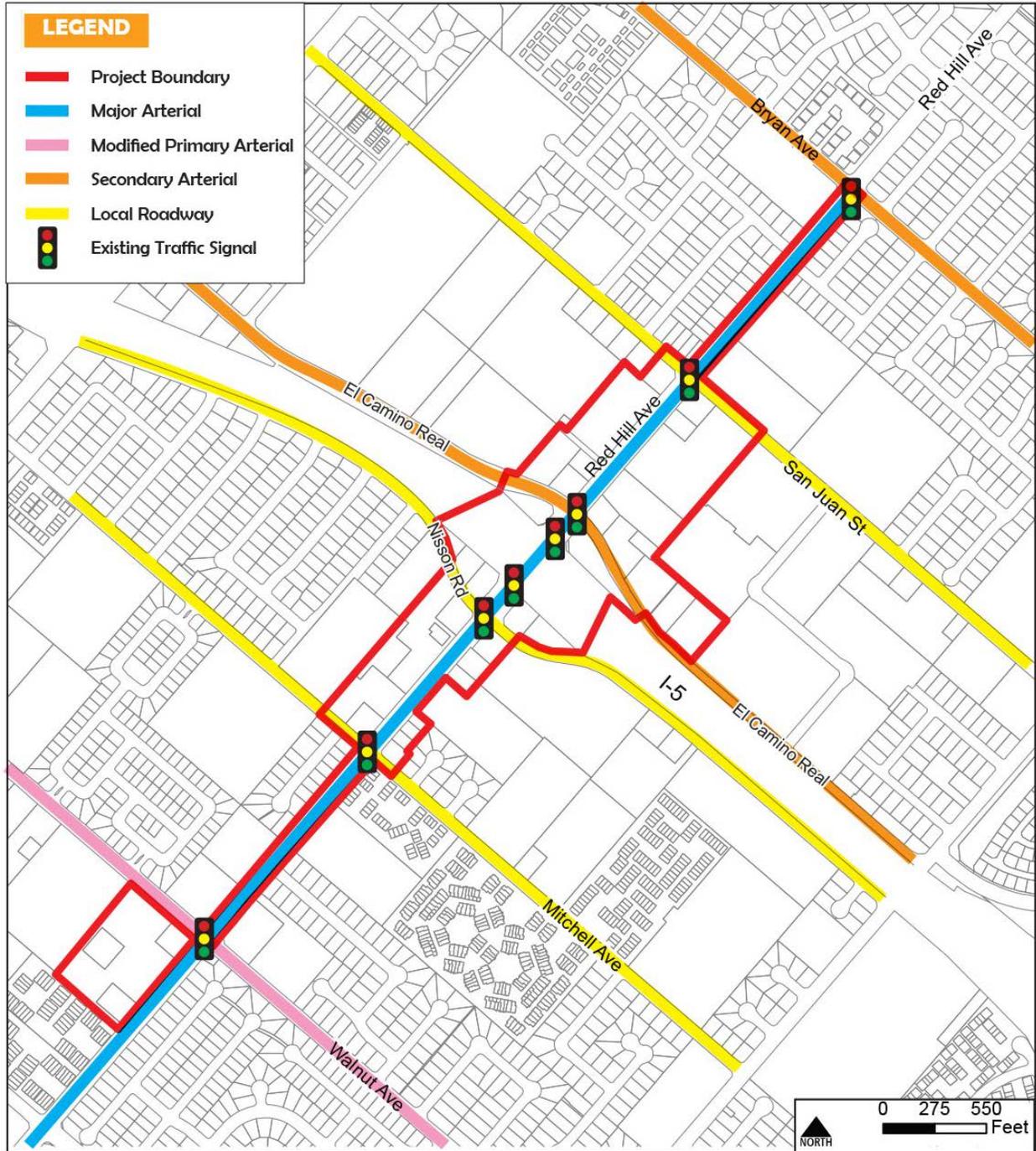
A market demand analysis was conducted for the Specific Plan area to evaluate the potential for future development opportunities. The analysis found that retail vacancy and retail lease rates in Tustin are slightly higher than the overall Orange County market average. Retail sales per capita for the City (~\$29,500) (Kosmont, 2017) is above average compared to other nearby regions and within Orange County. Higher performing sales categories include those in heavy commercial, such as, building materials, auto dealers and supplies, and gasoline stations, in addition to general merchandise stores, sporting goods, electronic stores, food and beverage stores, and restaurants. Lower performing retail categories include health and personal care, miscellaneous store retailers, and non-store retailers.

Overall retail sales in Tustin are higher than retail spending potential based on households and average household income, suggesting that, overall, the City is likely capturing a significant portion of Tustin resident retail purchases and additional retail spending by residents of other cities and unincorporated areas. Certain categories, however, are exhibiting a retail sales leakage, including: health and personal care stores, miscellaneous store retailers, grocery stores, gasoline stations, and non-store retailers. A market analysis for Tustin can be found in Appendix B: Kosmont, *Tustin Red Hill Corridor Retail Plan*.

## C. Circulation System

*Roadways.* The Specific Plan area roadways are illustrated by General Plan designation in Exhibit 1-4, *Existing Roadways*. These roadways consist of Red Hill Avenue, which is oriented in a northeast to southwest direction; and six roadways that cross Red Hill Avenue: Bryan Avenue, San Juan Street, El Camino Real, Nissan Road, Mitchell Avenue, and Walnut Avenue. The intersections of each of these streets within the Specific Plan are signalized. Interstate 5 bisects the Specific Plan area adjacent to the intersection of Red Hill Avenue/Nissan Road and Red Hill Avenue/El Camino Real. Northbound and southbound on- and off-ramps are present in a diamond style interchange configuration. Parking is currently prohibited along Red Hill Avenue except along the frontage of Pine Tree Park. There are painted medians on Red Hill Avenue throughout the Specific Plan area, and a raised, landscaped median between Bryan Avenue and Lance Drive. More detail on the existing roadways is provided in the Circulation Plan (see Chapter 3, *Development Plan*).

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 1-4 Existing Roadways**

# RED HILL AVENUE SPECIFIC PLAN

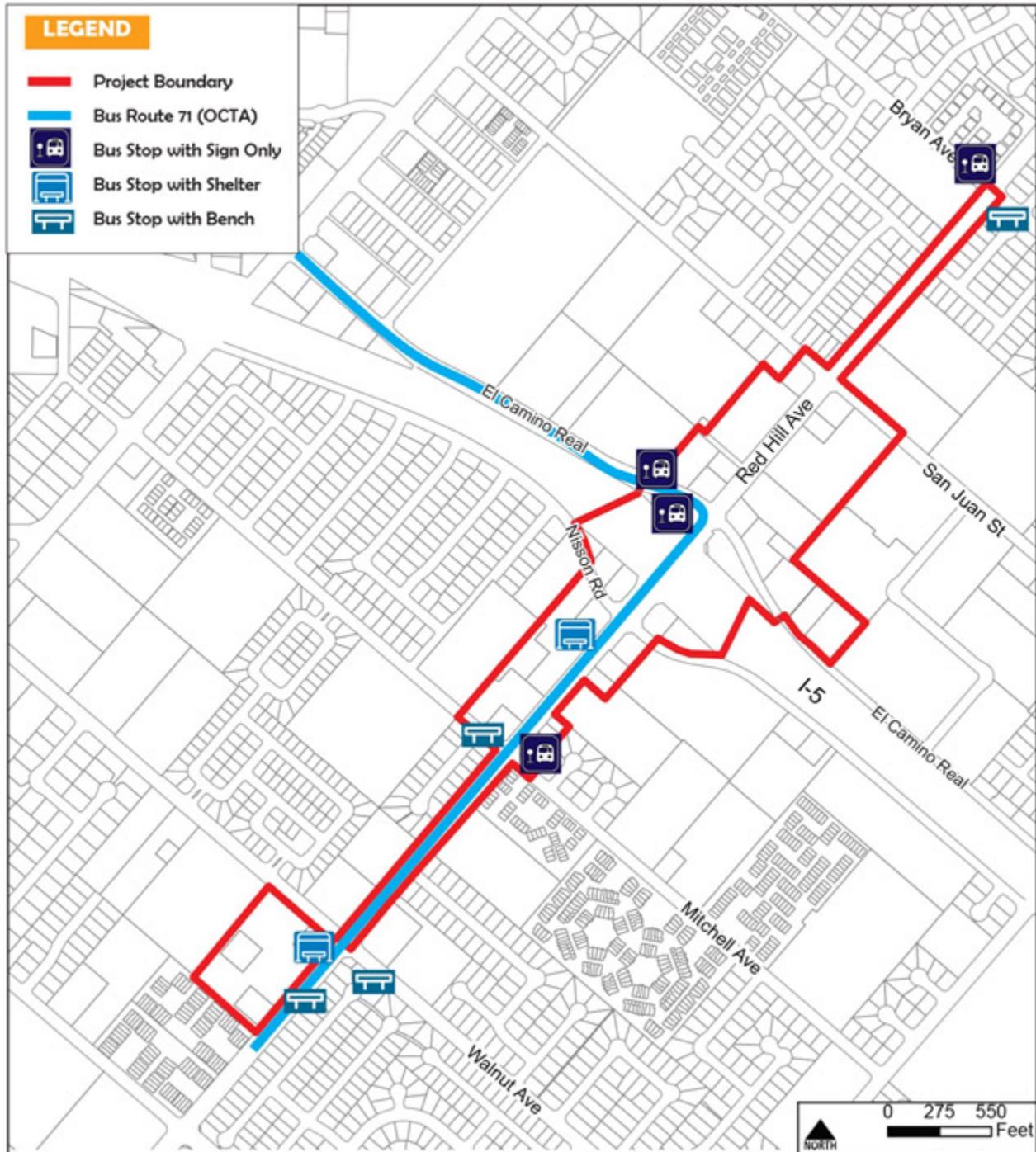


Exhibit 1-5 Public Transportation Facilities

*Transit.* The Orange County Transportation Authority (OCTA) operates local public transit services throughout Orange County, including the City of Tustin and through the Specific Plan area. Route 71 currently serves the Specific Plan area, as shown in Exhibit 1-5, *Public Transportation Facilities*. Route 71 operates between Newport Beach and Yorba Linda, traveling along Red Hill Avenue, El Camino Real, Newport Avenue, and First Street in Tustin. Service is provided daily with an approximate headway (measurement of time between vehicles in a transit system) of 50 minutes during the weekdays and 60 minutes during the weekends and holidays. Within the Specific Plan area, bus stops are provided on Red Hill in limited locations (see Exhibit 1-5, *Public Transportation Facilities*).

*Bikeways and Pedestrian Facilities.* Within the Specific Plan area, the only existing bike lane is a Class II bike lane (a striped, on-street bike lane adjacent to the travel lane) on Red Hill Avenue (in both directions), between El Camino Real and Nissan Road. Red Hill Avenue is a designated Class II bikeway between Irvine Boulevard and I-5 and a potential Class II bikeway between I-5 and First Street as shown in the City's Master Bikeway Plan. Existing pedestrian facilities within the Specific Plan area include sidewalks along all roadways and crosswalks across the signalized intersections. There are no unsignalized crosswalks across Red Hill Avenue within the Specific Plan area. A few restaurants along El Camino Real provide dedicated and landscaped pedestrian walkways from the sidewalk to the building.

## D. Urban Design

*Streetscape.* Overall the Specific Plan area is lacking in streetscape. The east side of Red Hill Avenue, north of San Juan Street, is landscaped with grass, plants, and trees adjacent to multi-family residential buildings. The west side of Red Hill Avenue, north of San Juan Street, consists of residential frontages and driveways, with minimal trees. The west side of Red Hill Avenue between San Juan Street and El Camino Real, directly adjacent to an existing vacant parcel, is landscaped with street trees; south of the vacant parcel there is limited landscaping, with several palm trees and grass in the parkway area. The east side of Red Hill Avenue between San Juan Street and El Camino Real does not have continuous streetscape. The large surface parking lot, located within Red Hill Plaza on the east side of Red Hill Avenue, north of the I-5, contains sparse trees and landscaping. Red Hill Avenue, south of the I-5, is landscaped around the perimeter of surface parking lots from I-5 to Mitchell Ave. The east and west sides of Red Hill Avenue from Mitchell Avenue to Walnut Avenue are a mixture of single- and multi-family residential uses and do not have continuous streetscape treatments. The west side of Red Hill Avenue south of Walnut Avenue to the southern boundary of the property area has evenly spaced street trees. A landscaped median is present south of Walnut Avenue (outside of the Specific Plan area).

The Specific Plan area lacks unified street furniture or other “placemaking” enhancements such as benches, lighting, bicycle racks, planters, pavement markings, etc. There are two existing bus shelters on the west side of Red Hill Avenue, south of the I-5; and two wayfinding signs located on the west side of Red Hill Avenue, north of El Camino Real, and at the OCTA bus stop on El Camino Real indicating the direction of Old Town Tustin. Generally, the Specific Plan area does not have a cohesive streetscape character and is often defined by large open areas of surface parking lots. Images on the following page illustrate the overall character of the Specific Plan area.

# RED HILL AVENUE SPECIFIC PLAN

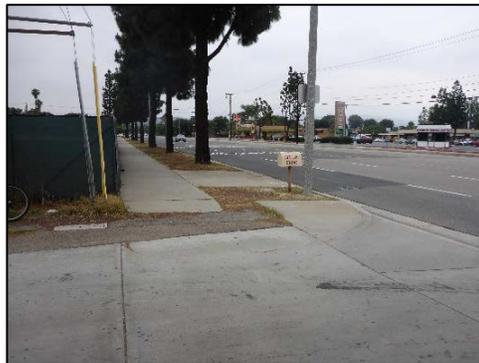
## Existing Character



*Large parking lots separating buildings and sidewalks. Many parking lots do not have consistent parking treatments or landscaping.*



*Landscape treatments along sidewalks are inconsistent. Sidewalks do not provide adequate buffers between pedestrians and vehicles.*



*Varying sidewalk widths and lack of unified treatments are prevalent.*



*Primary existing land uses consist of older and outdated retail/commercial buildings and centers.*

## 1.5 Purpose and Intent of the Specific Plan

### 1.5.1 Purpose

The RHASP provides planning regulations that connect General Plan policies with project-level development within the Specific Plan area. The purpose of the Specific Plan is to guide future change, promote high quality development, and implement the community's vision for the area. The Specific Plan provides long and short-term goals and objectives, a land use plan, regulatory standards, design criteria, and administration and implementation programs to carry out the vision of the Specific Plan area.

### 1.5.2 Vision Summary

The RHASP is envisioned to result in a distinctive, vibrant and thriving district with enlivened and interconnected commercial, office, and residential uses. The mixed uses represent an evolution of land uses that respond to the changing marketplace. The area will support multi-modal transportation practices and activate key areas where residents, visitors, or employees can safely and efficiently walk, ride a bike, catch a bus, or drive a car. The area will engage the surrounding neighborhoods at key locations and reflect the small-town spirit of Tustin.

Over time, the influx of retail, restaurants, services and new residents will enhance the economic and social vitality of the area and establish the RHASP area as a unique district with its own character. The streets and public spaces will be enhanced through landscaping and street furniture and framed by quality architecture expressed in a variety of building styles.

The Specific Plan will incorporate the following elements as part of this Vision:

- Sense of place;
- Compatible land uses in an integrated mixed-use environment;
- High quality architecture;
- High quality businesses; and
- Improved public streetscape.

The vision for the Specific Plan area is more fully described and illustrated in Chapter 2, *Vision, Goals, and Objectives*.

### 1.5.3 Specific Plan Goals

Overall goals for the RHASP, as further delineated in Chapter 2, *Vision, Goals, and Objectives*, are as follows:

**Goal 1** – Enhance streetscape, landscape, and public amenities throughout the Specific Plan area.

**Goal 2** – Improve visual and functional connections and linkages between Red Hill Avenue, surrounding residential neighborhoods, adjacent public and institutional uses, and Interstate 5.

**Goal 3** – Balance flexible and diverse land uses that foster economic development opportunities and support housing opportunities. Land use in the project area will maximize residential opportunities along with neighborhood-serving retail and commercial uses.

**Goal 4** – Streamline processes to support future development in the Specific Plan area.

**Goal 5** – Improve pedestrian and bicycle accessibility and vehicular circulation to minimize potential conflicts between different users and improve mobility throughout the Specific Plan area and connectivity with the greater community.

**Goal 6** – Implement parking standards that reflect verifiable demand and consider future land uses in the area.

**Goal 7** – Coordinate existing and future development with infrastructure capacity.

**Goal 8** – Ensure development within the Specific Plan area is sensitive to and compatible with surrounding land uses.

## 1.6 Authority and Scope

### 1.6.1 Statutory Authority

The RHASP is established through the authority granted to the City of Tustin by California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (Specific Plans). As expressed in California law, a Specific Plan may be adopted either by ordinance or by resolution. This allows jurisdictions to choose whether their specific plan will be policy driven (adopted by resolution) or regulatory in nature (adopted by ordinance). The RHASP will be adopted by Ordinance as a regulatory plan.

*Specific Plan – a tool for the systematic implementation of the General Plan. It effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area.*

### 1.6.2 Relationship to General Plan

The current City of Tustin General Plan was updated in 2013. A specific plan may not be adopted or amended unless the proposed plan or amendment is consistent with the General Plan, pursuant to Government Code Section 65454. The RHASP was adopted in conjunction with an amendment to the General Plan to make the two documents consistent. The RHASP's regulations and criteria comply with the directives of the Tustin General Plan's policies and programs as amended.

### 1.6.3 Relationship to Zoning Code

Upon its adoption, the RHASP will become the zoning for all properties within the Specific Plan area, replacing the existing zoning. Government Code Section 65455 requires that the adoption or amendment of a zoning ordinance be consistent with any applicable specific plan covering the same area. The provisions contained in the RHASP constitute the primary land use and development standards for the Specific Plan area. These regulations are applied in addition to the provisions set forth in the Tustin City Code (TCC). Where the Specific Plan is silent, the provisions of the TCC shall prevail.

## 1.7 Specific Plan Organization

The RHASP is organized into the following Chapters:

- **Chapter 1: Introduction** – This chapter provides a general introduction to the Specific Plan, including the purpose and intent of the plan, historical background, an overview of existing conditions, and scope and authority of the Specific Plan.
- **Chapter 2: Vision, Goals, and Objectives** – This chapter provides the policy foundation for the Specific Plan and includes the community vision, policy framework, and goals and objectives.
- **Chapter 3: Development Plan** – This chapter provides specific land use, circulation, urban design, and infrastructure plans.
- **Chapter 4: Land Use and Development Standards** – This chapter identifies the allowed land uses and the development standards for parcels in the Specific Plan area.
- **Chapter 5: Design Criteria** – This chapter provides design regulations and guidance for all private properties in the Specific Plan area, including criteria for site planning, building design, and signage.
- **Chapter 6: Implementation and Administration** – This chapter discusses the administrative process and procedures required for review and permitting of land use and development activity within the Specific Plan area as well as development incentives and funding mechanisms.
- **Appendices:** The Appendices provide background materials related to the Specific Plan.

## 1.8 User's Guide

The RHASP is designed to meet the needs of many users, including property owners, merchants, architects, designers, building contractors, City staff, future residents, investors, developers, and other interested persons in the community. Each of these interests plays a role in the future development of the Specific Plan area.

To most effectively use the Specific Plan, the following process is recommended:

- Step 1: Review Chapter 1 to gain an understanding of the study area, including history and an overview of existing conditions.
- Step 2: Review Chapter 2 to gain an understanding of the vision and guiding principles for this Specific Plan.
- Step 3: Property owners that are interested in developing/intensifying property or those interested in starting a new business or expanding/enhancing an existing business in the Specific Plan area should review Chapters 4, *Land Use and Development Standards* and 5, *Design Criteria*, to gain an understanding of the development standards and design criteria for their property.
- Step 4: Review Chapter 3, *Development Plan*, to gain an understanding of the land use, circulation, urban design, and infrastructure plans that will guide the potential buildout of the Specific Plan area.
- Step 5: Review Chapter 6, *Implementation and Administration*, to gain an understanding of the implementation procedures and administration of this Specific Plan.
- Step 6: Schedule a meeting with the City of Tustin Community Development Department to review concepts, determine project processes and requirements, identify other agencies that may need to be contacted, and discuss potential issues, solutions, and approaches.

It is highly recommended that interested property/business owners, developers, and investors contact the City to discuss project proposals before committing large amounts of time and financial resources for plans and drawings.

## 1.9 California Environmental Quality Act Compliance

The RHASP has been prepared in conjunction with a Program-level Environmental Impact Report (EIR), which identifies potential impacts resulting from the proposed development and establishes, standard conditions and mitigation measures that reduce them to a less than significant level, where feasible.

*California Environmental Quality Act (CEQA) – a California statute passed in 1970 to institute a statewide policy of environmental protection*

As the lead agency, the City of Tustin will implement a monitoring program for the approved standards and mitigation measures. To assist in this monitoring effort, a Mitigation Monitoring Program has been developed by the City as part of environmental findings which will be included in the Final Specific Plan approved by the Tustin City Council. The Mitigation Monitoring Program shall be included as an Appendix to this Specific Plan.

The RHASP EIR will serve as the primary environmental document for future development undertaken within the Specific Plan area. The EIR is anticipated to be the definitive environmental document for project implementation within the Specific Plan area, including serving as a Program EIR for purposes of backbone infrastructure improvements. Future development projects that require discretionary review will be examined against the analysis prepared for the EIR to determine if additional environmental documentation must be prepared. Developments that do not require additional discretionary review will not be subject to additional environmental documentation. However, a project applicant will be required to submit documentation substantiating the finding that said development is allowed and in conformance with the Specific Plan, and the potential environmental effects are within the parameters and timeframe analyzed within the Specific Plan EIR.

Future development projects proposed within the Specific Plan area may be required to prepare their own environmental documentation pursuant to State law. However, subsequent site-specific projects may use the “tiering” concept, as provided by § 15385 of the State CEQA Guidelines. The tiering concept is a process by which the City of Tustin, as lead agency, can adopt the programmatic EIR focusing on the “big picture,” and can then use streamlined CEQA review for subsequent individual development projects in the Specific Plan area. This streamlined CEQA review may be used for each site-specific future development so long as the project is consistent with the findings of the EIR, the mitigation measures described in the EIR, and the City’s General Plan and Zoning Ordinance. This tiering concept allows the City to address the broad environmental issues detailed in the EIR during the planning stages of the Specific Plan. Future site-specific development projects will be evaluated on a project-specific basis, and may be excused from repeating the broad environmental analysis examined in the comprehensive, programmatic EIR for the entire Specific Plan area.

# 2 VISION, GOALS, AND OBJECTIVES

## Vision, Goals, and Objectives

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## 2. Vision, Goals, and Objectives

### 2.1 Purpose

This chapter of the Red Hill Avenue Specific Plan (RHASP) provides the overall vision and policy foundation for the Specific Plan document, including a summary of the community outreach process, planning factors, and goals and objectives.

### 2.2 Community Engagement Overview

A primary purpose of the RHASP is to establish a regulatory document grounded in community participation and input. Throughout the development of the RHASP, the City of Tustin engaged with and collected input from community residents and stakeholders through various community outreach activities. Detailed summaries and notes from community engagement activities can be found in Appendix C: *Community Outreach Summary*.

#### 2.2.1 Community Workshops

##### A. Workshop #1 – July 21, 2016

On July 21, 2016, the City of Tustin held the first Community Workshop at Tustin High School. Workshop attendees learned about the planning process, project goals and objectives, and the overall project overview. Workshop attendees also participated in an interactive Post-It note exercise to identify treasures, challenges, and visions for the Specific Plan area. *At the time of the first workshop the Specific Plan area included the residential neighborhoods north of San Juan Street.*

**Treasures.** Workshop participants identified the following as some of the important “Treasures” in the Specific Plan area:

- Red Hill Avenue’s access to amenities, such as shopping, dining, and nearby schools;
- Existing mature hardwood trees in the area and at nearby Pine Tree Park;
- Shopping centers and other existing businesses that provide retail opportunities;
- Reduction of crime and efforts by Tustin Police Department to make the area safer; and
- Small town character and feel of many areas in the City, including the RHASP.

**Challenges.** Workshop participants identified the following as some of the existing “Challenges” in the Specific Plan area:

- Traffic congestion and noise along Red Hill Avenue;
- Lack of on-street parking in residential areas and streets close to nearby schools;

- Older, commercial centers and buildings needing rehabilitation;
- Lack of a comprehensive bike network;
- Safety issues, including freeway entrances and crime near the existing motels; and
- Deficient landscaping and trees within the area.

**The Future.** Workshop participants also provided their ideas and thoughts on the future of the Specific Plan area. The following “Visions” were identified as concepts that may help improve the area:

- Utilize sustainability principles, such as energy efficiency, green building, alternative energy, and water conservation;
- Provide adequate on-street and off-street parking (*related to existing residential neighborhood parking issues north of San Juan; the Specific Plan area was revised to eliminate this area*);
- Encourage other modes of transportation, especially walking and biking, by providing accessible and connected facilities;
- Remodel and rehabilitate existing shopping centers and attract a balanced mix of businesses;
- Address traffic flow and safety issues along Red Hill Avenue; and
- Preserve Tustin’s small-town feel even as new development is considered in the area.

**Visions.** Following the Post-It note exercise, workshop participants were placed in small groups and asked to develop group vision statements for the Specific Plan area. Vision statements described the community’s hopes, dreams, and aspirations. The following common themes emerged resulting in the following five (5) vision statements:

- Preserve the small town feel and community character of Tustin in all new development;
- Attract a variety of businesses that serve the community;
- Encourage an appropriate mix of commercial, retail, service, and residential uses;
- Incorporate sustainability principles that support the environment, the local economy, and Tustin’s quality of life; and
- Enhance future development by preserving existing street trees and incorporating additional landscaping and open space.

This workshop summary does not encompass all comments received. For detailed notes from Workshop #1, please refer to Appendix C: *Community Outreach Summary*.

## B. Workshop #2 – December 1, 2016

On December 1, 2016, the City of Tustin held a Community Open House at the Clifton C. Miller Community Center. Attendees learned about the planning process, project goals and objectives, a summary of Community Workshop #1, and the overall project overview. Community residents and stakeholders had the opportunity to participate in hands-on and interactive exercises to help develop a future vision for the Specific Plan area. Input from open house participants was gathered and covered the following topics: refined Specific Plan boundaries; priority areas and areas of concern within the Specific Plan area; potential future land uses and activities; potential transportation and circulation improvements; and streetscape, public amenities, architecture, and urban design improvements. *The refined Specific Plan area boundary was the result of concerns expressed during Workshop #1; the refined boundary eliminated the existing residential neighborhoods north of San Juan Street.*

The following is a summary of comments at this workshop.

### Specific Plan Boundaries:

- The community agreed with excluding the residential area north of San Juan Street and west of Red Hill Avenue from the Specific Plan area boundaries.
- The community asked for consideration of the inclusion of the parcels located on the southwest corner of Walnut Avenue and Red Hill Avenue.

### Future Land Uses and Activities:

- Allow mixed-use development within parcels north of El Camino Real and on both west side and east side of Red Hill Avenue;
- Mixed-use development is appropriate in the Stater Bros. shopping center and across the street at the 7-Eleven shopping center;
- Avoid architecture that looks too much like Irvine and buildings higher than three-stories;
- Commercial uses are appropriate along the entire Red Hill Avenue corridor between San Juan Street and Mitchell Avenue;
- Residential architecture should include canopies, patios, and overhangs for shade; blank, flat walls should be avoided;
- Residential uses should be concentrated in the vacant lot south of San Juan Street and west of Red Hill Avenue;
- Residential uses can also be considered on the parcel south of San Juan Street and east of Red Hill Avenue; and the parcel at the southwest corner of Red Hill Avenue and Walnut Avenue;
- Include adequate parking in future development and consider parking garages and underground parking where appropriate;

- Provide places to shop, retail areas, and family-friendly businesses where residents and visitors can hang out;
- Residential uses should not be more than 30 dwelling units per acre; and
- Commercial uses should be the focus.

#### Transportation and Circulation Improvements:

- Include safer bike lanes and buffered sidewalks to make the Specific Plan area a more comfortable and pleasant experience for non-motorists;
- Address parking issues in the neighboring residential areas and spillover to other areas;
- Reduce the number of driveways that ingress/egress onto Red Hill Avenue;
- Consider installing left turn arrows along Red Hill Avenue;
- Do not use roundabouts in the Specific Plan area; and
- Consider pedestrian bridges or additional pedestrian crossings to improve access across Red Hill Avenue.

#### Streetscape, Public Amenities, and Urban Design Improvements:

- Improve the aesthetics of Red Hill Avenue with landscaping, medians, public art, light posts, and themed signage;
- Add gateway features that are timeless, representative, and complements the identity of the area as well as the City of Tustin;
- Incorporate streetscape improvements along the freeway underpass; and
- Streetscape should have an overall theme that helps create an identity for the area.

This workshop summary does not encompass all comments received. For detailed notes from Workshop #2, please refer to Appendix C: *Community Outreach Summary*.

## 2.3 Specific Plan Vision

The RHASP is envisioned to result in a distinctive, vibrant and thriving district with enlivened and interconnected commercial, office, and residential uses. The mixed uses represent an evolution of land uses that respond to the changing marketplace. The area will support multi-modal transportation practices and activate key areas where people can safely and efficiently walk, ride a bike, catch a bus, or drive a car. The area will engage the surrounding neighborhoods at key locations and reflect the small-town spirit of Tustin. Over time, the influx of retail, restaurants, services and new residents will enhance the economic and social vitality of the area and establish the Specific Plan area as a unique district with its own character. The streets and public spaces will be enhanced through landscaping and street furniture and framed by quality architecture expressed in a variety of building styles.

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*The Red Hill Avenue Specific Plan area will be a distinctive, vibrant and thriving district with enlivened and interconnected commercial, office, and residential uses.*

---

The Specific Plan will incorporate the following elements as part of this Vision:

### A. Sense of Place

Present day Red Hill Avenue is a blend of older commercial development with no cohesive theme or character, with off-site parking dominating much of the street scene. The revitalization anticipated by this Plan will create a district that respects the City's past while providing modern amenities that attract residents, shoppers, and businesses. Because this redevelopment will occur over a period of years, the area will have a more organic feel as new buildings replace outdated structures, residential uses are introduced, existing buildings are remodeled, and landscaping along the streets matures. This evolution to a mixed-use community will result in an area that allows residents to live within walking distance to parks, restaurants, and daily services. Attractive streets and sidewalks will encourage pedestrians to stroll. Interesting commercial establishments will provide a draw to the area, creating a thriving area. The area will complement and connect to existing neighborhoods. It will add to the fabric that makes Tustin one of the most charming communities in Orange County.

"Charm" implies something old-fashioned to many. This district will respect Tustin's history while accommodating a mix of modern land uses. The area will have a comfortable feel that will appeal to residents, businesses, and visitors. The architecture in this area will not pretend to be "old" or introduce modern styles that might clash with surrounding land uses. Instead, a contemporary interpretation of traditional historical architectural styles found in Tustin will be employed when new buildings are constructed and existing buildings are updated. Buildings will be oriented to the street and ground floor uses will engage pedestrians. Parking will be off the street, but will still conveniently serve new businesses. Architectural features will add interest and reinforce the unique character of the area. Public outdoor spaces will be encouraged throughout the area to allow people to take advantage of the wonderful weather and invite pedestrians to linger.



**Exhibit 2-1 Vision – Sense of Place**

# RED HILL AVENUE SPECIFIC PLAN

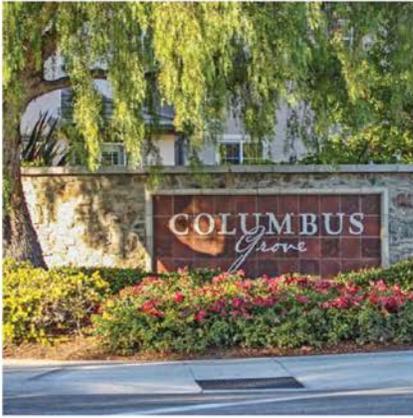


Exhibit 2-2 Vision – Public Streetscape

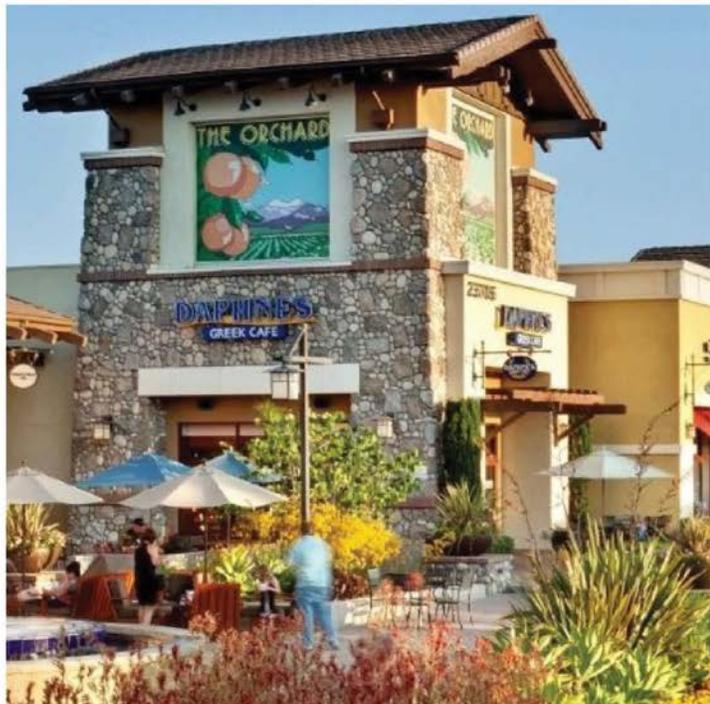


Exhibit 2-3 Vision – Architecture

# RED HILL AVENUE SPECIFIC PLAN



Exhibit 2-4 Conceptual Illustration – Red Hill Avenue North of I-5 Freeway



Exhibit 2-5 Conceptual Illustration – Red Hill Avenue South of I-5 Freeway

The City will support the creation of this district through new landscaping for the street parkways and medians with a landscape palette unique to this area. Over time, the landscaping will provide shade for pedestrians and frame the main access points to this area. In addition, entry gateways, and creative signs will be employed to reinforce the character of this district and the activities that take place within it. Banners will add color and interest to the street. Opportunities to add visual interest to public and private walls will be explored using methods such as murals, tiles, landscaping and lighting. The inclusion of a flexible amenity area will add and create a sense of place. Such features can create a sense of arrival as motorists exit the adjacent freeway and can further reinforce the unique character of this district.

## **B. Compatible Land Uses in an Integrated Mixed Use Environment**

The Specific Plan area is envisioned to retain a primarily commercial character along Red Hill Avenue, with vertical and horizontal mixed-use projects that activate the street by bringing new commercial uses and pedestrian activity to the street frontage. The evolution of land uses will enhance the economic and social vitality of the area through the addition of new residents and local-serving retail and service uses. Through the design review process, the City will ensure that new development complements existing residential neighborhoods surrounding the Plan area. Close attention to building location and size, architectural details, access, parking, and types of uses will ensure new projects are compatible with established neighborhoods. New development will introduce a high level of architectural detail and quality materials, setting the stage for overall revitalization of the area. As properties redevelop during the multi-year implementation of the Plan, pedestrian-friendly linkages to surrounding parks and neighborhoods will be provided.

With time, it is envisioned as an area which integrates existing commercial uses with new development to create a versatile mix of commercial, office, and residential uses, and will create a sense of place for this area of Tustin through enhanced and consistent streetscape elements, signage, and cohesive architectural styles.

## **C. High-Quality Architecture**

The City of Tustin and the Red Hill Avenue area have a varied architectural heritage, with Spanish roots as well as an agricultural heritage from past citrus and avocado groves. New development in the Specific Plan area is envisioned to draw from this heritage, with contemporary interpretations of three traditional architectural styles found in Tustin, including Spanish Mediterranean, Craftsman, and Farmhouse/Agricultural. This use of a mixture of compatible architectural styles – not a single mandated style, will allow for future flexibility in a community that evolves over time. While there may be variation over time, these styles will remain consistent, based on high-quality architectural forms, pedestrian-scaled architectural details at the street level and building elements, such as storefront windows, lights, and awnings that make visitors feel comfortable and want to visit and stay in the area.

## D. High-Quality Businesses

New uses and businesses established in the Specific Plan area will complement existing uses and will act as a catalyst for the improvement of aging commercial properties. The introduction of new residents and businesses will provide incentives for the revitalization of vacant or underperforming properties as surrounding properties improve and property values increase. The area is envisioned as having a focus on local-serving commercial and service uses rather than regionally serving uses.

## E. Public Streetscape

The public streetscape will be enhanced and activated through landscaping and framed by quality architecture in a variety of building types on the private side. The plan balances vehicular and pedestrian needs by providing for continued vehicular traffic flow, adding bike lanes, and enhancing the street with consistent landscaped parkways, sidewalks, street trees, landscaped medians, and street furniture. Provision of complete street elements will extend bike paths from the surrounding neighborhoods to connect with the Specific Plan area and provide additional modes of travel, enhanced through cohesive and informative wayfinding and entry monumentation signage. Public art elements in key locations will further enhance the unique character of the area.

## 2.4 Key Planning Factors

As demonstrated by input received during community engagement activities, several challenges, constraints, opportunities, and attributes exist within the Specific Plan area that may influence the development of the RHASP. These “planning factors” help form the vision for the Specific Plan.

*Planning Factors – Opportunities and constraints that influence and contribute to the successful implementation of the Specific Plan*

**Planning Factor 1:** Red Hill Avenue is a major arterial and auto-oriented corridor carrying approximately 21,000–26,000 vehicles per day. Future infrastructure improvements for bike, pedestrian, and public transit should balance the needs of all transportation users.

**Planning Factor 2:** Older and underutilized commercial retail centers within the Specific Plan area provide great potential for economic development and redevelopment of parcels with new uses or enhancement of existing uses.

**Planning Factor 3:** The City previously completed the *Red Hill Avenue Streetscape and Median Feasibility Study (see Appendix D)*, which analyzed Red Hill Avenue from El Camino Real to Bryan Avenue. The study provided recommendations for locations of medians, landscaped medians, bike lanes, sidewalks, utility undergrounding, and other landscaping features. This study has been used as a reference guide for the RHASP.

**Planning Factor 4:** The Specific Plan area is surrounded by existing residential neighborhoods. Access from these neighborhoods to new development will greatly enhance the ability of residents to patronize

future potential uses along Red Hill Avenue. Future development must also be compatible with existing adjacent residential neighborhoods and minimize potential negative impacts or conflicts.

**Planning Factor 5:** The proximity of the Specific Plan area to institutional and civic uses such as Frontier Park, Pine Tree Park, and schools including Tustin High School, Marjorie Veeh Elementary School and Benjamin F. Beswick Elementary School provide potential to extend connections beyond the Specific Plan area and provide complementary uses and amenities.

**Planning Factor 6:** Since Tustin, particularly in this area, is mostly a “built out” community, economic development and revitalization of Red Hill Avenue should optimize the potential of quality infill development within the Specific Plan area.

**Planning Factor 7:** Future development should consider employment-generating uses balanced with residential and community-supportive uses such as open space and gathering places along with sought-after amenities.

**Planning Factor 8:** The Specific Plan area’s proximity to Interstate 5 provides a key gateway opportunity. Improvements along the freeway entrance and exit ramps in the Specific Plan area are important to enhance the vibrancy of this area and reinforce its unique character.

**Planning Factor 9:** There are significant opportunities to improve the aesthetics and streetscape environment along Red Hill Avenue. Improvements may include, but are not limited to, signage, enhanced sidewalks, street furniture, appropriate building location and massing, enhanced medians, , pavement treatments, lighting, public art and pedestrian gathering spaces.

**Planning Factor 10:** Smooth traffic flow without congestion is a goal along Red Hill Avenue; therefore, improved traffic signalization and other traffic standards and mitigation measures should be considered as part of future development based on environmental review.

**Planning Factor 11:** Public investment in renewed streetscape should encourage an environment that creates a distinct identity for the Specific Plan area while complementing the character of Tustin. Future public improvements should encourage a positive identity, branding opportunities and serve as a catalyst for private investment.

**Planning Factor 12:** The infrastructure system affected by the Specific Plan area must be adequate to support future development. Therefore, land use growth assumptions for the Specific Plan area must consider the need for corresponding infrastructure improvements.

**Planning Factor 13:** Interstate 5 bisects the Specific Plan area and creates a potential disconnect between the northern and southern portions of the Specific Plan area. Efforts to connect these two areas along Red Hill Avenue should be encouraged as part of future development.

**Planning Factor 14:** Parking has been identified as a challenge in residential areas adjacent to the Specific Plan area and beyond. Future development needs to ensure parking needs do not affect the adjacent neighborhoods. Opportunities exist to optimize the future supply of parking through strategic management, such as the promotion of shared/joint-use parking, and other parking management programs. The design of parking areas should also consider pedestrian safety and aesthetics.

## 2.5 Specific Plan Goals and Objectives

A successful Specific Plan should accommodate and adapt to the desires and values of the community. Goals and objectives are used to provide a framework for the implementation of the RHASP.

**Specific Plan Goals** – Statements concerning an aspect of the City’s desired ultimate physical, social, and/or economic environment in this planning area. These goals serve as benchmarks for the analysis of future development proposals and design concepts to determine if they are consistent with the spirit and intent of this Specific Plan.

**Specific Plan Objectives** – Statements about the types of action that are necessary to achieve the stated goals and promote the overall spirit and intent of the RHASP.

**Goal 1** – Enhance streetscape, landscape, and public amenities throughout the Specific Plan area.

Objective 1-1: Establish a streetscape program using landscaping, signage, street furniture, entry statements, and other visual amenities compatible with the character of Tustin to achieve a distinct identity for the area.

Objective 1-2: Develop coordinated gateway design treatments that establish entry statements and a “sense of place” at key locations within the Specific Plan area.

Objective 1-3: Encourage a “sense of place” within the Specific Plan area through quality site design, architectural design, and public improvements as part of future development.

Objective 1-4: Coordinate a bus shelter and transit stop improvement program to ensure that all bus stops have the appropriate amenities.

**Goal 2** – Improve visual and functional connections and linkages between Red Hill Avenue, surrounding residential neighborhoods, adjacent public and institutional uses, and Interstate 5.

Objective 2-1: Identify ways to improve and enhance linkages and connections between new development in the Specific Plan area and surrounding neighborhoods.

Objective 2-2: Develop design criteria that encourage optimal building configuration and design, parking strategies, signage, pedestrian amenities, landscaping, and appropriate, timeless architectural styles.

**Goal 3** – Balance flexible and diverse land uses that foster economic development opportunities and support housing opportunities. Land use in the Specific Plan area will allow for residential opportunities along with neighborhood-serving retail, office and commercial uses.

Objective 3-1: Establish a land use program that encourages a mix of land uses responsive to market demands and Tustin community priorities.

Objective 3-2: Refine allowable land uses within the area to encourage the desired development envisioned by the Specific Plan.

Objective 3-3: Establish development standards for future land uses that are compatible with the surrounding area and preserve the small town feel and community character.

Objective 3-4: Develop land use standards that focus on retention and enhancement of commercial development, but supports integrated mixed-use development, sidewalk-adjacent development, parking behind building frontages and pedestrian activity.

**Goal 4** – Streamline processes to support future development in the Specific Plan area.

Objective 4-1: Adopt a program-level environmental clearance document to utilize in subsequent development proposals within the RHASP area.

Objective 4-2: Establish a tiered environmental review process, as described in Chapter 1, *Executive Summary and Introduction*, for discretionary development application review to streamline the approval process.

Objective 4-3: Establish development incentives such as tailored development standards or streamlined review processes, to encourage new development that fulfills the vision of the Specific Plan.

Objective 4-4: Identify local, State, and Federal grant and other funding opportunities that can provide business assistance and offer the City the means to upgrade the area.

**Goal 5** – Improve pedestrian and bike accessibility and vehicular circulation to minimize potential conflicts between different users and improve mobility throughout the Specific Plan area and connectivity with the greater community.

Objective 5-1: Improve and enhance pedestrian connections and facilities, particularly in areas that contain large, expansive parking lots. At these locations, accessible pedestrian connections from the sidewalk to building entrances should be encouraged.

Objective 5-2: Minimize curb cuts or driveways onto arterial roads and collector streets.

Objective 5-3: Promote and develop a transportation system which includes provisions for public transportation, bikes, and pedestrians.

**Goal 6** – Implement parking standards that reflect verifiable demand and consider future land uses in the area.

Objective 6-1: Promote the development and maintenance of adequate parking facilities commensurate with parking demand.

Objective 6-2: Monitor parking supply and utilization to identify deficiencies or conflicts with the movement of traffic as new development occurs.

**Goal 7** – Coordinate existing and future development with infrastructure capacity.

Objective 7-1: Ensure infrastructure capacity within the Specific Plan area meets future demands.

Objective 7-2: Coordinate future land use planning with sustainable transportation and infrastructure planning.

**Goal 8** – Ensure development within the Specific Plan area is sensitive to and compatible with surrounding land uses.

Objective 8-1: Ensure that the form, scale, and design of new development, including new construction, renovations, or additions, does not negatively impact the existing surrounding uses and structures.

Objective 8-2: Implement “four-sided architecture” principles that consider the aesthetic quality of development from all sides, whether visible from the public R.O.W. or not.

# 3 DEVELOPMENT PLAN

# Development Plan

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## 3. Development Plan

### 3.1 Purpose

The Development Plan is intended to achieve a specific pattern of development in accordance with the vision for the Specific Plan area as set forth in Chapter 2, *Vision, Goals, and Objectives*. This Chapter establishes the intent of the Specific Plan in terms of future land use for existing and new development. It provides an overall understanding and rationale for what is envisioned in the Specific Plan area, and the level of services and infrastructure to be provided. The purpose of this chapter is to lay the foundation for the development regulations and criteria provided in Chapter 4, *Land Use and Development Standards* and Chapter 5, *Design Criteria*.

The major components of the development plan include: the organization of land uses, circulation, urban design, and infrastructure. Some material in this chapter is intended to be directive and shall be implemented according to language contained herein. Other material is conceptual and is intended to guide and not restrict creative solutions.

The elements of this chapter implement several aspects of the Specific Plan vision as outlined in Chapter 2, *Vision, Goals, and Objectives*, including the land use, circulation, urban design, and infrastructure plans that will bring to life the vision of a distinctive, vibrant and thriving district with enlivened and interconnected commercial, office, and residential uses. The sections that follow outline these component plans.

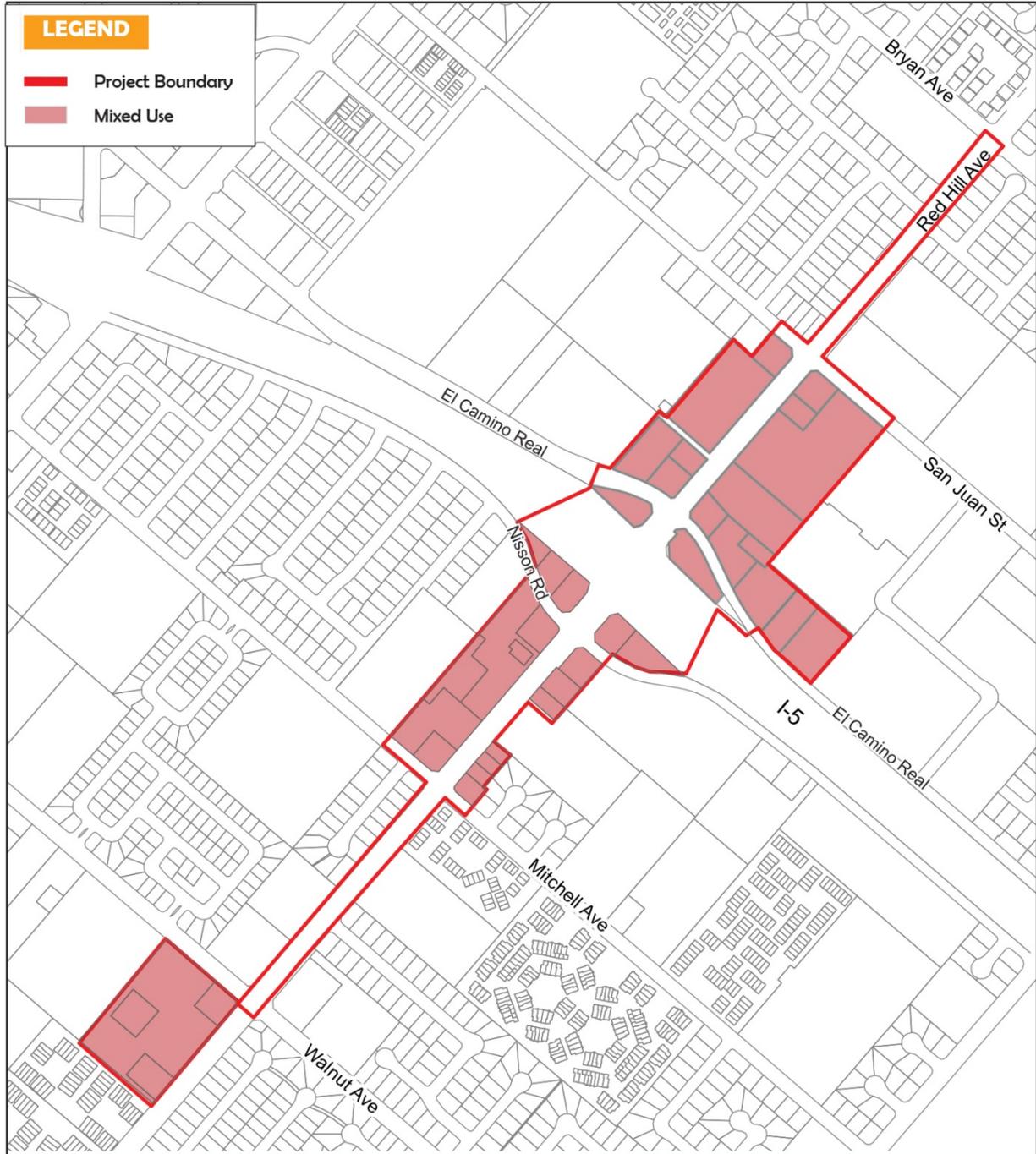
### 3.2 Land Use

The Land Use Plan responds to the Specific Plan goals stated in Chapter 2 by providing a balance of commercial, service, and residential uses. The Land Use Plan is shown on Exhibit 3-1, *Land Use Plan*.

The Red Hill Avenue Specific Plan (RHASP) provides planning regulations that connect General Plan policies with project-level development within the Specific Plan area. The purpose of the Specific Plan is to guide future change, promote high-quality development, and implement the community's vision for the area. The Specific Plan provides goals and objectives, a land use plan, regulatory standards, design criteria, and administrative and implementation programs to encourage high-quality development within the Specific Plan area.

The Specific Plan establishes procedures for the review and allocation of new residential development within the boundaries of the Specific Plan area. The mixed-use land use district is intended to provide a complementary mix of commercial, office and residential uses in an integrated, pedestrian-friendly environment. The mixed-use concept is intended to expand and enhance commercial opportunities to serve a primarily local customer base. The district is intended to physically and functionally integrate land uses in a manner that enhances livability through complementary relationships with existing uses, improved pedestrian environment and sidewalk-adjacent development.

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 3-1 Land Use Plan**

## 3.2.1 Mixed-Use

Land use for all properties within the Specific Plan area is classified as Mixed-Use, which provides for the following elements:

- **Commercial/Office:** Freestanding retail and service commercial and/or office uses are allowed within the mixed-use designation, with specific uses outlined in Chapter 4, *Land Use and Development Standards*. Freestanding commercial/office uses will likely continue to be the dominant pattern within the Specific Plan area, as many parcels are too small to accommodate the parking, common open space, and pedestrian-oriented requirements outlined in the Development Regulations and Design Criteria in an integrated mixed-use development.
- **Mixed-Use:** This use type provides for a variety of future development opportunities as market conditions are suitable for high-value use of the property. The focus of the Specific Plan area will continue to be commercial in character with the introduction of housing. It allows for mixed-use developments with commercial retail and/or office on the ground floor and either residential or office uses on upper floors in a vertical mixed-use environment or commercial/office uses and residential uses in a horizontal mixed-use setting on a single development site.
  - Residential Allocation Reservation (RAR). The Land Use Plan allows for 500 additional residential units within the Specific Plan area. The Specific Plan allocates 395 dwelling units to the Specific Plan area north of I-5 and 105 dwelling units to the Specific Plan area south of I-5 with transferability and conversions between the area north of the I-5 freeway and the area south of the I-5 freeway allowed as outlined in Section 6.6, *Administration and Enforcement* of this Specific Plan. Residential units must be high quality and integrated into a mixed-use development. The residential component of a mixed-use project requires City approval of units via the completion of the Residential Allocation Reservation (RAR) process defined in Chapter 4, *Land Use and Development Standards* and Chapter 6, *Implementation and Administration*.

The Specific Plan's Program EIR evaluates an additional 500 dwelling units and an additional 325,000 square feet of non-residential uses as a threshold of development intensity within the Specific Plan area. The total development in the Specific Plan area anticipated with the buildout potential of the Specific Plan in addition to the existing development is 521 dwelling units and 621,446 of non-residential development.

## 3.3 Circulation Plan

### 3.3.1 Existing Roadway Network

The Specific Plan area roadways, as shown in Exhibit 1-4, *Existing Roadways* consist of Red Hill Avenue, which is oriented in the northeast-to-southwest direction; and six roadways that cross Red Hill

# RED HILL AVENUE SPECIFIC PLAN

Avenue: Bryan Avenue, San Juan Street, El Camino Real, Nisson Road, Mitchell Avenue, and Walnut Avenue. The intersections of each of these streets within the Specific Plan area are signalized. Interstate 5 (I-5) bisects the study area. A brief description of each of the roadways is provided below.

- **Red Hill Avenue** – Red Hill Avenue is a six-lane divided roadway, with three travel lanes in each direction and a center two-way left-turn lane. Within the Specific Plan area, Red Hill Avenue provides access to the I-5 Freeway via a tight diamond interchange. The posted speed limit is 40 miles per hour (mph). Red Hill Avenue is designated as a Major Arterial on the Arterial Highway Plan of the City of Tustin’s General Plan. Right-of-way width varies along the length of the Specific Plan area from 100 to 120 feet as ultimate right-of-way dedications have been acquired. Red Hill Avenue crosses under I-5 and has several closely spaced intersections with the freeway ramps, Nisson Road and El Camino Real. Planned improvements to Red Hill Avenue are listed in Section 3.3.2.

The portion of Red Hill Avenue from El Camino Real north to San Juan Street was included in the *Red Hill Avenue Streetscape and Median Feasibility Study – El Camino Real to Bryan Avenue* (Tait & Associates, Inc., September 10, 2012, included in Appendix D of this Specific Plan). The following additional engineering details about Red Hill Avenue within the Specific Plan area are taken from that study:

- The existing street right-of-way along Red Hill Avenue is essentially 100 feet wide (50 feet on each side of the street centerline), with 84 feet of pavement and eight-foot parkways; however, the section on the west side of Red Hill Avenue, from 300 feet north of El Camino Real to San Juan Street, has a half-width right-of-way width of 60 feet, which is the ultimate half-width right-of-way for Red Hill Avenue. The property on the east side of the same section of Red Hill Avenue has an irrevocable offer to dedicate an additional 10 feet of right-of-way, from a 50-foot half street width to a 60-foot half street width.
- The street parkways include sidewalks ranging in width from four to nine feet. Portions of the parkway areas include landscaping and street trees. The parkway on the east side of Red Hill Avenue includes overhead power, telephone, and cable-TV wires; and street lights on wooden poles. No above-ground utilities are present on the west side of the roadway.
- **Bryan Avenue** – Bryan Avenue is a four-lane divided roadway with a two-way left turn lane at the intersection of Red Hill Avenue. The intersection with Red Hill Avenue is signalized. The posted speed limit is 40 mph. Bryan Avenue is designated as a Secondary Arterial in the City’s General Plan. This roadway intersection with Red Hill Avenue is the north end of the Specific Plan area but the roadway itself is not a part of the study area. No improvements are proposed for this roadway.
- **San Juan Street** – San Juan Street is a local two-lane undivided roadway which provides local access to adjacent residential and school uses. Its intersection with Red Hill Avenue is signalized. The posted speed limit along San Juan Street is 30 mph, with a 25-mph school

# RED HILL AVENUE SPECIFIC PLAN

zone, east of Red Hill Avenue associated with Marjorie Veeh Elementary School and a 25-mph school zone west of Red Hill Avenue associated with Tustin High School. The Tustin High School campus is immediately adjacent to the western border of the Specific Plan area. No improvements are proposed for this roadway.

- **El Camino Real** – El Camino Real is a four-lane divided roadway with a two-way left-turn lane northwest of Red Hill Avenue, and a two-lane divided roadway with a two-way left-turn lane southeast of Red Hill Avenue. The street has a 56-foot right of way east of Red Hill with a paved surface that varies from 36-40 feet. West of Red Hill the street has a wider right of way of 80 feet and a paved width of 64 feet. Its intersection with Red Hill Avenue is signalized. The posted speed limit along El Camino Real is 40 mph. El Camino Real is designated as a Secondary Arterial on the Arterial Highway Plan in the City’s General Plan. The eastbound approach of El Camino Real and Red Hill Avenue is posted with the following turn restrictions: “No Turn on Red, 7 – 9 am and 4 – 6 pm Weekdays”. No improvements are proposed for this roadway.
- **Nisson Road** – Nisson Road is a two-lane undivided roadway south of the I-5 freeway which provides local access to adjacent residential and business parcels. Its intersection with Red Hill Avenue is signalized. Nisson Road is not identified in the General Plan as an arterial highway and is considered a local roadway. The posted speed limit along Nisson Road is 35 mph. The westbound approach of Nisson Road at Red Hill Avenue is posted with the following turn restriction: “No Turn on Red, 7 – 9 AM, 4 – 6 PM Weekdays”. No improvements are proposed for this roadway.
- **Mitchell Avenue** – Mitchell Avenue is a two-lane undivided roadway which provides local access to adjacent residential and business parcels. Its intersection with Red Hill Avenue is signalized. Mitchell Avenue is not identified in the General Plan as an arterial highway and is considered a local roadway. The posted speed limit along Mitchell Avenue is 30 mph, with a 25-mph school zone west of Red Hill Avenue associated with Benjamin F. Beswick Elementary School. No improvements are proposed for this roadway.
- **Walnut Avenue** – Walnut Avenue is a four-lane divided roadway with a two-way left turn lane at the intersection of Red Hill Avenue. The intersection with Red Hill Avenue is signalized. Walnut Avenue is designated as a Modified Primary Arterial in the City’s General Plan. The posted speed limit along Walnut Avenue is 40 mph. This roadway is located at the southern edge of the study. No improvements are proposed for this roadway.
- **Interstate 5 (I-5)** – The I-5 freeway bisects the Specific Plan area adjacent to the intersection of Red Hill Avenue/Nisson Road and Red Hill Avenue/El Camino Real. Northbound and southbound on- and off-ramps are present in a diamond-style interchange configuration. The ramps are located at signalized intersections. The I-5 right-of-way is owned by the State of California.

## 3.3.2 Red Hill Avenue Roadway Improvements

### A. Multiple Objectives

There are several objectives related to the design of Red Hill Avenue which must be balanced in the planning for Red Hill Avenue roadway improvements within the Specific Plan area:

- **Need to Maintain Traffic Flow and Capacity.** As a Major Arterial, Red Hill Avenue provides local and regional connections within Tustin and is a point of access to and from I-5. Three (3) through traffic lanes must be maintained to accommodate existing and future traffic.
- **Desire to Provide for Bike Travel.** The General Plan Circulation Element and the desire for complete streets call for improved connectivity through the Specific Plan area and to the existing parks and schools in the vicinity. As discussed further below, complete streets are important because they allow for the safe and efficient use of the public right-of-way by pedestrians, cyclists, and vehicles. They promote healthy alternatives to driving and help to enliven the area for pedestrian foot traffic. Providing on-street (Class II) bike lanes uses roadway width and limits the space for any on-street parking and raised, landscaped medians.
- **Desire to Improve Aesthetics.** Existing Red Hill Avenue includes raised, landscaped medians in the northern reach (north of San Juan Street) but not within the central portions of the Specific Plan area. In addition, the existing streetscape is inconsistent or non-existent. Enhancing the streetscape with both proposed medians and proposed parkways is a high priority.

### B. Complete Streets

These multiple objectives relate to the planning concept of “complete streets.” The “complete streets” concept generally refers to streets, sidewalks, and public rights-of-way that are designed, operated, and maintained to enable safe access for all users – pedestrians, bicyclists, transit riders, and motor vehicle drivers. The implementation of complete streets concepts is intended to increase access and convenience for all users to adjacent land uses, including residential neighborhoods, commercial centers, and public institutions such as schools and parks, while still providing functional, multimodal street performance for all modes of transportation whether the users are traveling to, from, or through the area. The objectives also add to the livability and desire for residents and draws people and businesses to locate within the area.

Streets that include complete street elements are unique and respond to community context, hence there is no single design solution. Complete street components may include: sidewalks, bike lanes, comfortable and accessible public transportation stops, frequent and safe crossing opportunities, median islands, accessible pedestrian signals, narrower travel lanes and more.

Benefits of providing these design components have been identified by the National Complete Streets Coalition, including:

- **Improved Safety.** A Federal Highway Administration safety review found that streets designed with sidewalks, raised medians, better bus stop placement, traffic-calming measures, and enhancements for people with disabilities improve pedestrian safety. Other features, such as medians, enable pedestrians to cross busy roads more safely, reduce left-turning motorist accidents, and improve bike safety.
- **Encouragement of Walking and Biking for Health.** Safe places to walk close to home and places of employment encourages high activity levels and easy access to transit and bike lanes can also contribute to healthy physical activity.
- **Fostering Strong Communities.** A safe walking and biking environment is an essential part of improving public transportation and creating friendly, walkable communities.

## C. Road Improvements

The primary roadway improvements planned by this Specific Plan are for Red Hill Avenue itself (see Exhibit 3-2, *Circulation Plan*). As described below, the following improvements are planned (see additional detail in following sections):

- Modifications to the cross-section of Red Hill Avenue, including the following:
  - Restriping drive lanes to accommodate bike lanes;
  - Parkway improvements to add consistent parkway landscaping and sidewalks;
  - Addition of landscaped medians and modification of the median at Bryan Avenue;
- Potential for a traffic signal, if warranted, to accommodate anticipated new residential trips as shown on Figure 3-2, *Circulation Plan*;
- Dedications as development projects are processed to obtain the full 120-foot right-of-way.

**Roadway Cross Sections.** As shown in Exhibit 3-3, *Red Hill Avenue Street Cross Sections*, the proposed improvements to Red Hill Avenue consist of the provision of additional on-street (Class II) bike lanes, reduced lane widths through restriping, and the addition of landscaped medians. A consistent streetscape program has been developed (see Section 3.4.2, *Streetscape*). Proposed Red Hill Avenue street cross sections are as follows:

- **Baseline Section (Section A).** This is the minimum consistent cross-section throughout the length of Red Hill Avenue within the Specific Plan area. This section retains a consistent curb-to-curb width with three through-lanes (11'/10'/10'), adds an on-street Class II bike lane on both sides of the street, and allows for existing turn lanes. The street retains a 42-foot total half-width street to curb face. Medians would be provided where feasible as shown on

Exhibits 3-4, *Schematic Median Locations* and 3-5, *Potential Median Locations*. This baseline cross section incorporates a consistent streetscape along the entire reach consisting of four-foot minimum landscaped parkway and a four-foot minimum sidewalk.

- **Ultimate Section with Flexible Amenity Setback (Section B).** This ultimate cross-section retains the curb-to-curb width consistent with three thru-lanes (11'/10'/10'), adds an on-street Class II bike lane on both sides of the street, and accommodates the existing left turn lanes, with a 42-foot total half street to curb face. A four-foot (minimum) parkway and 14-foot flexible amenity space (for an 18-foot Flexible Amenity Setback) would accommodate a required four-foot minimum width sidewalk, and options for outdoor dining, plaza spaces, and/or enhanced landscape. The minimum four-foot parkway may be expanded to six feet, which is the preferred dimension. This section assumes the 120-foot right-of-way width and raised, landscaped medians where feasible.

All street sections require restriping within the paved width of the street to accommodate the reduced lane widths, existing turn pockets, and bike lanes. Parking on or adjacent to Red Hill Avenue on private property or within the flexible amenity area may be considered on a case-by-case basis by the Community Development Director and City Engineer as part of a development application. An application which includes on-street parking must demonstrate that the parking does not reduce travel lanes, impede traffic flow, or cause unsafe transitions. Any improvements within the Caltrans right-of-way near I-5 and its on- and -off ramps will require permitting through Caltrans.

As development projects are processed, dedications may be required to obtain the full 120-foot right-of-way.

A traffic signal is identified on Exhibit 3-2, *Circulation Plan*, based on the number of residential dwelling units anticipated in the area between I-5 and San Juan Street. This signal would be provided, if warranted, as development occurs, based upon a warrant analysis performed in conjunction with project-specific traffic analysis. Traffic signals must be synchronized in this area if a signal is warranted and constructed.

**Medians.** Raised medians can be accommodated in limited locations within the Specific Plan area, where they do not conflict with required turning movements (see Exhibits 3-4, *Schematic Median Locations* and 3-5, *Potential Median Locations*). The existing median north of San Juan Street near Bryan Avenue will be modified to accommodate increased northbound left turn lane length and to allow for the addition of the Class II bike lanes.

# RED HILL AVENUE SPECIFIC PLAN

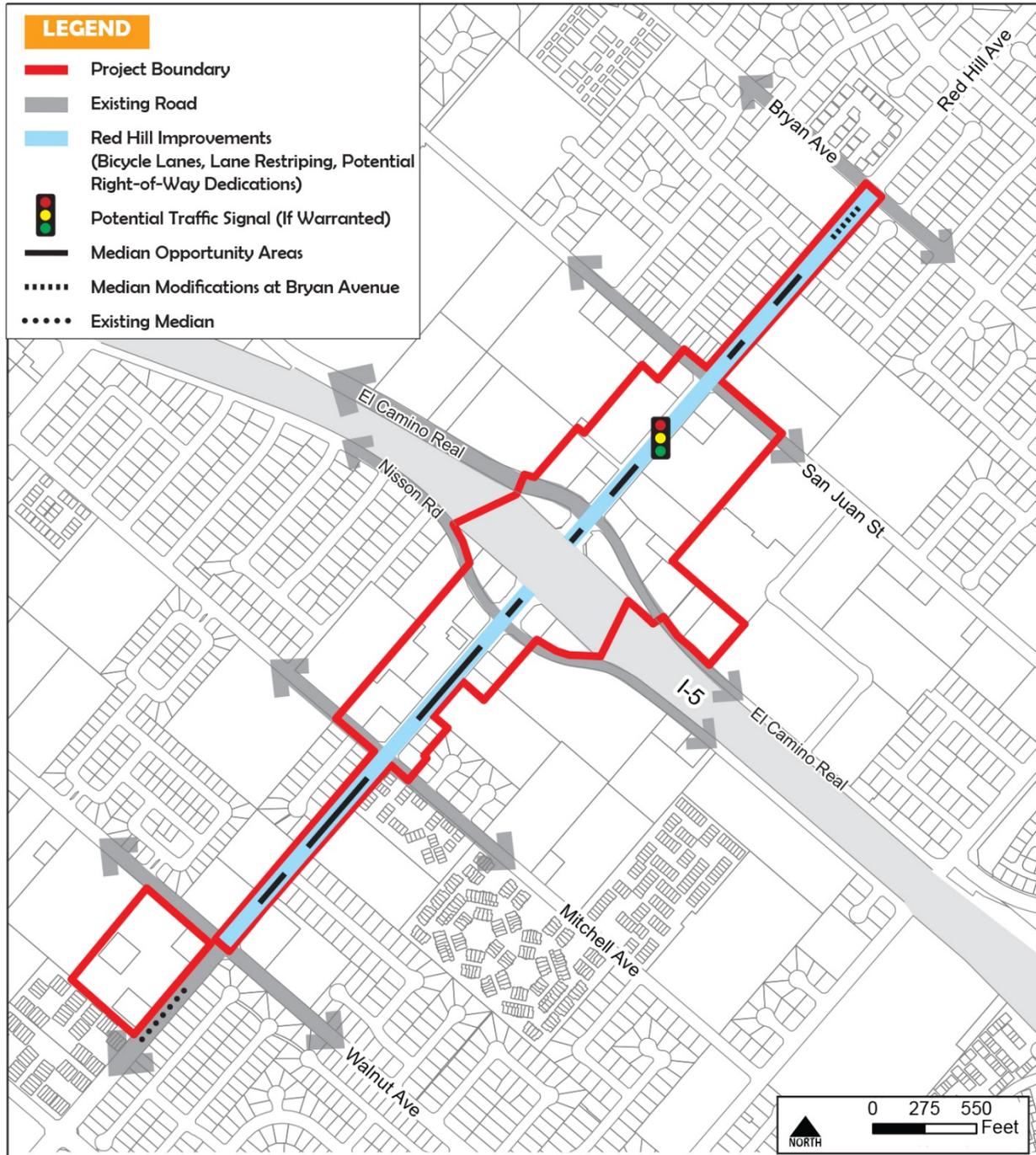
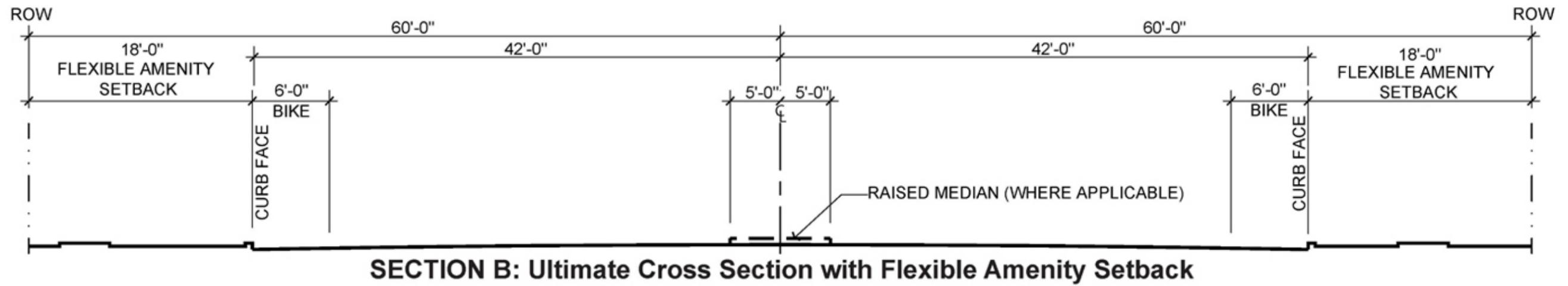
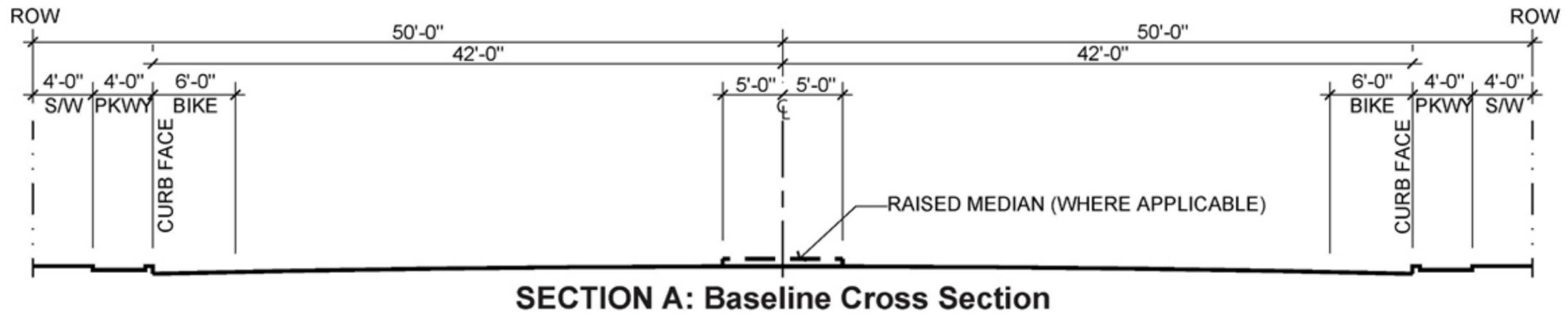


Exhibit 3-2 Circulation Plan

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S/W: Sidewalk (4' min)  
 PKWY: Parkway (4' min)

Note: 1) In all cases the minimum sidewalk width shall be 4' and the minimum parkway width shall be 4'.  
 2) For uses in the Flexible Amenity setback refer to Specific Plan Section 3.4.2 and Section 5.6.1. Uses may include additional landscaping, widening sidewalks, outdoor dining, or pedestrian-related uses.

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### 3.3.3 Bike and Pedestrian Improvements

#### A. Bike Lanes

Within the Specific Plan area, the single existing bike facility is a Class II bike lane (a striped, on-street bike lane adjacent to the travel lane) on Red Hill Avenue (in both directions), between El Camino Real and Nissan Road. The City of Tustin Master Bikeway Plan classifies the entire extent of Red Hill Avenue within the City limits as a designated or a potential Class II Bikeway.

*Class II Bike Lane: An on-street bike lane that uses painted stripes, stencils, and signs to delineate the right-of-way assigned to bicyclists and motorists*

The proposed circulation components of the Specific Plan include revisions to the roadway cross section for Red Hill Avenue to include a striped on-street bike lane the entire length of the study area to promote more multimodal travel opportunities.

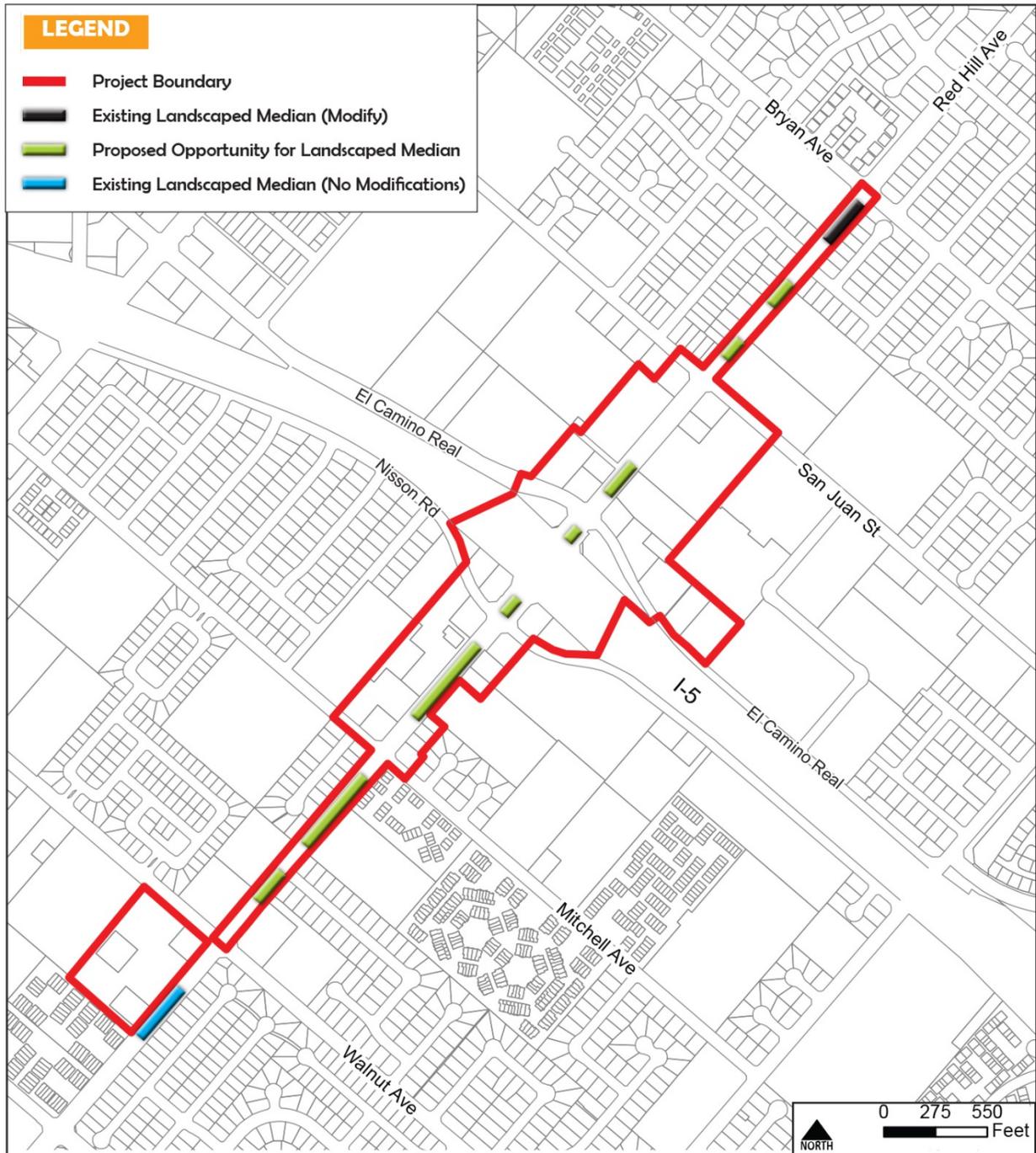
Enhanced bikeway signage will be introduced to promote bike usage and provide directions on how to connect to other bikeways or key points in the City.

Enhanced or decorative bike racks is another feature that is encouraged to be introduced within private developments. Going beyond conventional metal tubing, enhanced bike racks can be described as metal artwork. These types of racks enhance the right-of-way and serve as functional public art. They also present an opportunity to connect to the adjacent businesses; (e.g., a coffee cup-shaped bike rack could be provided in front of a coffee shop).

With more bikeway access and enhanced bike signage, more people know that they can bike safely. The intent of the recommended bikeway system improvements is to provide a safe, non-vehicular way for residents, employees, and students to travel.



# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 3-4 Schematic Median Locations**



Red Hill Avenue Segment from Walnut Avenue to Mitchell Avenue



Red Hill Avenue segment from Mitchell Avenue to I-5



Red Hill Avenue segment from I-5 to San Juan Street



Red Hill Avenue segment from San Juan Street to Bryan Avenue

Note: Median modification at Bryan Avenue to lengthen northbound Left turn pocket

**LEGEND**

 Potential Medians

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## B. Sidewalks

Consistent sidewalks are required along the length of the Specific Plan area, with a minimum width of four feet, sheltered from the street by a landscaped parkway as shown in the cross-sections (see Exhibit 3-3, *Red Hill Avenue Street Cross Sections*). Where traffic signals are either existing or proposed, pedestrian crosswalks are required.

### 3.4 Urban Design

Existing Red Hill Avenue includes inconsistent, minimal or no streetscape along its length, with landscaped medians only south of Walnut Avenue and south of Bryan Avenue. This section establishes a program of streetscape improvements within the public rights-of-way along the Red Hill Avenue corridor, as well as gateway signage enhancements.

The intent of these streetscape improvement concepts and gateway enhancements is to provide a “sense of place” or identity within the Specific Plan area, resulting in a consistent streetscape concept with expanded amenity areas adjacent to new development.

This section includes the following features:

- **Gateways and Signage** – Concepts for signage in the public right-of-way for the Specific Plan area.
- **Streetscape Enhancements** – Identification of streetscape improvements for both a consistent theme along the entire length of Red Hill Avenue and for new landscaped medians where they are feasible, considering the balance of desires for increased streetscape, adequate vehicular capacity, and the addition of on-street bike lanes.
- **Plant Palettes** – A plant palette for medians and parkways has been created which includes trees, shrubs, and groundcovers.

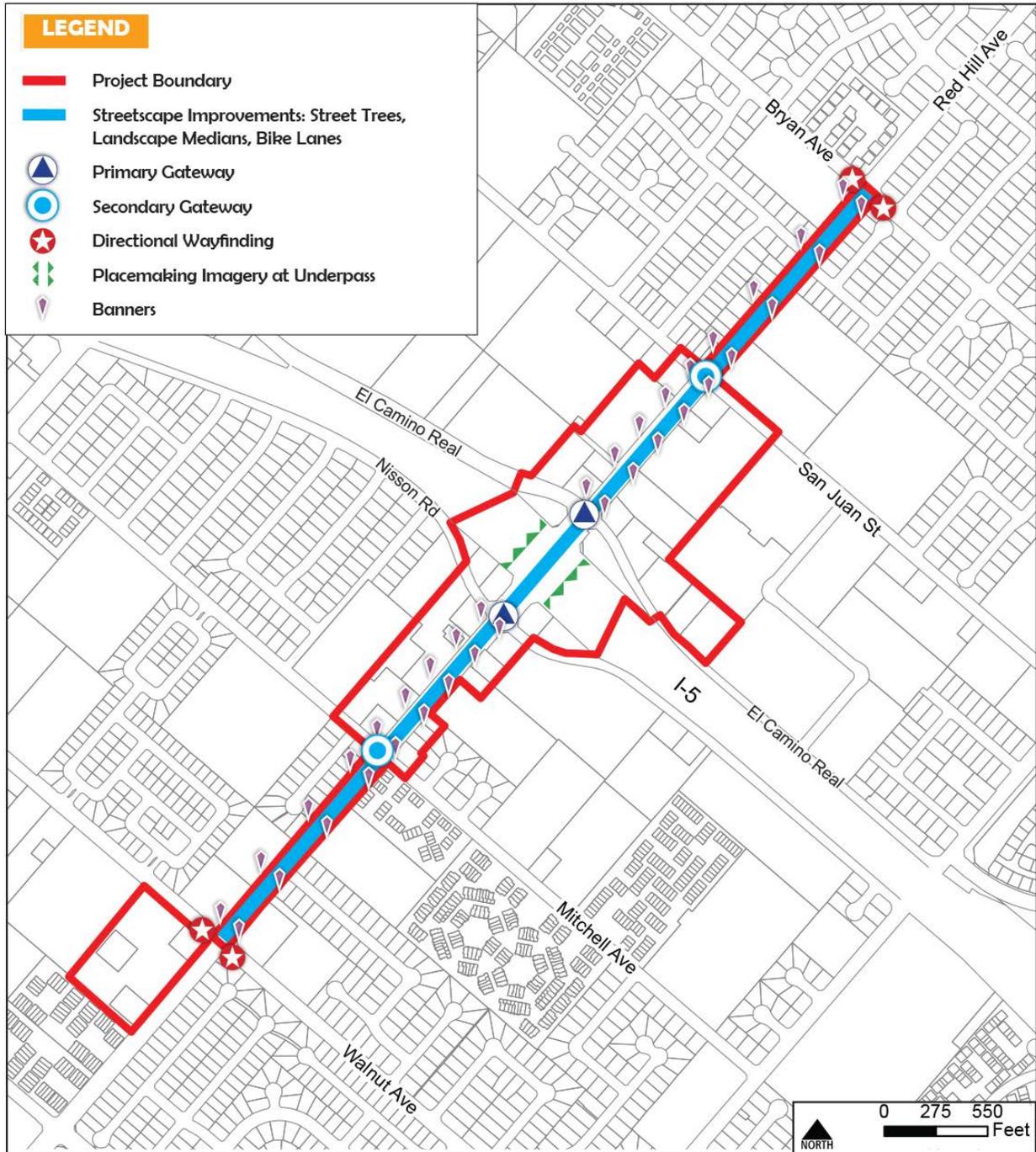
#### 3.4.1 Gateways

The proposed identity and wayfinding elements for Red Hill Avenue utilizes materials and colors which create a distinct sense of place, while maintaining a traditional look and feel throughout the Specific Plan area.

Although example images are shown, the final selection of the signage style and imagery will be undertaken as part of a separate City process. Gateway locations are illustrated in Exhibit 3-6, *Urban Design Plan*.

Four elements are illustrated in the Gateway Signage illustrations (see Exhibit 3-7, *Conceptual Illustration – Gateway & Secondary Gateway Signage* and Exhibit 3-8, *Signage Concepts*), including primary and secondary gateway elements, directional signage, and banners.

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 3-6 Urban Design Plan**

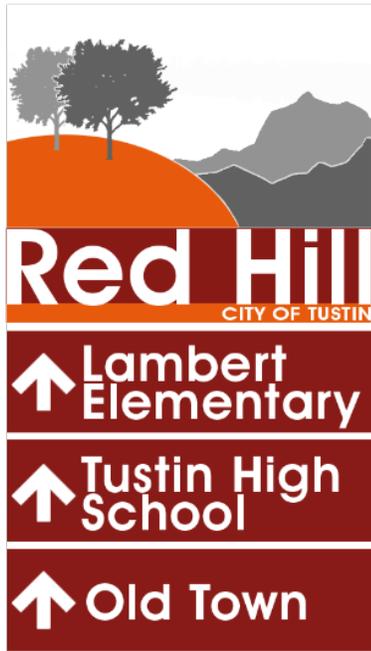
# RED HILL AVENUE SPECIFIC PLAN



Exhibit 3-7 Conceptual Illustration - Gateway & Secondary Gateway Signage



*Banners*



*Directional Signage*



*Freeway Underpass Imagery*

**Exhibit 3-8 Signage Concepts**

## 3.4.2 Streetscape

Phased streetscape improvements will be implemented that will contribute significantly to the enhancement of the visual appeal and identity of the Red Hill Avenue public realm. Streetscape improvements have been designed to promote attractive and compatible environments for the desired types of new development. The improvements will provide for shade and include human-scaled elements.

**Public Realm:** This term references the publicly-owned areas, typically including the street itself, sidewalks, parkways, and public parks.

The basic streetscape will consist of parkway plantings adjacent to the street along the entire length of Red Hill Avenue, with new landscaped medians where feasible.

- Section A of Exhibit 3-9, *Streetscape Cross Sections* illustrates the minimum streetscape standard along the length of the Specific Plan area. In all cases, a minimum four-foot parkway and four-foot sidewalk is required.
- Section B of Exhibit 3-9, *Streetscape Cross Sections* and Exhibit 3-12, *Streetscape Typical Plan* illustrate the general ultimate configuration adjacent to new development where the City has, or requires dedication of, the full 120-foot Red Hill Avenue right-of-way (referred to as the flexible amenity setback space). This section includes a required minimum four-foot sidewalk and a 14-foot flexible amenity space for a total 18-foot Flexible Amenity Setback. The Flexible Amenity Setback may be comprised of a widened walkway area, plaza space, enhanced landscaping, or outdoor dining space. Other uses of this space may be proposed as part of a development application subject to Community Development Director review and approval as outlined in Chapter 6, *Implementation and Administration*. Section 5.6 of Chapter 5, *Relationship to the Public Realm*, outlines the potential uses of the Flexible Amenity Setback in more detail.

Streetscape improvements will be installed and paid for through a combination of public and private investment. New private development will include the installation of (or bonding for the future installation of) sidewalk and landscaping improvements between the property line and curb. Implementation of public streetscape improvements will be phased over time, as financial resources allow.

Exhibit 3-12, *Streetscape Typical Plan*, illustrates one concept for design of the public realm streetscape. Actual design will be determined as part of a development application or a public improvement project by the City. Uses within the Flexible Amenity Setback may include outdoor dining, seating, plaza space or landscaping as approved through design review as discussed in Chapter 5.

## 3.4.3 Landscape Palettes

The RHASP plant palette is intended to provide a variety of landscape choices to complement the design and development of the Specific Plan area. The plant palette provides a range of trees, shrubs, groundcover and accents to enhance and promote attractive and water conserving plant materials suitable for Tustin's local climate zones. The plant palette identifies a streetscape theme for parkway

landscape and medians. Exhibits 3-10, *Public Realm Plant Palette (Trees)* and 3-11, *Public Realm Plant Palette (Shrubs and Groundcovers)*, show the intended plant materials for use in the public right-of-way, including the Flexible Amenity Setback.

The proposed landscape palette promotes a cohesive aesthetic streetscape throughout the Red Hill Avenue public realm by utilizing low-water species of trees and shrubs consistent with a Southern California streetscape character. Final design will be subject to a separate City process.

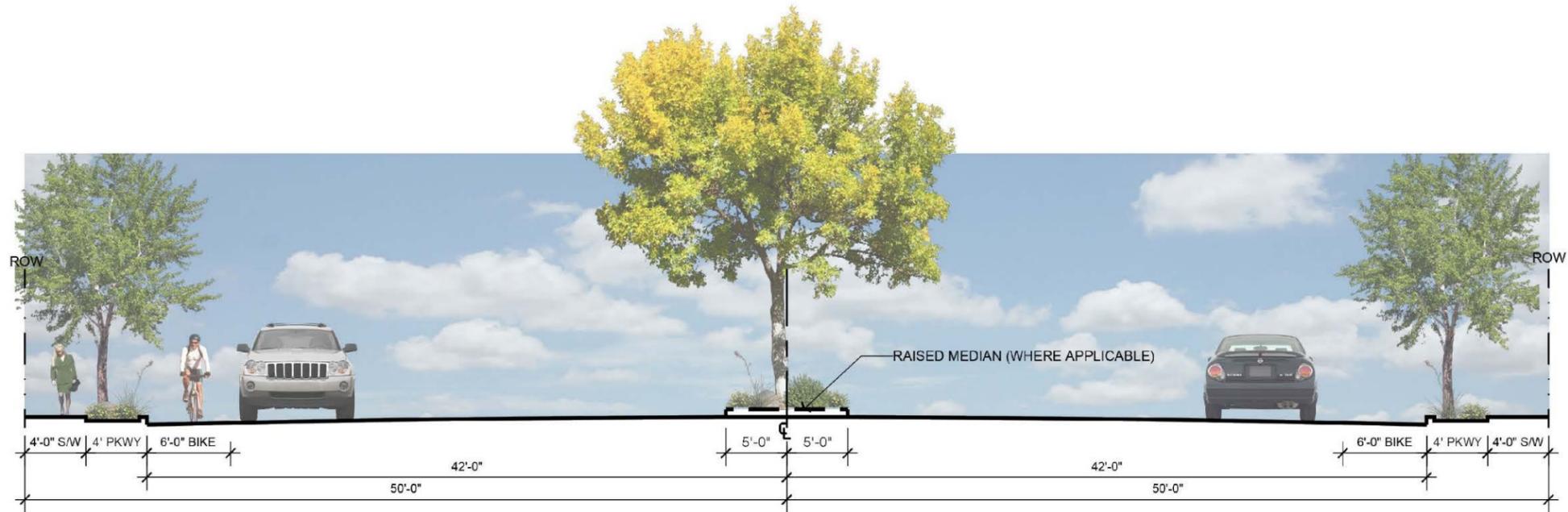
Landscaping installed with adjacent future development shall expand upon this character with a palette which is complimentary but does not distract from or disrupt the proposed streetscape for Red Hill Avenue. There is no specific required plant palette for private development.

### 3.4.4 Street Furniture

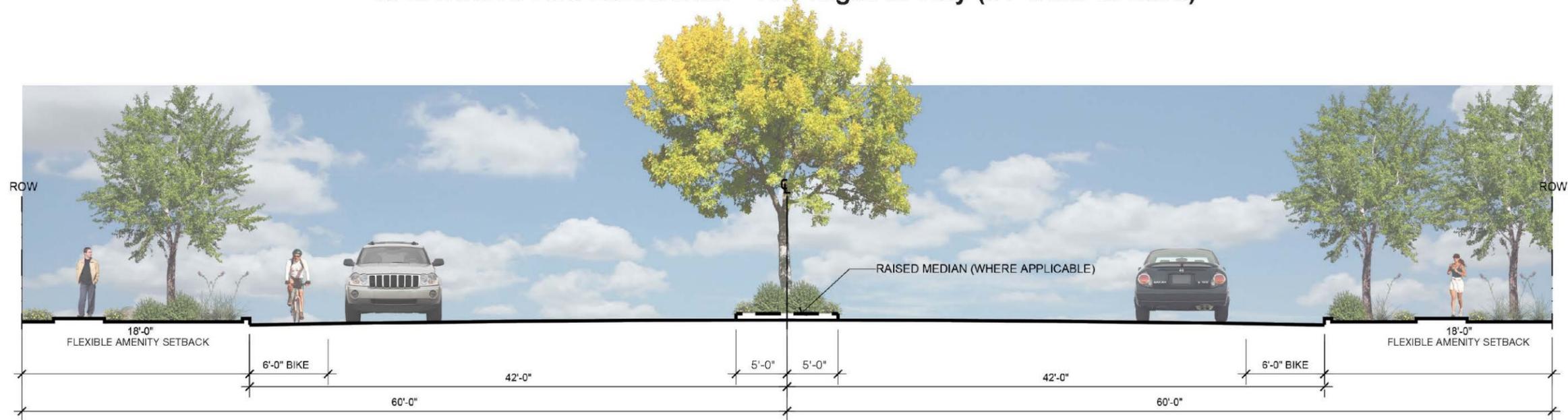
Street furnishings can enliven and provide variety to spaces used by the public, serving an aesthetic and utilitarian function. Street furniture in the public realm (i.e. public right-of-way) includes bike racks, trash receptacles, benches, bollards and bus shelters/stops.

As the City implements public street improvements along the corridor, the following factors should be taken into consideration:

- A unified appearance is encouraged. Street furniture products shall be made of durable, high-quality materials. The appearance and color of these items shall complement other design elements.
- Items should be securely anchored to the sidewalk and a graffiti-resistant coating should be applied to street furniture surfaces.
- The design and selection of street furniture should include considerations for the security, safety, comfort and convenience of the user.
- Benches should be placed to conserve sidewalk width, ensure free pedestrian flow, and accommodate the needs of the disabled. Space should be provided adjacent to walkways for wheelchairs.
- Benches on sidewalks and other rights-of-way shall be made of a durable material such as concrete, powder coated metal, or hardwood and be designed to minimize the effects of vandalism and skateboarding.
- Trash receptacles should be located in high-activity areas, such as plazas and other pedestrian areas within the Flexible Amenity Setback. Provision of trash receptacle and/or seating shall be reviewed by the City as part of site plan/design review approvals.
- The style and color of trash receptacles should be compatible with the desired style of other street furnishings.
- New bus shelters should be installed at transit stops where no benches are currently provided. Older bus benches or shelters should be replaced with models of a uniform design and color. Bus shelter design will require coordination with OCTA.



**SECTION A: Red Hill Avenue - 100' Right-of-Way (84' Curb to Curb)**



**SECTION B: Red Hill Avenue - 120' Right-of-Way with Flexible Amenity Setback (84' Curb to Curb)**

SW: Sidewalk (4' min)  
 PKWY: Parkway (4' min)

Note: 1) In all cases the minimum sidewalk width shall be 4' and the minimum parkway width shall be 4'.  
 2) For uses in the Flexible Amenity setback refer to Specific Plan Section 3.4.2 and Section 5.6.1. Uses may include additional landscaping, widening sidewalks, outdoor dining, or pedestrian-related uses.

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Median Tree



Native California Sycamore  
*Platanus racemosa*



Chinese Pistache  
*Pistacia chinensis*



Honeylocust  
*Gleditsia triacanthos*

**PROPOSED PLANT PALETTE**

The proposed landscape palette promotes a cohesive aesthetic throughout the corridor, by utilizing species consistent with a traditional Southern California, low-water species, streetscape character. Off-site and adjacent future development shall expand upon this character with a palette which is complementary but does not distract or disrupt the proposed streetscape for Red Hill Ave. Additionally, the City will create a landscape plan for the area which will identify trees from this list to be used in future development. All identified species in the proposed plant palette comply with the City of Tustin's Water Efficient Ordinance.

Parkway Trees



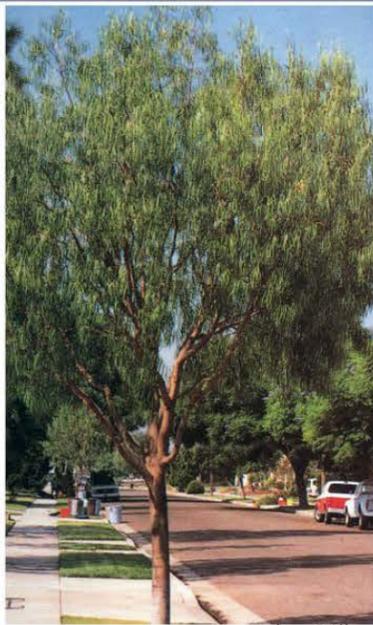
Brisbane Box  
*Lophostemon confertus*



Japanese Zelkova  
*Zelkova serrata*



After Dark Peppermint Willow  
*Agonis flexuosa 'After Dark'*



Australian Willow  
*Geijera parviflora*



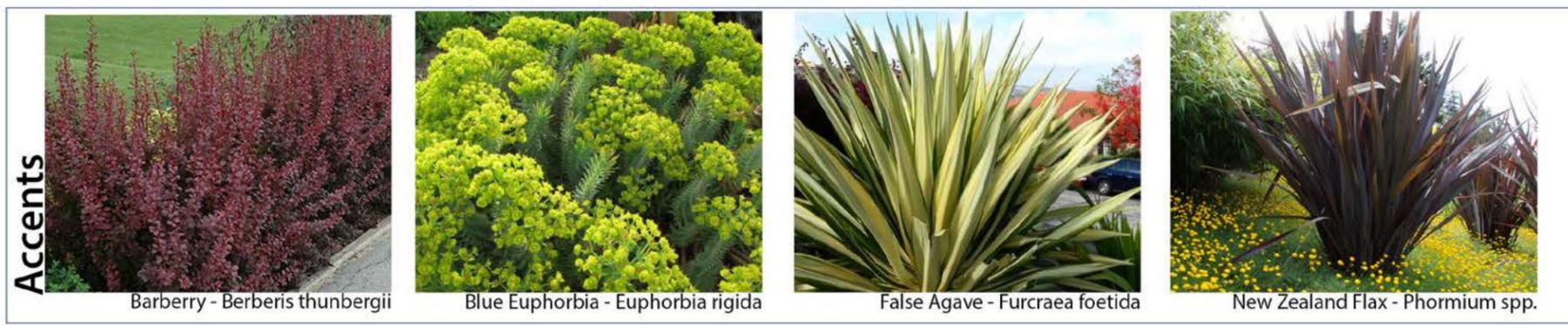
Crape Myrtle  
*Lagerstroemia*



Fruitless Olive  
*Olea europaea*

**Exhibit 3-10 Public Realm Plant Palette (Trees)**

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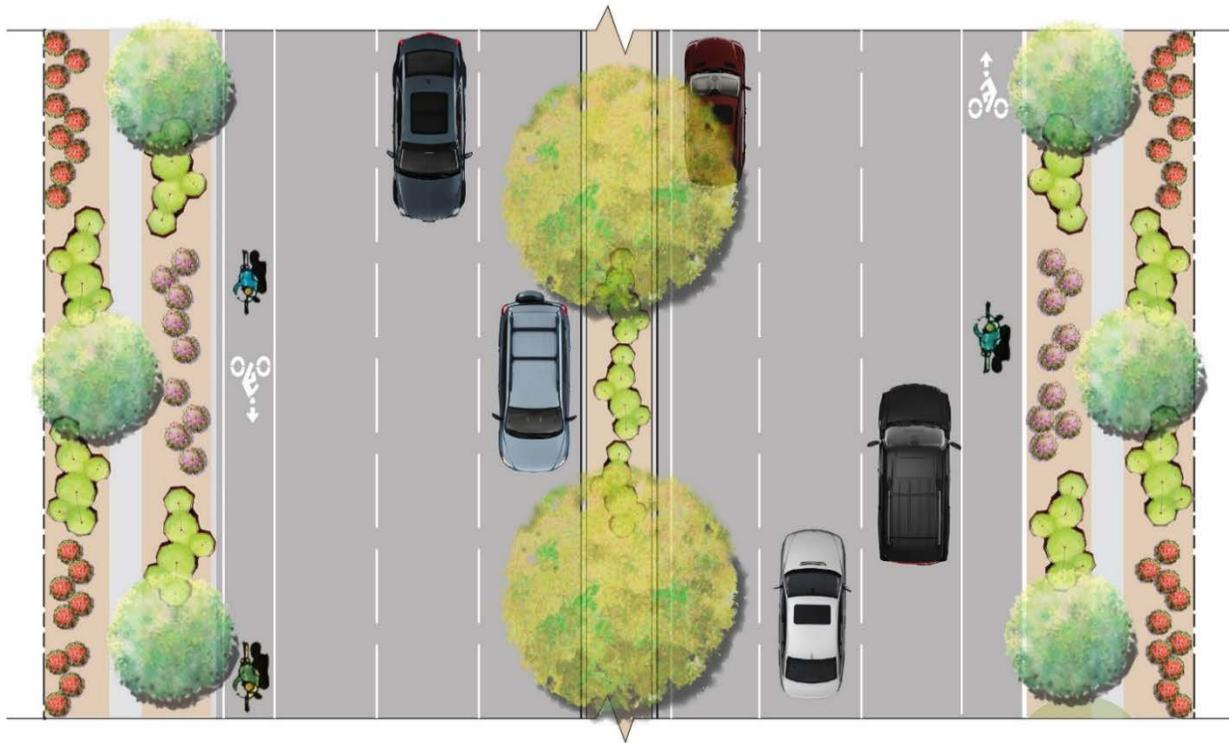
**PROPOSED PLANT PALETTE**  
 The proposed landscape palette promotes a cohesive aesthetic throughout the corridor, by utilizing species consistent with a traditional Southern California, low-water species, streetscape character. Adjacent future development shall expand upon this character with a palette which is complementary but does not distract or disrupt the proposed streetscape for Red Hill Ave. All identified species in the proposed plant palette comply with the City of Tustin's Water Efficient Ordinance.



**Exhibit 3-11 Public Realm Palette (Shrubs and Groundcovers)**

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# RED HILL AVENUE SPECIFIC PLAN



*The proposed streetscape enhancements are designed to create a cohesive and coordinated aesthetic throughout the Red Hill Avenue corridor, by utilizing materials and amenities that promote a timeless look and feel. Selected colors and materials should be light in color to help minimize the urban heat island effect and create a pleasant summer-time experience for pedestrians. All amenities should have powder-coated coverings to promote ease of maintenance.*

*The Typical Plan shown is one concept for the design of the public realm area, showing an enhanced sidewalk and landscaped area as part of the Flexible Amenity Setback. Other concepts are allowable, including seating areas, plaza spaces, outdoor dining areas (subject to City licensing). Chapter 5, Design Criteria, Section 5.6 Relationship to the Public Realm, and Exhibit 5-6, Flexible Amenity Setback Options, include additional detail of potential uses within this area.*

**Exhibit 3-12 Streetscape Typical Plan**

## 3.5 Infrastructure and Public Services

The public improvements described in this section are utilized as guidelines, with specific improvements to be determined as part of a City Capital Improvement Program or requirements imposed as part of discretionary approval of private development. The infrastructure improvements described in this section include:

- **Potable Water** – Description of the water conveyance system and additional anticipated water infrastructure in the Specific Plan area.
- **Sewer** - Description of the sewer conveyance system in the Specific Plan area.
- **Drainage System and Water Quality** – Description of the stormwater conveyance system and a menu of water quality features appropriate for an urbanized environment such as the Specific Plan area.
- **Other Utilities and Services** – Description of dry utilities and services.

### 3.5.1 Potable Water

The domestic water system is owned and operated by the City of Tustin. The City is part of the East Orange County Water District (EOCWD), a wholesale water district, and a member agency of the Metropolitan Water District of Orange County (MWDOC). The Specific Plan area lies within the City's Pressure Zone 1, with a hydraulic grade of 308 feet. The City receives approximately 85 percent of its water from its own groundwater wells that tap the Lower Santa Ana Groundwater Basin and the remaining 15 percent from EOCWD. By 2035, the City expects to produce approximately 71 percent of its own water through groundwater extraction and import the remaining 29 percent from EOCWD. The City's 2016 Urban Water Management Plan (UWMP) concludes that water supply is available and reliable for the City of Tustin through 2035, including an aggregate seven percent increase in City population. Buildout of the proposed Specific Plan would be served from existing entitlements and new or expanded water entitlements would not be needed due to diversified supply and conservation measures. The City is capable of meeting all customers' demands within its service area through the purchase of significant reserves held by Metropolitan, local groundwater supplies, and through implementation of conservation measures in multiple dry years from 2020 through 2040. In addition to City water lines, the Metropolitan Water District (MWD) has a transmission main that crosses through the Specific Plan area but will not be altered or affected by actions associated with implementation of the Specific Plan.

The water facilities within the Specific Plan area are identified in Exhibit 3-13, *Water Plan*. In addition to the pipe infrastructure in the streets, there are sites that contain public water mains in water easements across private property.

To provide potable water and fire service to the existing and proposed land uses within the Specific Plan area, additional water infrastructure is required. It is anticipated that the section of existing six- and

eight-inch water mains in Red Hill will be replaced with a larger diameter pipe and will extend east from the Interstate 5 (I-5) to the terminus at San Juan Street as a condition of development of the adjacent properties. The City also has a long-range plan for the upgrade of other sections of water mains in the corridor. Other anticipated improvements include public meters and backflow devices that would be required for domestic water service and/or separate fire lines for individual developments as they occur.

The Specific Plan area is within the jurisdiction of the Orange County Fire Authority (OCFA). As development progresses, some plans may require review by the OCFA to ensure compliance with its standards. These standards include providing minimum fire department access and fire flow requirements.

Fire flow requirements are based upon building size and building construction type. The latest fire regulations require all buildings to be equipped with a fire sprinkler system, including residential uses. Site-specific hydraulic analysis shall be performed as individual developments move forward. This analysis will help determine specific water and fire needs.

### 3.5.2 Sewer

The local sanitary sewer mains within public streets are owned and operated by EOCWD. Orange County Sanitation District (OCSD) owns and maintains a network of regional sewer trunk mains, lift stations, and two wastewater treatment plants, one in Fountain Valley, and the other in Costa Mesa. OCSD has identified a capacity deficiency in its Mitchell Avenue and Red Hill Avenue trunk mains located just downstream of the Specific Plan area. OCSD indicated that the deficiency issues are due to wet weather flows emanating from the City of Santa Ana and that dry conditions flows do not cause system surcharges. No other system deficiencies have been identified by the Sanitation District. In Fall 2017, the Orange County Sanitation District (OCSD) completed work on its Gisler-Red Hill System Improvements Project, which consisted of new sewer pipe construction on Red Hill Avenue between Warner Avenue and Edinger Avenue and rehabilitation of existing sewers and manholes between McGaw Avenue and Mitchell Avenue. With the exception of the segment between Walnut Avenue and Mitchell Avenue, these improvements are south of the Specific Plan area. Existing and proposed sewer pipe infrastructure is shown in Exhibit 3-14, *Sewer Plan*.

All new developments within the Specific Plan area will be reviewed on a project-by-project basis by the City of Tustin to determine if local sewer lines have sufficient capacity to accommodate waste from new development.

In conjunction with new development within the Specific Plan area, the property owner will need to apply for a new sewer connection permit through the EOCWD. The property owner is responsible for the lateral from the property line to the local sewer main. The owner will also be responsible for obtaining will-serve letters from EOCWD.

Significant regional sewer service upgrades for this Specific Plan area are not anticipated as capacity exists within the current system and can accommodate the demand created by the anticipated new development.

OCSO discourages direct connection to its regional trunk line and only allows connection to regional OCSO manholes. However, special discharge permits may be obtained when no alternative discharge point exists or they pose an environmental impact. Trunk connection permits will only be issued by EOCWD.

- Developers will be charged for connecting into existing sewer laterals and for any upgrade resulting from the anticipated new development.
- Any system deficiencies that will be impacted by new development must be corrected at the expense of the developer and/or property owner.
- All future development will be required to adhere to the provisions regarding sewer capacity allotment in the City.
- Any new sewer lateral will be designed in accordance with the Tustin City Code.

### 3.5.3 Drainage

The Specific Plan area lies within the Peters Canyon tributary area of the San Diego Creek watershed. Regional drainage facilities are owned and operated by Orange County Public Works, Flood Division (OCPW). Local drainage facilities are owned and operated by the City of Tustin. The Specific Plan area lies within FEMA flood zone X, and is not subject to flood insurance requirements.



The following may be required in conjunction with future development:

- Applicants for all future development within the Specific Plan area shall prepare a hydrology and hydraulics analysis demonstrating that the existing condition flow rates are not exceeded by proposed project flow rates.
- Direct connection to the City’s existing storm drain system is preferable provided that the existing tributary areas and flow rates to the existing drains are not exceeded by new development. Alternatively, applicants may provide hydraulic analyses of the downstream storm drain system that demonstrate no significant impacts to the City storm drain infrastructure.
- Where existing storm drains are not available for connection, applicants shall propose drainage systems utilizing parkway drains to direct runoff directly to the adjacent street curb and gutter section. In all cases, stormwater quality requirements must be met.

- Applicants for future development within the Specific Plan area will be required to apply for encroachment permits through the City of Tustin for connection into the City storm drain infrastructure. Existing drainage infrastructure is shown in Exhibit 3-15, *Drainage Plan*.

### 3.5.4 Storm Water Quality

Improvement of water quality is a critical issue for all development. Local, State and Federal laws include requirements for the treatment of stormwater runoff to reduce pollutants entering the environment.

The Specific Plan area lies within a hydromodification zone, as defined in the Stormwater Quality Technical Guidance Document (TGD) prepared by the County of Orange. The purpose of hydromodification management is to incorporate hydrologic controls within a proposed development such that post-development two-year peak flows do not exceed pre-development conditions. Reducing hydromodification can protect and restore the downstream receiving waters. Receiving waters for the Specific Plan area consist of Peters Canyon Channel, San Diego Creek, and Newport Bay. Hydromodification will likely be a minimal concern since current regulations allow for discharge up to the existing flows generated from a project site.

As an urbanized area, opportunities for large-scale water quality improvements (such as stormwater basins) within the Specific Plan area are limited. Best Management Practices (BMPs) are structural or engineered devices and systems used to treat stormwater runoff before it is discharged into a drainage system (storm drain or channel).

BMPs appropriate for an urbanized setting may include the following menu of items as defined in the TGD prepared by the County of Orange:

- **Underground Infiltration:** Underground infiltration systems consist of a network of perforated pipes or vaults and are constructed in naturally pervious soils, as confirmed by infiltration testing, with a flat bottom.
- **Permeable Pavement:** Permeable pavements contain small voids that allow water to pass through to a gravel base. They come in a variety of forms; they may be a modular paving system (concrete pavers, grass-pave, or gravel-pave) or poured in place pavement (porous concrete, permeable asphalt).

*Hydromodification refers to changes in runoff characteristics caused by altered land use and increase of impervious areas.*

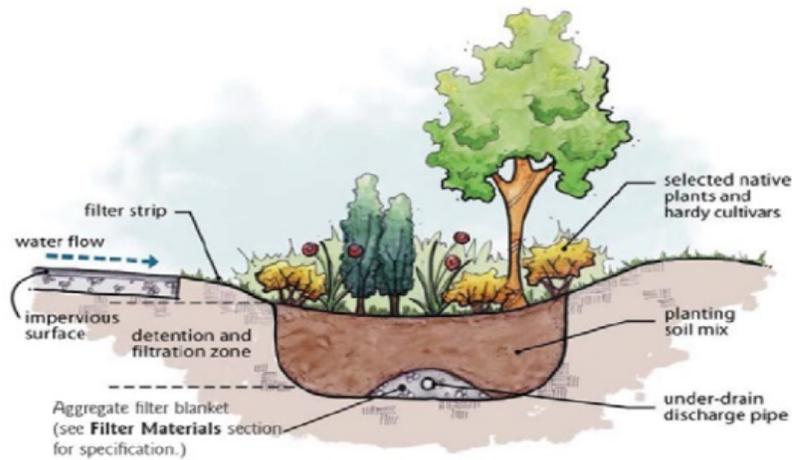
*Too much hydromodification can cause erosion of stream banks and beds. Best Management Practices (BMPs) can include structural BMPs to reduce flows or volumes thus reducing impacts to downstream channels.*



*Permeable Pavers*

# RED HILL AVENUE SPECIFIC PLAN

- **Biofiltration/Bioretention:** Bioretention stormwater treatment facilities are landscaped shallow depressions that capture and filter stormwater runoff. These facilities function as a soil and plant-based filtration device that removes pollutants through a variety of physical, biological, and chemical treatment processes. Vegetated swales, filter strips, and planter boxes fall within this category.



*Biofiltration Vegetated Filter strip/swale*



*Biofiltration/Bioretention*



*Green Roof*

- **Green Roofs:** Green roofs reduce stormwater runoff volume by retaining a portion of rainfall. Green roofs also provide biotreatment/biofiltration of water draining through and over roofs, removing pollutants deposited from the atmosphere or from adjacent transportation land uses.

Prior to issuance of grading permits for any development project within the Specific Plan area, a Water Quality Management Plan (WQMP) must be submitted for review and approval to the City Public Works Department. A preliminary WQMP will be submitted as part of the entitlement process for individual development projects; it will outline the required quantities of stormwater required to be treated and the appropriate treatment methods. A final WQMP will be submitted as part of final construction documents, which will describe the final selection of BMPs for the proposed development.

Low Impact Development (LID) principles shall be incorporated into the design of all new developments. The hierarchy of preferred treatment methods are as follows: infiltration, harvest-and-reuse, evapotranspiration, or biofiltration of the 85th percentile storm event on-site.

## 3.5.5 Other Utilities and Services

### A. Electrical Service

Southern California Edison Company (SCE) is the distribution provider for electricity in Tustin. Electrical structures and power lines are owned and operated by SCE. New electrical service within SCE's service area is provided on an as-needed basis. SCE is regulated by the California Public Utilities Commission (CPUC), which protects customers from overcharge and promotes energy efficiency, system reliability, and financial integrity of utilities. SCE delivers power to 15 million people in 50,000 square-miles across central, coastal and Southern California, excluding the City of Los Angeles and some other cities (SCE, 2017).

The Specific Plan area is currently connected to the SCE power grid. Existing overhead services are located on the eastern edge of Red Hill Avenue north of El Camino Real and along El Camino Real. The aboveground facilities include a single four-wire 12 kV system for SCE, four to six communication lines, and local street lights and their electrical distribution system, under a pole-sharing arrangement. At the intersection of Red Hill and Bryan Avenues, the pole line joins an existing SCE 66KV transmission pole on Bryan, continuing to the northeast along Red Hill Avenue. At the intersection of Red Hill and El Camino Real these facilities are underground. South of I-5, electrical lines are underground.

As part of future development in the Red Hill Specific Plan area north of I-5, the overhead power lines will be undergrounded from the I-5 to the terminus of San Juan Street in accordance with City policies.

### B. Gas

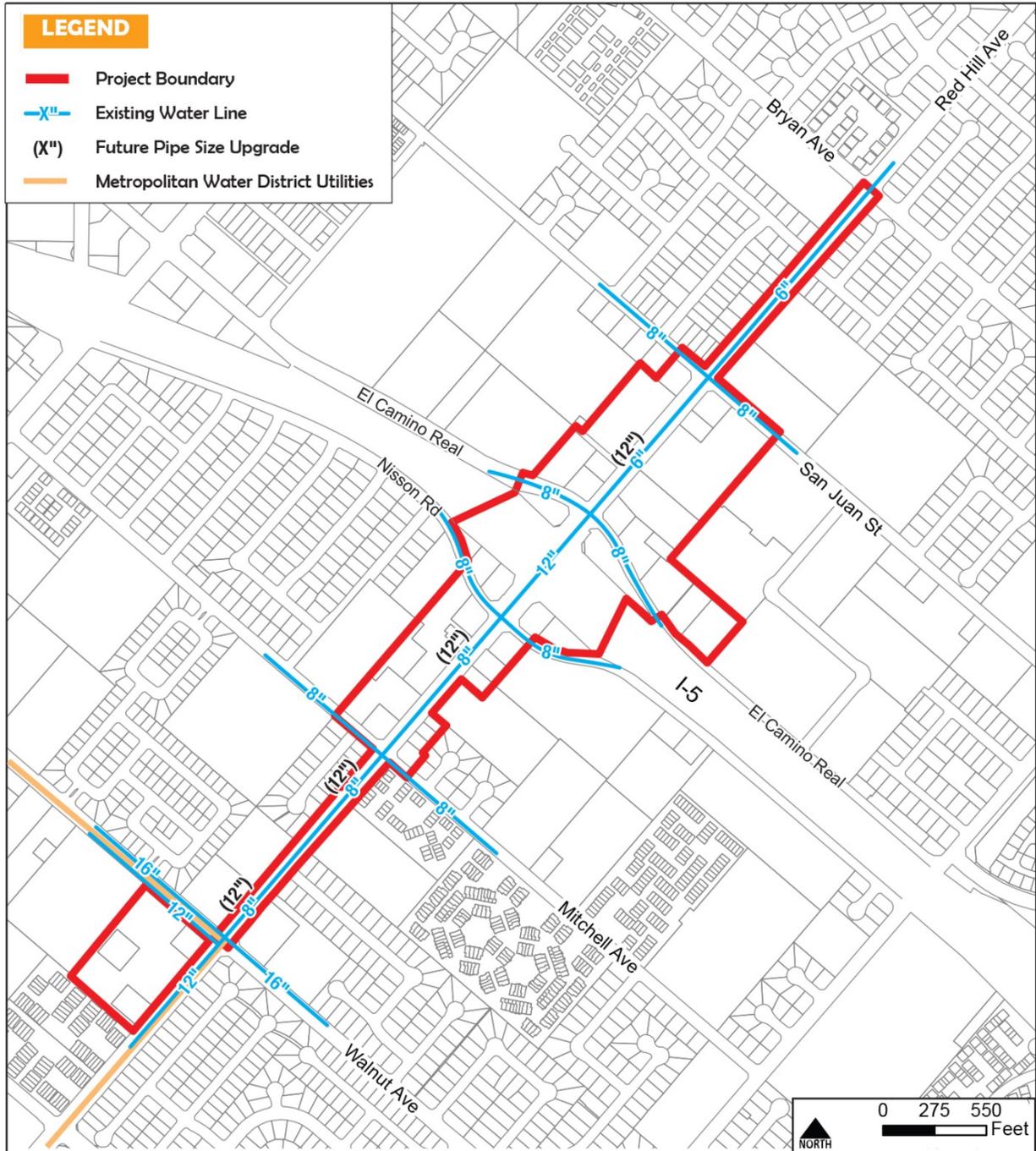
Southern California Gas Company (SoCalGas) provides natural gas to the City. The company's service territory encompasses approximately 23,000 square miles in most of central and Southern California. The Specific Plan area currently has natural gas service.

### C. Solid Waste

The City currently contracts for refuse collection. The materials are then transported to a Materials Recovery Facility where they are sorted for recyclables. CR&R Environmental Services operates five Material Recovery, Transfer and Disposal locations in California. The nearest centers are in the cities of Stanton and San Juan Capistrano.

The County of Orange owns and operates three landfills: Olinda Alpha Landfill in Brea, Frank R. Bowerman in Irvine, and Prima Deshecha in San Juan Capistrano (OC Waste & Recycling, 2016a). Currently, only the Frank R. Bowerman Sanitary Landfill, located at 11002 Bee Canyon Access Road in Irvine, serves the City.

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 3-13 Water Plan**

# RED HILL AVENUE SPECIFIC PLAN

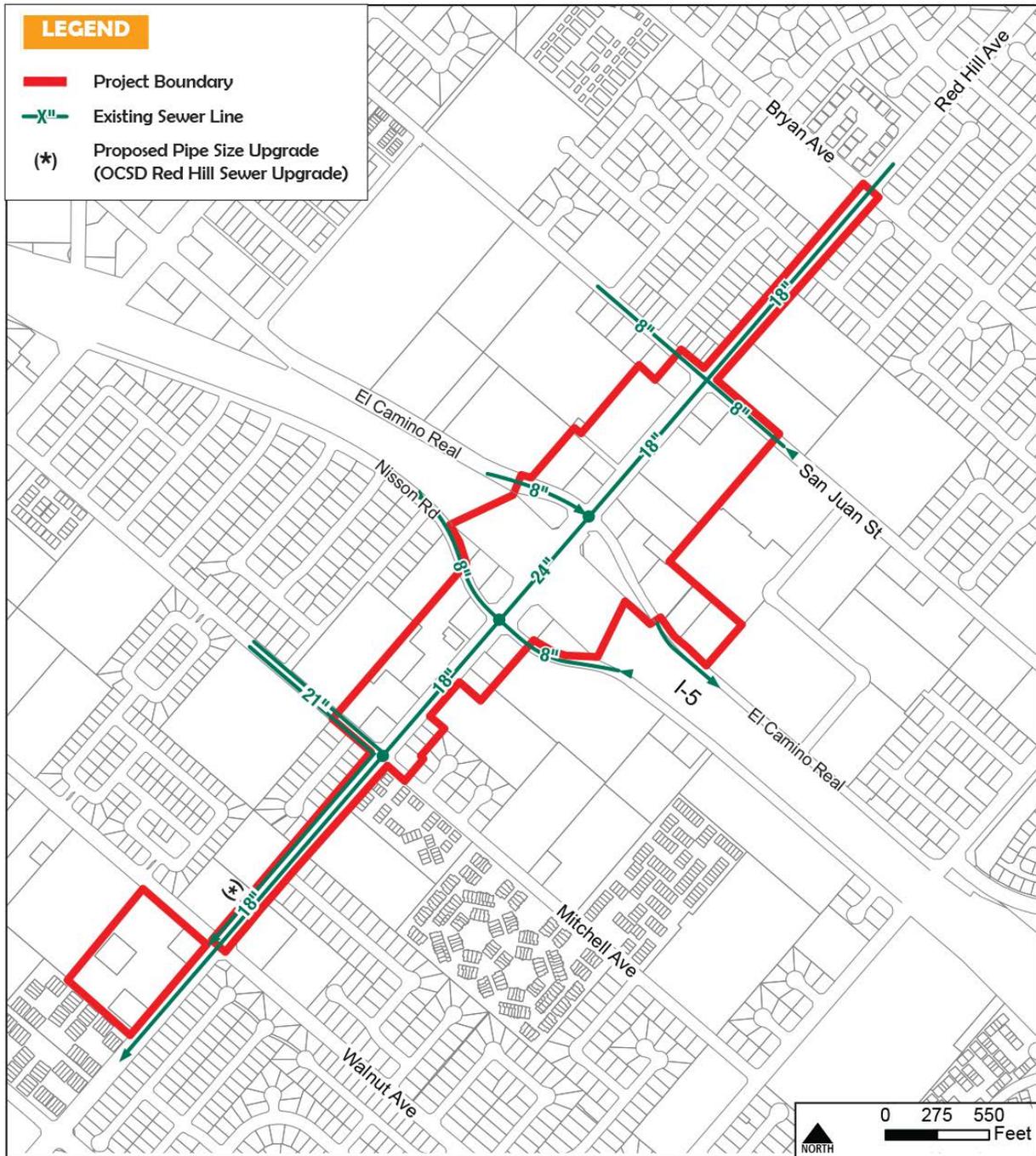
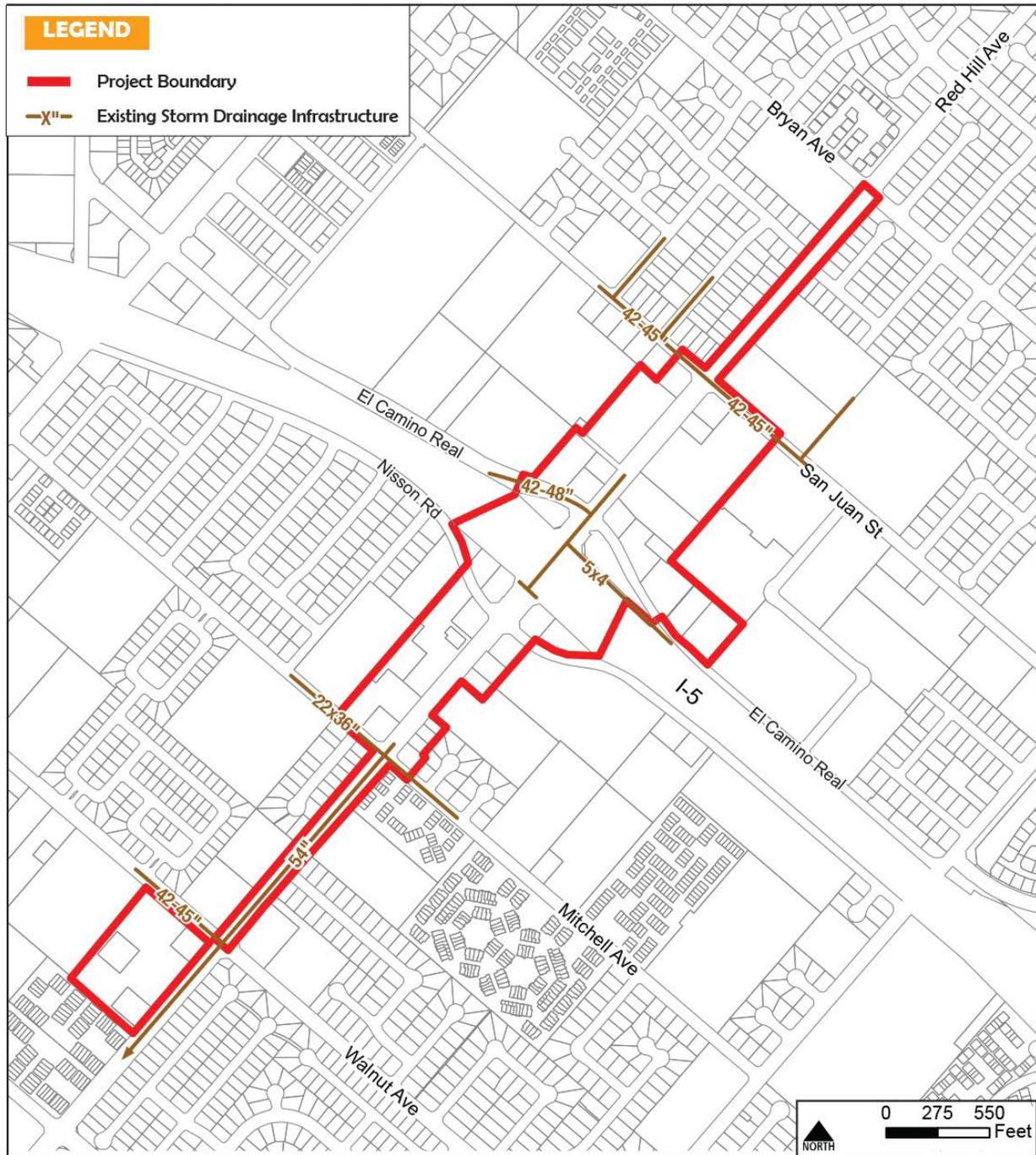


Exhibit 3-14 Sewer Plan

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 3-15 Drainage Plan**

## D. Schools

The Specific Plan area lies within the boundaries of the Tustin Unified School District (TUSD), which serves elementary, middle, and high school students in the City. The closest elementary schools to the Specific Plan area are Benjamin Beswick Elementary School at 1362 Mitchell Avenue, and Marjorie Veeh Elementary School at 1701 San Juan Street. The closest middle school is C.E. Utt Middle School, at 13601 Browning Avenue. Tustin High School is located near the Specific Plan area at 1171 El Camino Real. School mitigation fees will be collected when building permits are issued for new residential and commercial projects developed within the Specific Plan area to mitigate impacts to schools.

## E. Fire Protection and Emergency Medical Services

The Orange County Fire Authority (OCFA) is a regional fire service agency that serves 23 cities in Orange County, including the City of Tustin, and all unincorporated areas. The OCFA protects over 1,680,000 residents from its 71 fire stations located throughout Orange County. Response times in the City vary based on the level of emergency; however, the response time goal is for the first unit to arrive on scene within five minutes of receipt of the call, ninety (90) percent of the time. OCFA also operates an Emergency Medical Services Section that manages the delivery of medical services by OCFA emergency medical technicians and paramedics.

## F. Police Protection

Police protection services in the plan area are provided by the Tustin Police Department, headquartered at 300 Centennial Way. The Tustin Police Department has approximately 100 sworn officers and 55 civilian support personnel. Tustin Police Department work in partnership with other City departments, the residential and business communities, and other governmental and non-profit agencies to reduce crime, provide safety and security, and improve the quality of life for those who visit, live, and work in the City of Tustin. As of the date of this Specific Plan, there are no specific plans for expansion of police facilities or addition of staff or equipment inventory (Tustin Police Department, 2017).

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# 4 LAND USE AND DEVELOPMENT STANDARDS

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## 4. Land Use and Development Standards

### 4.1 Purpose

This Chapter establishes the land use program, Planning Areas for the Red Hill Avenue Specific Plan (RHASP), the allowable land uses (permitted and conditionally permitted), and the development standards that apply within each area. Together, the table of permitted uses and the development standards prescribe the allowed development for the RHASP area. The intent of the development standards is to implement the goals, objectives, and policies of the RHASP.

The RHASP area is envisioned as a distinctive, vibrant and thriving district with enlivened and interconnected commercial, office, and residential uses. The mixed-uses represent an evolution of land uses that respond to the changing marketplace. The area will support multi-modal transportation practices and activate key areas where people can safely and efficiently walk, ride a bike, catch a bus, or drive a car. The area will engage the surrounding neighborhoods at key locations and reflect the small-town spirit of Tustin. Over time, the influx of commercial retail and service businesses, restaurants, office and new residents will enhance the economic and social vitality of the area and establish the Specific Plan area as a unique district with its own character. The streets and public spaces will be enhanced through landscaping and street furniture and framed by quality architecture expressed in a variety of building styles. The land use regulations, development standards and design criteria are consistent with the goals and policies of the General Plan.

### 4.2 General Provisions

#### 4.2.1 Minimum Requirements

The land use and development standards contained herein are minimum requirements. In reviewing individual projects requiring discretionary approval, more restrictive standards or conditions may be applied, if deemed necessary, to accomplish the goals and objectives of this Specific Plan.

#### 4.2.2 Applicability of Development Standards and Design Criteria

The land use and development standards contained in this Chapter shall apply to all new construction projects or development proposals, including additions or modifications to existing buildings, and changes in use, as provided for in Chapter 6, *Implementation and Administration*. The Design Criteria contained in Chapter 5 shall supplement the development standards.

### 4.3 Establishment of Permitted Land Uses

The RHASP provides planning policies and regulations that connect General Plan policies with project-level development within the Specific Plan area. The purpose of the Specific Plan is to guide future change, promote high quality development, and implement the community's vision for the area. The Specific Plan provides long and short-term goals and objectives, a land use plan, regulatory standards, design criteria,

and administrative and implementation programs to encourage integrated mixed-use, development within the RHASP area.

The RHASP is intended to provide a complementary mix of commercial, office and residential uses in an integrated, pedestrian-friendly environment. The Specific Plan area is intended to expand and enhance commercial opportunities to serve a local and regional customer base. The district is intended to physically and functionally integrate land uses in a manner that enhances livability through complimentary relationships with existing uses, improved pedestrian environment, and sidewalk-adjacent development. Proposed development and improvements would be subject to a set of design criteria and standards that are described in the Specific Plan Land Use Regulations and Development Standards Chapter.

### 4.3.1 Definitions

The RHASP's permitted land uses shall comply with the definitions contained in Section 9297 of the Tustin City Code (TCC). Additional definitions for the RHASP include:

- **"Biofiltration Facilities"** shall mean landscaped shallow depressions that capture and filter stormwater runoff.
- **"BMPs (or Best Management Practices)"** shall mean structural or engineered devices and systems used to treat stormwater runoff before it is discharged into a drainage system (storm drain or channel).
- **"CEQA/California Environmental Quality Act"** shall mean the California statute passed in 1970 to institute a statewide policy of environmental protection.
- **"Class II Bike Lane"** shall mean an on-street bike lane that uses painted stripes, stencils, and signs to delineate the right-of-way assigned to bicyclists and motorists.
- **"Complete Streets"** shall mean streets, sidewalks, and public rights-of-way that are designed, operated, and maintained to enable safe access for all users – pedestrians, bicyclists, transit riders, and motor vehicle drivers.
- **"Flexible Amenity Setback Area"** shall mean a component of the public right-of-way that adjoins private development in areas that have or will be required to dedicate the full 60-foot half-width right-of-way for Red Hill Avenue. Because the driving surface (curb-to-curb width) of the roadway will not change as part of this Specific Plan, in the interim, if an activity needs the full dedication for roadway purposes, the additional right-of-way may be used for a variety of pedestrian-oriented purposes: additional walkway width, additional landscaping, seating areas or plaza spaces, or outdoor dining (with a license from the City).
- **"Flexible Format Retail"** shall mean an enclosed space within a development project that permits and is designed for residential and/or commercial uses and may be used as such depending on market conditions and the preferences of the owner or tenant.

# RED HILL AVENUE SPECIFIC PLAN



- **“Freestanding Signs”** shall mean signage installed on posts or other supports that are not attached to any building or structure. Freestanding signs include monument, pylon, and poles signs.
- **“Hydromodification”** shall mean changes in runoff characteristics caused by altered land use and an increase of impervious areas.
- **“Long-Term Stay” also “Extended-Stay”** shall mean a hotel or motel unit providing lodging or lodging and meals for an occupancy of more than 30-days.
- **“Mixed-use”** shall mean a complimentary mix of land uses including, commercial retail, office and residential uses that are part of an integrated single development. Land uses may be arranged vertically in one or multiple story buildings or horizontally within separate buildings on a lot, site or parcel.
- **“Outdoor Activity Area”** shall mean an area (not including primary circulation space), located outside of a building or in a courtyard that is provided for the use or convenience of patrons of a commercial establishment, including but not limited to, sitting, eating, drinking, dancing and food service activities.
- **“Outdoor Dining”** shall mean the extension of services of an existing restaurant or eating/drinking establishment to be provided at tables placed on the public sidewalk or private common area adjacent to and within the confines of any frontage of that portion of the building that the restaurant use is situated.
- **“Planning Factors”** shall mean opportunities and constraints that influence and contribute to the successful implementation of the Specific Plan.
- **“Public Realm”** shall mean the publicly owned areas, typically including the street itself, sidewalks, parkways, and public parks.
- **“Residential Allocation Unit(s)”** shall mean units that are allocated by the Community Development Director in compliance with Section 6.6 of the RHASP.
- **“Residential Allocation Reservation (RAR)”** shall mean the process, described in Section 6.6 of this Specific Plan, by which residential units are allocated by the City to development projects as part of the entitlement process.
- **“Residential Allocation Bank”** specifies the potential residential dwelling unit allocation within the Specific Plan area and for the transfer of either between Red Hill Avenue – North and Red Hill Avenue – South. The Residential Allocation Bank establishes a maximum number of 500 new dwelling units that may be developed within the Red Hill Avenue Specific Plan area.
- **“Specific Plan”** shall mean a tool for the systematic implementation of the General Plan. It effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area.

## 4.3.2 Permitted Land Uses

This section describes the permitted land uses and activities in the Specific Plan area. Table 4-1, *Permitted Land Use and Activities*, identifies the permitted land uses and activities exclusive to the RHASP.

### A. Determination of Uses

As described in Section 6.6, *Administration and Enforcement*, the Community Development Director or his or her designee shall have the authority to interpret if a land use or activity that is not explicitly listed in Table 4-1, *Permitted Land Use and Activities*, is a permitted, conditionally permitted or prohibited use.

### B. Land Use and Activities

If a use or activity is not explicitly stated in this section, it is deemed a prohibited use.

**Table 4-1 Permitted Land Use and Activities**

<p><b>Permitted land uses.</b> The land uses and activities permitted by this Specific Plan, and the land use permit required to establish each use.</p> <p><b>Prohibited land uses.</b> Any table cell indicated with an “N” symbol indicates that the listed land use is prohibited. Any use not listed may be determined by the Community Development Director to be permitted, conditionally permitted or prohibited.</p> <p><b>Notes.</b> Where the last column in the table states (“See standards in Section”) it includes a Section number, any applicable regulations in the referenced section apply to the use; however, provisions in other sections of this Specific Plan or applicable section of the TCC may also apply.</p> <p>“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted</p>		
Land Use or Activity	RHASP Planning Area	Notes
<b>Retail Uses</b>		
Alcohol Sale (off-site)	C	Within a building and permitted business with at least 15,000 square feet of gross floor area and where the alcoholic beverage sales area within the building occupies no more than 10 percent of the gross floor area
Alcohol Sale (on-site) (for purchase and consumption on-site)	C	
Antiques and Curios	P	
Art Galleries	P	
Bicycle Shops (including repairs w/in enclosed building)	P	
Books and Stationery	P	

# RED HILL AVENUE SPECIFIC PLAN



Table 4-1 Permitted Land Use and Activities

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“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
Ceramics (not including commercial-scale on-site molding, casting or manufacturing by any process)	P	
Clothing, Shoes & Accessories Stores	P	
Convenience Stores	P	For alcohol sales, see standards in TCC Section 9271, <i>Specific Provisions</i>
Department Stores	P	
Drug Stores (including general retail and pharmacy)	P	For alcohol sales, see standards in TCC Section 9271, <i>Specific Provisions</i>
Drive-Thru, Drive-In and Drive-Up Facilities	C	See criteria in RHASP Section 5.4
Flexible Format Retail Stores	P	See criteria in RHASP Section 5.4
Florist Shops	P	
Food Service (including bakeries, confectioners, food markets, supermarkets, specialty food markets, etc.	P	
Furniture Sales	P	
General Retail Stores (including drug stores and pharmacies)	P	For alcohol sales see standards in TCC Section 9271, <i>Specific Provisions</i>
Hardware Stores	P	
Jewelry Stores	P	
Music Stores	P	

# RED HILL AVENUE SPECIFIC PLAN

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Land Use or Activity	RHASP Planning Area	Notes
Liquor Stores	C	For alcohol sales see standards in TCC Section 9271, <i>Specific Provisions</i>
Office Supplies	P	
Paint and Wallpaper Stores	P	
Restaurants (not including Drive-Throughs)	P	
Reverse Vending Machines	C	See standards in TCC Section 9271, <i>Specific Provisions</i>
Secondhand Sales without Pawn	P	
Sporting Goods (not including onsite marine and motor sales)	P	
Variety Stores/Gift Shops	P	
Vendor Cart and Outdoor Commercial Kiosks	C	See standards in TCC Section 9271, <i>Specific Provisions</i>
<b>Services</b>		
Animal Hospitals/Clinics (including boarding – indoor only)	C	
Assisted Living (including independent living and memory care)	C	
Automated Teller Machines (ATM) (accessory use to primary business only)	P	

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“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
Alternative Fuels and Recharging Facilities (ancillary use only)	P	
Banks, Financial Institutions, Credit Unions (w/out drive-through)	P	
Barber, Beauty and Nail Salons	P	
Body Art Facilities	P	As defined in TCC Section 3141, <i>Definitions</i> and Article 4, Chapter 1, Part 4, <i>Body Art Facilities</i> for standards. See standards in TCC Article 3, Chapter 6 Part 5, <i>Body Art</i> and Sections and Article 4 Chapter 1 Part 4 <i>Body Art Facilities</i>
Day Care Centers (child or elder care)	C	See standards in TCC Section 9271, <i>Specific Provisions</i>
Dry Cleaning or Laundry Services (pick-up, drop-off, and delivery only)	P	
Health and Athletic Clubs, Gyms (indoor only)	C	
Home Occupations	P	See standards in TCC Section 9271, <i>Specific Provisions</i>
Insurance Agencies	P	
Instructional Studios (including visual art, music, tutoring)	P	
Land/Property Management	P	
Laundromats (non-commercial self-service only)	C	

# RED HILL AVENUE SPECIFIC PLAN

Table 4-1 Permitted Land Use and Activities

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“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
Locksmith	P	
Massage Establishments	P	See standards in TCC Section 3668, <i>Massage Establishments - Operations</i>
General Office	P	As defined in TCC Section 9297, <i>Definitions</i>
Professional Office (includes medical offices uses)	P	
Pet Store, Supplies and Grooming	P	
Pet Services (including veterinarian, pet daycare or overnight boarding – indoor only)	C	
Photography Studio	P	
Print and Reprographics Shops	P	
Restaurants (not including Drive-Throughs)	P	
Seamstress, Tailor and Alterations Shops	P	
Shoe or Luggage Repair	P	
Skilled Nursing, Extended Care, convalescent/rehabilitation hospitals	C	
Tanning Studios/Salons	P	

# RED HILL AVENUE SPECIFIC PLAN

Table 4-1 Permitted Land Use and Activities

**Permitted land uses.** The land uses and activities permitted by this Specific Plan, and the land use permit required to establish each use.

**Prohibited land uses.** Any table cell indicated with an “N” symbol indicates that the listed land use is prohibited. Any use not listed may be determined by the Community Development Director to be permitted, conditionally permitted or prohibited.

**Notes.** Where the last column in the table states (“See standards in Section”) it includes a Section number, any applicable regulations in the referenced section apply to the use; however, provisions in other sections of this Specific Plan or applicable section of the TCC may also apply.

“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
<b>Hospitality and Entertainment</b>		
Bowling Alley	C	
Indoor Amusement/Entertainment/ Recreation Centers/Arcades	C	
Hotels, Motels, and Extended/Long-Term Stay Hotels	C	Refer to Section 4.3.1 of this Specific Plan for definition of extended stay
Theatres (cinema or performance)	C	
<b>Residential and Mixed-use</b>		
Flexible Format Retail	P/R	See provision for allocation of residential uses.
Mixed-Use Developments	P/R	Residential Use Permit required for all residential uses. Freestanding residential uses are not permitted at the ground floor frontage along Red Hill Avenue.
<b>Civic, Cultural and Public Uses</b>		
Art Galleries/Museums (non-retail)	P	
Community/Recreational Center	C	
Government Enterprises (local, State, or Federal)	P	
Cultural Facilities	P	
Performing Arts Theaters, Auditoriums, Meeting Halls (Not including adult entertainment)	C	
Parking Lots/Structures, Public	P	
Parks, Public	P	
Places of Worship/Assembly Uses	C	

# RED HILL AVENUE SPECIFIC PLAN

Table 4-1 Permitted Land Use and Activities

**Permitted land uses.** The land uses and activities permitted by this Specific Plan, and the land use permit required to establish each use.

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“P” = Permitted Use “C” = Conditionally Permitted “R” = Residential Use Permit “N” = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
Publicly-Owned Facilities and Structures	P	
Schools (includes private schools, vocational or training schools)	C	
Temporary Uses	P	Subject to the granting of a Temporary Use Permit in compliance with TCC Article 9, <i>Land Use</i> .
<b>Medical</b>		
Acute Care Rehabilitation Facilities	C	
Medical - Office	P	
Medical Services – Laboratories and Clinics	P	
Medical Services	P	
<b>Automotive and Auto Related</b>		
Automobile Service Stations	C	
Car Wash (including full & self-service drive-through)	C	
Motor Vehicle Parts and Supplies (retail only) and Sales (no onsite repairs)	C	
Minor Motor Vehicle Repair and Maintenance (must be conducted within an enclosed building)	C	
Tire Installation, Smog Check and Oil Change Facilities (must be conducted within an enclosed building – excludes bodywork)	C	

**Table 4-2 Major Prohibited Land Uses and Activities**

"N" = Not Permitted

Land Use or Activity	RHASP Planning Area	Notes
Sexually-Oriented Businesses	N	
Donation Centers and Donation Boxes	N	
Storage (includes mini-storage, vehicle and recreational vehicle storage, personal storage or self-storage)	N	
Single Family Residential	N	
Hazardous Materials Storage	N	
Motor Vehicle Sales and Service (new and/or used)	N	
Schools (includes public K-12 schools)	N	
Manufacturing	N	
Industrial	N	

## C. Temporary Uses and Activities

Temporary uses or activities such as grand openings, promotional events, fundraising events, and similar special events may occur occasionally in the Specific Plan area. The Specific Plans vision for a vibrant, pedestrian-scaled mixed-use environment encourages any reasonable and safe activity or event that promotes the success of business(es) in the Red Hill area. All Special Events and Temporary Uses and Activities shall be subject to the granting of a Special Event and/or Temporary Use Permit in compliance with Article 9 of the TCC.

### 4.4 Development Standards

This section provides standards and provisions for the use of land within the Specific Plan area. The standards contained within this Chapter shall govern all land uses and activities in the Specific Plan area. In addition, the standards and provisions of the TCC shall also apply. When a conflict exists between the provisions of the TCC and the Specific Plan, the Specific Plan shall take precedence.

# RED HILL AVENUE SPECIFIC PLAN

## 4.4.1 Development Standards Table

New land uses, activities, structures, and alterations to existing land uses and structures, shall be designed, constructed, and/or established in compliance with the requirements in Table 4-3, *General Development Standards & Requirements* and Exhibits 4-1 and 4-2, below:

Table 4-3 General Development Standards & Requirements (Standards shown are minimums unless specified)			
	Development Standards	Commercial (1)	Mixed-Use (2)
A	Minimum Required Street Frontage/Lot Width	70 feet	<ul style="list-style-type: none"> <li>• 100 feet</li> </ul>
B	Maximum Building Height	50 feet	<ul style="list-style-type: none"> <li>• 4 Stories and 50 feet</li> </ul>
<b>Setbacks</b>			
C	Front Yard (Building)	None	<ul style="list-style-type: none"> <li>• None with required pedestrian accessible amenities (3)</li> <li>• 5-foot (aggregate) setback above the second story;</li> <li>• Additional upper story setbacks may be imposed during design review process;</li> </ul>
D	Side Yard (Interior)	None 10 feet from an adjoining residential lot or mixed-use lot that contains residential uses;	<ul style="list-style-type: none"> <li>• 10 feet;</li> <li>• 5 feet aggregate setback at the third story;</li> <li>• Additional upper story setbacks may be imposed during design review process.</li> </ul>
E	Side Yard (Corner/Street Side)	None	<ul style="list-style-type: none"> <li>• None with required pedestrian accessible amenities (3);</li> <li>• 5-foot aggregate setback above the second story;</li> <li>• Additional upper story setbacks may be imposed during design review process.</li> </ul>
F	Rear Yard	None 10 feet from an adjoining residential lot or mixed-use lot that contains residential uses	<ul style="list-style-type: none"> <li>• 10 feet</li> <li>• Additional upper story setbacks may be imposed during design review process</li> </ul>

# RED HILL AVENUE SPECIFIC PLAN



**Table 4-3 General Development Standards & Requirements**  
(Standards shown are minimums unless specified)

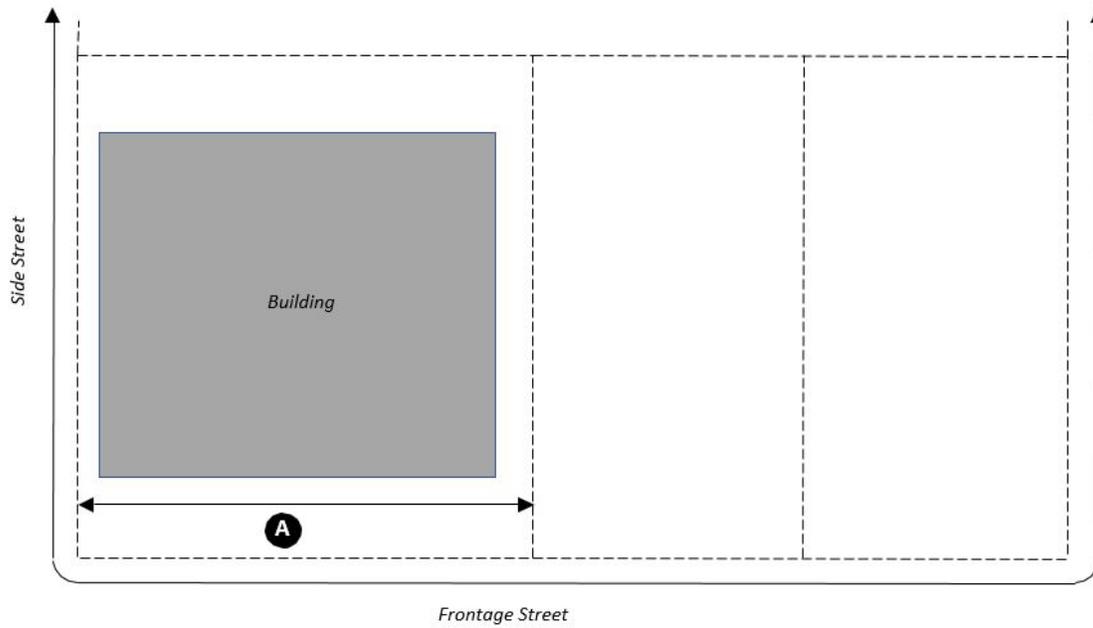
	Development Standards	Commercial (1)	Mixed-Use (2)
G	Building Separation (freestanding buildings containing residential and adjacent building(s))	N/A	<ul style="list-style-type: none"> <li>• 10 Feet <sup>5</sup></li> </ul>
H	Open Space (4) Private Common	N/A N/A	<ul style="list-style-type: none"> <li>• 100 sf per unit</li> <li>• 200 sf per unit</li> </ul>
I	Landscaping Requirements	Refer to Tustin City Code for landscaping requirements	<ul style="list-style-type: none"> <li>• See Section 5 (Design Criteria) for mixed-use landscaping requirements.</li> </ul>

Notes:

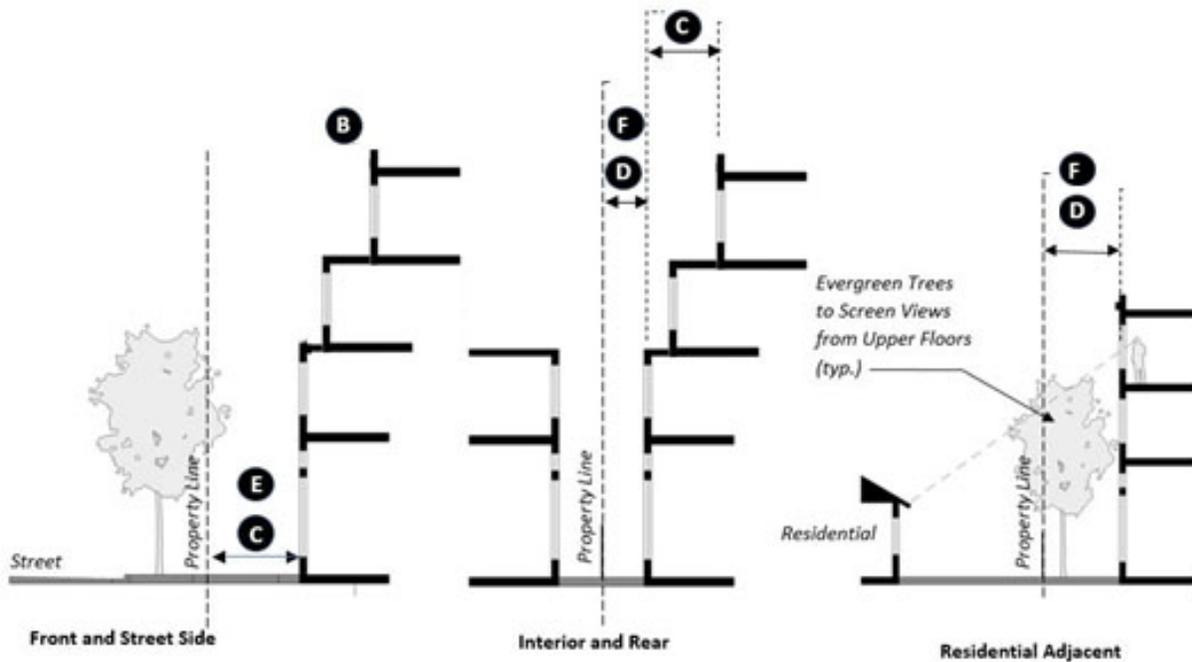
- (1) Commercial references stand-alone commercial/office uses, without a residential component. This may be a commercial center or smaller commercial or office building(s).
- (2) Mixed-Use references an integrated horizontal or vertical combination of commercial/office uses with a residential component. Residential units are subject to the Residential Allocation Reservation process.
- (3) Pedestrian accessible amenities may include outdoor seating, outdoor dining (subject to a City license as specified in TCC Section 9277), public art, plaza space or architectural features that allow for additional pedestrian activity.
- (4) Refer to the Open Space standards in Chapter 4, Section 4.4.3 (Mixed-Use Development) for additional requirements.
- (5) A minimum distance of 10 feet shall separate exterior walls of separate buildings containing dwelling units on the same lot. The windows or window/doors of any dwelling unit shall not face the windows or window/doors of another dwelling unit unless separated by a distance of 10 or more feet.

N/A – Not Applicable

# RED HILL AVENUE SPECIFIC PLAN



**Exhibit 4-1 Frontage/Lot Width Requirements**  
*(refer to Table 4-3)*



**Exhibit 4-2 General Development Standards**  
*(refer to Table 4-3)*

## 4.4.2 Commercial

Commercial development shall comply with the Development Standards as shown in Table 4-3 of this Chapter, and the following standards;

- A. Office uses shall be consistent with and complement the retail, mixed-use character of the RHASP.
- B. Office uses shall not utilize street adjacent frontage at the ground floor, unless design and other functional considerations maintain an active street interface similar to a retail commercial frontage.

## 4.4.3 Mixed-Use Development

Mixed-use projects are comprised of a complementary mix of land uses including, commercial retail, office, and residential uses that are part of an integrated single development. Land uses may be arranged vertically in multiple story buildings, or horizontally within separate buildings on a lot, site or parcel.

Mixed-use projects shall combine residential uses with retail commercial/office uses. Mixed-use projects may provide commercial and/or office space on the ground floor with residential units above, or distributed horizontally on a project site in individual structures. Stand-alone residential uses are not permitted to face onto Red Hill Avenue.

### A. General Development Standards

- 1. Trash Collection Areas. Trash collection areas shall be contained within an enclosed structure. Trash collection areas shall be designed, located or screened so as not to be readily identifiable from adjacent streets.
- 2. Loading and Storage Facilities. Commercial loading areas and trash collection areas shall be located as far as possible from the on-site residential units and shall be completely screened from view from adjacent residential, as well as other residential portions of the project. The location and design of the trash collection areas shall ensure no potential nuisances such as odors and noise or conflicts of vehicle accessibility from collection vehicles result. Sensitivity to adjacent residential shall also be considered.
- 3. Private Storage Space. A minimum 90 cubic feet of private storage space shall be provided for each residential dwelling unit. Private storage space shall have a minimum horizontal surface area of 24 square feet and shall be fully enclosed and lockable. Storage space may be counted towards the minimum private open space requirement.
- 4. Exterior Lighting. Parking lot lighting and security lighting for the commercial uses shall be appropriately shielded as not to spill over into residential areas. Illuminated commercial signs and associated fixtures shall also be shielded to prevent light spillover onto adjacent residential uses as well as residential portions of mixed-use projects.

5. Exterior Equipment. All exteriorly mounted equipment shall be screened from public view. Special consideration shall be given to the location and screening of noise generating equipment (e.g., air conditioning, exhaust fans, refrigeration units, alternative fuel stations, etc.) Noise reducing screens and insulation may be required where equipment has the potential to impact residential uses.
  - a. Satellite dish systems must be screened from view on all sides through use of architectural elements integrated into the design of the building
6. Rooftop Equipment
  - a. Rooftop equipment, except solar energy equipment or alternative energy systems requiring access to solar or wind, shall be completely enclosed on all sides or screened from view of public rights-of-way.
  - b. All roof appurtenances including but not limited to, air conditioning units and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets, and adjacent properties. Screening should be designed to be compatible with the architectural design of the building.
7. Open Space
  - a. A minimum private open space of 100 square feet shall be provided for each dwelling unit.
  - b. A minimum common open space of 200 square feet shall be provided for each dwelling unit.
  - c. A total of 300 square feet of combined private and common open space shall be provided. If the design of a unit does not provide private open space, a minimum of 300 square feet of common open space shall be provided for each unit.
  - d. Open space shall be provided as common and private space. A reasonable proportion of the required open space shall include courtyards, pool/spa areas, rooftop decks, paseos and other similar uses.
  - e. Open space and courtyards located in the commercial areas of mixed-use development shall be accessible to residential occupants and visitors and shall not be counted towards the required minimum common open space.
  - f. Landscaping and seating shall be permanently integrated into all required common open spaces.
8. Hours of Operation. The Community Development Director may restrict the hours of operation of nonresidential uses to mitigate adverse impacts to residential uses.

## 9. Landscaping

- a. A minimum of 10 percent of the total commercial gross lot size shall be permanently landscaped with a variety of live landscape, hardscape materials, and other design elements (i.e. fountains, sculptures, planters, decorative rock, etc.). If a parking area that is within a required setback is landscaped, the landscaped area may be counted toward meeting the minimum landscaping area requirement for the project.
- b. For the purposes of this section, permanent landscaping shall consist of landscaped areas at the ground level.
- c. Rooftop landscaping that is permanent, including “green roofs” that are permanently landscaped, irrigated and usable by building tenants can be counted towards meeting required landscape coverage.
- d. Pedestrian walks and vehicular access ways shall be permitted in landscaped areas but shall not be counted as landscaped areas.
- e. Permanent and automatic irrigation facilities shall be provided in all permanent landscaped areas except potted containers.
- f. Landscaping shall be permanently maintained in substantial conformance with the approved plan, including potted containers.

## 10. Lighting

- a. Lighting shall comply with the standards and provisions contained in the California Building Code, as referenced in Section 8102 of the TCC, in addition to the following standards:
  - (i) Light poles for uncovered parking areas, vehicle access ways and walkways shall not exceed a height of 16 feet, except that the maximum height on the rooftop of any parking structure located on a lot adjacent to any residential zone shall not exceed a height of 8 feet.
  - (ii) The overall height shall be measured from the paved parking area surface to the uppermost part of the light standard, including the light globe.
  - (iii) Lighting shall be directed onto the driveways, walkways and parking areas within the development and away from adjacent properties and public rights-of-way. Appropriate shields shall be incorporated into lighting fixtures to ensure lighting does not spill onto adjoining properties.

## 11. Parking for Flexible Format Retail in Mixed-Use Development

- a. Flexible Format Retail shall be defined as an enclosed space within a development project that permits residential and/or commercial uses and may be used as such depending on

market conditions and the preferences of the owner or tenant. Flexible Format Retail is intended to be occupied by business operators who live in the same structure that contains the commercial activity. Flexible Format Retail is intended to function in whatever capacity is deemed appropriate for the owner or tenant and must be consistent with the Specific Plan and per California Building Code requirements.

12. **Applicability and Allowed Uses.** The provisions of this section shall apply to Flexible Format Retail development as permitted by Table 4-1, *Permitted Land Use and Activities*. The nonresidential component of Flexible Format Retail shall only be a use permitted in accordance with Table 4-1, *Permitted Land Use and Activities*, of this Specific Plan.
13. **Limitations on Use.** Flexible Format Retail shall not be established or used in conjunction with any of the following activities or any use deemed prohibited per the TCC or determined by the Community Development Director to be not allowed:
  - a. Adult businesses;
  - b. Vehicle maintenance or repair (e.g., body or mechanical work, including boats and recreational vehicles), vehicle detailing and painting, upholstery, etc.;
  - c. Storage of flammable liquids or hazardous materials beyond that normally associated with a residential use; and
  - d. Other activities or uses, not compatible with residential activities and/or that have the possibility of affecting the health or safety of residents, because of dust, glare, heat, noise, noxious gasses, odor, smoke, traffic, vibration, or other impacts, or would be hazardous because of materials, processes, products, or wastes, as determined by the Community Development Director.
14. **Development Standards.** The following development standards shall apply to Flexible Format Retail:
  - a. **Location of Flexible Format Retail.** The location of Flexible Format Retail Space is limited to the following:
    - (i) Flexible Format Retail shall be limited to the ground floor directly facing a public sidewalk or internal circulation path.
    - (ii) Flexible Format Retail shall be designed as independent non-residential space.
    - (iii) Flexible Format Retail shall be limited to occupy no more than 50% of the total assignable square feet of a mixed-use project.

# RED HILL AVENUE SPECIFIC PLAN



- b. Street Frontage Treatment. Flexible Format Retail shall only contain commercial at the ground level when fronting Red Hill Avenue, and shall have a pedestrian-oriented frontage that publicly displays the interior of the nonresidential or intended nonresidential areas of the structure.
  - c. Access to Flexible Format Retail. Where more than one Flexible Format Retail unit is proposed within a single structure, each Flexible Format Retail shall be separated from other Flexible Format Retail space and other uses in the structure. Access to Flexible Format Retail space shall be from common access areas, corridors, or hallways and shall be clearly identified to provide for emergency access.
  - d. Notice to Occupants at Occupancy. The owner or developer of any structure containing Flexible Format Retail shall provide written notice to all adjacent occupants and users on the subject property that the surrounding area may be subject to levels of dust, fumes, noise, or other impacts associated with commercial uses at higher levels than would be expected in more typical residential area.
  - e. Integral layout. The living space within the Flexible Format Retail unit shall be complementary in design and function of the space, with direct access between the two areas.
  - f. The Flexible Format Retail space shall be assigned only one street address.
15. Sale or Rental of Flexible Format Retail. No portion of a Flexible Format Retail unit may be separately (i.e. independently) sub-leased or sold.
- a. For Sale units shall be required to establish Conditions, Covenants, and Restrictions (CC&Rs) as part of the property's Joint Owner's Association documents to address limitations on the subleasing, renting and other similar transactional issues.
16. Findings for Flexible Format Retail. The approval of a Flexible Format Retail Space shall be subject to the following findings:
- a. The establishment of Flexible Format Retail will not conflict with nor inhibit other uses in the area where the project is proposed.
  - b. All commercial components of the Flexible Format Retail shall be built to commercial standards.
  - c. The use of the Flexible Format Retail shall provide for active commercial frontage at the ground level.

## 4.4.4 Off Street Parking and Loading Standards

### A. Purpose

The purpose of this section is to ensure that sufficient parking and loading areas are provided and properly designed and located in the Specific Plan area. Bike parking is an important vision factor for the area. Every use, including a change in or expansion of an existing use or structure, shall have appropriately maintained off-street parking and loading areas in compliance with the standards and criteria as described in this Specific Plan. Off-street parking and loading for uses within the RHASP area shall be provided in accordance with Article 9, Chapter 2, Part 6 of the TCC. For standards and provisions that are not explicitly stated, the standards and provisions of the TCC shall apply.

### B. Regulations for Off-Street Parking

Mixed-Use. Off-street parking standards for mixed-used development are provided in Table 4-4, *Parking Standards for Mixed-Use (Residential and Non-Residential/Commercial) Development*. These standards shall apply to mixed-use development in the RHASP area only.

1. Residential Use. For mixed-use development only, the parking ratios in Table 4-4, *Parking Standards for Mixed-Use (Residential and Non-Residential/Commercial) Development*, shall apply. There are no exceptions to these requirements, except as noted in footnotes and in this Specific Plan.
2. Non-Residential Use. Off-street parking for non-residential uses shall be provided for each separate use in compliance with the TCC, unless applicant is granted an alternative ratio provided in Table 4-4, *Parking Standards for Mixed-Use (Residential and Non-Residential/Commercial) Development*.
3. Mixed-Use. For mixed use development, which includes residential and non-residential uses, a parking management plan shall be required as part of the RAR application. The contents of the parking management plan shall be determined by the Community Development Department.
4. Flexible Format Retail parking standards shall be calculated as a residential use. Refer to Table 4-4, *Parking Standards for Mixed-Use (Residential and Non-Residential/Commercial) Development*, for parking standards.
5. Fractional Spaces. Any use requiring less than a full parking space shall be rounded up to the nearest whole number.
6. Location of Parking. Required parking spaces for commercial or mixed-use shall be located on the same parcel they are intended to serve. On-site parking areas shall not be located within the required front setback or in the front along Red Hill Avenue.
7. Parking and Access Criteria
  - a. All parking spaces required for the residential use shall be provided on site.

- b. Parking spaces to serve residential uses shall be specifically designated and shall be reserved for the exclusive use of the residents.
- c. If structured parking is provided for the entire complex, separate dedicated and accessible areas shall be provided for residential and commercial uses.
- d. Separate site access drives shall be provided for the residential uses and commercial uses whenever possible.
- e. Security gates shall be strongly encouraged for access to residential uses and residential parking areas, as well as for securing commercial parking areas when businesses are closed.

Table 4-4 Parking Standards for Mixed-Use (Residential and Non-Residential/Commercial) Development <sup>(3)</sup>			
Land Use Type	Resident Parking	Guest Parking (4)	Total Parking Required (1) (2)
Residential	2.0 space/unit	0.25 space/unit	2.25 space/unit
Non-Residential	Refer to TCC standards for non-residential use types		
<b>Notes:</b> (1) Total parking spaces required shall be rounded up to the nearest whole number. (2) Joint use parking shall be permitted subject to the requirements of Chapter 4 of this Specific Plan. Development application requesting joint-use parking shall require a parking management plan subject to the requirements of Chapter 4. (3) Applicants may propose alternative parking standards when an applicant believes the proposed development supports consideration of alternative parking standards. Applicants must provide a parking demand analysis supporting the proposed alternative standards, subject to the requirements of Chapter 4 of this Specific Plan. (4) Guest parking per dwelling unit shall be located on-site or within 500-feet of the property. No tandem parking is permitted.			

### C. Alternative/Shared Parking

Alternative Shared Parking Standards. Applicants may propose alternative shared parking standards for parcels with a mixed-use development or development that is utilized at non-traditional hours. The number of off-street parking spaces is stated in Article 9, Chapter 2, Part 6 of the TCC and Table 4-4, *Parking Standards for Mixed-Use Residential and Non-Residential/Commercial Development*, of this Specific Plan. Alternative standards for parking shall be considered by the Planning Commission and applicants shall be required to comply with the following procedures:

1. Mixed-use alternative parking standards are alternatives to required parking as specified in the Specific Plan and the TCC and shall only be permitted where shared parking is to be authorized/approved.
2. Requests for Shared Parking. The utilization of shared parking facilities within the RHASP area is encouraged. Shared parking standards are based on the assumption that patrons will use a single parking space for more than one destination in certain locations within the Specific Plan area and that the parking space will be open and available for short-term parking to serve different uses which may have different peak hours.

3. Ineligible Development. The following types of uses are not eligible to use shared parking standards:
  - a. New or existing development that is not a component of commercial or mixed-use.
  - b. New construction of hotel or office uses on parcels of 30,000 square feet or greater.
4. Parking Study Required. Commercial uses, and mixed-use development requesting shared parking shall be required to prepare a parking and loading study that demonstrates adequate parking and loading facilities for average daily demand (rather than peak daily demand).
  - a. The parking study may be used in lieu of the required parking standards and provisions of this Specific Plan provided the following conditions are met:
    - (i) The parking study provides sufficient acceptable justification and evidence supporting modifications to the prescribed standards;
    - (ii) The project demonstrates methods to reduce parking demand, including but not limited to mixed-use, shared parking facilities, transit access, pedestrian amenities, and bike amenities.
5. Adequacy of Alternative Parking. Upon build-out of a development project, if alternative parking measures are not working adequately to address parking for a project, then required parking must be either provided on-site, off-site or by other means deemed acceptable by the Community Development Department.

## **D. Loading**

As approved by the Community Development Director, applicants requesting the use of common loading areas shall comply with the following requirements:

1. A loading study, conducted by a qualified individual, describing the location and extent of off-street common loading facilities, location of parking, pedestrian features and methods to protect pedestrians and cyclists and not impede on normalized traffic flow.
2. Uses within a mixed-use development may utilize common loading facilities when demonstrated that the loading activity will not be detrimental to the public health, safety, or welfare, or adversely affect traffic patterns.

## 4.4.5 Signs

This section provides standards and provisions for signs within the Specific Plan area. For all sign types, the provisions of the TCC shall apply, in addition to the standards and provisions provided within this section and the criteria outlined in Chapter 5, *Design Criteria*, of this Specific Plan.

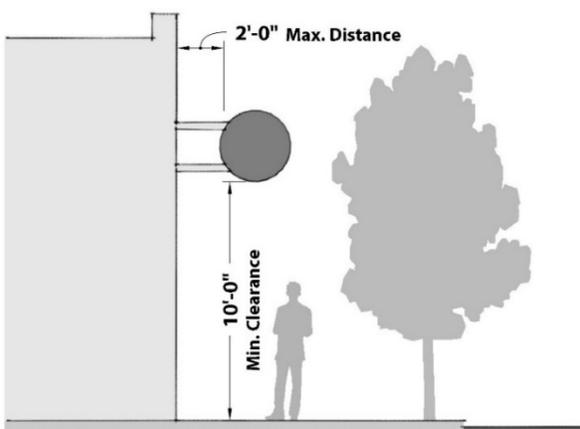
### A. Projecting Signs

Projecting signs shall be allowed subject to the following requirements:

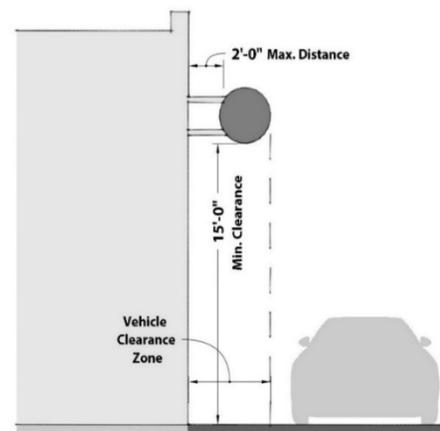
1. Projecting signs shall be located only on wall frontages with a primary entrance to the structure. This may include buildings with multiple frontages.
2. A clear vertical distance of 10 feet shall be maintained from the lowest point of the projecting sign to the ground level below. For projecting signs over public driveways, alleys and thoroughfares a clear vertical distance of 15 feet shall be maintained from the lowest point of the projecting sign to the ground level below.
3. A sign shall be attached to the wall no more than two (2) feet from the nearest point of the sign to the wall.
4. All mounting hardware shall be architecturally compatible.
5. No part of a sign shall be located within two (2) feet of a curb.
6. Signs may comprise or be configured with logos, symbols, or figures in addition to or instead of written words.
7. The maximum area of each sign face shall be 20 square feet.



*Example of Projecting Signs*



**Exhibit 4-3 Projecting Sign at Sidewalk**



**Exhibit 4-4 Projecting Sign at Driveway or Drive Aisle**

## B. Window Signs

Window signs shall be allowed subject to the following requirements:

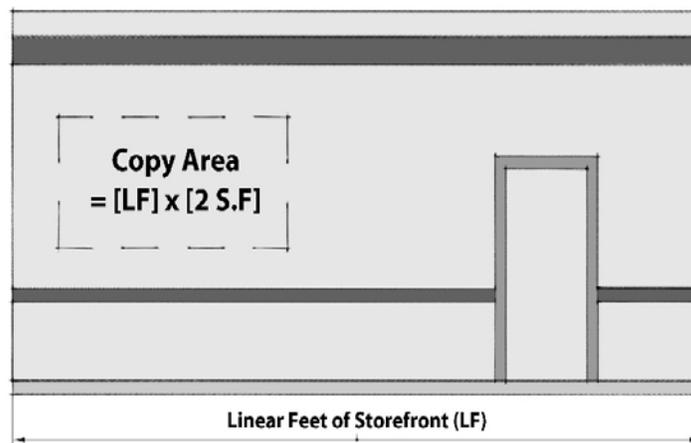
1. Placement of window signs shall be subject to review and approval of the Community Development Director.
2. Signs shall be allowed only on the ground floor level and second story of a structure's frontage.
3. Signs shall not occupy more than 25 percent of the window area.
4. The maximum area for window signs shall be calculated in conjunction with the requirements for wall signs in Section 4.4.5C, below.

## C. Wall Signs

1. Wall signs shall be located only on walls having frontage along streets, sidewalks, alleys, parking lots and not located directly across from a residential use.
2. Can, box, cabinet or signs designed with a raised raceway shall not be permitted.
3. Wall signs shall not project from the surface from which they are attached more than required for construction purposes and in no case more than 6 inches.
4. Signs shall be mounted in such a way so as not to obstruct any portion of a window or storefront.
5. Wall signs shall have a maximum of 2 square feet of sign area per linear foot for ground floor storefronts with direct access to the street, alley, sidewalk or on-site parking lot.



*Example of Projecting Sign as a Symbol*



**Exhibit 4-5: Wall Sign Area Calculation**

6. One address identification sign for the structure per street frontage shall be permitted up to a maximum of 15 square feet.

## D. Awning Signs

Awning signs shall be permitted subject to the following requirements.

1. Awnings containing signage shall be primarily located on a structure's frontage where building ingress/egress occurs, including those fronting a parking lot or pedestrian way. This may include buildings with multiple frontages
2. Awnings containing signage shall be limited to the ground level and second story commercial businesses only.
3. A minimum clear vertical distance of 8 feet shall be maintained from the lowest part of the awning sign to the ground level below.
4. Maximum area of an awning sign shall be calculated in conjunction with the requirements for wall signs contained in Section 4.4.5F, below.
5. No pricing, products, phone number, etc.



*Example of Awning Sign*

## E. Neon Signs

1. Neon signs shall comply with the standards and provisions of Article 9, Chapter 4, Section 9404, *Sign Regulations*, of the TCC.

## F. Monument Signs

1. Illumination. Monument signs shall be permitted to be internally illuminated, provided only the sign copy is illuminated. However, external illumination of monument signs is the preferred method of sign illumination. External illumination shall only illuminate the monument sign and provide consistency with the architectural design of the primary structure on the site.
2. Design. Monument signs shall be consistent with the RHASP Design Criteria provided in Chapter 5 of this Specific Plan.
3. Spacing. There shall be a minimum of 75 feet between monument signs to ensure proper visibility for all signs from the public right of way. The Community Development Director may waive this requirement in situations where its enactment may be impractical due to the location of existing signs on adjacent properties.

4. Location. Monument signs shall not project over or into public property or easements. Monument signs shall not obstruct traffic sight lines or create any visual obstruction that may create life, health or safety hazards.
5. Landscaping. Monument signs shall be located with a landscaped area, with the landscaped base area of the sign equal to twice the square footage of the monument sign's face. A permanent irrigation system shall be provided and landscaping maintained to preclude obstruction of the sign copy.



*Example of Landscape Base*

## G. Portable Signs

1. Portable signs shall mean freestanding signs or signage not attached to any object, structure or the ground. Portable signs shall be permitted in the public right of way and within private development. Portable signs located in the public right-of-way shall be allowed in compliance with the following conditions:
  - a. A-Frame or similar signs shall be permitted along sidewalks in the public right-of-way (which includes flexible amenity setback) provided a 4-foot minimum horizontal clearance is maintained.
  - b. A-Frame signs shall be permitted along sidewalks within private development, subject to all provisions of this section.
  - c. One freestanding sign per business.
  - d. Maximum of two faces per sign.
  - e. Maximum height of 4 feet and maximum width of 3 feet.
  - f. No external or internal illumination.
  - g. No highly reflective or fluorescent colors.
  - h. Not permanently affixed to any object, structure or the ground, including utility poles, light standards, trees or any merchandise or products displayed outside of building.
  - i. No sign shall be on display during non-business hours.
  - j. Located directly in front of the building/business.
  - k. Not placed in such a way to interfere with pedestrian or vehicular sight lines, corner visibility or ADA requirements

- l. Not placed in such a way as to obstruct access to a public sidewalk, public street, parking space, fire door, fire escape, disabled access or any way that obstructs the free passage over any portion of the public right of way.
- m. Not obscure or interfere with a public safety device (fire alarms, hydrants, etc.), traffic device (traffic signals, signs, etc.) or other similar devices.
- n. Maintain a minimum of four (4) feet over the entire length of the sidewalk or public right-of-way in front of the business for pedestrian clearance.
- o. Placement of portable signs in the public right-of-way

Portable signs shall be subject to staff level review. Placement of signs shall require the submittal of and are subject to the requirements of the TCC in addition to the following:

- i. Submit a brief description of the proposed sign and its proposed location within the public right-of-way.
- ii. A statement the portable signs shall be displayed only during business hours.
- iii. Submit an appropriately scaled and dimensioned site plan showing existing conditions, including street and sidewalk width and location/dimension of all street furniture or other elements on the sidewalk
- iv. The description and site plan shall be reviewed and approved at the sole discretion of the Community Development Director.
- v. An approved sign may be revoked by the City after a 30-day written notice.

## H. Pole Signs.

Pole signs shall not be permitted within the Specific Plan area.

## I. Master Sign Plan

Integrated office complexes, commercial centers or mixed-use developments shall require an approved Master Sign Plan in compliance with Section 9403h of the TCC

## 4.5 Residential Allocation Reservation (RAR)

Allocation of residential units within the RHASP is approved by the Community Development Director subject to the criteria outlined in this Section, the criteria outlined in Chapter 6.6.2, *Approval Authority* and/or the TCC shall apply.

This section establishes the procedures for the review and allocation of new residential units within a mixed-use development within the boundaries of the RHASP.

## A. Allocation Administration

All residential units within a mixed-use residential development within the RHASP shall be allocated by the Community Development Director pursuant to this Section and Chapter 6, *Implementation and Administration*. Transfers and conversions up to 25% of the original residential unit count and commercial square footage may be approved by the Community Development Director, but transfers and/or conversions greater than 25% require approval by the Planning Commission. Approval of units shall be made by the highest approving authority in conjunction with a submitted application.

## B. Mixed-Use Residential Unit Allocations

From the effective date of adoption of the RHASP, the following units are available within the defined area as noted in Table 4-5, *Residential Unit Allocations*, below:

Planning Areas	Maximum Permitted Units
North Area: North of I-5	395 dwelling units
South Area: South of I-5	105 dwelling units
TOTAL	500 dwelling units

The mixed-use residential unit allocations described in Table 4-5, *Residential Unit Allocations*, represent the maximum number of additional permitted residential units in the RHASP area. At no time, shall the number of new units approved under this Specific Plan exceed those identified in Table 4-5, unless by separate action of the Tustin City Council.

## C. Allocation Procedures

1. Procedures, process and application submittal requirements for the Residential Allocation Reservation process are outlined in Section 6.7.2, *Residential Allocation Reservation (RAR) Process and Findings*, of this Specific Plan.
2. Approval. Approval of unit allocations shall be made by the Community Development Director. The Community Development Department has the authority to refer any application for residential allocations to the Planning Commission in compliance with the TCC.

## D. Transfer of Mixed-use Residential Unit Allocations between the North and South Areas

In the event an applicant requests residential unit allocations within any area of the Specific Plan and there are no further units available or enough to accommodate the applicant's request, the applicant shall have

the opportunity to request the transfer of unit allocation between the north and south areas, as defined in Section 6.6.5, *Transfers*, of this Specific Plan.

## E. Conversion of Commercial Square Footage to Residential Units

Conversion of commercial square footage to residential units may be permitted subject to the procedures outlined in Section 6.6.6, *Land Use Conversion*, of this Specific Plan.

## F. Right of Appeal

Any person or persons shall have the right to appeal any decision (with or without units allocated to an applicant). Appeals are subject to provisions of Section 9294, *Appeal*, of the TCC.

## 4.6 Nonconforming Uses, Structures, and Parcels

This section establishes uniform provisions for the regulation of legal nonconforming land uses and structures. Within the planning area established by this Specific Plan, there exist land uses and structures that were legally established before the adoption, or amendment of the TCC, but which would be prohibited, regulated, or restricted differently under the terms of this Specific Plan or subsequent amendments thereof. Existing uses shall be permitted to continue and need not comply with the new standards in the RHASP, subject to the provisions of TCC Section 9273, Legal Nonconforming Structures and Uses.

When land uses intensify to a different land use classification (examples include: from retail to assembly, daycare, medical, restaurant, etc. or any other parking/traffic intensification) or are extended to occupy a greater land area or new structure, or new development is proposed, conformance with the development standards as specified in Table 4-3 related to the use will be required.

## 4.7 Specific Development Standards

### 4.7.1 Flexible Amenity Setback Provisions

The Flexible Amenity Setback applies to sites which include or are able to include (as part of a development application), the full 60-foot half-width right-of-way along Red Hill Avenue. The Flexible Amenity Setback is intended to allow a range of uses, including expanded walkways, additional landscape, public seating/plaza areas or other similar uses proposed as part of a Design Review application for a development project. Outdoor dining as part of a commercial food service use is a permissible use within this amenity setback, subject to a license obtained from the City. Refer to Chapter 5, *Design Criteria* for additional design considerations.

## 4.7.2 Outdoor/Sidewalk Dining

Outdoor/sidewalk dining areas that encroach into the public right-of-way shall be subject to approval of Design Review and a public right-of-way Revocable License Agreement. The standards and provisions provided in Article 9, Chapter 2, Part 7, Section 9277 of the TCC shall apply, in addition to the following:

- A. Outdoor/Sidewalk dining must maintain accessible minimum clearance at public sidewalk or interior walkways, exclusive of landscape areas, fire hydrants, streetlights, other street furniture, on-street auto overhang or parking lot auto overhang.
- B. Menu boards, or other temporary or movable signage shall not encroach into areas outside of the sidewalk dining area and may not be displayed after business hours.

## 4.7.3 Limitations and Exceptions for Storage and Outdoor Uses

### A. Outdoor Storage

No material or equipment shall be stored within the space between the street and the building setback line except for temporary storage during construction on the same premises. No required setback area shall be used to store any boat, camper, equipment, motor vehicle, or trailer, or parts thereof, or any type of antenna except as provided for in this Specific Plan.

### B. Limitation on Outdoor Uses

1. All uses except outdoor dining areas, public gathering areas/plazas, parking, loading, the growing of plants, cut flowers, approved temporary uses and other similar uses specifically permitted, shall be conducted entirely within a completely enclosed building which is attached to a permanent foundation.
2. Outdoor storage of auxiliary equipment, utilities, and items related to the maintenance of such facilities shall be prohibited. Storage areas shall be enclosed by a solid architecturally compatible masonry wall with a height adequate to fully screen such areas from public view.
3. Outdoor wholesaling of goods and materials shall not be permitted. The retail sales of goods and materials to the general public on a temporary basis shall be permitted with the approval of a Temporary Use Permit.
4. Kiosks and similar movable facilities shall be permitted as part of an integrated retail development
5. All outdoor uses shall be conducted in a manner so as not to be objectionable to a person of normal sensitivity due to dust, fumes, noise, odor, smoke, vibrations, or other similar causes.

## 4.7.4 Grand Openings, Special Events, and Other Temporary Circumstances

Grand openings, special events or other activities deviating from normal standard operations require and approved Temporary Use Permit. For temporary signage associated with a temporary use, a Temporary

Sign Permit is required. Refer to Article 9, *Land Use*, of the TCC for standards and provisions for the issuance of a Temporary Use and Temporary Sign Permits.

#### 4.7.5 Entertainment

Entertainment Uses shall comply with the provisions contained in Article 3, Chapter 2 of the TCC.

#### 4.7.6 Parking Structures

The following standards shall apply to enclosed parking structures, including above grade and below grade.

##### A. Site Organization

1. A minimum 5-foot landscaped setback shall be provided on all sides of the parking structure except where ground floor retail space is provided. Landscaping must provide adequate facilities to ensure proper watering and maintenance.

##### B. Access and Circulation

1. Vehicle stacking areas for entering and exiting traffic shall be of sufficient length to minimize vehicle stacking onto surrounding streets, within the parking structure, or impeding circulation on-site. A minimum of 2 vehicle lengths or 40 feet of stacking distance shall be provided between the street or other access way and the control gate.
2. One inbound lane shall be provided for a garage with a capacity of up to 500 vehicles. At least 2 inbound lanes shall be provided for garages with a capacity of 500 or more vehicles.
3. Exit lanes shall be provided at a ratio of one lane for every 200-250 vehicles. The maximum aisle length shall not exceed 400 feet without providing a cross aisle.
4. Ramp grades shall not exceed 10 percent and parking areas shall not exceed a slope of 4 to 5 percent.

##### C. Lighting and Security

A minimum of 1 foot-candle of illumination shall be provided inside parking structures and for exterior parking areas per the applicable section of the TCC. Higher levels are recommended for remote areas subject to security considerations (e.g., stairways, elevators, and other pedestrian access points).

#### 4.7.7 Landscaping, Walls, and Fences

This section provides standards for the provision of landscaping, walls, and fences within private property in the Specific Plan area.

## A. Landscaping and Irrigation Required

All projects shall provide and maintain landscaping and irrigation in compliance with applicable sections of the Specific Plan and the TCC. Standards for the provision of landscaping within the public right-of-way in conjunction with any private development shall be in compliance with the TCC.

## B. Walls and Fences

1. Height and Location. Perimeter fences or walls that are within a front yard setback shall not exceed a height of three (3) feet unless specifically allowed to a height of six (6) feet by the Community Development Director to achieve a specific screening purpose.
2. Design. The design of walls and fences shall be consistent and compatible with the overall project design and/or adjacent streetscape.
3. Articulation. Perimeter walls and fences shall be articulated by providing a minimum 3-foot deep by 6-foot long recession for every 50 feet of continuous wall length. The design of the wall shall include an appropriate mix of materials and landscaping.
4. Prohibited Fence Types. Chain link or similar fencing shall not be permitted within the public view, other than temporary fencing associated with construction activity.

### 4.7.8 Auxiliary Structures, Equipment, and Utilities

This section provides standards for the location, mounting and screening of structural features, mechanical equipment for utilities, fire, life and safety equipment, utility boxes, devices, cabinets and similar facilities associated with the function and use of buildings. The following standards apply:

- A. Auxiliary structures, equipment, and utilities (electrical, solar, HVAC, mechanical equipment and supporting structures), shall not be located directly adjacent to the street frontage of any property.
- B. All ground mounted mechanical/electrical equipment, including heating and air conditioning units and refuse disposal areas shall be completely screened in a solid enclosed structure from surrounding properties by use of a wall, fence or landscaping, or shall be enclosed within a building.

### 4.7.9 Refuse Plan

Prior to the approval of any development project, a refuse plan shall be submitted and approved by the Community Development Director as part of Design Review. The refuse plan shall include, at a minimum, the following:

- A. Location and storage of refuse facilities on the property.
- B. Schedule for maintenance and cleaning of refuse facilities.
- C. Planned pickup times and dates of refuse.

- D. Access, parking, and loading for refuse and circulation of vehicles.
- E. Distance indicators of users to facilities and path of travel.

#### 4.7.10 Access/Circulation Plan

Prior to the approval of any development project, an access plan shall be submitted and approved by the Community Development Director as part of Design Review. The access plan shall, at a minimum, include the following:

- A. Location of site ingress and egress to and within the site.
- B. Location of controlled access (electronic key, FOB, lock boxes or code access).
- C. Location of emergency vehicle access.
- D. Location of bike racks and lockers to accommodate estimated bike user needs.
- E. Site signage plan indicating location of exit, entrance and emergency vehicle access and parking areas.

## 4.8 Development Incentives

Development incentives will provide specific guidance for the granting of additional development potential, provided that certain conditions are met. Development incentives are intended to encourage the effective utilization and consolidation of parcels to encourage more viable development opportunities, implanting the vision of the Specific Plan area as a vibrant mixed-use district.

Development incentives are contained in Section 6.8A of Chapter 6, *Mixed Use Development Incentives*.

## 4.9 Development Review Procedures

The implementation and administration of this Specific Plan and procedures for development review shall be subject to the requirements as set forth in Article 9, Chapter 2, Part 9 of the TCC and Chapter 6, *Implementation and Administration* of this Specific Plan.

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# 5 DESIGN CRITERIA

# Design Criteria

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## 5. Design Criteria

### 5.1 Introduction

#### 5.1.1 Purpose

The Design Criteria implement the objectives and vision of the Red Hill Avenue Specific Plan (RHASP) by providing design direction for new development to shape and enhance the evolving character of the private property within the Specific Plan area. The RHASP Design Criteria (“Design Criteria”) allow creativity in new infill development that is compatible with Tustin’s character and the overall design aesthetic the community would like to preserve and perpetuate. The Design Criteria are intended to promote high-quality design and to ensure that new development and rehabilitation of existing structures promote a clear community identity and sense of place. It is not the intent of the Design Criteria to eliminate freedom in design, discourage innovation, or impose an overriding style, color palette, or artificial theme.

The Design Criteria provide a tool for developers, builders, architects, engineers, and landscape architects and others to use in the design of new development.

The Design Criteria provide a defined framework of design principles that supplement development standards by providing direction and guidance on the more qualitative aspects of a development project. A project may not be required to meet all design criteria, as not all may be applicable on a case-by-case basis. In addition, alternative measures may be considered if the measures meet or exceed the intent of the Design Criteria. The purpose is to ensure that plans for new development or rehabilitation of existing structures carefully consider the community context and make a conscious effort to develop a compatible relationship with neighboring properties and to respond to design intentions of the Specific Plan.

#### 5.1.2 Applicability

The Design Criteria will be utilized during the City’s design review process to encourage the highest level of design quality, while at the same time providing flexibility in their application to specific projects. The design elements of each project (including, but not limited to, site design, architecture, landscaping, lighting, and signage) will be reviewed comprehensively by the applicable review authority (Community Development Director, Zoning Administrator, Planning Commission, or City Council).

The Design Criteria apply to all parcels within the Specific Plan area. Where these criteria are most applicable to a certain area or project type, it is noted in the text. During the review process, the review authority may interpret these Design Criteria with some flexibility in their application to specific projects, as not all design criteria may be workable or appropriate for each project. Projects will be evaluated on the degree to which the project demonstrates substantial compliance with the intent of the Design Criteria, leading to a recommendation of project approval or denial. Findings required for approval of projects are outlined in Chapter 6, *Implementation and Administration*, of this Specific Plan.

### 5.1.3 Using the Design Criteria

The Design Criteria are designed to meet the needs of many users, including property owners, merchants, real estate professionals, architects, building contractors, vendors, craftspeople, City of Tustin staff, and other interested persons and organizations in the community. Each group plays a vital role in establishing quality development in the Specific Plan area.

The Design Criteria supplement the development standards and regulations in Chapter 4, *Land Use and Development Standards*. The Design Criteria provide guidance on a variety of design elements and are intended to further define the desired character and imagery within the RHASP area.

To effectively utilize the Design Criteria, and gain an optimum understanding of the Specific Plan's intent and conformance requirements, the following basic process is recommended:

- When preparing a new project in the Specific Plan area, or proposing changes to an existing building or site, users of the Design Criteria should first consult the Specific Plan Development Plan (Chapter 3), including required streetscape, and consult the Specific Plan Land Use and Development Standards (Chapter 4).
- The users should identify any specific Design Criteria that are applicable, paying close attention to criteria that may be unique to the proposed use or building type.
- Schedule a pre-application meeting with the Planning Division of the Community Development Department to review ideas, identify processes and requirements, ask questions, and discuss potential issues, solutions, and approaches.

The Design Criteria are applicable to the following types of projects within the Specific Plan area:

Applications for Design Review are generally required for all projects and shall be processed in accordance with Section 6.6.1, Approval Authority of this Specific Plan and Section 9272, Design Review of the TCC.

Such projects would include conformance with applicable design criteria and guidelines related to the proposed improvements. A new construction project, enlargement of existing structures and building relocation would comply with all provisions of the Design Criteria, while new or modified signage, exterior facade improvements and major exterior improvements would comply with the architectural provisions. Design Criteria are not applicable to interior remodels of existing buildings.

Within the Design Criteria:

- "Shall" indicates a mandatory provision. Required provisions are noted with (R) before the provision.
- "Should" indicates a provision that is encouraged but not required. Such items are guidelines and are noted with (G).
- "May" indicates a provision that is permitted but not required. Such items are guidelines and are noted with (G).

- Illustrations and photographs are provided as general guidance and are not intended to indicate required or preferred design.

## 5.2 Design Principles

### 5.2.1 Purpose

This section provides the basic framework for project design and the goals that each project is expected to achieve. New construction projects typically have greater opportunities to address the design principles more completely, while projects that involve renovations or additions to existing buildings are typically more limited in their ability to address each specific principle. For renovations or enlargements of existing buildings, the Design Criteria should be followed to the greatest extent feasible based on the area and magnitude of work being conducted. For example, a façade rehabilitation project or major exterior alternation should, at minimum, follow guidance on architectural imagery, detailing, colors and materials. For a façade rehabilitation project, the limited area of the project may not allow for the provision of new gathering spaces or other site amenities. Conversely, full redevelopment of a site should incorporate amenities, landscaping, etc., as the scope of work allows the project to integrate such items into the design. For this reason, the requirement of substantial conformance is a finding of approval for any project.

The following design principles contribute to future development and redevelopment ensuring that development activity is integrated, sensitive to surrounding neighborhoods, provides positive imagery, and creates an inviting environment for residents and visitors, meeting the intent of the Specific Plan.

#### **Design Principle 1: Provide for High Quality Development**

The RHASP area is anticipated to redevelop over an extended period, thus development should not be thought of as temporary. Building materials and landscaping should be chosen for their ability to be maintained in a cost-effective manner at the same high quality as when they were originally constructed and installed. Planning and design of buildings should consider longevity and adaptability to future conditions and trends.

#### **Design Principle 2: Promote a Sense of Place within the Specific Plan area**

New development and redevelopment within the Specific Plan area should create an inviting environment that encourages people to enjoy public spaces and patronize shops and businesses. Public gathering spaces, cohesive architecture, landscaping and signage can all contribute to creating a sense of place.

#### **Design Principle 3: Provide Development that is Human Scale**

The RHASP area has traditionally been oriented towards the automobile. Development in the area should be inviting and scaled for its users, while acknowledging the importance of safety for both pedestrians and vehicles along with access and mobility through pedestrian connectivity, public transit and bikes. The extension of human-scale character throughout the area is a design principle to be

addressed by all projects. In addition, new architecture should be designed with elements which are scaled to the pedestrian, such as awnings, store-front windows, and first-floor design elements.

#### **Design Principle 4: Create Linkages between the Specific Plan area and Surrounding Neighborhoods**

The Specific Plan area is adjacent to existing residential neighborhoods and public/institutional uses (parks, including Pine Tree and Frontier Parks, and schools). Development should demonstrate sensitivity to these existing uses while providing access to new amenities, employment areas and shopping. Linkages should consider different types of transportation modes, including vehicles, bicycles, and pedestrians.

#### **Design Principle 5: Create Cohesive Thematic Elements**

The RHASP area is bisected by Interstate 5, creating a potential disconnect between the northern and southern areas of the plan. New development and redevelopment should provide thematic elements that create a cohesive environment. Elements can include; consistent and cohesive streetscape; strong relationships between buildings, sidewalks, and other outdoor spaces; comprehensive signage and wayfinding; gateway elements; and pedestrian and bicycle connections.

### **5.3 General Area-Wide Design Criteria**

The following Design Criteria apply to all development within the Specific Plan area and should be used as a guide for new development or improvements to existing development.

#### **5.3.1 Architectural Style**

The architectural criteria and guidelines have been prepared to provide the framework for high quality design for development projects within the Specific Plan area. The architectural criteria and guidelines express the desired character of future development, and address architectural character and site planning. These criteria will encourage community identity and sense of place within the Specific Plan area. The guidelines are not intended to limit innovative design, but rather provide clear direction.

In order to maintain a development pattern that is consistent with the surrounding environs, buildings should evoke a suburban character in form and function, reflect straightforward geometry and show an expression of floor levels and structure. As described in the following sections of this document, buildings should follow sound design principles by incorporating massing and proportion, structure, simple roof forms, fenestration, balconies, accent elements, high quality materials and colors into a unified architectural expression. Within the Specific Plan area, architecture may draw inspiration from such locally relevant traditional styles as Spanish/Mediterranean, Modern Craftsman (in certain applications) and agriculturally-based styles such as Farmhouse/Agricultural style expressed as a contemporary interpretation. Building design and execution should be sensitive to current construction practices and should not attempt to literally replicate historic styles. Ornate and heavily themed styles, such as Tudor, Victorian and Beaux Arts are not allowed.

# RED HILL AVENUE SPECIFIC PLAN



**Architectural Style:**  
Spanish/Mediterranean

**Characteristics:**

- Stucco with brick or stone accents
- Terracotta roof tiles
- Light, earth-tone colors
- Curvilinear shapes and arches
- Tower-like chimneys/vertical elements
- Arcades and balconies
- Tile accents, decorative vents, and/or ironwork



Exhibit 5-1 Examples of Spanish/Mediterranean Architectural Style

**Architectural Style:**

Modern Craftsman

**Characteristics:**

- Flat or low-pitched, multiple gable roof
- Heavy use of wood, brick, and stone materials
- Exposed roof rafters and decorative beams
- Strong, clean lines
- Decorative window trim and details (i.e. transom, repetition)



Exhibit 5-2 Examples of Modern Craftsman Architectural Style

# RED HILL AVENUE SPECIFIC PLAN

## Architectural Style:

Farmhouse/Agricultural

## Characteristics:

- Use of visually strong materials like wood, stone, steel
- Warm, vibrant color palettes
- Exposed awnings
- Iconic farmhouse elements (grain silo, windmill, water tower)
- Flat or low-pitched roof
- Strong lines and minimal decorative detailing

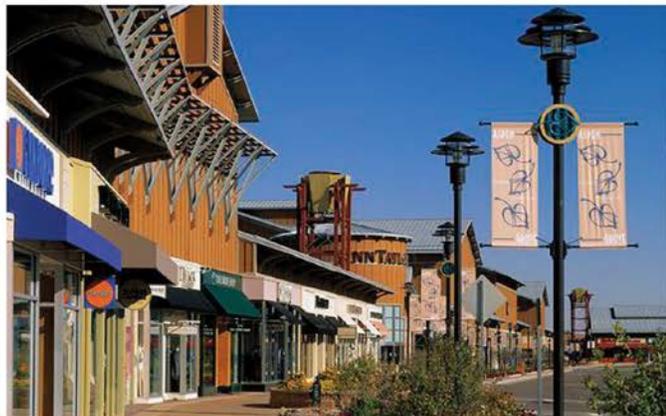


Exhibit 5-3 Examples of Farmhouse/Agricultural Style

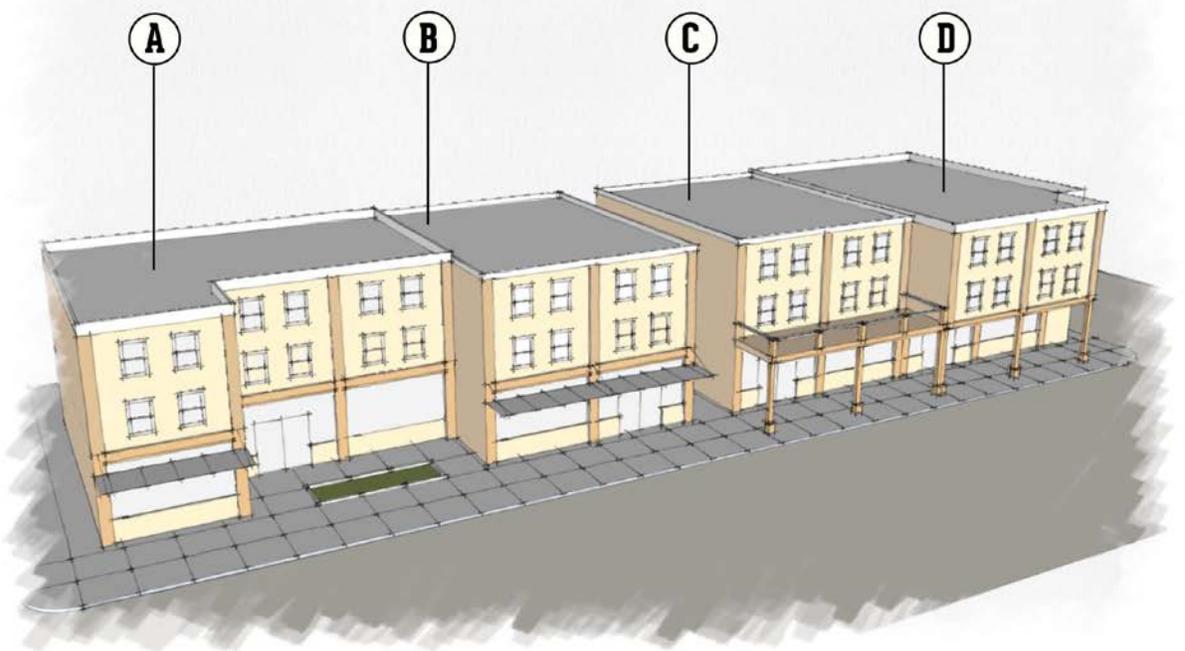
## 5.3.2 Architectural Criteria and Guidelines

The proper consideration of the scale, massing, and detail on individual buildings will contribute to a coherent streetscape and enjoyable public environment. Regulating appropriate building proportion will contribute to a coherent building design and promote architectural unity in the Specific Plan area. Multiple storefronts, shop entrances, building articulation, and activities enliven the environment and provide a safe and interesting pedestrian environment. New development should include well-built structures that are compatible with Tustin's community character, and particularly the overall RHASP architectural palette.

## 5.3.3 Building Mass and Scale

- (R) Each building shall have a recognizable base, body, roof lines and entry.
- (R) Upper floors shall have varied setbacks to accommodate balconies, seating and other architectural treatments.
- (R) Multi-story buildings should be made less imposing with physical step-backs on upper floors and architectural details and materials at ground floor that are pedestrian-scaled such as arches, trellises or awnings. Upper story step-backs are required for mixed-use buildings (see upper story step-back requirements in Table 4-3, *Development Standards* in Chapter 4).
- (R) Due to the high visibility of corner properties, corner buildings shall have prominent architectural features to anchor their location while still allowing clear lines of sight at corners. Architectural features and materials shall wrap corners and provide the same level of detail as the primary façade.
- (R) Design features must be consistent on all elevations of a structure. Side and rear elevations visible from public rights-of-way, schools, adjacent residential, parks or other similar spaces shall receive the same design considerations as the primary public elevation. Four-sided architecture is required for all buildings.
- (G) Building frontage types provided in Exhibit 5-4, *Frontage Types*, and Exhibit 5-5, *Forecourt Frontage Types (Open Space)* address the elements of the ground floor as it relates to the street and outline the elements of each type. The design of building frontages reinforces human scale and determines the character of the pedestrian environment. The frontage types shown are those most common for new development within the Specific Plan area. The choice of frontage type, design and architectural style is the decision of the property owner. At the time of a development application an applicant may suggest a different approach that achieves the same goals of pedestrian scale and orientation.
- (G) Recessed entries and articulation in the storefront mass should be included.
- (G) Tall structures should be broken up by creating horizontal emphasis using trim, awnings, eaves, or other ornamentation and by using a combination of complementary colors.

# RED HILL AVENUE SPECIFIC PLAN



**A - FORECOURT**



**B - STOREFRONT**



**C - GALLERY**



**D - ARCADE**



**Exhibit 5-4 Frontage Types**

### 5.3.4 Rhythm and Proportion

- (G) When new development or rebuild is proposed, the building façade size, horizontal elements (e.g. cornice line, window height/width, and spacing), and setback distance should be consistent with the existing neighboring or adjacent structures to maintain façade rhythm.
- (G) When there are no neighboring or adjacent structures near a new development or rebuild, the proposed development should look to establish a façade rhythm that is consistent with well-designed structures found throughout the Redhill Specific Plan area.
- (G) When an infill building or building rehabilitation is proposed, the common horizontal elements (e.g. cornice line, window height/width, and spacing) found in well-designed neighboring or adjacent structures may be used to maintain a similar architectural rhythm.
- (G) When a proposed infill building façade is wider than the existing facades on the street, the infill façade should be broken down into a series of appropriately proportioned components to continue and/or complement the existing façade rhythm.

### 5.3.5 Wall Articulation and Interest

- (R) Long, blank, unarticulated street-facing facades shall be avoided. Monolithic street wall facades shall be “broken” by vertical and horizontal design elements.
- (G) Building facades should be detailed to create visual interest and promote pedestrian scale. This can be partly achieved by articulating the separate floor levels and increasing the level of detail at the street level.
- (G) Pedestrian-level exterior walls should maintain elements of building depth and character. Emphasize windows, trellises, roof overhangs, recessed or projected stories, columns, balconies, wainscots, and awnings.
- (G) Where possible, public art (murals, art installations) are encouraged to break up long buildings or exterior walls.



*Multi-story structures should be broken up by using different types of building ornamentation*



*Building facades should be detailed to create visual interest*

## 5.3.6 Roof Articulation

- (R) Roof-mounted mechanical or utility equipment is required to be screened. The method of screening should be architecturally integrated with the structure in terms of materials, color, shape, and size. Equipment should be screened by solid building elements (e.g., parapet wall) instead of add-on screening (e.g., wood or metal slats). Mechanical equipment should not be visible from any angle or any height outside of the building.
- (R) Roofline elements including parapet walls shall be developed along all elevations, regardless of orientation away from the right-of-way.
- (G) Roofs should be given design considerations and treatment equal to that of the rest of a building's exterior.
- (G) Roofs and rooflines should be continuous in design except where there is a major change in an element of a building elevation.
- (G) Roofs may be flat or sloped. Radical roof pitches that create overly prominent or out-of-character buildings such as A-frames, mansards, and geodesic domes should be avoided.
- (G) Flat roofs should include ornamental cornices and edge details.



*Roofs should be given equal treatment to that of the rest of the building's exterior*



*Roofs and rooflines should have continuous design*



*Flat roofs should include edge details*

- (G) Roofline elements such as exposed roof rafters are encouraged with the appropriate architectural styles.
- (G) The visible portion of sloped roofs should utilize materials complementary to the architectural style of the building and other surrounding buildings.

### 5.3.7 Materials and Colors

- (R) Exterior materials, textures, and colors shall be appropriate for the architectural style or theme of the building and contribute to the visual quality of the streetscape.
- (R) Colors and materials shall be high quality, durable and not readily deteriorate if exposed to the elements.
- (G) Materials should be compatible with materials used on the adjacent buildings, assuming they meet the guidelines for high-quality materials outlined in Design Principle 1.
- (G) Use color to accent architectural details.
- (G) No more than three colors should be used on any given façade, including “natural” colors such as unpainted brick or stone.
- (G) Chimneys, roof flashing, rain gutters, downspouts, vents and other roof protrusions should be finished to complement or accent adjacent colors and materials, or incorporated into the building design.
- (G) Materials and technologies that minimize environmental impacts, reduce energy and resource consumption, and promote long-lasting development are highly encouraged. Use of graffiti-resistant materials and coatings is encouraged.



*Natural, light-colored materials are encouraged*



*Pedestrian entrances via alleyways is encouraged*

## 5.3.8 Windows, Doors and Storefronts

- (R) High quality, durable materials should be used for all doors, windows, and other storefront elements, including trim and accent features.
- (R) For buildings on corners, include storefront design features such as windows, window treatments, vertical/horizontal banding and accent materials for at least 50% of the wall area on the sidewall.
- (R) To reduce glare, recess windows and large areas of glass by inseting glass a minimum of 3 inches from the exterior wall surface to add relief. Clear glazing is strongly recommended, while reflective glazing and tinting is prohibited.
- (G) Generally, base storefronts on modules of approximately 25 feet in width. For new buildings that are wider than existing facades on the street, break them down visually into clearly distinguishable segments through the use of columns or piers.
- (G) Treat each storefront like a small building, with its own base, roofline, and door and window pattern.
- (G) Building recesses are encouraged to define entryways and window openings along a building's exterior and to provide weather protection and a transition zone from sidewalk activity into the store. Recommended treatments are special paving materials such as textured ceramic tile, pavers, or brick, ornamental ceilings such as coffering and decorative light fixtures.
- (G) Clearly articulate entries to commercial structures with substantial, well-detailed doors that match the materials, design, and character of the display window framing. Windows to retail shops should contain a high percentage of glass to display retail contents.
- (G) Providing rear pedestrian entrances via alleys/private drives and parking lots is encouraged where feasible. Improvements to rear facades should be subtle and modest, and may include signs, landscaping, and awnings.



*Utilize architectural details at ground floor*



*Building recesses are encouraged to define entryways and window openings*

## 5.3.9 Site Planning and Compatibility

### A. General Requirements

- (R) Establish clear pedestrian connections on-site that are well-marked; direct pedestrians from surface or structured parking to buildings, streets, and public spaces.
- (R) Establish well-defined outdoor spaces, such as on-site plazas, interior courtyards, patios, terraces and gardens; these are especially encouraged in association with major developments.
- (R) The siting or placement of buildings shall recognize the characteristics of the site and relate to the surrounding built environment in pattern, function, scale, and character.
- (R) Buffer adjacent sensitive land uses from negative impacts that may originate from a site; buffers may be landscaped and/or architectural in character.
- (G) On larger sites, focal points should be developed to create thematic features. Plazas, landscaping, fountains, artwork, textured pavement, changes in pavement levels, and vertical architectural features may be combined to create a sense of place.
- (G) Pedestrian linkages to nearby neighborhoods, other commercial properties, and public amenities, such as pocket parks, walkways, or seating areas, should be provided whenever appropriate and feasible.
- (G) Create a pleasant microclimate, including shade from summer sun and access to winter sunlight; locate seating with consideration to noontime sun and shade.
- (G) Site furniture and light fixtures in private developments should reflect complementary design concepts as the public right-of-way street furniture and light fixtures.



*Clear on-site pedestrian connections that are well-marked*



*Site furnishings and seating provide pleasant meeting places*

- (G) Commercial and residential uses should include buffers at their interface. Building orientation, landscaping, and increased setbacks may be used to provide compatibility between these uses.
- (G) When adjacent uses can mutually benefit from connection, appropriate linkages (e.g. common landscape areas, building orientation, pedestrian paseos, and unfenced property lines) are recommended.



*Buffer commercial and residential uses using low walls and landscaping*

## B. Access, Circulation and Parking

- (R) Surface parking lots shall be located behind buildings or interior to a block, not to the front and preferably not to the side of a building in relation to the street; surface parking shall not be located at a street corner. When parking must occur at the street edge due to variation in building placement or lotting patterns, proper screening of automobiles from public view and adjacent sensitive land uses is required. This may be accomplished with dense, tiered landscaping or low-profile walls while in keeping with line of sight and visual clearance requirements.
- (R) Parcels that include parking and buildings should provide cohesive interaction between automobile and pedestrian circulation within the site and between adjacent properties and activities.
- (G) Minimize the number of curb cuts and access points to parking areas from the street; curb cuts should be consolidated and placed at mid-block, or provide alley access to service and parking, where feasible.



*Provide pedestrian connections within parking lots*

- (G) Developments should integrate and encourage use of alternative modes of transportation, such as bicycles and transit, by providing safe, convenient, and attractive facilities.



*Developments should integrate bike parking*

## C. Utilities, Loading and Service Areas

- (R) Avoid siting of noise and odor-generating functions in locations that may create a nuisance for the adjacent properties or for the pedestrian paths. Buffer adjacent sensitive land uses from the undesirable impacts of service facilities; use landscape or cohesive architectural treatment to screen service access and facilities. Hours of operation for deliveries shall comply with the Tustin City Code (TCC).
- (R) Locate service, loading, and storage areas away from public streets and public spaces, wherever possible.
- (G) Combining or grouping trash dumpsters at central locations behind buildings is encouraged.
- (G) Landscaping, green (garden) walls and fences should be used to screen non-public areas, trash bins, bin loading areas, utilities, mechanical equipment, and other support facilities.

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*Screen on-site trash bins and utilities*



*Locate service and loading areas away from public streets*

### 5.3.10 Parking Structures

Parking structures are an efficient way of providing parking without allocating land for surface parking. Parking structures, both above and below grade, may occur in a variety of settings, both for commercial or office uses (for example a commercial assisted living building), or for vertical mixed-use buildings in a “wrap” format. In any situation, the treatment of these structures affects the aesthetics of a development.

The following criteria apply:

#### A. Site Planning

- (R) A minimum 5-foot landscaped setback shall be provided on all sides of the parking structure except where ground floor retail space is provided. Landscaping must provide adequate facilities to ensure proper watering and maintenance.
- (G) Where appropriate, parking garages should incorporate ground floor retail adjacent to the public sidewalk.

#### B. Building Design

- (R) Parking structures shall be designed as an integral component of a coordinated site plan and architectural theme.
- (R) The interior treatment of a parking structure shall be of a light color. The structure shall include a coordinated interior sign program designed to identify parking levels and locate stairwells, elevators, phones, exits and other interior features.
- (R) Parking structures shall be designed to help reduce the mass and scale of the garage and to ensure their compatibility with surrounding uses.
- (R) Vehicles shall be concealed from view through a combination of screen walls and plantings.
- (R) The elevations shall provide an adequate level of design detail to reduce a monolithic appearance. This can be accomplished through a combination of the following methods:
  - Provision of a balance of both horizontal and vertical elements.
  - Incorporation of simple, clean geometric forms, and coordinated massing. Step back upper levels of the garage.
  - Use of ground floor retail or other uses adjacent to setbacks.
  - Coordinate openings in the parking garage with the size and modulation of adjacent windows, structural bays, and/or storefronts.

- Design openings in the parking garage to resemble architectural elements of the adjoining structure.
- Use of masonry materials are the same or similar color as the adjoining structure. The use of unpainted concrete shall be minimized.
- Use of landscaped walls to soften the appearance of buildings is encouraged.
- Avoid a sloping ramp appearance by providing level and uniform spandrels.
- Visually define and differentiate between pedestrian and vehicular entrances through appropriate architectural detailing.



*Parking structure architecture integrated into building design*

## C. Security and Lighting

- (R) Lighting fixtures shall be designed and placed to provide uniform illumination over all parking areas (see lighting regulations in Chapter 4, *Land Use and Development Standards*).
- (R) Light sources shall be shielded so that the source of the illumination is not seen from outside the structure.
- (R) The architectural design of the garage shall eliminate possible hiding places and openings that could allow random pedestrian access.
- (R) During periods when parking activity is substantially less than the garage capacity, as during night operations, there shall be a means of securing unused parking levels from use, including stairwells and elevators. If the garage is not operated on a 24- hour basis the entire facility shall be secured from access during hours when the facility is closed to normal business activities.
- (R) At a minimum, stair towers shall include glass, or visually penetrable material running vertically the height of the tower. Elevators should be provided with glass-back cabs and shafts.
- (G) Stairs and elevators should be located adjacent to a street on the exterior of the structure where lobbies can be exposed to outside view.

## 5.4 Specific Design Criteria

The following sections include specific design criteria for commercial uses (Section 5.4.1) and for mixed-use projects (Section 5.4.2). As appropriate, specific criteria within the commercial subsection may be applied to commercial uses within a mixed-use project.

## 5.4.1 Commercial Uses

### A. Site Planning

- (R) Loading and service areas shall be separate from the primary public access.
- (G) Buildings should be oriented to face either a public street, private street or pedestrian outdoor space and provide pedestrian access from the public sidewalk.
- (G) When feasible and appropriate, interior spaces are encouraged to extend into the outdoors, both physically and visually. This may be achieved through awnings and overhangs, outdoor dining spaces or other features.
- (G) Commercial development should provide primary access from the front of the building. Avoid public access to the rear of structures when adjacent to potentially incompatible uses.
- (G) Commercial development should provide outdoor pedestrian activity and gathering areas such as courtyards and plazas which are accessible to the public.
- (G) When commercial development is adjacent to residential uses, commercial access should not front onto residential uses unless in a horizontal mixed-use setting.
- (G) When commercial development is situated on a corner, a wrapped building design is encouraged for a continuous retail presence on both street frontages.

### B. Building Design

Commercial buildings shall be subject to the following:

- (R) The architectural style shall consist of contemporary interpretations of traditional architectural styles found in Tustin as outlined herein. Unique or landmark architecture may be considered during design review.
- (R) Enhanced architectural elements shall be provided for buildings located at the intersection of public streets in the Specific Plan area.
- (R) Building facades shall be articulated with architectural elements and details on all sides visible from on-site pedestrian areas or off-site property or rights-of-way.
- (R) Primary building entries shall provide a prominent sense of entry for easy



*Buildings should present an attractive and interesting facade*

# RED HILL AVENUE SPECIFIC PLAN

identification. The use of architectural projections, columns, entry lobbies or other design elements are strongly encouraged. This applies to all entrances adjacent to the public R.O.W.

- (R) For buildings with two or more entrances, the primary entrance should be clearly delineated through architectural detailing, lighting, or signage.
- (R) Roofline elements, including parapet walls, shall be developed along all elevations.
- (R) Step-backs or articulation of building planes shall be provided for commercial uses which exceed two stories, subject to design review. This would apply to hotel or other unique commercial uses such as assisted living which may exhibit elements of residential-style architecture.



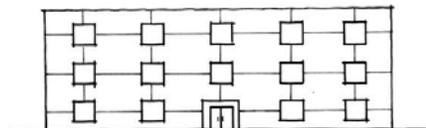
*Roofs should provide equal design treatment*

- (R) First floor primary elevations must include pedestrian-scaled elements such as storefront design, overhangs, awnings, arcades, etc.
- (G) The size of door and windows should relate to the scale and proportion of the overall structure. Monumental entries that are not pedestrian scaled are discouraged.
- (G) The use of security grilles on windows is discouraged. If security grilles are necessary, they should be placed inside the building behind the window and not be visible from public view.
- (G) Upper story window to wall ratios should be lower than the ground floor.
- (G) Window frames and sills should be used and must be proportional to the window frame.
- (G) Public rear entrances should be visible and easily located. Rear entrances should not be more prominent than the front, primary entrance.
- (G) Roofs should be given design consideration and treatment consistent with the primary building and the rest of the building exteriors. Roof form and rooflines should be continuous in design throughout a commercial development.



APPROPRIATE

Variation in vertical and horizontal wall planes



INAPPROPRIATE

Minimal variation along building facade

**Office Uses.** Office uses have physical and functional characteristics that are not typical of commercial development. Office uses are typically within multi-story buildings that may be large in scale. There are fewer public entries and higher utilization of on-site parking. In addition to the previous criteria for commercial uses, office uses have specific criteria:

- (R) Windows on the first floor shall encompass a minimum of 40 percent and a maximum of 50 percent of the building façade.
- (G) Office buildings should have the primary entry visible from the public street and accessible from pedestrian pathways and parking areas.
- (G) Entry lobby interiors should be well-designed and of high-quality when visible from the building’s exterior.



*Primary office building entries should be well-defined*

**Flexible Format Retail.** “Big box” retail has evolved, with retailers providing smaller-format stores which are compatible with smaller retail or mixed-use centers and require smaller floor area. These stores provide a variety of products under one roof. Flexible format stores are often located on sites with multiple buildings of various sizes. In addition to the previous criteria for commercial uses, the following guidelines are specific to this type of retail use:

- (R) Building façade articulation and a mixture of materials shall be used to avoid large wall expanses of unrelieved stucco material.
- (G) Design of outdoor sales or display areas (i.e. garden centers, etc.) should be consistent with the architecture of the primary building, utilizing articulated walls or fencing to enclose the space.
- (G) Loading and service areas should be located and designed to minimize direct exposure to public view. Walls or tiered landscape screening should be used to reduce visual impacts. Loading area location and design should prevent truck back-up maneuvers from or onto public rights-of-way of parking aisles.
- (G) Exterior wall treatments, such as arcades, porticos, offset planes and colonnades should be used to break up the mass and scale of the building.
- (G) Facades of buildings located on a corner should wrap around to ensure a continuous aesthetic quality.



*Exterior parapet walls should terminate with a building cap*



*Exterior wall treatments are encouraged to break up mass and scale of building*

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**Hotel Uses.** Small-scale hotels and motels are classified as a commercial use, but often include residential-scaled elements in building design. Therefore, the design and orientation of hotels and motels should consider both the impact of the use on adjacent development and the impacts of adjacent development use. In addition to the previous criteria for commercial uses, the following guidelines are specific to this type of use:

- (R) Delivery and loading areas shall be located at the rear of the building lot and be screened to minimize impact on incompatible uses.
- (R) Hotels and motels shall provide landscaped setbacks from interior property lines.
- (G) The main building and driveway should be the primary visual presence along Red Hill Avenue; parking lots should not be adjacent to the street frontage.
- (G) Recreational facilities such as swimming pools should be designed to offer privacy to facility users and to minimize noise impacts on adjacent uses. Mechanical equipment of all types, including swimming pool equipment, should be located to minimize impacts on adjacent uses.
- (G) Landscaped areas should separate ground floor units from pedestrian walkways, project amenities and drive aisles/parking areas.
- (G) For structures over two stories, access to guestrooms should be provided from interior hallways. Avoid room entrances directly adjacent to parking lots or exterior walkways.
- (G) Exterior building materials should include natural stone (marble, granite, brick, slate, etc.), stucco, and/or cultured (i.e. manufactured) stone. The choice of materials should be complementary to the design of the structure.



*Main building and driveway should be primary presence on major streets*



*Architectural treatment of building should be equally applied on all facades of hotel*

- (G) Air conditioning units should not be visible from public streets.
- (G) Walkways, stairways, and balcony railing and other similar details should be architecturally consistent with the building design.

- (G) Walls and fences along property lines should be designed to complement the architecture of the primary buildings on a site. Decorative elements, or flowering vines should be incorporated.

**Drive-Thru Businesses.** A proliferation of drive thru businesses is not encouraged in the Specific Plan area. Drive-through businesses may include restaurants, coffee shops, banking institutions with drive-up teller/ATM access, pharmacies and other similar facilities. Drive-through businesses have additional design considerations related to on-site circulation, noise impacts, vehicular access, outdoor seating, signage, landscape, and other parameters. In addition to the previous criteria for commercial uses, the following criteria apply to new construction and major renovation of existing uses:

- (R) The primary visual reference along the Red Hill Avenue frontage shall be the building, not a drive-through lane or parking.
- (R) Curb cuts shall be minimized.
- (R) Drive-through lanes for food and beverage establishments shall accommodate seven (7) vehicle stacking at the menu board and pickup windows without impacting other on-site circulation.
- (R) Safe pedestrian walkways shall be provided from the parking areas and from street frontages. Pedestrian pathways to the use should avoid conflicts with vehicular drive-through lanes through siting or clearly delineated paving.
- (G) Drive-through aisles should be separated from adjacent streets and parking areas.
- (G) Tiered levels of landscaping, including the mixing of groundcovers, shrubs, and trees, and/or low masonry walls should be utilized.
- (G) Public art should be displayed on walls where possible.
- (G) Outdoor eating areas are encouraged. Outdoor eating areas should provide details such as low walls, trellis elements, furniture, umbrellas/awnings, refuse area placement, etc.
- (G) Franchise-identifying features should only be located on the main structure.
- (G) Exterior doors, equipment rooms, and service/employee entries should be designed with complementary architectural treatment.



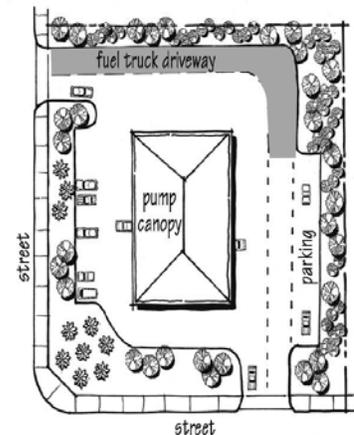
*Drive-through lanes shall accommodate appropriate vehicle stacking based on the specific use*



*Outdoor eating areas are encouraged at drive-through businesses*

**Service Stations and Car Washes.** Service stations are often characterized by unique site features, such as intensive on-site vehicle utilization; and servicing; large expanses of paving; use of equipment and machinery; and use of potentially hazardous materials. Refer to TCC Service Station Guidelines for a complete list of regulations and guidelines.

- (R) The location of filling pumps, carwash bays, or other on-site facilities shall be designed to avoid vehicle stacking or overflow onto adjacent streets.
- (R) High quality, durable building materials shall be used. Service stations and car washes shall incorporate façade material to produce textured design. Reflective, glossy, and fluorescent surfaces are prohibited.
- (R) When service stations/car washes front public streets, a berm and/or hedge or other landscaping shall be provided for screening.
- (G) Structures on site should be grouped together and integrated into the overall design of a site.
- (G) When commercial development borders a service station, two-way vehicular access that is integrated with adjacent development should be provided.
- (G) Site design for projects located at street corners should provide structural or strong design elements that anchor the corner.
- (G) Storage facilities should be enclosed within the primary structure on the site.
- (G) All required perimeter walls and fencing should be decorative and consistent with adjacent architecture.
- (G) A fuel delivery truck lane should be provided through the site with sufficient turning radius.
- (G) The siting of self-serve carwash bays, drying and vacuuming areas should be small in scale and not conflict with on-site vehicle circulation. These areas must also be located away from residential uses and designed to mitigate noise impacts on adjacent properties.
- (G) Buildings facing public streets should be architecturally detailed. Building design should be given equal design consideration on all sides.
- (G) Service station building design should consider the context of the site and area.
- (G) Self-service car wash bays, and drive-through car wash facilities should be designed to ensure all drainage is confined on-site. When service stations/car washes include retail uses, a



*Fuel truck driveways should not obstruct on-site circulation*

clearly defined pedestrian path should be provided from required on-site parking to the primary customer entrance.

- (G) Self-service carwash/service bays should not face the street.
- (G) Pump island canopies should be designed to match the architecture of primary structures on the site.



*Car wash bays should not conflict with on-site vehicle circulation*

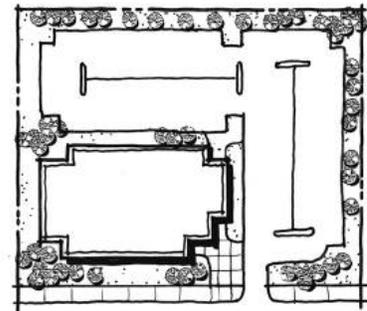


*Pump island canopies should be designed to match architecture of primary structure*

## C. Parking, Access, and Circulation

Commercial parking and vehicular/pedestrian circulation areas shall be subject to the following:

- (R) For new development, parking shall not be located between the front of the building and the street. Surface parking shall be located at the rear of the site or the side of the building and not visible from streets. Where limited areas of surface parking are in view of adjacent streets, it shall be screened with tiered landscaping.
  - When existing commercial properties within the Specific Plan area are renovated, improvements to existing surface parking areas will be required, to increase/add parking lot landscaping for shade, and increase/add buffer landscaping at the street interface.
  - Creative site planning of the commercial center should not re-create the strip-mall design of the past, with a row of commercial uses fronted by a “sea” of parking, but should provide a variety of building placement, breaking up parking areas and providing an interesting street scene.
- (R) Surface parking shall not be located at street corners/intersections.



*APPROPRIATE  
Parking located at rear or side  
of building*

- (R) Parking area lighting and security lighting for commercial uses shall be shielded to prevent glare and spillover into residential areas. Residential units shall also be shielded from illuminated commercial signs.
- (G) Site access and internal circulation should promote safety, efficiency, convenience and minimize conflict between vehicles, bicycles and pedestrians.
- (G) The number of site access points to a parking lot should be minimized and located as far as possible from intersections. Parking lot access points should not interfere with the function of adjacent roadways.
- (G) Use of common or shared driveways between adjacent uses is strongly encouraged.
- (G) Pedestrian access from surrounding residential neighborhoods and parks should be provided. This access should be designed to promote safety by providing unobstructed sight lines into the access way from surrounding public areas and be well-lit.
- (G) Safe, convenient pedestrian links should be designed between parking areas and businesses.
- (G) Unobstructed visibility and clear delineations between pedestrian paths and vehicular travel aisles should be provided. Use of landscaping, lighted walkways, and decorative hardscape to delineate pedestrian circulation is encouraged.
- (G) Access to transit stops should be integrated into the pedestrian circulation network.
- (G) Bicycle storage such as racks and lockers should be provided where feasible and easily accessible from the street, transit stops, and pedestrian routes.



*Pedestrian walkway within commercial settings*

## D. Site Amenities

Commercial open space areas shall be subject to the following:

- (G) Site furnishings (i.e. benches, bollards, trash receptacles, tables/chairs, umbrellas, kiosks, directories and/or bicycle racks) within development projects should be compatible with the design of the adjacent development and similar in quality to furniture used in the public streetscape improvements.

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- (G) The siting of site furnishings should not create pedestrian/vehicular conflicts. Legible and appropriately scaled kiosks/directories should be sited near vehicular and pedestrian entrances.
- (G) Seating areas or plaza spaces within commercial developments are strongly encouraged; in large commercial centers with multiple buildings it is a requirement. This may include provisions for formal plazas, urban gardens, covered colonnades, outdoor dining, or pedestrian walkways.



*Larger commercial development should provide outdoor gathering spaces*

## E. Support Facilities

Commercial support facility areas shall be subject to the following:

- (R) Storage, refuse and equipment areas shall be screened from publicly accessible spaces and/or neighboring residential uses. Landscaping and/or architectural enclosures can be used to screen these areas.
- (R) Rooftop equipment shall be fully screened from public view by parapet walls or roof elements.

- (R) Screening shall be compatible with the design of adjacent architecture. Enclosures shall use forms, materials and color applications similar to the primary building(s).
- (R) Refuse storage areas that are visible from upper stories of adjacent structures shall provide an opaque or semi-opaque horizontal cover or screen to reduce unsightly views. The horizontal screen/cover shall be integrated into the enclosure design and compatible with adjacent development.
- (G) Refuse storage enclosures should be located so that the doors do not interfere with landscaping and pedestrian and vehicle circulation.
- (G) Dimensions of refuse enclosures should not exceed the reasonable space required for anticipated uses.
- (R) Utility equipment should be located at the rear of buildings. When equipment is required to be located along street frontages or elsewhere, it shall be screened from public view.



*Refuse areas shall be screened*

## 5.4.2 Mixed-Use

Mixed-use projects are defined as developments that combine commercial/office and residential uses in structures on a single lot, or as components of a single development. The uses may be combined either vertically within the same structure, or spread horizontally on the site in different areas and structures. Horizontal mixed-use projects, which include multifamily residential units in the Specific Plan area, should contribute to the sense of community by carefully relating to the scale and form of adjacent properties, and by designing street frontages that create architectural and landscape interest for the pedestrian and adjacent neighborhoods. Any ground floor development along Red Hill Avenue must be commercial in character to continue the focus of this retail corridor.

### A. Site Organization

- (R) Parking facilities for residential uses and non-residential uses in a vertical mixed-use setting shall be separated and clearly delineated using gates/key card entries (or other mechanisms), and clear signage.
- (G) Commercial loading areas and refuse storage facilities should be placed as far as possible from residential units and be completely screened from adjacent residential development.



*Main building and driveway should be primary presence on major streets*

- (G) Commercial spaces should have the primary entry visible from the public street and be accessible from pedestrian pathways and parking areas.
- (G) Developments should relate directly to the adjacent street, and present an attractive and interesting façade.
- (G) The residential component of a horizontal mixed-use project should be compatible with other development in the immediate area using complementary building arrangements, buffers, and avoidance of overwhelming building scale and visual obstructions.
- (G) Buildings should be generally oriented parallel to streets with varying setbacks to provide visual interest and varying shadow patterns.
- (G) In the residential component of a horizontal mixed-use project, buildings should be oriented to promote privacy for individual residential units to the greatest extent possible.
- (G) Clustering of multi-family units should be a consistent site-planning element. Large projects should be broken up into groups of structures.
- (G) When commercial development is situated on a corner, a wrapped building design is encouraged for a continuous retail presence on both street frontages.



*Primary entry for commercial spaces should be accessible from pedestrian pathways*

## B. Building Design

Mixed-Use development shall be subject to the following:

- (R) Separate entrances, clearly marked, shall be provided when residential and commercial uses are provided in the same structure.
- (R) Building design and operations shall incorporate measures to reduce the impacts of business activities, such as noise, hours of operation, delivery/loading times, etc, on residential uses in a mixed-use setting.
- (R) Structures with heights greater than three stories shall step back upper floors to minimize the appearance of mass and bulk. Chapter 4, *Land Use and Development Standards*, herein require an aggregate five-foot step-back between the second floor and upper floors.



*Design of storefronts should be consistent with general commercial development*

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- (G) The architectural imagery and use of materials should be consistent throughout the entire mixed-use project. However, differences in materials and/or architectural details should be used to differentiate the residential portion of the project from the commercial portion of the project.
- (G) The design of storefronts should be consistent with the Design Criteria for general commercial development.



*Building facades should be detailed to create visual interest*

- (G) The residential component of a horizontal mixed-use project should use varied building heights to provide visual interest and give the appearance of a collection of smaller structures. The development's building height should create a transition from the heights of adjacent development, rather than abrupt height changes.
- (G) The residential component of a horizontal mixed-use project should use architectural elements such as recessed or projecting balconies, porches or other elements that add visual interest, human scale and character to the neighborhood.
- (G) Courtyard doors or gates used as building entries should be attractively designed as an important architectural feature of the building or development.



*Buildings should present an attractive and interesting facade*

- (G) The residential component of a horizontal mixed-use project should incorporate individual unit entries that have a strong relationship with a fronting street, internal walkway or courtyard as appropriate to the overall siting concept. A transition area from the public space or walkway to the private dwelling unit entry such as a porch, steps or landscaped walkway, should be provided.
- (G) Building facades that enclose stairways should include residential-type windows to reduce the visual bulk of the stairwell and enhance safety.
- (G) Building facades enclosing elevator shafts should use architectural treatments, such as glass or other materials, to reduce visual mass.

- (G) The architecture for the residential component of a horizontal mixed-use project should be consistent with that of the community facilities and, where parking structures are used, unified by a consistent use of building materials, textures and colors.



*Architectural treatment of building should be equally applied on all facades*



*Recessed balconies add visual interest*

- (G) For the residential component of a horizontal mixed-use project, roof pitches and materials should appear residential in character and should consider the prevailing roof types in the neighborhood.
- (G) For the residential component of a horizontal mixed-use project, garage and carport roofs should be compatible with the roof pitch and materials of the primary building(s).

**Flex Format Retail Space.** Flex Format Retail Space buildings allow for commercial or residential uses within a building. The building type may allow for incubator businesses needing smaller commercial areas, home occupation uses, or other combinations of residential, office, and commercial within a building. In all cases, the first-floor frontages must exhibit a commercial storefront character. In addition to the mixed-use provisions above, the following items apply:



*Each flex space unit should have separate access from other units*

- (G) Each unit should have a clearly identified, separate access from other units within the structure or development. Access to individual units should be from common access areas, parking lots or walkways.
- (G) Parking shall be separate and secure, similar to provisions for separating commercial/retail parking from residential parking in other mixed-use developments.

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*Units fronting pedestrian walkways should have pedestrian-oriented frontage*

- (G) The architectural imagery and use of materials should be consistent throughout the entire Flex Format Retail Space project.
- (G) Units fronting public spaces or pedestrian walkways should have a pedestrian-oriented frontage that allows views into the interior of the non-residential areas of the unit. Units should include display windows and other elements that are inviting to pedestrians.
- (G) Living and work areas should have access to sufficient daylight, through use of windows or skylights.
- (G) Residents or commercial users should have access to usable open space and be consistent with open space requirements for mixed-use residential development, as stated in Chapter 4, Section 4.4.3).

## C. Parking, Access, and Circulation

Mixed-Use parking and vehicular/pedestrian circulation areas shall be subject to the following:

- (R) Circulation conflicts between vehicles, bicycles and pedestrians shall be minimized. A continuous, clearly marked walkway should be provided from the parking areas to main entrances of buildings



*Examples of Flex Space Building Fronts*

*A frontage that reinforces both residential and work activities that can occur in the building. The elevation of the ground floor is located at or near the grade of sidewalk to provide direct public access to the building. Entrances and windows are provided on the front of the facade to provide eyes on the street and direct sidewalk access to commercial and office uses. The front setback (if provided) may be improved with landscaping or as an extension of the public sidewalk to create a more pedestrian-friendly environment.*

- (G) Where provided, surface parking should be divided into a series of small parking areas with convenient access that relates to dwelling units or commercial uses.
- (G) Parking areas should be in the development’s interior and not along street frontages. Screening measures, such as tiered landscaping and walls, should be used to minimize visual impacts.
- (G) Convenient pedestrian connections should be provided between residential buildings, community amenities, parking areas, and any adjacent commercial and retail developments.
- (G) Access to transit stops should be integrated into the pedestrian circulation network.
- (G) Bicycle storage such as racks and/or lockers should be provided and be easily accessible from the street and the pedestrian routes.

## D. Open Space, and Site Amenities

Mixed-Use open space areas shall be subject to the following:

- (R) Building numbers and individual unit numbers shall be readily visible, in a consistent location, well-lighted at night, and compatible with the overall design of the development.
- (R) Site furnishings shall not create pedestrian/bicycle/vehicular conflicts.
- (R) Signage shall be consistent with the overall design of the development including color, materials and form.
- (R) Clear legible entry signs shall be provided to identify the development. Internal circulation signs and visitor parking areas should also be clearly indicated. A directory that shows the location of buildings and individual dwelling units within the development is encouraged.
- (G) Use of landscaping and decorative pavement treatments is encouraged to define and accent specific areas such as building and parking lot entrances and the main walkways to common open space areas and other useable open space.



*Building numbers should be readily visible*



*Pedestrian connection should be provided between residential buildings and community amenities*

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- (G) Residents should have access to usable open space for recreation and social activities. Open spaces should be conveniently located for most units.
- (G) Landscaping should be used to define the edge between public and private space, buffer adjacent uses when appropriate, and screen service areas.



*Private and common open space should be clearly defined*



*Residents should have access to usable open space*

- (G) Open space areas should be sheltered from the noise and traffic of adjacent streets or other incompatible uses.
- (G) Open space should be configured and sized to be usable areas and not merely leftover areas.
- (G) Open space intended for private use should be provided adjacent to the units it serves.
- (G) Boundaries between private and common open spaces should be clearly defined by low walls or plant materials.
- (G) Site furnishings (i.e. benches, bicycle racks, bollards, trash receptacles, etc.) should be compatible with the design of adjacent buildings.



*Courtyard doors should be designed as an important feature of the building*



*Seating areas and site furnishings should provide sheltered areas for pedestrians*

## E. Support Facilities

Mixed-Use support facility areas shall be subject to the following:

- (R) The following shall be screened from views from publicly accessible spaces:
  - Electric and water utility meters and cabinets
  - Heating/ventilation/cooling equipment
  - Irrigation and pool pumps and cabinets
  - Rooftop mechanical equipment
  - Refuse areas
  - Other mechanical equipment and cabinets



*Central mailbox design should be consistent with primary building*

- (R) Refuse containers must be covered.
- (R) Refuse storage areas that are visible from upper stories of adjacent structures shall provide an opaque or semi-opaque horizontal cover or screen to reduce unsightly views. The horizontal screen/cover should be integrated into the enclosure design and compatible with adjacent development.
- (R) Mailboxes shall be in highly visible, conveniently accessible areas, per United States Postal Service guidelines.
- (G) Refuse storage areas should be convenient to residential units and commercial uses, located within parking garages or to the rear of buildings and screened from publicly accessible areas.
- (G) Refuse enclosures should be located so that the doors do not interfere with landscaping and pedestrian and vehicle circulation.
- (G) Dimensions of refuse enclosures should not exceed the reasonable space required for anticipated uses.
- (G) Incorporation of mailbox design features, such as a built frame consistent with the primary building's architectural design, is encouraged.
- (G) Appropriate methods of screening include fencing, landscaping, roof parapets and



*Enclosures for refuse storage should have equal architectural design and include an integrated cover*

architectural enclosures. The design of screening devices should be compatible with the primary building(s).

- (G) Architectural enclosures should be used to enclose outdoor refuse containers.
- (G) The design of enclosures should be architecturally compatible with other buildings on the site, and design should use similar forms, materials and colors.

### 5.4.3 Landscaping

All development within the Red Hill Avenue Specific Plan area are subject to the following regulations and guidelines, as well as any applicable landscape requirements of the TCC.

- (R) The following areas of a commercial or mixed-use development, or any other landscaped area to be maintained, shall be landscaped and maintained in a weed and trash-free condition:
  - Required yards;
  - Green roofs;
  - Common open space areas such as, but not limited to pool, clubhouse and outdoor seating areas; and
  - Parking lots.
- (R) Plant materials shall be planted and any walls or fencing designed and maintained to keep sight lines at driveways and intersections clear. Plantings in the sight clearance area shall be pruned to a height of 24 inches, consistent with the TCC.
- (R) Place landscaping so that it does not interfere with the lighting of the project area or restrict access to utilities (such as electrical boxes) or emergency apparatus (such as fire hydrants or fire alarm boxes) or interfere with installation and maintenance utilities.



*Parking lot landscaping for shading is required*

- (R) Root barriers shall be provided where trees are planted adjacent to pedestrian walkways.
- (R) Parking lots shall include shade trees planted at a ratio of one 24-inch box sized tree for every six (6) parking spaces; the total number of trees may be averaged across a parking lot to account for unique site conditions, however most of the parking field must include trees.

- (R) Drought tolerant plants and irrigation systems shall comply with the Tustin Water Efficiency Ordinance.
- (R) Parking lots shall be separated from the street frontage by landscape buffers and berms to reduce visual impacts. Landscaping should be tiered with multiple horizontal and vertical layers of plant material for screening and aesthetics.
- (G) Landscaping should enhance the quality of development by framing and softening the appearance of buildings, enhancing the overall image, screening undesirable views, and providing shade and wind protection.
- (G) Landscaping in parking areas are encouraged to be planted to avoid direct views of parked vehicles from public rights-of-way, and to minimize noise, light, exhaust fumes, and other negative effects on pedestrians.
- (G) Keep landscaping in scale with adjacent buildings and of appropriate size at maturity. Tiered landscaping is encouraged.
- (G) Use trees and plants appropriate to Tustin and the surrounding region. See Plant Palettes in Chapter 3, *Development Plan* as a guide to coordinate with the public right-of-way plantings.
- (G) Group plantings according to common environmental conditions, such as soil type, water needs, sun, temperature, and precipitation, etc.
- (G) Plantings should be of a more significant size for faster canopy results where appropriate.
- (G) Placing planters and pots in building recesses and adjacent to blank walls is encouraged as they provide visual interest and color accents, and enrich sidewalks, courtyards, and plazas. Planter and pot materials should complement the building's architectural design.
- (G) Areas not utilized by structure, storage, paved walks, plazas, driveways or parking should be landscaped.
- (G) Landscaping should be used to separate parking from buildings, pedestrian walkways, and bikeways and to reduce the visual impact of paved surfaces.
- (G) Landscaping at the base of buildings is encouraged to soften any transition. Consideration should be given to the scale and bulk of a building and its relationship to the scale of adjacent development.



*Layered landscape screening of surface parking lots is required*

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- (G) Use of landscaping is encouraged to define and accent specific areas such as building and parking lot entrances, pedestrian walkways, bikeways and the main walkways to common open space.
- (G) Landscaping should be used to define the edge between public and private space, buffer adjacent uses when appropriate, and screen service areas.



*Rooftop parks provide outdoor gathering spaces in urban areas*



*Green roofs can improve storm-water management and reduce runoff*



*Landscaping should frame and soften buildings*



*Group plantings to support design schemes*



*Utility equipment and refuse collection areas should be screened from public view*

## 5.4.4 Lighting

- (R) Lighting shall not produce glare or spill over onto adjacent properties; consider the latest technical and operational energy conservation concepts in lighting designs.
- (R) Lighting shall be provided, at a minimum, in the following locations for safety and for crime prevention:
  - Parking Lots;
  - Parking Structures;
  - Plazas or outdoor seating areas;
  - All building entries;
  - Refuse disposal areas (may be provided as part of parking lot lighting); and
  - Walkways.
- (R) Wall mounted lights shall not extend above the height of the wall or parapet to which they are mounted.
- (G) Lighting should be designed to accommodate the scale of the user. Thus, there should be taller lighting fixtures for vehicular traffic (parking lots) and shorter lighting fixtures for pedestrian movement.
- (G) Use low-level, decorative lighting to provide appropriate nighttime visibility for safety and pedestrian movement as well as accent detail.
- (G) Lighting levels should vary depending on the specific use and conditions, but the overall consideration should be to provide lighting levels sufficient to provide security and safety.
- (G) Light fixtures should be designed or selected to be architecturally compatible with the main structure and the theme of the area.
- (G) Where appropriate, design exterior lighting as part of the overall architectural style of the building to highlight interesting exterior architectural features. Lighting of full facades or roofs is discouraged.
- (G) Low voltage lighting should be incorporated into landscaped areas.
- (G) Lighting should provide consistent light levels to prevent dark areas.
- (G) Lighting fixtures should be covered or otherwise directed so that bulbs are not directly visible to pedestrians or drivers.
- (G) On the top floor of a parking structure with no roof, lighting should be positioned appropriately to create a safe environment for pedestrians, while not negatively impacting the users of surrounding uses and developments.



*Lighting should be designed to be compatible and integral with the architecture*



*Lighting should be designed to accommodate the scale of the user*



*Parking lots should have sufficient lighting to provide security and safety*

## 5.4.5 Walls and Fences

- (R) The design of fences and walls, as well as the materials used, shall be consistent with the overall development's design and must be compatible with the architecture.
- (R) Wall and fence height shall comply with the provisions of the TCC. (G) Walls and fences should be kept as low as possible while performing their intended function. Walls should be kept to a minimum height to avoid a "fortress-like" appearance.
- (G) Fences and walls should have an articulated design. Articulation can be created by having regularly spaced posts, changes in height, and by using different building materials at the base, posts, or the cap of the fence/wall.
- (G) Fences and walls should be painted to match or complement the color of the building. However, walls constructed with stone or brick may remain unpainted to display the natural color of the materials.
- (G) Masonry block walls seen from the public right-of-way should be constructed of "split-face," beige or similar brown tone colored blocks. Chain link fences are not permitted.
- (G) Temporary construction fencing should be aesthetically treated with a green screen or project imagery.



*Walls and fences should be kept as low as possible*

## 5.5 Signs

Signs are important because they communicate the type of goods and services being offered at a commercial establishment and/or identify a property and convey important information, especially directional. All are important to the image of the neighborhood in general. Attractive, creative, and clearly-understandable signs oriented towards pedestrians and cyclists will help create a more pleasing visual environment in the RHASP area.

### 5.5.1 Sign Materials and Colors

- (G) Sign materials and colors should complement the materials and colors on the façade and the architectural style of the building. Appropriate sign materials may include:
  - Wood (carved, sandblasted, etched, properly sealed and painted or stained),
  - Tile (painted, sealed, inlaid tiles),
  - Metal (formed, etched, cast, engraved, and properly primed or factory coated to protect against erosion), and
- (G) For sign hardware, use of decorative iron brackets or wood should be used.
- (G) The selected materials should contribute to the legibility of the sign; for example, glossy finishes are often difficult to read due to glare and reflections.
- (G) Sign materials should be very durable and graffiti resistant; paper and cloth signs are not suitable for outside use.
- (G) Limited application of neon signs may be appropriate if they are consistent with the character of the area, location within a commercial center, and meet the requirements of TCC.
- (G) Limit use of accent colors to increase legibility, and limit colors to no more than three on a single sign.
- (G) Contrast is an important influence on the legibility of signs; the most aesthetic and effective graphics are produced with light-colored letters and images on a dark, contrasting background.



*Sign materials and color should complement the architectural style of the building*



*Limit colors to no more than three on a single sign*

## 5.5.2 Sign Types

Roof signs, pole signs, and internally illuminated box signs are prohibited.

The following sign types are encouraged for new projects in the RHASP area. A coordinated signage program shall be required for new construction where multiple buildings, businesses, or mixed-use projects are proposed.

### A. Projecting Signs

- (R) Hang projecting signs at a 90-degree angle from the face of the building, and use sign supports and brackets that are compatible with the design and scale of the building. Decorative iron and wood brackets are encouraged.
- (G) Use projecting signs for ground floor uses only; on a multi-storied building, suspend the sign between the bottom of the second story windowsill and the top of the doors or windows of the first story. On a one-story building, position the top of the sign at the same height as the lowest point of the roof and ensure that the sign does not hang low enough to be a safety concern for pedestrians.
- (G) The scale of projecting signs should not detract from the architectural character of the building.



*Use small, pedestrian-oriented signs*

### B. Hanging Signs

- (G) Where overhangs or covered walkways exist, pedestrian-oriented hanging signs are encouraged. Securely hang signs over the pedestrian right-of-way.
- (G) Keep hanging signs simple in design and avoid using them to compete with existing signage at the site, such as wall signs.



*Sign supports and brackets should complement the design of the building*

### C. Window Signs

- (R) Window signs shall be primarily individual letters placed on the interior surface of the window and intended to be viewed from outside. Glass-mounted graphic logos may also be applied, if they comply with the 25 percent (25%) limitation in accordance with TCC.



*Limit the text to the business name and a brief message identifying products or service*

- (G) Limit the text or sign copy of a window sign to the business name and a brief message identifying the product or service (e.g., “maternity wear” or “attorney”) or pertinent information (e.g., “reservations required”).

## D. Wall Signs

- (R) Wall signs shall not project from the surface upon which they are attached more than that is required for construction (i.e. mounting, framing) purposes and, in no case, more than six (6) inches.
- (G) Locate wall signs where the architectural features or details of the building suggest a location, size, or shape for the sign. The best location is generally a band or blank area between the storefront and the parapet.
- (G) Place new wall signs for individual businesses in a shopping center or multi-tenant building consistent with the location of signs for other businesses, which will establish visual continuity among storefronts and create a unified appearance. The required Master Sign Plan should ensure this requirement is met.



*Best location for wall sign is the area between the storefront and parapet*

## E. Awning Signs

- (R) Awning signs shall be placed only on the valance portion of the awning.
- (R) The use of adhesive/press-on lettering is prohibited; lettering shall be an integral part of the awning.
- (G) Letter color should be compatible with the awning and the building color scheme.
- (G) Carefully design the shape, design, and color of awnings to coordinate with, and not dominate, the architectural style of the building. Where multiple awnings are used on the building, keep the design and color of all sign awnings consistent.
- (G) Use only permanent signs that are an integral part of the canopy or awning. To avoid having to replace awnings or paint out previous tenant signs when a new tenant moves in, consider using replaceable valances.



*Shape, design and color of awnings should coordinate with the architectural style of the building*

## F. Monument/Freestanding Signs

- (G) The sign area and height should be in proportion to the site and surrounding buildings; signs should not be overly large or be a dominant feature of the site.
- (G) Place monument and freestanding signs perpendicular to the street so that the sight lines at entry driveways and circulation aisles are not blocked.
- (G) Design monument and freestanding signs to create visual interest and complement their surroundings, incorporating architectural elements, details, and articulation.
- (G) Incorporate materials and colors into the sign support structures that match or are compatible with materials and colors of the development the sign serves so it does not appear out of scale with adjacent building(s).



*Freestanding monument signs can be placed in landscaped medians at private project entries to a commercial center*

## 5.6 Relationship to the Public Realm

The public realm is defined as those areas within the public right-of-way: the street itself, parkway, and sidewalk. Where private development adjoins, and interacts with this public area, the following criteria should guide design.

### 5.6.1 Urban Design

#### A. Public Gathering Spaces

- (R) Development shall consider the location, scale, and massing of buildings to consistently frame and define public spaces and private open spaces adjoining the public right-of-way.
- (R) Development shall make public or publicly accessible spaces visible from the street or link them to the street via a clear pedestrian connection, such as an open passage, paseo, or covered arcade.
- (G) Decorative paving that adds visual interest and a sense of place is encouraged.
- (G) Incorporate courtyards, plazas, outdoor eating areas, mid-block pedestrian paseos, and other amenities into development designs to interplay with the public realm.
- (G) Private open space types on street frontages may take several forms as outlined in the Development Regulations section, Chapter 4, including variations on a forecourt design as shown on Exhibit 5-5, *Forecourt Frontage Options (Open Space)*. Forecourts are various

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different building frontage orientations that embrace and incorporate outdoor spaces. Where these open spaces front on the public right-of-way, landscape palettes and treatments and furnishings should be compatible with and complement the treatment of the streetscape.



*Decorative paving should be utilized to create a sense of place*



*Private open space may be integral to the architecture and public realm using balconies, roof decks, or colonnades*

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**FORECOURT**



**FORECOURT WITH PASS-THROUGH OPEN SPACE**



**FORECOURT WITH SIDE OPEN SPACE**



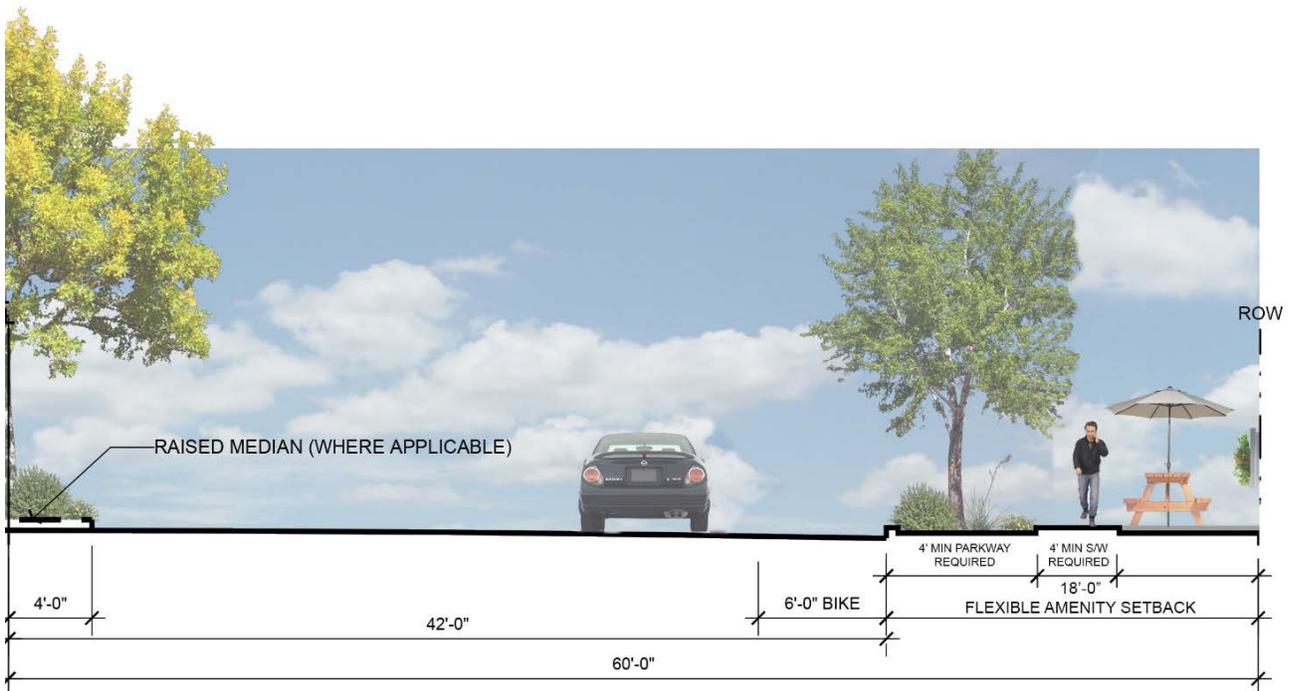
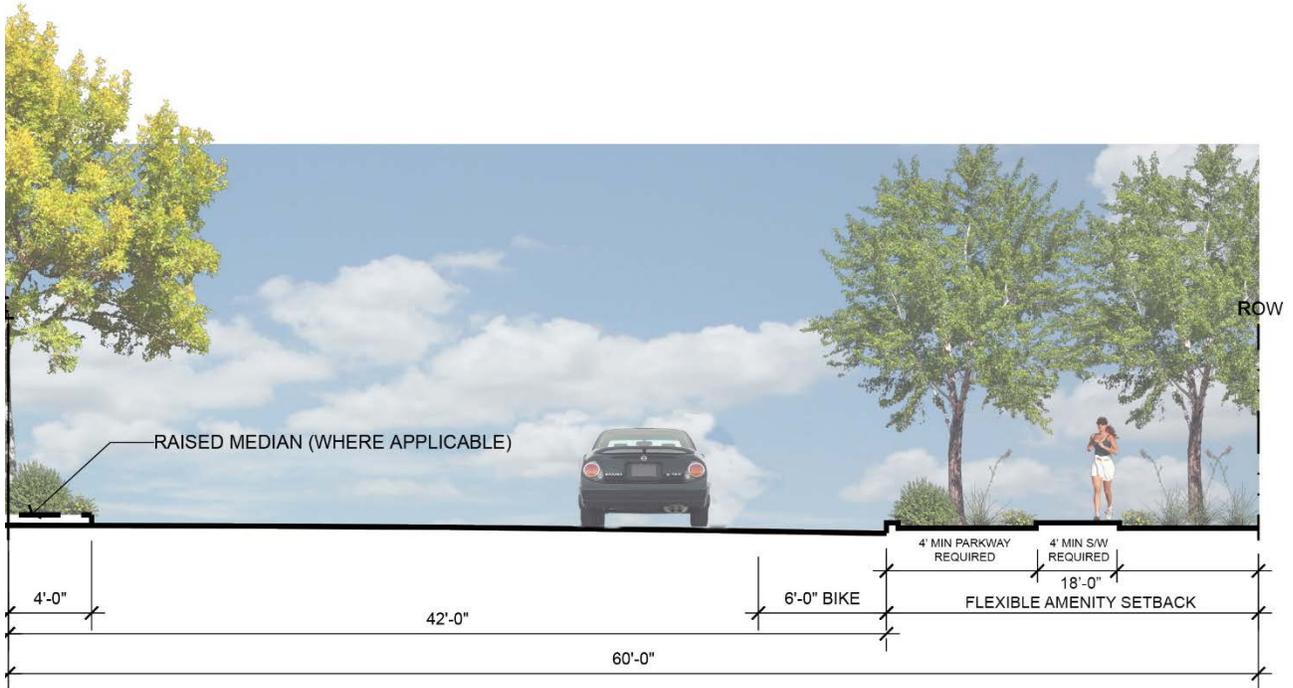
**Exhibit 5-5 Forecourt Frontage Options (Open Space)**

## B. Pedestrian Connectivity and the Flexible Amenity Setback

The Flexible Amenity Setback is a component of the public right-of-way that adjoins private development in areas that have or will be required to dedicate the full 60-foot half-width right-of-way for Red Hill Avenue. Because the driving surface (curb-to-curb width) of the roadway will not change as part of this Specific Plan, additional right-of-way may be used for a variety of pedestrian-oriented purposes in the interim of use of full right-of-way: additional walkway width, additional landscaping, seating areas or plaza spaces, or outdoor dining (with a license from the City) (see Exhibit 5-6, *Flexible Amenity Setback Options*).

- (R) Provide ample width and design for ADA access along sidewalks and pathways.
- (R) Design sidewalks and pathways as unifying elements throughout the Specific Plan area and from and between adjacent parcels.
- (R) Outdoor dining areas within the Flexible Amenity Setback must be kept at the same grade as the adjacent pedestrian walkway.
- (G) Locate pathways to promote continuity, connecting with the public sidewalks on adjacent streets.
- (G) Encourage new development that extends existing circulation patterns and connects important centers of activity.
- (G) Introduce shading elements to improve pedestrian access and comfort such as landscaping, umbrellas, awnings, etc.

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**Gathering Space  
 (Plaza, outdoor seating)**

**Walkway  
 (4' min)**

**Parkway  
 (4' min)**

*The Flexible Amenity Setback may include a number of pedestrian-oriented uses within the 18-foot setback area: seating, plaza space, outdoor dining, additional landscaping, and/or widened walkways.*

*In all cases a minimum four-foot wide sidewalk and four-foot wide planted parkway are required. Where possible the parkway should be expanded to six feet.*



*Examples of pedestrian-oriented uses in the Flexible Amenity Setback*

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*Flexible  
Amenity  
Setback  
Options*



**Exhibit 5-6 Flexible Amenity Setback Options**

## C. Gateways

- (R) Corner lots shall provide, as part of development projects, where gateway monumentation is identified, in the Development Plan herein, area for gateway monumentation, to be constructed and maintained by the property owner. A corner triangular-shaped setback range of 15-25 feet shall be provided for gateway monumentation subject to review and approval by the Community Development Director. All gateway monumentation shall comply with TCC visual clearance requirements.
- (R) The property owner(s) or their representatives shall be responsible for the installation, future preservation, maintenance, and replacement if necessary, of gateway monumentation on private property for the life of the development project.
- (G) Gateways should be designed to announce arrival into the RHASP area, consistent with the requirements outlined in Chapter 3, *Development Plan*.
- (G) Gateway signs and treatments should not obstruct motorists view and shall adhere to the TCC regarding appropriate sight and visual clearance distances.
- (G) Gateway treatments, such as architectural features, public art, special paving treatment, lighting, and mature, full-sized landscaping, is encouraged and should be coordinated to create a unifying theme.

## D. Park Interface

If mixed-use redevelopment occurs at the commercial center at 14131-14245 Red Hill Avenue adjacent to Frontier Park, it is highly encouraged that the uses and site design engage the park through clearly defined frontage directly onto the park, or siting of buildings and outdoor spaces to provide visibility and pedestrian access between the park and the proposed development. Development on this site and its adjacency to Frontier Park is a unique opportunity within the Red Hill Avenue Specific Plan. Comprehensive site planning and development of this property will be key to furthering the vision of the Specific Plan area, specifically to the area south of the I-5.

### 5.6.2 Furnishings

Furnishings can enliven and provide variety to outdoor spaces used by the public or adjacent to the public sidewalk areas. They serve an aesthetic and utilitarian function. Furnishings include items such as benches, trash receptacles, plant containers, tree grates and guards, bicycle racks, bollards, kiosks and fountains. Proper design and placement of such amenities can reinforce the identity of the district and create a lively and active atmosphere within the Specific Plan area.



*Furnishings should be provided in higher-use pedestrian traffic areas*



*Street furniture should have a unified theme*



*Group furniture together, wherever possible*

- (R) Provisions to accommodate Americans with Disabilities Act (ADA) requirements must be incorporated into the design and siting of furnishings. This includes a provision for space adjacent to walkways for wheelchair and/or stroller parking.
- (G) The interface between the public realm and private development (including seating, outdoor dining, canopies, planters, etc.) should not be defined by hard edges such as bollards or fencing.
- (G) A unified “look” is encouraged. The color and appearance of furniture products should be selected to complement other design elements. Items should be securely anchored to the sidewalk and a graffiti-resistant coating should be applied to surfaces.
- (G) Locate street amenities except seating in a zone along or near the curb as a barrier to automobile traffic; this is especially applicable to trash receptacles, and heavy planters.

- (G) The design and selection of street furniture should include considerations for the security, safety, comfort and convenience of the user.
- (G) Street furniture should be placed to conserve existing sidewalk width and ensure free pedestrian flow.
- (G) The location and layout of the different elements of street furniture should ensure that each article or structure is designed and situated to be in harmony with both the surrounding furnishings and the overall Specific Plan area.
- (G) Where possible, furnishings should be grouped together. A greater number and type of furnishings should be provided in higher-use pedestrian traffic areas.

### 5.6.3 Public Art

Public art contributes significantly to the public identity of a place. Public art can stimulate creativity, imagination and add a unique human quality to the outdoor environment. It enriches the urban landscape and provides a means to express the City’s cultural and social heritage. Public art is an increasingly important element of the built environment.

- (R) The property owner(s) or their representatives shall be responsible for the installation, future preservation, maintenance, and replacement if necessary, of artwork on private property for the life of the development project.

- (G) Integrate works of art into new development projects in areas visible to the public and at gateway intersections as defined in Exhibit 3-6, *Urban Design Plan*. Public art projects are subject to review by the Community Development Director if visible from the public right-of-way.
- (G) The placement of freestanding pieces of art should avoid locations where it would compete with a storefront, obstruct a pedestrian path, create a traffic hazard, compete with another piece of art, or adversely impact adjacent buildings. The placement of artwork should avoid locations where it would conflict with monument signage.



*Public art can break up large facades with little articulation while expressing the character of the community.*

- (G) Public art should add local meaning; interpret the local culture, environment, and/or history; and capture or reinforce the unique character of place.
- (G) The physical setting of public art should be considered in its design. The impact of physical space and nearby structures on public art should also be considered.
- (G) Art should be sited to complement other features, such as a plaza or architectural components, and make the art an integral part of site development.
- (G) Public art should be constructed using durable materials and finishes such as stone or metal or use of materials that are functional furnishings (i.e. bench/wall) and treated to resist graffiti.

- (G) Murals, bas-relief or other artistic treatments are strongly encouraged to be used to enhance otherwise blank walls (including parking structures) and are subject to the requirements of the TCC.
- (G) Public art location(s) should be clearly visible and freely accessible during daylight hours.
- (G) Exterior artwork should be adequately lit to be clearly visible from sidewalks during evening hours.



*Murals may be used to enhance blank walls*

- (G) Interior artwork should be adequately lit during all hours of public access.

# 6 IMPLEMENTATION AND ADMINISTRATION

## Implementation and Administration

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## 6. Implementation and Administration

### 6.1 Purpose

As discussed in the vision section of the Red Hill Avenue Specific Plan (RHASP) (see Chapter 2, *Vision, Goals, and Objectives*), the Specific Plan area is envisioned to retain a primarily commercial character along Red Hill Avenue, with vertical and horizontal mixed-use projects that activate the street by bringing new commercial and residential uses and pedestrian activity. The evolution of land uses will enhance the economic and social vitality of the area through the addition of new residents and local-serving retail and service uses. Through the design review process, the City will ensure that new development complements existing residential neighborhoods surrounding the plan area. The provisions of this chapter are applicable to development activity and land uses within the boundary of the RHASP and serves to implement the vision for the Specific Plan area.

This chapter explains the procedures to implement this Specific Plan. The RHASP applies to all existing and future development and land uses in the area. Existing uses shall be permitted to continue and need not comply with the new standards in the RHASP, subject to the provisions of TCC Section 9273, *Legal Nonconforming Structures and Uses*.

When land uses intensify to a different land use classification (examples include: from retail to assembly, daycare, medical, restaurant, etc. or any other parking/traffic intensification) or are extended to occupy a greater land area or new structure, or new development is proposed, conformance with the development standards as specified in Table 4-3 related to the use will be required.

### 6.2 Phasing

Future new development and redevelopment within the Specific Plan area will occur over a multi-year timeline because public and private development and/or redevelopment will be responsive to prevailing market conditions. Owners of private property will determine when, and if, development occurs. This makes forecasts of the timing and extent of future conditions challenging; however, buildout is assumed by 2035. In the near term, improvements such as expansions or façade improvements will be subject to the development standards and design criteria outlined herein. The City may implement the public improvements outlined in the Specific Plan, including, public streetscapes, landscaped medians, and gateway/wayfinding signage, in advance of, or concurrent with, private development. High priority improvements would include restriping and the addition of the Class II bike lanes, followed by public streetscape improvements and the addition of landscaped medians.

## 6.3 General Plan Amendment

The Tustin General Plan was amended by Resolution concurrent with the adoption of the RHASP to provide consistency between the two documents. The following amendments to the General Plan were made:

- Update to the Land Use Map to show the boundaries of the RHASP; and
- Updates to the General Plan Land Use Element (GPA 2017-01), and other related conforming amendments to General Plan to ensure that the RHASP and the General Plan, as amended, are internally consistent.

## 6.4 Zoning Map Amendments

The RHASP was adopted by Ordinance and defines the zoning for the properties within its boundaries. The adoption of the Specific Plan was accompanied by a concurrent zoning map amendment to designate the area “Red Hill Avenue Specific Plan (SP-13).”

## 6.5 California Environmental Quality Act Compliance

The RHASP has been prepared in conjunction with a Program-level Environmental Impact Report (EIR), which identifies potential impacts resulting from the proposed development and establishes mitigation measures that reduce them to a less than significant level, where feasible.

As the lead agency, the City of Tustin will implement a monitoring program for the approved mitigation measures. To assist in this monitoring effort, a Mitigation Monitoring Program has been developed by the City as part of environmental findings and included in the Final EIR certified by the Tustin City Council. The Mitigation Monitoring Program will also be included as an Appendix of this Specific Plan when adopted.

The RHASP EIR will serve as the primary environmental document for development within the RHASP and all future development undertaken within the Specific Plan area. The EIR is anticipated to be the definitive environmental document for project implementation within the Specific Plan area, including serving as a Program EIR for purposes of backbone infrastructure improvements. Future development projects that require discretionary review will be examined against the analysis prepared for the EIR to determine if additional environmental documentation must be prepared. Developments that do not require additional discretionary review will not be subject to additional environmental documentation. However, a project applicant will be required to submit documentation substantiating the finding that said development is allowed and in conformance with the Specific Plan, and the potential environmental effects are within the parameters and timeframe analyzed within the Specific Plan EIR.

Additional environmental documentation may be required for future development projects proposed within the Specific Plan area pursuant to State law. However, documents for subsequent site-specific projects may use the “tiering” concept, as provided by § 15385 of the State CEQA Guidelines.

The tiering concept is a process by which the City of Tustin, as lead agency, can adopt the programmatic EIR focusing on the “big picture,” and can then use streamlined CEQA review for subsequent individual development projects in the Specific Plan area. This streamlined CEQA review may be used for each site-specific future development so long as the project is consistent with the findings of the EIR, the mitigation measures described in the EIR, and the City’s General Plan and Zoning Ordinance. This tiering concept allows the City to address the broad environmental issues detailed in this EIR during the planning stages of the proposed Specific Plan. Future site-specific development projects are evaluated on a project-specific basis, and may be excused from repeating the broad environmental analysis examined in this comprehensive, programmatic EIR for the entire Specific Plan area.

## 6.6 Administration and Enforcement

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities to utilize specific plans for purposes of implementing the goals and policies of the City’s General Plan. The RHASP establishes a set of regulations, standards, design criteria, and processes for proposed development within the Specific Plan boundaries, and constitutes the zoning for development within the Specific Plan area. Where provisions of the RHASP conflict with the Tustin City Code (TCC), the provisions of the RHASP shall prevail. Where the RHASP is silent, the relevant provisions of the TCC shall apply.

### 6.6.1 Responsibility

The Community Development Department shall be responsible for the administration of the RHASP, including processing of applications, providing assistance to applicants, administration of the Residential Allocation Reservation (RAR), and reviewing projects for compliance with the RHASP. The Public Works Department shall be responsible for overseeing improvement projects involving streets, public rights-of-way, and other public properties or improvements within the RHASP. The Community Development Director acts as the Zoning Administrator as outlined in the TCC. The Community Development Director may refer an action to the Planning Commission.

### 6.6.2 Approval Authority

Approving bodies for actions implementing the RHASP include the following (note that the final approving authority for projects that include multiple actions will be the highest approval authority):

Implementing Action	Approval Authority
Specific Plan Approval, General Plan Amendments or Amendments thereafter	City Council, per TCC
Subdivision Maps	Planning Commission and/or City Council per TCC
Site Plan/Design Review	Community Development Director, per TCC
Residential Allocation Reservation (RAR)	Community Development Director
Conditional Use Permits	Zoning Administrator or Planning Commission, per TCC

### 6.6.3 Unlisted Uses and Interpretation

Uses not specifically listed in the Land Use and Development Standards Chapter 4 of the RHASP, are subject to determination by the Community Development Director as permitted, conditionally permitted, or prohibited, consistent with the intent and vision of the RHASP. Whenever the provisions contained in the Specific Plan conflict with the TCC, the provisions of the Specific Plan shall take precedence. Any ambiguity concerning the content or application of the Specific Plan shall be resolved by the City's Community Development Director, or his/her designee, in accordance with Section 9298 of the TCC, *Interpretation and Enforcement*. Such interpretations shall consider the stated goals, objectives, policies, and intent of the Specific Plan.

### 6.6.4 Appeals

Any decision of the Community Development Director or Zoning Administrator may be appealed to the Planning Commission. Any decision of the Planning Commission may be appealed to the City Council in accordance with TCC Section 9294, *Appeals*.

### 6.6.5 Transfers

Transfer of residential dwelling units or commercial intensity between the areas south of I-5 and north of I-5 may be permitted subject to approval by the Community Development Director. A maximum of 25 percent of the original 500 residential unit count and/or 325,000 square feet of new commercial development allocated north or south of the I-5 freeway can be either converted or transferred from one area to the other with approval by the Community Development Director. Transfers/conversions greater than 25 percent shall require approval by the Planning Commission.

Transfers of dwelling units or intensity may be approved subject to the following criteria:

- A. A completed application has been submitted for a specific development project.
- B. The requested transfer implements the intent and vision of the RHASP.
- C. The proposed development on the receiving site complies with all development regulations and design criteria set forth in the RHASP.
- D. The requested transfer does not negatively impact the area where the transfer is taken from ("donor" area) or the area receiving the transfer ("receiving" area).
- E. The donor area retains enough unit availability to allow for viable development.
- F. Environmental review has been conducted to determine whether the project changes are consistent with the assumptions of the RHASP's Program EIR and no new significant impacts or a substantial increase in known significant impacts will occur; or, if new substantially greater impacts occur, that subsequent environmental documentation and/or mitigation has been provided.

- G. The applicant receiving the transfer must be the one developing the project, unless otherwise authorized by the City.

### 6.6.6 Land Use Conversion

To maintain the flexibility to respond to changing community needs and market conditions over the build-out duration of the RHASP, certain contemplated/proposed land uses may be converted from one use to another use in accordance with the terms of this section.

The RHASP provides for a total of 500 additional residential units in the RHASP in a horizontal or vertical mixed-use setting, as well as an additional 325,000 square feet of non-residential uses. The precise allocation of density/intensity and type of development shall be determined as the Specific Plan is built out through the City's approval of private development applications. The precise number of units allocated to a development project is determined by the City as part of a Residential Allocation Reservation (RAR) process (see Section 6.7.2, *Residential Allocation Reservation (RAR) Process and Findings*, below).

After the 500 units allowed by the RHASP have been allocated, commercial intensity may be converted into residential units based on an evaluation of the vehicular trip assumptions in the Program EIR. A traffic study evaluating this conversion must be submitted with any request for a RAR in excess of the initial 500 units. The conversion would be subject to approval by the Community Development Director as part of a request for residential allocation. If new or different impacts occur as a result of the traffic analysis, new analysis, environmental review or amendments to the Specific Plan may be required.

## 6.7 Development Process

### 6.7.1 Site Plan, Design Review Process and Findings

Individual development projects will require Design Review as required by the TCC and specific procedures outlined in the project Land Use and Development Standards herein (see Exhibit 6-1, *Residential Allocation Request Process*, below). The Community Development Department has the following responsibilities for Design Review of development projects in accordance with Section 9272 of the TCC Design Review:

- To provide for the review of building design, site planning, and site development in order to protect the increasing value, standards, and importance of land and development in the City due to the urbanization of Orange County.
- To retain and strengthen the unity and order of the visual community.
- To ensure that new uses and structures enhance their sites and are harmonious with the highest standards of improvements in the surrounding area and total community.

## A. Findings for All New Development Projects

The Community Development Director may approve the submitted plans if he/she finds that the location, size, architectural features, and general appearance of the proposed development will not impair the orderly and harmonious development of the area, the present or future development therein, the occupancy thereof, or the community as a whole. In making these findings, the Community Development Director shall consider the following for all new development projects within the RHASP:

1. The development is consistent with the overall vision, goals, and intent of the RHASP.
2. The development is consistent with the policies and intent of the Development Plan (Chapter 3), including the Urban Design plan (Section 3.4).
3. The project, including the proposed uses and requested entitlements, comply with the Permitted Land Use and Activities Table (Table 4.1) and complements and contributes to the project area.
4. The development substantially complies with or obtained approval of a modification to conformance with the Commercial and/or Mixed-Use Development Standards (Chapter 4).
5. The development substantially complies with or has obtained approval of a modification to conformance with the Commercial and/or Mixed-Use Design Criteria (Chapter 5).
6. Compliance or modification thereto with applicable Development Standards and Design Criteria is demonstrated and ensured through Design Review of the project site plan, building elevations, floor plan, parking plan, landscaping plan, lighting plan, access plans, refuse plans, and any other applicable plan(s) or document(s).
7. Required parking for the project is provided entirely on-site, or by an alternative provided in Section 4.4.4, *Off Street Parking and Loading Standards*, which is approved concurrently or made a condition of approval. Approval of a parking alternative will have a beneficial impact and does not adversely affect development or the surrounding area.
8. The development demonstrates high-quality architectural design and site planning and reflects the nature of the site and the surrounding area through the following:
  - a. Incorporating roof forms and facades that provide building articulation, create visual interest, and reduce the appearance of uniform building massing.
  - b. Creating a design that is both cohesive and varying with respect to architectural style, architectural details, windows, doors, colors, and materials.
  - c. Facilitating pedestrian orientation through building placement, building scale, and architectural design.

- d. Ensuring associated elements including parking, service areas, landscaping, lighting and pedestrian access, and amenities are functional and serve to enhance the overall appearance and experience of the project.
  - e. Siting and designing structures that relate to and respect adjacent development and sensitive land uses.
9. The development is supported by existing infrastructure or will be supported by infrastructure improvements paid for by the developer.
  10. The developer consents to enter into a mutually agreeable Development Agreement if the City requests one.
  11. The project impacts are assessed through the approved RHASP Program EIR or through subsequent environmental documentation in compliance with the CEQA guidelines.

## B. Findings for Mixed-Use Development

For mixed-use development which includes a residential component, the Community Development Director may approve the submitted plans if he/she finds that the Residential Allocation Reservation (RAR) has been obtained per Section 6.7.2 below, and the location, size, architectural features, and general appearance of the proposed development will not impair the orderly and harmonious development of the area, the present or future development therein, the occupancy thereof, or the community as a whole. In making these findings the Community Development Director shall consider the following in addition to the findings for all development (see item A, above):

1. The project complies with the Residential Allocation Reservation (RAR) application process as provided in Section 6.7.2 (Reservation Allocation Reservation Process and Findings) and units are available within the applicable area.
2. The project, including its proposed uses and requested entitlements, comply with the Permitted Land Use and Activities Table (Table 4.1) and complements and contributes to the project area.
3. The development substantially complies with the Mixed-Use Development Standards (Chapter 4).
4. The development substantially complies with the Mixed-Use Design Criteria (Chapter 5).
5. The development provides a sufficient mix of uses, allocated as follows:
  - a. Commercial space is sufficient and placed along the principal street frontage of Red Hill Avenue.
  - b. Ground floor design of buildings facing Red Hill Avenue precludes office space, unless the office use meets one of the following criteria:

- I. Located behind retail or commercial development;
  - II. Designed with prominent display windows similar to retail uses; or
  - III. Located on upper floors.
- c. Residential units are exclusively located on upper floors in the case of vertical mixed-use or on non-principal street frontages, such as San Juan Avenue, Mitchell Avenue or Walnut Avenue, or are internal to a development site.
6. The proposed development is consistent with the City's affordable housing requirements.
  7. The proposed development provides at least one of the following public benefits as part of the development, subject to the review and approval by the project approval body:
    - a. Provision of affordable housing significantly greater than the mandatory requirements contained in the City's affordable housing ordinance.
    - b. A public amenity that is easily accessible is included such as a public plaza that provides, at a minimum, seating, landscaping, and lighting.
    - c. Provision of additional combined common/private open space in excess of the standards outlined in Table 4-4 of this Specific Plan.
    - d. Prominent public art, which is located and featured within easy public view.
    - e. Installation of specific public infrastructure above any required by the project.
    - f. Off-site parking improvements above any required by the project.
    - g. Recreational amenities above any required by the project.
    - h. Other, as proposed and deemed important to and acceptable by the City.
  8. The development facilitates multi-modal transportation through building siting and design that provides convenient access for transit users, pedestrians, and bicyclists. Where a development is adjacent to an existing or planned transit stop, building siting and orientation provide convenient access to transit users.
  9. The developer submits building plans to the Building Division within six months after project approval to fully construct the project in a timely manner within the period specified in the RAR conditions of approval.
  10. The applicant has sufficiently demonstrated its level of development experience to the City.

## C. Findings for Mixed-Use Applications with Residential Allocation Reservation Transfers or Conversions

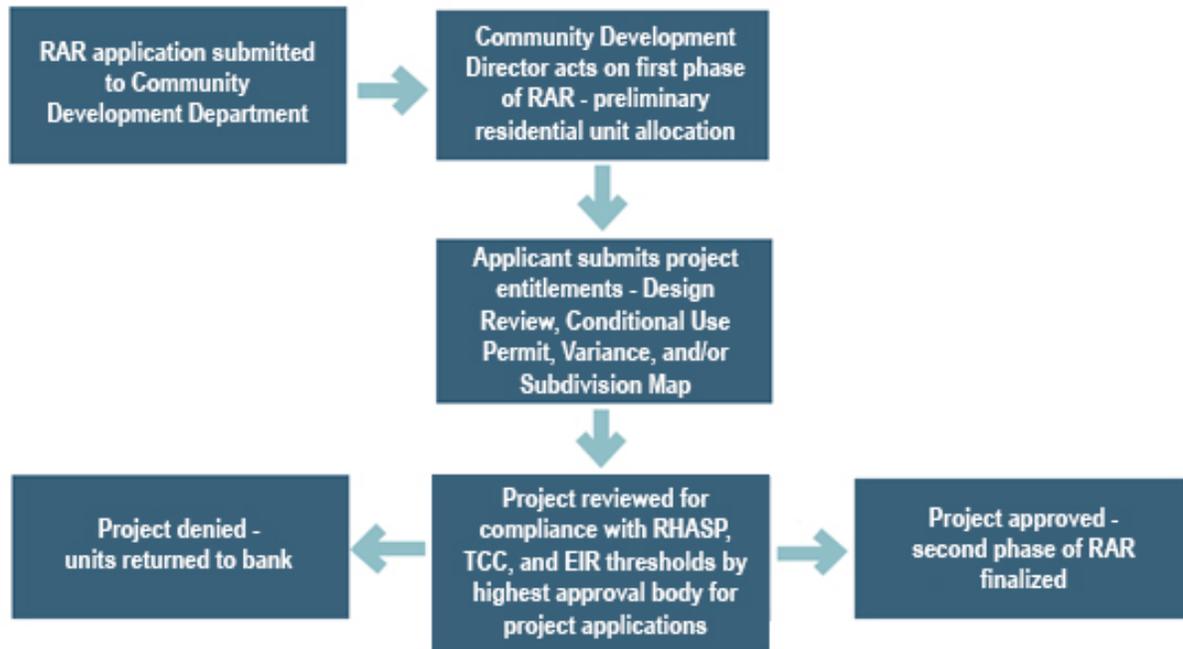
In addition to the findings in A and B above, the following findings shall be required for mixed-use applications for Residential Allocation Reservations (RARs) that include a request for a transfer or conversion of residential units from another area within the RHASP area in compliance with Section 6.6.5 and Section 6.6.6, *Transfers* and *Land Use Conversions*, respectively.

1. The Community Development Director has identified units available for transfer or commercial square footage available for conversion from Red Hill Avenue – North to Red Hill Avenue – South or vice versa, which are not part of a foreseeable development project within the transferring development area.
2. A transfer of residential units or conversion of commercial square footage is desirable because it creates a better high quality development project providing additional housing units for a project, including affordable units or units restricted to special occupancy (seniors, disabled persons, or veterans), yet maintains compliance with the Development Standards (Chapter 3), Design Criteria (Chapter 5) and all other applicable provisions of the RHASP.
3. A transfer of residential units or conversion of commercial square footage will benefit the project area by creating a more desirable project and will not negatively impact the donating or receiving development areas, as determined by the approval authority. The approval authority has no knowledge of pending development applications within the development area that desire to utilize the units approved for the transfer or conversion.

## 6.7.2 Residential Allocation Reservation (RAR) Process and Findings

### A. Process

Allocation of residential units within the RHASP is approved by the Community Development Director subject to a preliminary review and the standards outlined in Chapter 4, *Land Use and Development Standards* and the criteria outlined in Chapter 5, *Design Criteria*.



RHASP = Red Hill Avenue Specific Plan  
 TCC = Tustin City Code  
 EIR = Environmental Impact Report

**Exhibit 6-1: Residential Allocation Request Process**

The following describes the RAR process in sequential order:

1. Submittal of a RAR application to the Community Development Department (CDD). A RAR application must include the required items to be considered complete for the CDD to begin the review and make the determination. The RAR submittal shall include the following:
  - a. Scaled and dimensioned site plan;
  - b. Architectural Elevations; a
  - c. Parking Management Plan; and
  - d. Project Description.
2. The CDD shall review and consider whether a proposed project substantially complies with the RHASP Development Plan (Chapter 3), Land Use and Development Standards (Chapter 4),

# RED HILL AVENUE SPECIFIC PLAN



Design Criteria (Chapter 5) and required findings (Chapter 6) and whether residential units are available in the Residential Allocation Bank.

3. If the CDD determines that the project substantially complies and residential units are available, the Director shall make the Required Findings (refer to Section 6.7.2B) and approve the first phase of the RAR process, which shall provide a preliminary allocation of residential units for the project. The first phase approval shall be accompanied by conditions of approval establishing the time limitation for completion of the second phase of the RAR process, which shall include applications and submittals required for the project such as Design Review, Conditional Use Permit, Variance, and/or Subdivision Map(s). Following phase one approval of the RAR, final approval of the RAR (phase two) shall not exceed a period of 180 days. A time extension may be considered at the discretion of the CDD. The RAR preliminary allocation of units in the first phase shall not be considered final until approval of the requested entitlement(s) in the second phase.
4. Applicant shall submit remaining project entitlement materials (Design Review, Conditional Use Permit, Variance, Subdivision Map, etc).
5. The approval authority indicated in RHASP Section 6.6.2 shall make the determination if a RAR application is approved, approved with conditions, or denied. This determination is subject to appeal pursuant to the appeal procedures in the TCC.
6. Following final approval of the RAR and requested entitlement(s), all construction related permits, including demolition, grading, and building permits, must be obtained within 180 days. Following building permit issuance, construction must commence within 180 days. Time extensions may be considered at the discretion of the CDD.
7. The allocation of units shall expire and the units redeposited into the Residential Allocation Bank for use by other projects if any of the following occur:
  - a. The time limits established in the conditions of approval expire without submittal of the required entitlement applications;
  - b. The entitlement(s) associated with the RAR application are denied;
  - c. Construction related permits are not obtained within the required time limits or approved for extension; or
  - d. Permits are not utilized within 180 days.
8. The Community Development Department shall monitor the Residential Allocation Bank and track all reservations and transfers on an annual basis.

Dwelling units may be transferred from one area (i.e. either Red Hill Avenue – North or Red Hill Avenue – South) to another with the approval of a RAR application and entitlement application, as

described in RHASP Sections 6.6.5 and 6.6.6., *Transfers* and *Land Use Conversion*, respectively. RARs and entitlement applications may be processed concurrently subject to Community Development Director approval. Dwelling units may not be approved as part of an entitlement action without completing the RAR process.

## B. Findings

Findings required for approval of the RAR include the following:

1. The project is substantially consistent with the uses, design criteria, and development regulations of the RHASP.
2. The project implements the vision of the Specific Plan related to excellence in architectural design, provision of substantial usable common open space, provision of public art (which may consist of murals, sculpture, decorative fountains or other art deemed acceptable) connectivity to adjacent parks and/or schools if appropriate, and pedestrian connections.
3. The number of units requested is within the thresholds established by the Specific Plan's Program EIR.
4. In allocation of the 500 Residential Allocation Reservations units, the City shall consider an equitable distribution within the Specific Plan Area such that no one parcel receives a disproportionate number of units.
5. The overall project meets the findings for Design Review outlined in Section 6.7.1 above.

### 6.7.3 Flexible Amenity Setback Provisions

The Flexible Amenity Setback applies to sites which include, or are able to include (as part of a dedication for a new project), the full 60-foot half-width right-of-way specifically along Red Hill Avenue. The Flexible Amenity Setback is intended to allow a range of uses, including expanded walkways, additional landscape, public seating/plaza areas or other similar uses approved by the Community Development Director as part of a Design Review application for a development project. Outdoor dining, as part of a commercial food service use, is a permissible use within this amenity setback, subject to a license obtained from the City. Maintenance of the Flexible Amenity Setback shall be the responsibility of the owner/tenant of the property.

### 6.7.4 Subdivisions

All divisions of land within the Specific Plan area shall be processed in accordance with the TCC regarding subdivision and parcelization of land as well as the State of California Subdivision Map Act. Findings for subdivisions within the Specific Plan area shall be made in accordance with the Subdivision Ordinance of the TCC and this Specific Plan.

## 6.7.5 Variances and Conditional Uses

Applications for variances or conditional use permits shall be processed in accordance with the TCC. Findings for such variances or conditional uses shall comply with required findings of the TCC and this Specific Plan.

## 6.8 Development Incentives

The Specific Plan area is presently under-developed. This section describes the availability of incentives and provides specific guidance for the granting of additional development potential, provided that certain conditions are met. The development incentives contained in this section shall encourage the effective utilization and consolidation of parcels to encourage more viable development opportunities, implanting the vision of the Specific Plan area as a vibrant mixed-use district.

The following are development incentives to encourage new mixed-use development. Note that State authorized incentives for affordable housing or deed restricted senior housing are not listed herein but would also apply.

### A. Mixed-Use Development Incentives

1. **Purpose.** To provide incentives for the encouragement of mixed-use development within the Specific Plan area. Mixed-use development shall meet the requirements of applicable sections of the RHASP and the TCC. The City of Tustin encourages the development of mixed-use projects within the Specific Plan area to maximize development and redevelopment potential that is consistent with the purpose and intent of this Specific Plan.
2. **Mixed-Use Requirement.** Mixed-use development incentives shall apply only to projects that combine residential and non-residential development in a vertical or horizontal configuration. Any development, excluding integrated residential use, shall not be considered mixed-use for purposes of this section.
3. **Incentives**
  - a. **Lot Consolidation.** At the discretion of the Community Development Director, residential unit allocations may be granted if the project substantially meets the findings outlined above for both Site Plan/Design Review and Residential Allocation Reservation.

## 6.9 Minor Specific Plan Modifications

Minor modifications to the provisions of the RHASP shall require the approval of the Zoning Administrator, in accordance with Section 9299 of the TCC. The following list of modifications shall be considered minor, subject to a determination by the Zoning Administrator or Public Works Director as appropriate:

- A. Changes in the location of infrastructure and public improvements (such as median locations).

- B. Minor change of landscaping materials and/or locations, wall materials, wall alignment, and streetscape design as set forth in Chapter 5, *Design Criteria*, which are intended to be flexible in nature.
- C. Limited deviations from site development standards outlined in the TCC. Deviations not exceeding 10 percent from the development standards set forth in Chapter 4, *Land Use and Development Standards* are permitted, subject to the review and approval of the Community Development Director.
- D. Other minor modifications like those listed above and deemed minor by the Community Development Director, which are in keeping with the intent of the Specific Plan.

When a request for deviations from development standards and design criteria exceeds the allowances of the TCC, a variance or Specific Plan Amendment may be required.

### 6.9.1 Exceptions

- The Community Development Director shall have the authority to issue Exceptions to the Commercial and/or Mixed-Use Development Standards in compliance with the limitations established under Zoning Administrator duties in TCC Section 9299b, *Zoning Administrator, Duties*, provided Exceptions are consistent with the required findings.
- The Community Development Director shall have the authority to issue Exceptions that provide for flexibility in fulfilling the intent of the Development Plan (Chapter 3) or the Commercial or Mixed-Use Design Criteria (Chapter 5), provided Exceptions are consistent with the required findings.

### 6.9.2 Required Findings

- A. An Exception to the provisions of the Development Plan (Chapter 3) and Commercial or Mixed-Use Design Criteria (Chapter 5) meets at least one of the following required findings. The granting of an Exception will not be detrimental to the health, safety, or general welfare of residents, businesses, workers, property, or improvements in the area or to the City in general.
  - 1. An Exception is due to a physical limitation on the site caused by one or more of the following:
    - i. Lot shape, topography, size or orientation
    - ii. Lot location, including the nature of the adjacent street
    - iii. Location or design of adjacent structures
  - 2. Strict compliance without an Exception would preclude fulfillment of the Development Plan and/or applicable Design Criteria. The granting of the Exception will not constitute a grant of special privilege inconsistent with the limitations upon other similar properties.

3. An Exception provides a design solution that implements the intent of the RHASP, specifically the Development Plan and the applicable Design Criteria.
4. With an approved Exception, the development fulfills the intent of the provision in an alternative, high-quality manner that positively affects the development and does not have negative effects on adjacent properties, as determined by the review authority. Or, alternatively, the review authority deems the provision is not applicable to the proposed development. Said Exception meets Section 6.8. of the RHASP. All other required provisions are met.

## 6.10 Specific Plan Amendments

Substantial modifications to the RHASP will require an amendment. A Specific Plan Amendment may be initiated by the City Council, Planning Commission, Community Development Department, or a property owner. An amendment to the Specific Plan is required if any of the following changes are proposed:

- A. Changes to the overall Specific Plan boundaries to include properties not included in the Specific Plan at the time of approval;
- B. Any increase in the overall development density and intensity thresholds within the Specific Plan;
- C. Any change to the development standards outlined in Chapter 4.

The RHASP may be amended by Ordinance, utilizing the procedure by which it was originally adopted as set forth in the TCC, which would include a public hearing by both the Planning Commission and City Council. In addition, any amendment must meet the intent of the Specific Plan's policy framework, including its Design Criteria and Specific Plan Goals and Objectives.

All sections or portions of the RHASP to be changed or that may be affected by the change, including changes to the project boundary must be included in the application for a Specific Plan Amendment. A concurrent amendment to the General Plan would not be required if the Community Development Director determines that substantive changes would not influence the goals, objectives, policies or programs of the Tustin General Plan or the General Plan Land Use Map.

Allowing flexibility in the administration of the Specific Plan enhances the effectiveness of the Specific Plan as a "living" planning document.

## 6.11 Affordable Housing

Development projects within the RHASP will be required to comply with citywide policies, programs, and ordinances related to affordable housing which are in place at the time of an application for building permits.

## 6.12 Severability

If any regulation, condition, program, policy or any other portion of this Specific Plan or the application thereof to any person or circumstance is held to be invalid or unconstitutional by any court of competent jurisdiction, such portions shall be deemed a separate, distinct, and independent provision and shall not affect the validity of the remaining provisions of this Specific Plan or applications thereof which can be implemented without the invalid provision or application.

## 6.13 Funding Mechanisms

Technological advances are changing the way we live and shop. Greater automation has led to a replacement of jobs and retail has been adapting by focusing on trips and use of internet commerce. Simultaneously, the State is placing an emphasis on a “green” economy and in addressing the existing housing shortage. New economic development strategies are necessary to respond to accelerated changes in climate action controls, technology, and lifestyle preferences, and cities will need assistance from the private sector to create further jobs, tax revenue, and housing.

To that end, the following post economic development strategies may be considered to facilitate public-private projects, and promote development along Red Hill Avenue in the Specific Plan area. In the future, other programs may be available for consideration. Note that none of these mechanisms assume or allow for the use of eminent domain for economic purposes, which is not allowed under State law.

Program	Description
Dedications and Exactions	Developers may be required to dedicate land or make cash payments for public facilities required or affected by their project (e.g., road right-of-way fronting individual properties). Dedications are typically made for road and utility right-of-way, park sites, and land for other public facilities. Cash contributions are made for other public facilities that are directly required to serve their projects (e.g., payments for a traffic signal).
General Fund Revenues	The City may elect to use General Fund revenues to help offset the cost of public infrastructure provisions.
Enhanced Infrastructure Financing Districts (“EIFDs”)	An EIFD is a financing district governed by a city, county, or special district (except school districts) that offers a new platform to leverage Tax Increment Financing (“TIF”) and finance an array of economic development projects. These districts do not require a public vote to be created and require just 55% of the vote amongst landowners or registered voters to issue tax increment bonds. EIFDs mandate a regional approach by encouraging multiple local agencies (cities, counties, special districts) to cooperate in order to use TIF. Once approved, EIFDs can combine tax increment with other

# RED HILL AVENUE SPECIFIC PLAN



Program	Description
	<p>regional and state-authorized financing programs (GGRF, PACE) to finance economic development projects of communitywide significance, including, but not limited to, affordable housing, mixed-use developments, transit priority projects, parks and open space, childcare facilities, and roads.</p>
<p>Statewide Communities Infrastructure Program (SCIP)</p>	<p>Infrastructure financing for small development projects. The Statewide Community Infrastructure Program (“SCIP”) is a program of the California Statewide Communities Development Authority (the “Authority”). The SCIP Program can be used to issue all sizes of land-secured bonds and is well suited as a conduit for smaller bond issues to take advantage of the economies of scale associated with a larger bond pool. SCIP can combine small financing secured by individual land development projects located in participating municipalities throughout the State into one large bond pool financing which is sold using California Communities as its Issuer. With the major exception of school fees, SCIP can finance most development impact fees and public improvements that provide benefit within a designated assessment district.</p>
<p>Special Districts (Business Improvement Districts “BIDs”)</p>	<p>A Business Improvement District, or “BID”, is a revitalization tool for commercial neighborhoods such as shopping malls and regional business districts. Established by law in the late 1980’s and early 1990’s, BIDs are public/private sector partnerships that perform a variety of services to improve the image of their cities and promote individual business districts. They also carry out economic development services by working to attract, retain and expand businesses. BIDs enable a city to establish a BID and levy annual assessments on businesses within its boundaries. Improvements which may be financed include parking facilities, parks, fountains, benches, trash receptacles, street lighting, and decorations. In addition, a BID also allows financing of streets, rehabilitation or removal of existing structures, and security facilities and equipment, as well as financing of marketing and economic development, and various supplemental municipal services.</p>
<p>Property Assessed Clean Energy Finance Program (“PACE”)</p>	<p>The Property Assessed Clean Energy (PACE) model is an innovative mechanism for financing energy efficiency and renewable energy improvements on private property. PACE programs exist for both residential properties (commonly referred to as residential PACE or R-PACE) and commercial properties (commonly referred to as commercial PACE or C-PACE). Commercial and residential PACE programs share a</p>

# RED HILL AVENUE SPECIFIC PLAN

Program	Description
	<p>common foundation. PACE programs allow local governments, state governments, or other inter-jurisdictional authorities, when authorized by state law, to fund the up-front cost of energy improvements on commercial and residential properties, which are paid back over time by the property owners. PACE financing for clean energy projects is generally based on an existing structure known as a "land- secured financing district," often referred to as an assessment district, a local improvement district, or other similar phrase. In a typical assessment district, the local government issues bonds to fund projects with a public purpose such as streetlights, sewer systems, or underground utility lines.</p>
Bicycle Transportation Account (BTA)	<p>Caltrans administers the Bicycle Transportation Account (BTA), an annual program providing state funds for city and county projects that improve safety and convenience for bicycle commuters. Cities and counties are eligible to apply for BTA funds. Eligibility is based on pre-adoption of a Bicycle Transportation Plan (BTP) that complies with Streets and Highways Code Section 891.4 that has been pre-approved by the appropriate Metropolitan Planning Organization (MPO) or Regional Transportation Planning Agency (RTPA). Eligible uses include bikeways and related facilities, planning, safety, and education. The BTA is a reimbursement program, which requires allocated funds to be matched by at minimum 10 percent of the total project cost.</p>



# RED HILL AVENUE SPECIFIC PLAN

Draft | July 2018

## Appendices

# 7 APPENDICES

A

EXISTING CONDITIONS

# Red Hill Avenue Specific Plan

## Existing Conditions



*April 2017*

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## 1. Introduction

### Project Description

The City of Tustin is embarking on a major planning effort along Red Hill Avenue as recent development interests have spurred the City's desire in developing a comprehensive plan to guide future development in the area. This portion of Red Hill Avenue provides a key gateway into the City due to its proximity to a major regional freeway and existing retail centers and buildings in the area need revitalization and repair. **Figure 1.1, Specific Plan Project Area** illustrates the Tustin Red Hill Avenue Specific Plan boundaries. The Specific Plan establishes a policy and regulatory framework to create an economically vibrant and sustainable environment that complements the character and desires of Tustin residents. The Specific Plan seeks to:



- Create a mixed-use, multi-modal environment;
- Provide an attractive gateway to the community;
- Enhance the pedestrian scale and function of the built environment;
- Promote sustainability principles in design and development;
- Establish a complementary mix of land uses, public spaces and outdoor activities;
- Provide better functionality and connectivity with adjacent neighborhoods;
- Provide family-oriented community spaces and activities;
- Support and enhance economic development;
- Support future regional transportation and transit planning objectives;
- Reinforce the character of the Tustin community;
- Enhance the streetscape and public realm; and
- Look at opportunities to set this area apart as a unique, organic, branded area.

The Specific Plan will establish an effective and implementable policy and regulatory mechanism that introduces a complementary mix of land uses including residential, retail, public space, and commercial that capitalizes on the area's current land use, transportation, and infrastructure opportunities.

## Existing Conditions Report Purpose & Outline

The Existing Conditions Report provides a “snapshot” of existing conditions and opportunities and constraints in the Specific Plan area. This report is intended as a working document that establishes baseline information for the project area and gives guidance for project decisions. The report is organized in the following Chapters:

1. Introduction
2. Demographics and Employment
3. Land Use and Zoning
4. Mobility and Parking
5. Infrastructure
6. Urban Form and Streetscape
7. Opportunities and Constraints Map

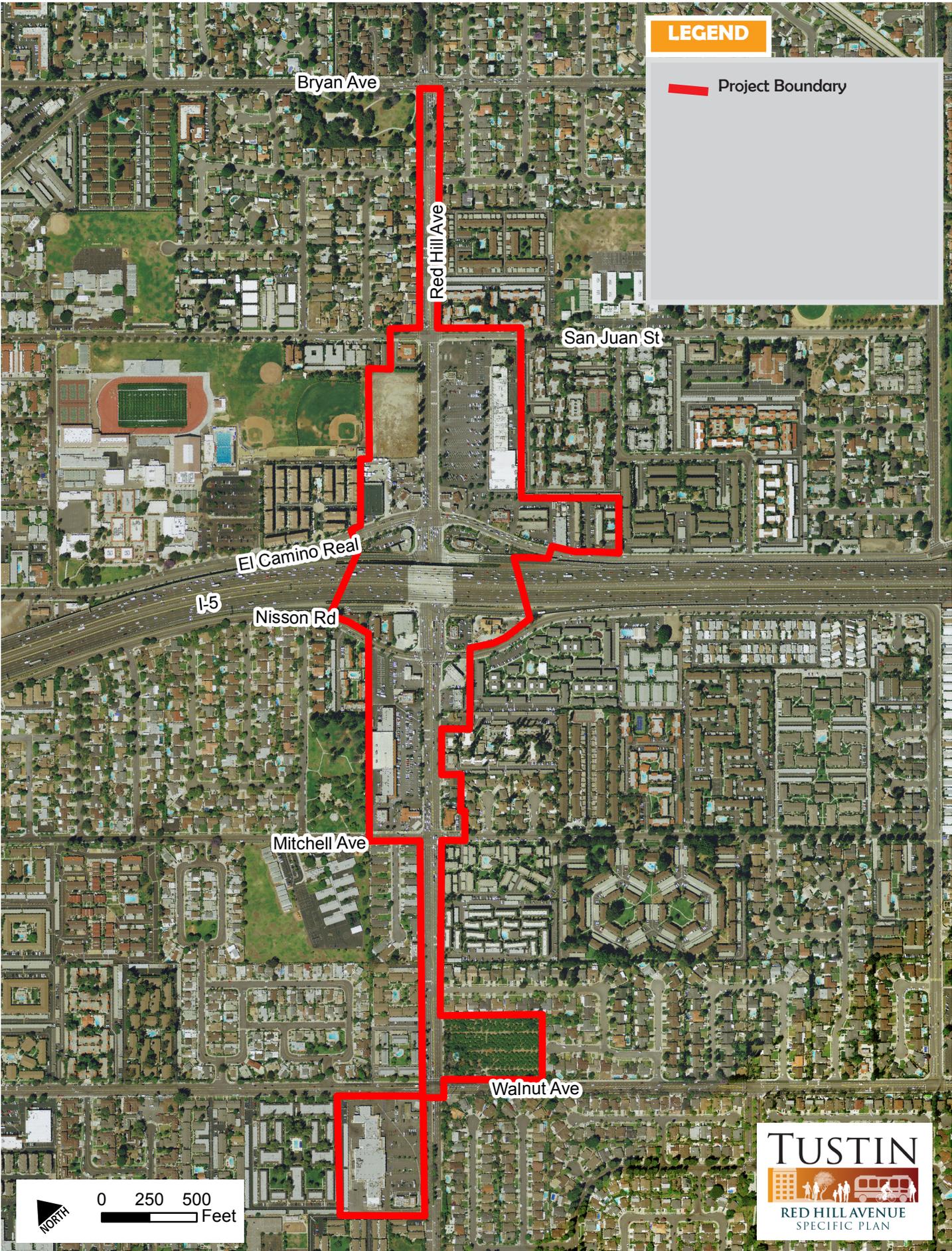


Figure 1.1: Red Hill Avenue Specific Plan Project Area

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## 2. Demographics & Employment

The following discussion provides an overview of the demographics and employment patterns in the City of Tustin. Please refer to the *Tustin Red Hill Avenue Retail Plan, March 2017* prepared by Kosmont Companies under separate cover, which is the source for the following information.

### Demographics

In 2015, there was a population of 80,796 residents and approximately 26,400 households living in the City of Tustin. From 2010 to 2015, the City's population increased by 8.2 percent. By comparison the population of Orange County increased by 3.8 percent from 2010 to 2015. Average household size in Tustin was 3.04 persons per household in 2015.

Approximately 41 percent of residents in Tustin identify as Hispanic. The percentage of Hispanic residents in the City has increased from 34.34 percent in 2000 to 40.6 percent in 2015. The median age in the City is lower than the County and the state at 33.4 years old. The median household income in the project area is \$75,011, compared to \$77,676 in the county. **Table 2-1, Population and Income 2015.**



*Typical Retail Center within the Study Area*

**Table 2-1: Population and Income 2015**

	City of Tustin	County of Orange
Population	80,796	3,124,130
Households	26,374	1,026,508
Average Household Size	3.04	3.00
Median Age	33.4	36.9
% Hispanic Origin	40.6%	34.8%
Per Capita Income	\$33,672	\$35,179
Median Household Income	\$75,011	\$77,676
Average Household Income	\$102,356	\$106,158
<b>2015-2020 Annual Growth Rate</b>		
Population	1.05%	0.82%
Median Household Income	2.53%	2.36%

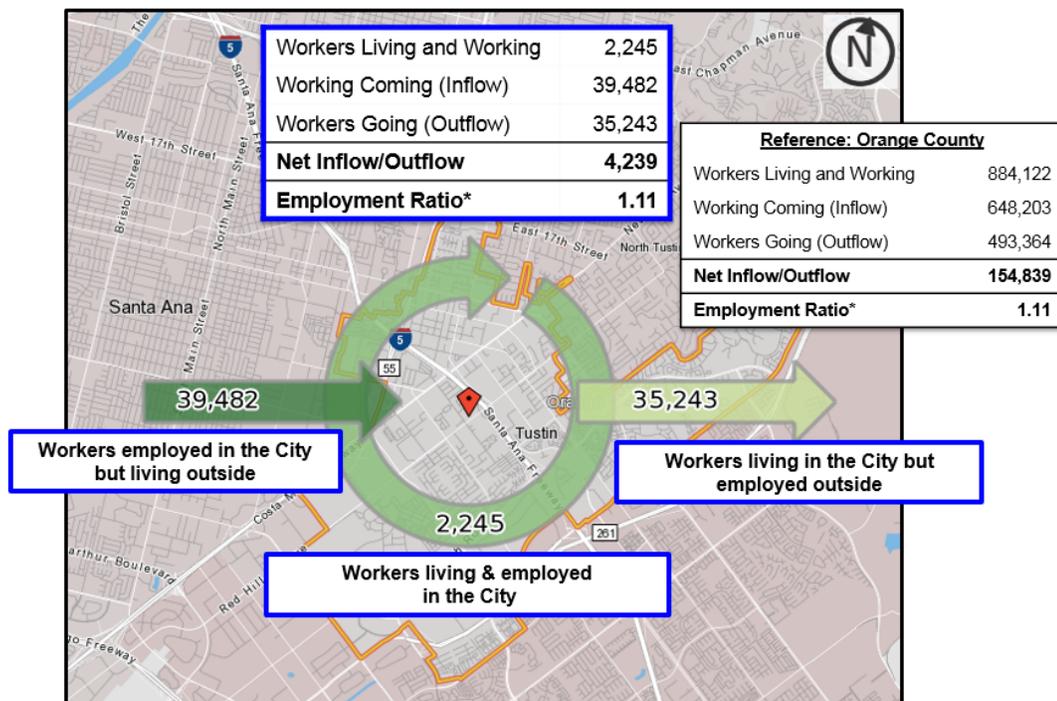
Source: Kosmont Tustin Red Hill Corridor Retail Plan, March 2017

## Employment and Commute Patterns

The unemployment rate in Tustin for April 2016 was 3.6 percent, which is lower than the County's unemployment rate of 3.9 percent (Kosmont, 2017). The top job categories within the City include retail trade; professional, scientific, and technical services; and hotel accommodation and food services.

Approximately 35,243 workers, or 44 percent of Tustin residents work outside of the City and approximately six percent of the working population who live in Tustin also work in the City (Kosmont, 2017). **Figure 2.1, Worker Inflow/Outflow**, illustrates the count and characteristics of worker flow into and out of Tustin and shows that Tustin has a net inflow of workers, consistent with Orange County as a whole.

**Figure 2.1, Worker Inflow/Outflow (Kosmont, 2017)**



\*Employment Ratio= People employed within the City (living and working in the City + those who come into the City for work)/Employed population of City (living and working in City + workers who live in the City, but work outside of the City)

### Summary of Existing Conditions

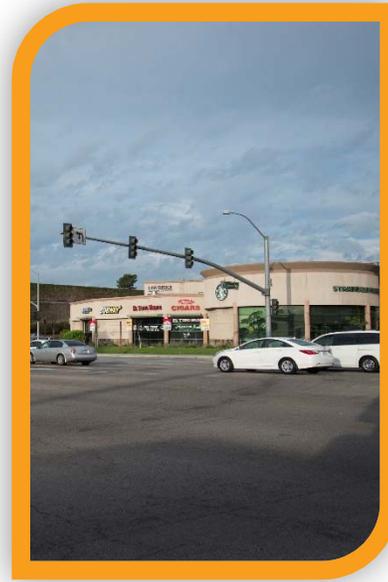
- Relatively young, diverse population.
- Average household size comparable to the County of Orange and slightly higher than the State average.
- Lower unemployment rate compared to the State and slightly less than Orange County with most workers employed in retail trade; professional, scientific, and technical services; and hotel accommodation and food services.
- Many residents are employed in Irvine, Santa Ana, Tustin, Anaheim, Los Angeles, and other cities, however the City has a net inflow of jobs, consistent with the County as a whole.

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### 3. Land Use & Zoning

#### General Plan Land Use

The Tustin General Plan Land Use Map shows a mix of commercial and professional office land use designations within the project area. As shown in **Table 3-1, General Plan Land Use Designations within the Red Hill Avenue Specific Plan**, the Community Commercial (CC) land use designation accounts for the highest percentage of acreage within the project area. As shown in **Figure 3.1, General Plan Land Use**, commercial land use designations are located along the entire length of Red Hill Avenue within the project area.



**Table 3-1: General Plan Land Use Designations within the Red Hill Avenue Specific Plan**

Land Use	Acres	Percentage
Community Commercial (CC)	36.05	90.8%
Planned Community Commercial/Business (PCCB)	2.86	7.2%
Professional Office (PO)	0.78	2.0%
<b>TOTAL</b>	<b>39.69</b>	<b>100%</b>

#### Zoning Districts

Zoning districts in the project area consists mainly of commercial and professional office zones. As shown in **Table 3-2, Zoning Districts within the Red Hill Avenue Specific Plan**, Retail Commercial (C1), and Central Commercial (C2) zones account for the highest percentage of acreage within the project area. As shown in **Figure 3.2, Zoning Districts**, commercial zones are located along the entire length of Red Hill Avenue within the project area.

**Table 3-2: Zoning Districts within the Red Hill Specific Plan**

Zoning District	Acres	Percentage
Retail Commercial (C1)	16.51	41.6%
Central Commercial (C2)	17.0	42.8%
Commercial General (CG)	5.40	13.6%
Professional (PR)	0.78	2.0%
<b>TOTAL</b>	<b>39.69</b>	<b>100%</b>

### Existing Land Use – Study Area

Existing (on-the-ground) land uses include a mix of low intensity commercial and retail and office uses in an auto-oriented setting. There are currently non-conforming uses along Nisson Road with multi-family residential uses located on parcels zoned for commercial uses, and two single-family homes north of Mitchell Avenue on parcels zoned for professional office uses. In addition, an existing home and agricultural operation are present near the southern boundary of the study area; this parcel is zoned for community commercial uses.

Existing land uses are shown on **Figure 3.3, Existing Land Use**. Based on land use analysis, the study area contains approximately 296,446 square feet of non-residential uses and 21 dwelling units.

The property located at 14462 Red Hill includes a structure constructed in 1915. As part of the City's Historic Resources Survey, the building was evaluated and identified as a significant resource (Status Code 3S – eligible for the National Register) due to its architecture and association with early Tustin residents. Any physical change to this building would require environmental analysis and review.

### Adjacent Land Use

#### Northern Area

Land uses adjacent to the study area north of Interstate 5 are characterized by high density residential uses (a mixture of attached single family and multifamily units) and the Tustin High School. General Plan and Zoning Designations are as follows:

**North:** Low Density Residential (LDR) and Park, zoned R1, P & I and PD

**South:** Interstate 5 and the southern portion of the study area

**East:** High Density Residential (HDR), zoned R2, R3, R4 and PD

**West:** High Density Residential (HDR), zoned R3, and Tustin High School, zoned P&I

#### Southern Area

Land uses adjacent to the study area south of Interstate 5 are characterized by a mixture of medium, low, and high density residential uses and Frontier Park. General Plan and Zoning Designations are as follows:

**North:** Interstate 5 and the northern portion of the study area

**South:** Planned Community (PCR) and High Density Residential (HDR), zoned PC RES, R3 and R4.

**East:** High Density Residential (HDR), pockets of Low Density Residential (LDR) and a planned community area (PCR), zoned R1, R3, R4, and PCR.

**West:** High Density Residential (HDR) Medium Density Residential (MDR), zoned R-2, R3, R4, and R2 and Frontier Park, zoned P&I

### Summary of Existing Conditions

- Commercial and retail uses are concentrated along Red Hill Avenue south of San Juan Street.
- There are currently non-conforming uses along Nisson Road with multi-family residential uses located on parcels zoned for commercial uses and two single family dwelling units along Red Hill north of Mitchell Avenue.
- Future changes to structures of historical significance will require environmental review.

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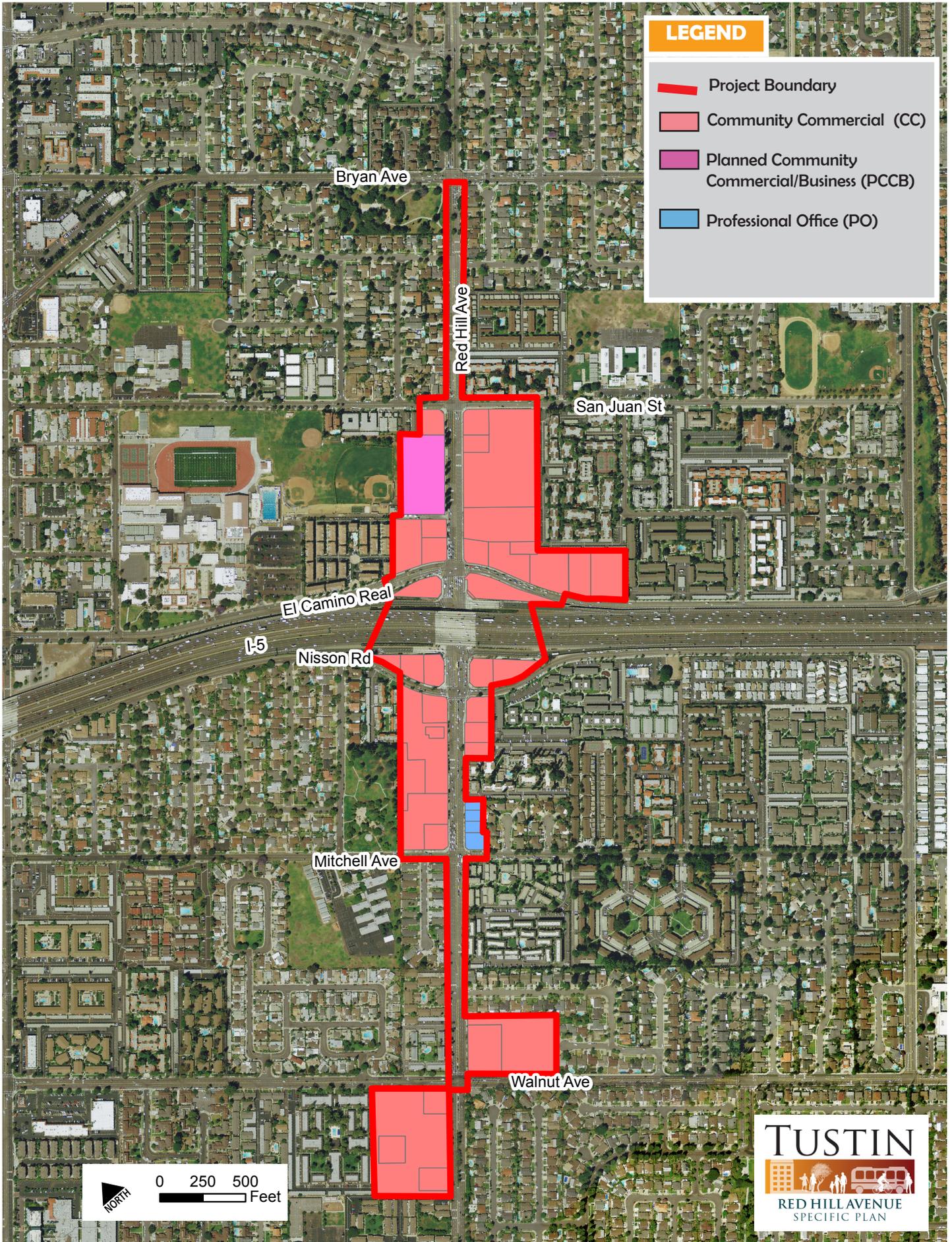


Figure 3.1: General Plan Land Use

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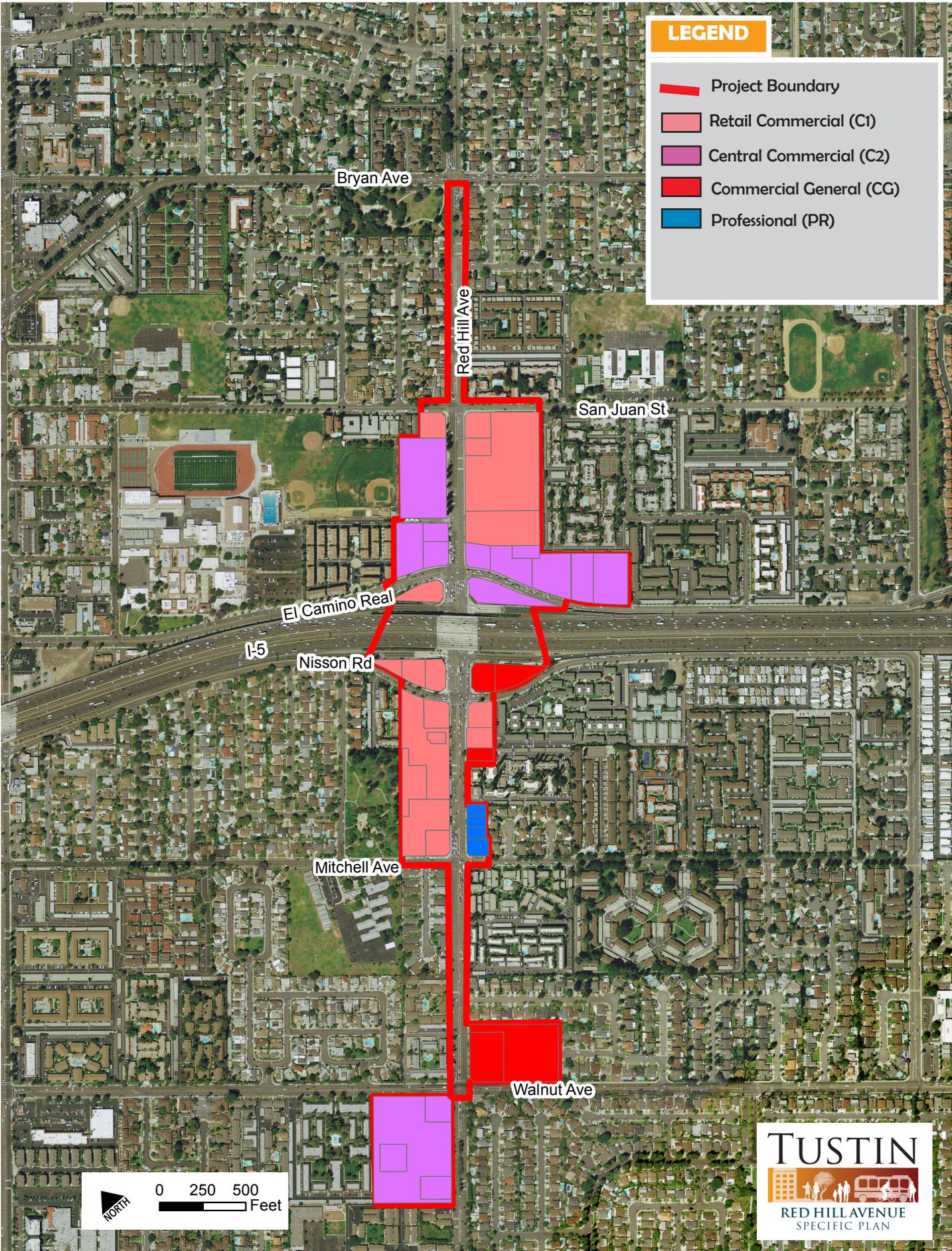


Figure 3.2: Zoning Districts

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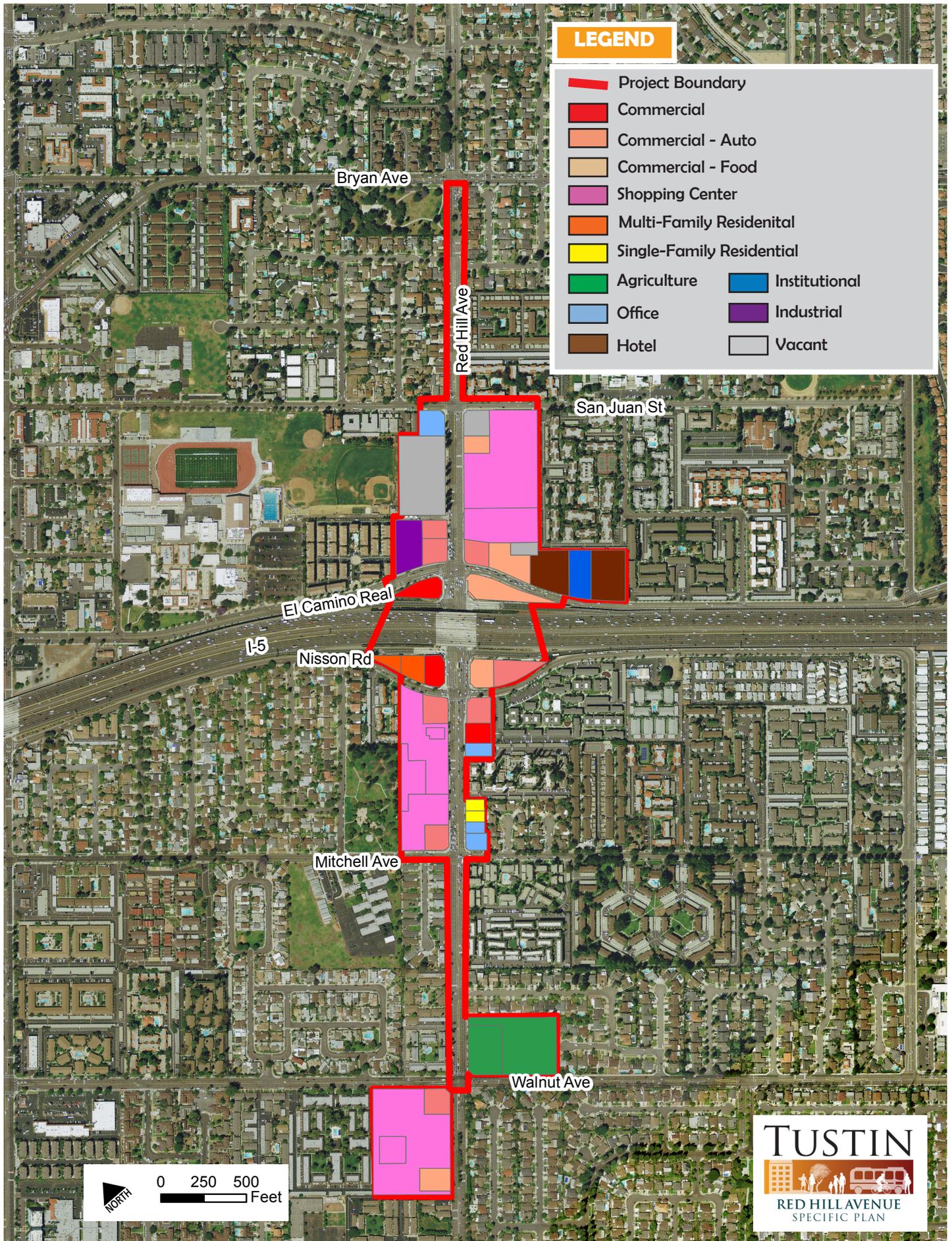


Figure 3.3: Existing Land Use

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## 4. Mobility & Parking

### Existing Roadways

The project area roadways are shown on **Figure 4.1, Existing Roadways and Intersections**. They consist of Red Hill Avenue, which is oriented in the northeast-to-southwest direction; and six roadways that cross Red Hill Avenue in the northwest-to-southeast direction: Bryan Avenue, San Juan Street, El Camino Real, Nissan Road, Mitchell Avenue, and Walnut Avenue. The intersections of each of these streets within the project area signalized. Interstate 5 bisects the study area. A brief description of each of the roadways is provided below.

**Red Hill Avenue** – Red Hill Avenue is a six-lane divided roadway, with three travel lanes in each direction and a center two-way left-turn lane. Within the project area, Red Hill Avenue provides access to the Interstate 5 (I-5) Freeway via a tight diamond interchange, and carries approximately 21,400-26,300 vehicles per day (vpd). The posted speed limit along Red Hill Avenue is 40 miles per hour. Red Hill Avenue is designated as a 6-Lane Major Arterial on the Arterial Highway Plan of the City of Tustin’s General Plan. A 6-Lane Major roadway would provide 3 travel lanes in each direction with a raised center median within 120 feet of right-of-way per the Tustin General Plan.

The portion of Red Hill Avenue from El Camino Real to San Juan Street was included in the Red Hill Avenue Streetscape and Median Feasibility Study – El Camino Real to Bryan Avenue (Tait & Associates, Inc., September 10, 2012). The following additional engineering details about Red Hill Avenue within the project area are taken from that study:

The existing street right-of-way along Red Hill Avenue is essentially 100 feet wide (50 feet on each side of the street centerline), with 84 feet of pavement and 8-foot parkways; however, the section on the west side of Red Hill Avenue, from 300 feet north of El Camino Real to San Juan Street, has a half-width right-of-way width of 60 feet, which is the ultimate half-width right-of-way for Red Hill Avenue.

The street parkways include sidewalks ranging in width from 4 feet to 9 feet. Portions of the parkway areas include landscaping and street trees. The parkway on the east side of Red Hill Avenue includes overhead power, telephone, and cable-TV wires; and street lights on wooden poles. No above-ground utilities are present on the west side of the roadway.



*Typical roadway edge condition*

**Bryan Avenue** – Bryan Avenue is a four-lane divided roadway with a two-way left turn lane at the intersection of Red Hill Avenue. The intersection with Red Hill Avenue is signalized. Bryan Avenue is designated as a Secondary Arterial in the City’s General Plan. A Secondary would provide 2 travel lanes in each direction within 80 feet of right-of-way; or may include Class II bicycle lanes, within 92 feet of right-of-way. This roadway borders the study area to the north but is not a part of the study area itself.

**San Juan Street** – San Juan Street is a local two-lane undivided roadway which provides local access to adjacent residential and school uses. Its intersection with Red Hill Avenue is signalized. San Juan Street carries between 2,600 and 3,100 vpd near Red Hill Avenue. The posted speed limit along San Juan Street is 30 miles per hour, with a 25-mph school zone, east of Red Hill Avenue. The Tustin High School campus is immediately adjacent to the western border of the project area.

**El Camino Real** – El Camino Real is a four-lane divided roadway with a two-way left-turn lane northwest of Red Hill Avenue, and a two-lane divided roadway with a two-way left-turn lane southeast of Red Hill Avenue. The street has a 56 foot right of way east of Red Hill with a paved surface that varies from 36-40 feet. West of Red Hill the street has a wider right of way of 80 feet and a paved width of 64 feet. Its intersection with Red Hill Avenue is signalized. The posted speed limit along El Camino Real is 40 miles per hour. El Camino Real carries between 9,400 and 14,900 vpd near Red Hill Avenue. El Camino Real is designated as a Secondary Arterial on the Arterial Highway Plan in the City’s General Plan. A Secondary would provide 2 travel lanes in each direction within 80 feet of right-of-way; or may include Class II bicycle lanes, within 92 feet of right-of-way. The eastbound approach of El Camino Real and Red Hill Avenue is posted with the following turn restrictions: “No Turn on Red, 7 – 9 am and 4 – 6 pm weekdays”.

**Nisson Road** – Nisson Road is a two-lane undivided roadway which provides local access to adjacent residential and business parcels. Its intersection with Red Hill Avenue is signalized. Nisson Road carries between 4,300 and 5,700 vpd near Red Hill Avenue. The posted speed limit along Nisson Road is 35 miles per hour. The westbound approach of Nisson Road at Red Hill Avenue is posted with the following turn restriction: “No Turn on Red, 7 – 9 AM, 4 – 6 PM Weekdays”.

**Mitchell Avenue** – Mitchell Avenue is a two-lane undivided roadway, which provides local access to adjacent residential and business parcels. Its intersection with Red Hill Avenue is signalized. Mitchell Avenue carries between 4,900 and 6,500 vpd near Red Hill Avenue. The posted speed limit along Mitchell Avenue is 30 miles per hour, with a 25-mph school zone west of Red Hill. The Tustin High School campus is immediately adjacent to the western border of the project area.

**Walnut Avenue** – Walnut Avenue is a four-lane divided roadway with a two-way left turn lane at the intersection of Red Hill Avenue. The intersection with Red Hill Avenue is signalized. Walnut Avenue is designated as a Primary Arterial in the City’s General Plan. A Primary would provide two

travel lanes in each direction within 100 feet of right-of-way. This roadway is located at the southern edge of the project area.

**Interstate 5** – Interstate 5 bisects the project area adjacent to the intersection of Red Hill Avenue/Nisson Road and Red Hill Avenue/El Camino Real. Northbound and southbound on- and off-ramps are present in a diamond-style interchange configuration. The ramps are located at signalized intersections. The Interstate right-of-way is owned by the State of California.

**Intersections**

The following intersections are major intersections within the project area shown on **Figure 4.1, Existing Roadways and Intersections**.

- Red Hill Avenue at San Juan Street (Signalized)
- Red Hill Avenue at El Camino Real (Signalized)
- Red Hill Avenue at I-5 NB Ramps (Signalized)
- Red Hill Avenue at I-5 SB Ramps (Signalized)
- Red Hill Avenue at Nisson Road (Signalized)
- Red Hill Avenue at Mitchell Avenue (Signalized)
- Red Hill Avenue at Bryan Avenue (Signalized)
- Red Hill Avenue at Walnut Avenue (Signalized)

Intersection spacing along Red Hill Avenue can affect the flow of traffic and requires signal timing and coordination. The distance between each of the signalized intersections (measured centerline to centerline along Red Hill Avenue) are shown on **Table 4-1, Distance between Major Intersections**. The close spacing of intersections (less than 300 feet) between Nisson Road and El Camino Real poses queuing and signal timing challenges, particularly during the peak hours.

Table 4-1: Distance between Major Intersections	
Intersections	Distance
Mitchell Avenue to Nisson Road	950 feet
Nisson Road to I-5 Southbound Ramps	240 feet
I-5 Southbound Ramps to I-5 Northbound Ramps	260 feet
I-5 Northbound Ramps to El Camino Real	220 feet
El Camino Real to San Juan Street	955 feet
Walnut Avenue to Mitchell Avenue	1,320 feet
San Juan Street to Bryan Avenue	1,325 feet

**Accident History**

The City of Tustin provided accident data along Red Hill Avenue, between Bryan Avenue and Walnut Avenue, from January 1, 2014 to May 16, 2016. A summary of the accident data is provided on **Table 4-2, Summary of Accident Data, Red Hill Avenue (Bryan Avenue to Walnut Avenue)**. There was a total of 85 accidents in the 2-1/2-year reporting period, with roughly half

being property damage only (PDO), and the other half involving injury (I). There were no fatality accidents during the reported period. The area within the project area with the most accidents was the stretch of Red Hill Avenue between El Camino Real and the I-5 Southbound Ramps, where 36 of the 85 accidents occurred.

Table 4-2: Summary of Accident Data, Red Hill Avenue (Bryan Avenue to Walnut Avenue)												
Location <sup>1</sup>	2014			2015			2016			2014-2016		
	PDO	I	Total	PDO	I	Total	PDO	I	Total	PDO	I	Total
Bryan Avenue	2	1	3	1	1	2	1	0	1	4	2	6
Lance Drive	0	0	0	0	1	1	0	0	0	0	1	1
San Juan Street	2	1	3	0	5	5	1	0	1	3	6	9
El Camino Real	3	1	4	4	7	11	2	0	2	9	8	17
I-5 NB Ramp	2	3	5	0	4	4	2	1	3	4	8	12
I-5 SB Ramp	2	0	2	3	0	3	0	2	2	5	2	7
Nisson Road	2	1	3	1	0	1	3	1	4	6	2	8
Mitchell Ave.	2	3	5	0	5	5	0	1	1	2	9	11
Walnut Ave.	3	1	4	2	5	7	1	2	3	6	8	14
<b>TOTAL</b>	<b>18</b>	<b>11</b>	<b>29</b>	<b>11</b>	<b>28</b>	<b>39</b>	<b>10</b>	<b>7</b>	<b>17</b>	<b>39</b>	<b>46</b>	<b>85</b>

<sup>1</sup>Location of crashes which occurred at or near the intersection  
 PDO= Property Damage Only  
 I= Injury

### Parking

On-street parking is prohibited along the entire length of Red Hill Avenue throughout the project area, with “No Stopping Entire Block” signage. Parking is also prohibited on El Camino Real on either side of Red Hill Avenue with “No Stopping Anytime” signage. On-street parking is also prohibited by painted red curbs along portions of some roadways within the project area as shown on **Figure 4.2, Existing Parking Facilities**.

On-street parking is generally allowed along San Juan Street, Nisson Road, and Mitchell Avenue, with the following specific, signed restrictions primarily related to street sweeping:

- Nisson Road: No Parking 7 AM to 11 AM Monday (Street Sweeping) on the roadway’s western reach and the southern edge of the eastern reach, and 7 AM to 11AM Wednesdays on the northern edge of the roadway’s eastern reach (also for street sweeping). In addition, on Nisson Road’s eastern reach, commercial vehicles over 3 tons are limited to 2-hour parking.

- Mitchell Avenue: No Parking 4 AM to 7 AM Monday (Street Sweeping)

Private off-street parking for local businesses within the project area is provided in surface parking lots, as shown on **Figure 4.2, Existing Parking Facilities.**



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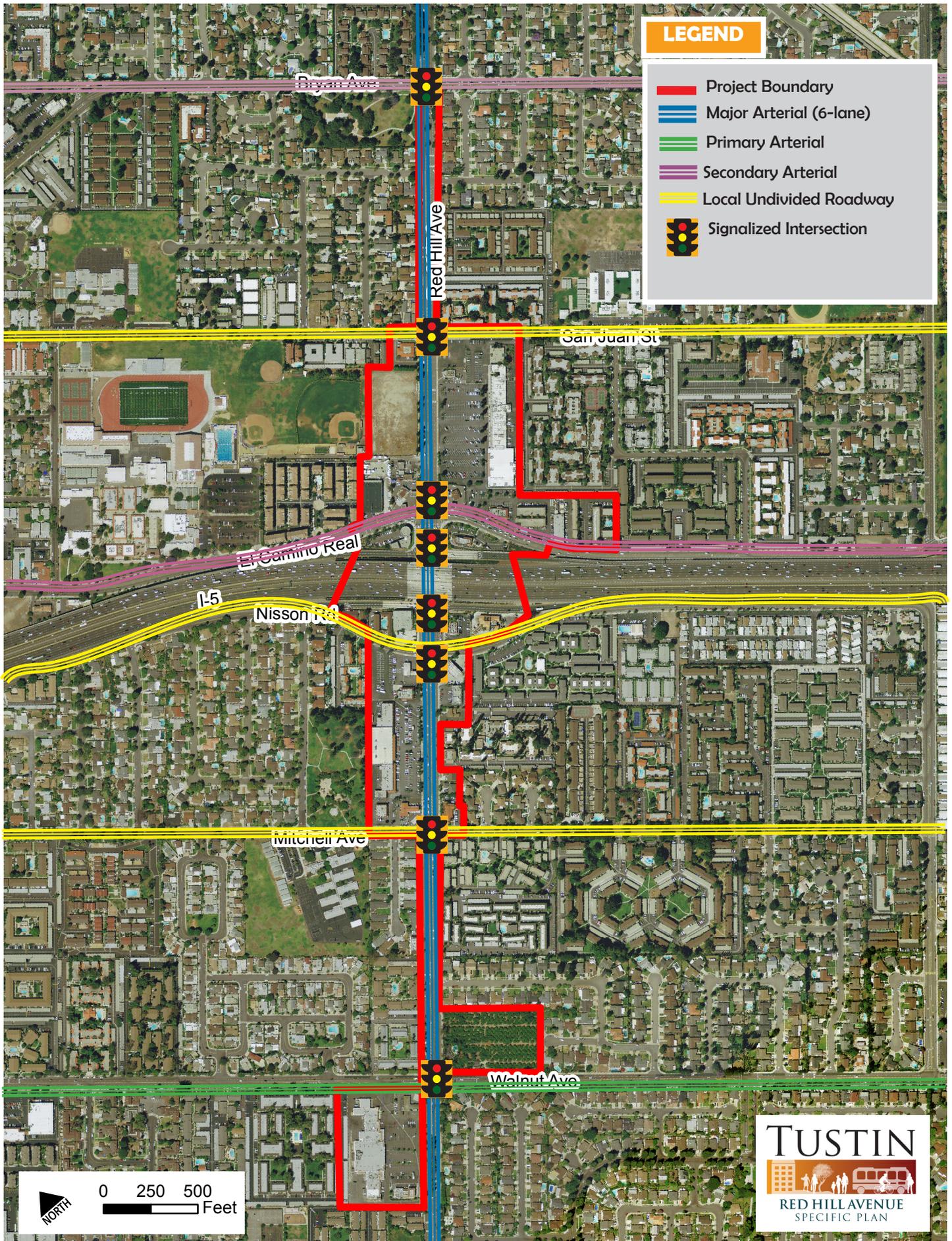


Figure 4.1: Existing Roadways and Intersections

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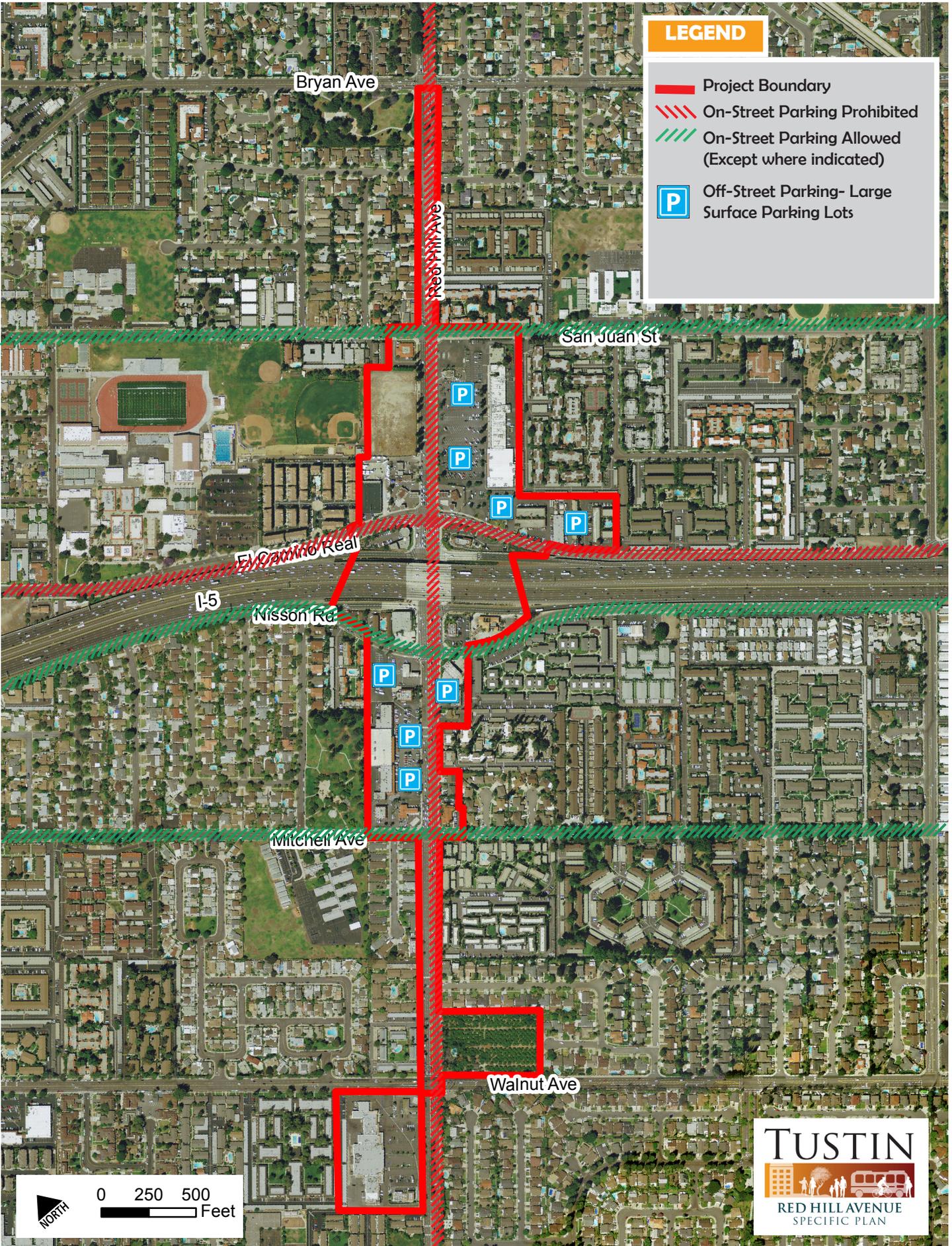


Figure 4.2: Existing Parking Facilities

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### Public Transportation

The Orange County Transportation Authority (OCTA) operates local public transit service throughout Orange County, including the City of Tustin and through the project area. Route 71 currently serves the project area, as shown on **Figure 4.3, Public Transportation Facilities**.

Route 71 operates between Newport Beach and Yorba Linda, travelling along Red Hill Avenue, El Camino Real, Newport Avenue, and First Street in the City of Tustin. Service is provided every day with an approximate headway<sup>1</sup> of 50 minutes during the weekdays and 60 minutes during the weekends and holidays.

Within the project area, bus stops are provided on Red Hill Avenue, north and south of Mitchell Avenue; on the north side of Red Hill Avenue, south of Nissan Road; and on both sides of El Camino Real, west of Red Hill Avenue.

### Bicycle Facilities

Within the project area, the only existing bicycle facility is a Class II bike lane (a striped, on-street bicycle lane adjacent to the travel lane) on Red Hill Avenue (in both directions), between El Camino Real and Nissan Road. The City of Tustin Master Bikeway Plan shows the entire extent of Red Hill Avenue within the City limits as a designated or a potential Class II bikeway.

### Pedestrian Facilities

Existing pedestrian facilities within the project area are shown on **Figure 4.4, Bicycle and Pedestrian Facilities**. Sidewalks are located along all roadways within the project area. Crosswalks are located across some or all legs of each of the signalized intersections within the project area, as indicated on **Table 4-3, Crosswalk Provisions**, below.

There are no unsignalized crosswalks across Red Hill Avenue within the project area.

Table 4-3: Crosswalk Provisions				
Intersection	Intersection Leg			
	North	South	West	East
Red Hill Avenue at Bryan Avenue	Y	Y	Y	Y
Red Hill Avenue at San Juan Street	Y	N	Y	Y
Red Hill Avenue at El Camino Real	Y	N	Y	Y
Red Hill Avenue at I-5 NB Ramps	N	N	Y	Y
Red Hill Avenue at I-5 SB Ramps	N	N	Y	Y
Red Hill Avenue at Nissan Road	N	Y	Y	Y
Red Hill Avenue at Mitchell Avenue	Y	Y	Y	Y
Red Hill Avenue at Walnut Avenue	Y	Y	Y	Y

<sup>1</sup> Headway is a measurement of time between vehicles in a transit system.

## Summary of Existing Conditions

- The intersections of each of the major streets within the project area are signalized.
- Intersection spacing along Red Hill Avenue can affect the flow of traffic and requires signal timing and coordination. The close spacing of intersections (less than 300 feet) between Nisson Road and El Camino Real poses queuing and signal timing challenges, particularly during the peak hours.
- The area within the project area with the most accidents was the stretch of Red Hill Avenue between El Camino Real and the I-5 Southbound Ramps, where 36 of the 71 accidents occurred.
- On-street parking is prohibited along the entire length of Red Hill Avenue throughout the project area, with “No Stopping Entire Block” signage. Parking is also prohibited on El Camino Real on either side of Red Hill Avenue with “No Stopping Anytime” signage.
- On-street parking is generally allowed along San Juan Street, Nisson Road, and Mitchell Avenue.
- Within the project area, bus stops are provided on Red Hill Avenue, north and south of Mitchell Avenue; on the north side of Red Hill Avenue, south of Nisson Road; and on both sides of El Camino Real, west of Red Hill Avenue.
- Within the project area, the only existing bicycle facility is a Class II bike lane (a striped, on-street bicycle lane adjacent to the travel lane) on Red Hill Avenue (in both directions), between El Camino Real and Nisson Road.
- Sidewalks are located along all roadways within the project area. Crosswalks are located across some or all legs of each of the signalized intersections within the project area.
- There are no unsignalized crosswalks across Red Hill Avenue within the project area.

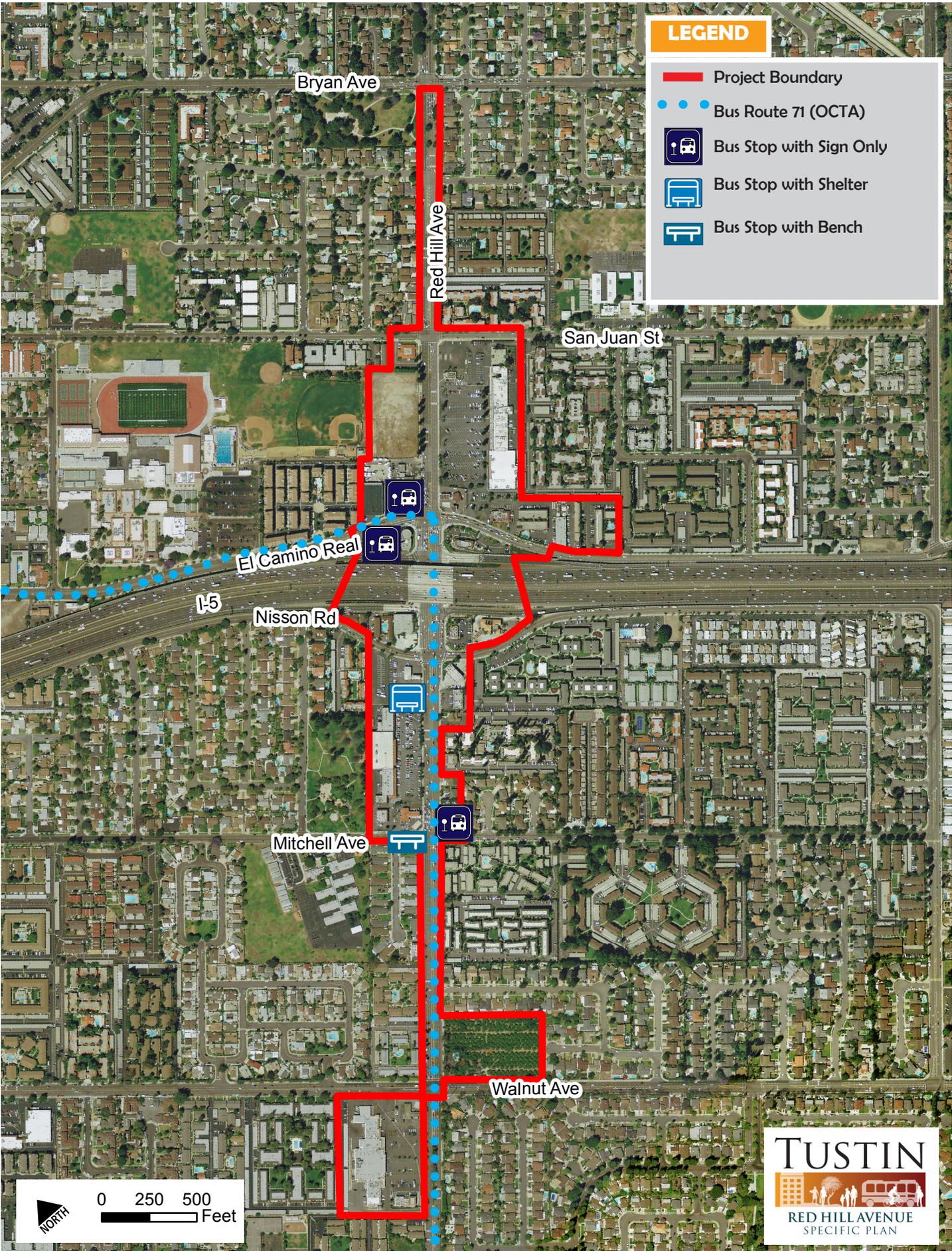


Figure 4.3: Public Transportation Facilities

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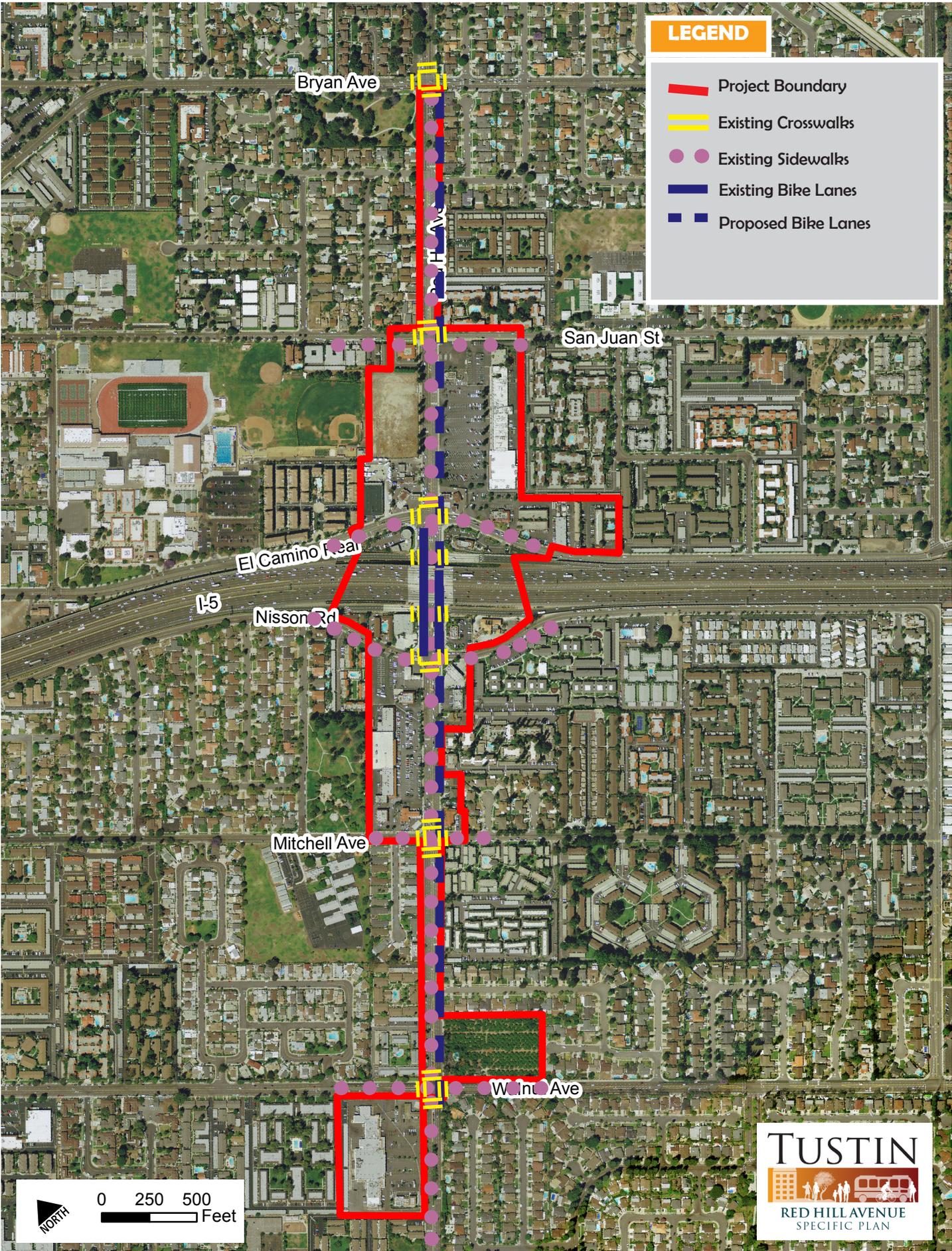


Figure 4.4: Bicycle and Pedestrian Facilities

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## 5. Infrastructure

### Domestic Water System

The domestic water system is owned and operated by the City of Tustin. According to the City's 2010 Urban Water Management Plan (UWMP), The City is part of the East Orange County Water District (EOCWD), a wholesale water district, and a member agency of the Metropolitan Water District of Orange County (MWDOC). The Specific Plan area lies within the City's pressure zone 1, with a hydraulic grade of 308 feet. The City receives approximately 85 percent of its water from its own groundwater wells that tap the Lower Santa Ana Groundwater Basin and the remaining 15 percent from EOCWD. By 2035, the City expects to produce approximately 71 percent of its own water through groundwater extraction and import the remaining 29 percent from EOCWD. The UWMP concludes that water supply is available and reliable for the City of Tustin through 2035, including an aggregate 7 percent increase in City population.



The City's water atlas and as-builts indicate the following pipe infrastructure as shown in **Figure 5.1, Existing Water System:**

- Red Hill Avenue (El Camino Real to Bryan Avenue): 6" asbestos cement pipe (ACP)
- Red Hill Avenue (Nisson Road to El Camino Real): 12" ductile iron pipe (DIP)
- Red Hill Avenue (Walnut Avenue to Nisson Road): 8" ACP
- El Camino Real: 8" ACP and 8" polyvinyl chloride (PVC) pipe
- Nisson Road: 8" ACP and 8" polyvinyl chloride (PVC) pipe
- Mitchell Avenue: 8" ACP
- San Juan Street: 8" PVC (West of Red Hill) and 8" ACP (East of Red Hill)
- Walnut Avenue: 16" ACP

In addition to the pipe infrastructure in the streets, there are sites that contain public water mains in private easements. The majority of the mains are 6" ACP. The City has indicated that the 6" ACP water main in Red Hill Avenue is undersized and that significant development would likely require upsizing of the 6" portion, subject to future engineering analysis.

### Sanitary Sewer System

The sanitary sewer mains within the public streets are owned and operated by East Orange County Water District (EOCWD). Orange County Sanitation District (OCSD) owns and maintains a network of sewer trunk mains, lift stations, and two wastewater treatment plants, one in Fountain

Valley, and the other in Costa Mesa. OCSD has identified a capacity deficiency in its Mitchell Avenue and Red Hill Avenue trunk mains located just downstream of the Specific Plan area. OCSD indicated that the deficiency issues are due to wet weather flows emanating from the City of Santa Ana and that dry conditions flows do not cause system surcharges. No other system deficiencies have been identified by the Sanitation District. The District plans to construct a parallel sewer relief main in Red Hill Avenue in phases in the summers of 2016 and 2017 to alleviate the deficiency. In 2016 a new sewer pipe was placed in Red Hill Avenue between Warner Avenue and Valencia Avenue. In 2017, construction will occur in the segment between McGaw Avenue (outside of the project area) and Mitchell Avenue (within the project area), continuing through fall 2017.

OCSD's and EOCWD's sewer as-builts indicate the following pipe infrastructure as shown in **Figure 5.2, Existing Sanitary Sewer System:**

- Red Hill Avenue (El Camino Real to Bryan Avenue): 18" vitreous clay pipe (VCP)
- Red Hill Avenue (Nisson Road to El Camino Real): 24" VCP
- Red Hill Avenue (Mitchell Avenue to Nisson Road): 18" VCP
- San Juan Street: 8" VCP
- Nisson Road: 8" VCP
- El Camino Real west of Red Hill Avenue: 8" VCP
- El Camino Real east of Red Hill Avenue: No sewer along Specific Plan frontage
- Mitchell Avenue west of Red Hill Avenue: Dual 21" VCP (per plan 7-4-1 and the Tustin-Orange Trunk Sewer Plan)
- Mitchell Avenue east of Red Hill: No sewers along Specific Plan frontage

OCSD does not prefer new connections to its trunk sewers. Existing laterals that connect to the trunks are depicted on the trunk plans in some cases and other laterals were approved by the City of Tustin. Property owners are responsible for the lateral serving their property from the property line to the main. OCSD will likely discourage new connections to its system and encourage the use of existing laterals.

### Storm Drain System

The Specific Plan area lies within the Peters Canyon tributary area of the San Diego Creek watershed. Regional drainage facilities are owned and operated by Orange County Public Works, Flood Division (OCPW). Local drainage facilities are owned and operated by the City of Tustin, as shown in **Figure 5.3, Existing Storm Drain System.** In general, drainage on the north side of I-5 drains into local storm drains and is directed into OCFD's F07P07 facility located in Browning Avenue. Drainage on the south side of I-5 drains into local storm drains and is directed into OCFD's F10P01 facility located in Red Hill Avenue, south of Mitchell Avenue. The Specific Plan area lies within FEMA Flood Zone "X", and is not subject to flood insurance requirements.

The City's storm drain as-builts and storm drain atlas indicate the following City pipe infrastructure:

- Red Hill Avenue: Catch basins and laterals at Mitchell Avenue and 54" Reinforced Concrete Pipe (RCP) south of Mitchell Avenue.
- Bryan Avenue: 54" to 66" RCP.
- San Juan Street: 42" to 45" RCP draining from west to east.
- Mitchell Avenue West of Red Hill Avenue: 22"X36" arched corrugated metal pipe (CMPA) draining east to OCPW's Red Hill Avenue F10P01 and 48" RCP draining east to OCPW's Red Hill Avenue F10P01 is abandoned and no longer in use. Segment between curb inlet catch basin at northwest corner of Mitchell Avenue and Red Hill Avenue and the storm drain in Red Hill Avenue is still active.
- Mitchell Avenue East of Red Hill: 22"X36" arched corrugated metal pipe (CMPA) draining east to OCPW's Red Hill Avenue F10P01 is abandoned and no longer in use. Historically, the arch pipe crossed Red Hill Avenue and continued flowing easterly before it was intercepted at Red Hill Avenue. A City storm drain remains active in Mitchell Avenue from Quent Drive extending eastward.
- Walnut Avenue: 42-45" RCP west of Red Hill Avenue.

The City's storm drain as-builts and storm drain atlas indicate the following Caltrans pipe infrastructure:

- Red Hill Avenue: Several RCP storm drain laterals and catch basins.
- El Camino Real west of Red Hill Avenue: 42" to 48" RCP.
- El Camino Real east of Red Hill Avenue: A 5'X4' box culvert is in Caltrans right-of-way south of, and parallel to, El Camino Real.

OCFD's storm drain atlas indicates the following OCFD pipe infrastructure:

- Red Hill Avenue: 48" RCP F10P01 facility with upstream limits at Mitchell Avenue.

The Specific Plan area lies within a hydromodification zone, as defined in the Stormwater Quality Technical Guidance document (TGD) prepared by the County of Orange. Sites will be required to detain and retain runoff to existing condition levels. Infiltration will be required if technically feasible, and no defined infiltration constraints are depicted in the TGD for this area.

### **Above-Ground Facilities<sup>2</sup>**

Existing overhead services are located on the eastern edge of Red Hill Avenue north of El Camino Real. The aboveground facilities include a single 4-wire 12 kV system for Southern California

<sup>2</sup> Utility Specialists. Existing Overhead Utility Services Investigation and Cost Estimate, May 2012.

Edison, four to six communication lines, and local street lights and their electrical distribution system, under a pole sharing arrangement. At the intersection of Red Hill Avenue and Bryan Avenue, the pole line joins and existing SCE 66 kV transmission pole that comes from Bryan and joins the pole line, continuing to the northeast along Red Hill. At the intersection of Red Hill Avenue and El Camino Real these facilities are undergrounded.

### Potential Proposed Improvements

No street widenings are proposed on the City's 2015-2017 Capital Improvement Program (CIP). Routine pavement management will occur on Red Hill Avenue and Mitchell Avenue in the CIP timeframe. In addition, a potential storm drain project on San Juan Street is listed in the CIP, but the project is undefined and a limited budget is allocated for the improvements.

The City also retained in 2012, Tait and Associates to prepare the "Red Hill Avenue Streetscape and Median Feasibility Study." The Study outlines several different alternatives for undergrounding overhead facilities, improvement vehicular traffic flows, and enhancing the pedestrian experience from El Camino Real to Bryan Avenue. None of the alternatives involve right-of-way dedication from adjacent property owners; however, sidewalk easements behind right-of-way are recommended in some of the alternatives. To that extent, adjacent development sites could be impacted by implementation of certain Study alternatives.

### Summary of Existing Conditions

- The City has indicated that the 6" ACP water main in Red Hill Avenue is undersized and that significant development would likely require a replacement of the 6" portion with a larger diameter main, with sizing subject to engineering analysis.
- OCSD does not prefer new connections to its trunk sewers. Existing laterals that connect to the trunks are depicted on the trunk plans in some cases and other laterals were approved by the City of Tustin. Property owners are responsible for the lateral serving their property from the property line to the main. OCSD will likely discourage new connections to its system and encourage the use of existing laterals.
- The Specific Plan area lies within a hydromodification zone, as defined in the stormwater quality technical guidance document (TGD) prepared by the County of Orange. Sites will be required to detain and retain runoff to existing condition levels. Infiltration will be required if technically feasible, and no defined infiltration constraints are depicted in the TGD for this area.

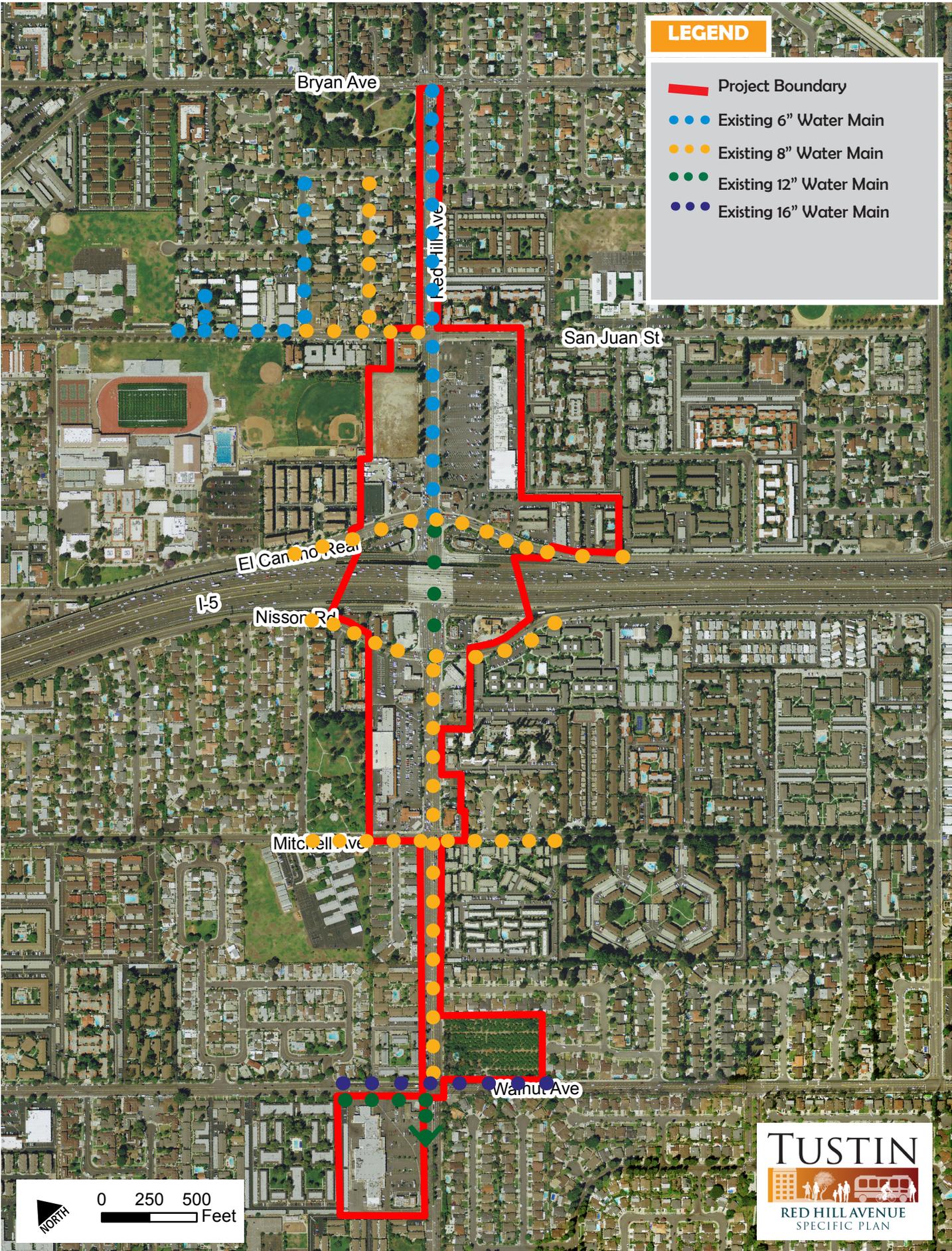


Figure 5.1: Existing Water System

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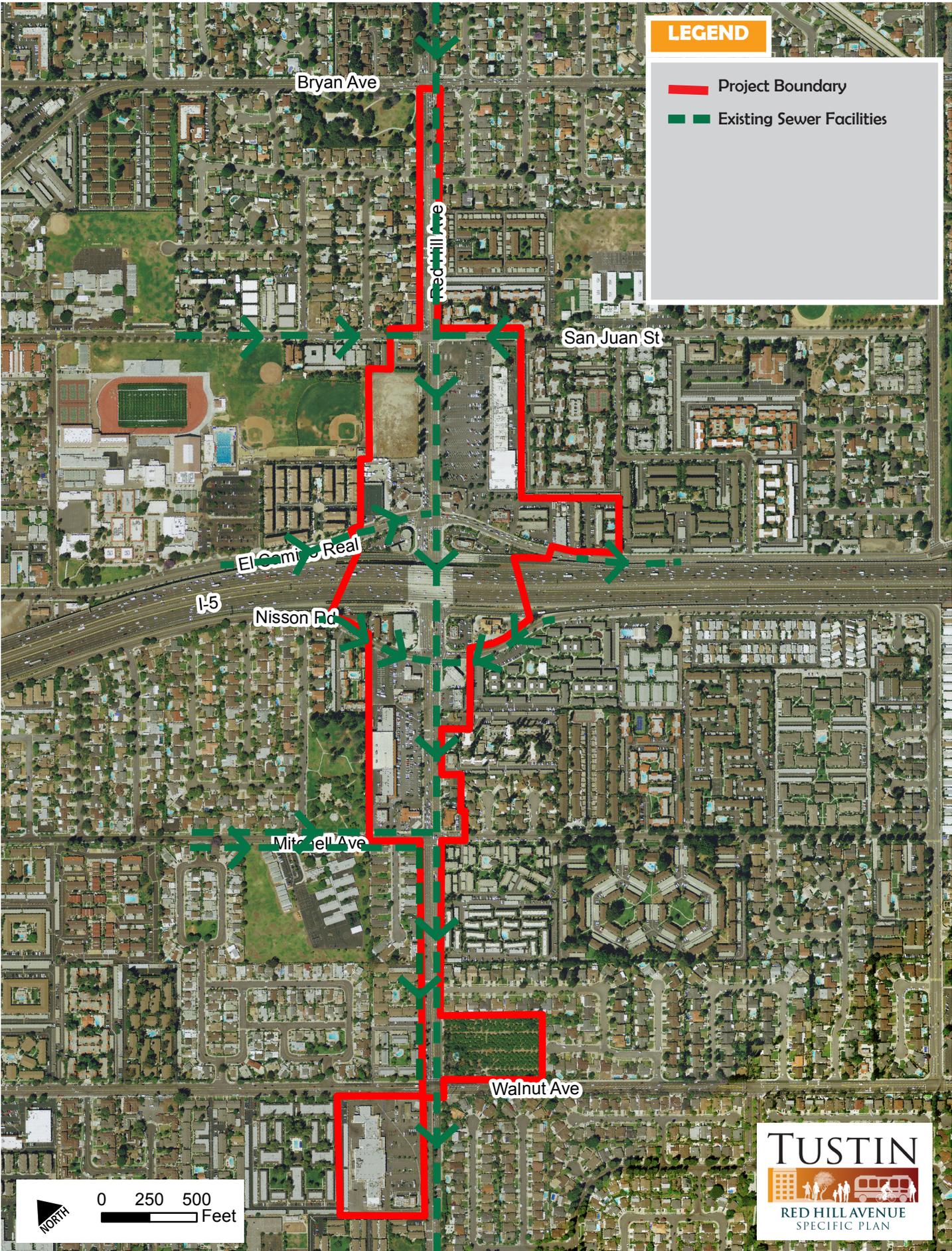


Figure 5.2: Existing Sanitary Sewer System

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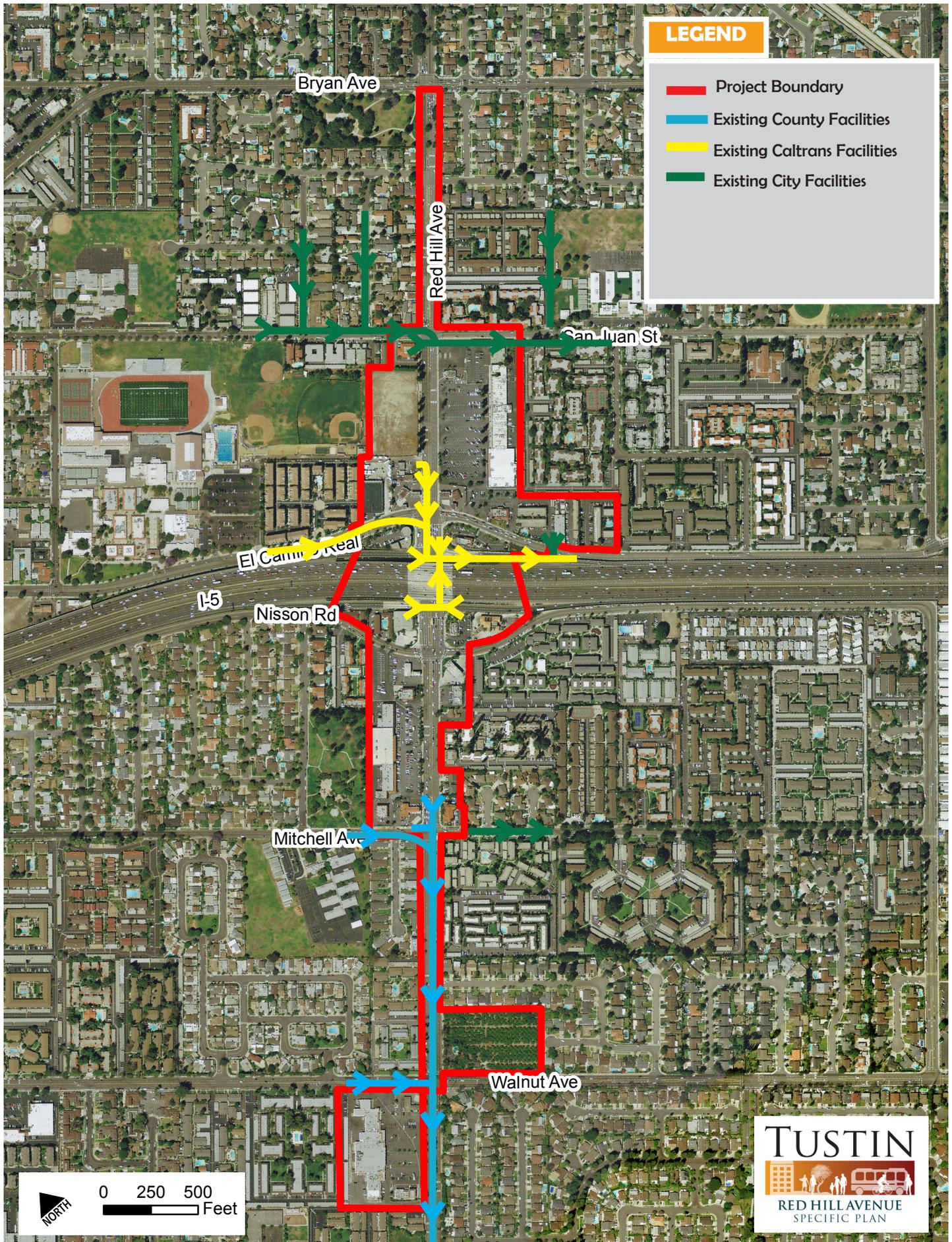


Figure 5.3: Existing Stormwater System

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## 6. Urban Design & Streetscape

### Building Footprints

Urban form refers to the physical layout and design of an area taking into consideration building size, scale, and layout. **Figure 6.1, Building Footprints**, illustrates the size and scale of existing building footprints and the spaces around buildings. This map was developed using aerial imagery of the Red Hill Avenue Specific Plan area accessed online in June 2016.

Commercial and retail uses have large building footprints as can be seen in the center and southern part of the project area.



Most buildings within the project area are located at a distance from the sidewalk often to accommodate large areas of surface parking. However, there are smaller buildings located in the southeast portion of the project area that are set adjacent to the street with landscaped setback areas.



*Commercial centers with large surface parking areas*



*Smaller commercial buildings with landscaped setbacks*

### Street Network

**Chapter 4: Mobility & Parking**, describes the street network that serves the Red Hill Specific Plan area. Main streets within the project area include Red Hill Avenue, Bryan Avenue, San Juan Street, El Camino Real, Nissan Road, Mitchell Avenue, and Walnut Avenue. The I-5 freeway separates the northern and southern portions of the project area and can be accessed via a tight diamond interchange from Red Hill Avenue.

Most commercial and retail businesses are found along Red Hill Avenue with multiple driveways for access, which can pose points of conflict between vehicles, bicyclists, and pedestrians. Smaller local roads can be found within the residential areas and may terminate at cul-de-sacs inside and outside of the project area.

Within the project area, the only existing bicycle facility is a Class II bike lane (a striped, on-street bicycle lane adjacent to the travel lane) on Red Hill Avenue (in both directions), between El Camino Real and Nissan Road. The lack of bicycle lanes within the project area may force bicyclists to share a traffic lane with vehicles or utilize the sidewalk potentially restricting pedestrian movements.



*Lack of bicycle facilities along Red Hill Avenue*



*Wide signalized intersections*

### Pedestrian Connections

During a site visit, it was observed that pedestrian activity within the project area occurs within surface parking lots at the main retail centers and around the main points of access to public transportation, specifically the OCTA bus stops on El Camino Real. Site observations also indicate that people walk through surface parking lots to connect from the sidewalk to commercial and retail building entrances. The larger shopping and commercial centers along Red Hill Avenue lack dedicated pedestrian access from the sidewalk to building entrances, though a few restaurant uses along El Camino Real provide dedicated and landscaped pedestrian walkways and ADA access from the sidewalk.

Sidewalks are located along all roadways within the project area, ranging from a width of 4 feet to 16 feet. Many sidewalk segments are directly adjacent to the travel lane and do not provide a buffer between the pedestrian and vehicles. There are several sidewalk segments within the project area that adequately provide a buffer and pedestrian paths: east side of Red Hill Avenue, from Lance Drive to San Juan Street; west side of Red Hill Avenue, from San Juan Street to the end of the vacant lot; north side of San Juan Street, from Red Hill heading east; and San Juan Street, from Red Hill heading west, as shown on **Figure 6.2, Pedestrian Connections**. These sidewalks provide useful examples to improve all sidewalks within the project area and enhance pedestrian access to existing retail and commercial centers.

Crosswalks are located across some or all legs of each of the signalized intersections within the project area, and there are no unsignalized crosswalks across Red Hill Avenue. All marked crosswalks are “standard” painted parallel lines, which may not be as visible as “continental” or “ladder” style crosswalks that may be considered in future improvements. Mid-block crossings are a potential consideration along Red Hill Avenue subject to traffic engineering review, specifically along the blocks of San Juan Street to El Camino Real and Nisson Road to Mitchell Avenue. These two blocks are approximately 1,000 feet in length and the introduction of a mid-block crossing could improve pedestrian access and activity in the project area, if combined with a proposed landscaped median identified in the Red Hill Avenue Streetscape and Median Feasibility Study – El Camino Real to Bryan Avenue (Tait & Associates, Inc., September 10, 2012).



*Existing examples of pedestrian and ADA compliant walkways*



*Majority of sidewalks are not buffered from vehicle traffic*



*Opportunities to provide pedestrian access from the sidewalk to building entrances*



*Some landscaping along sidewalks*

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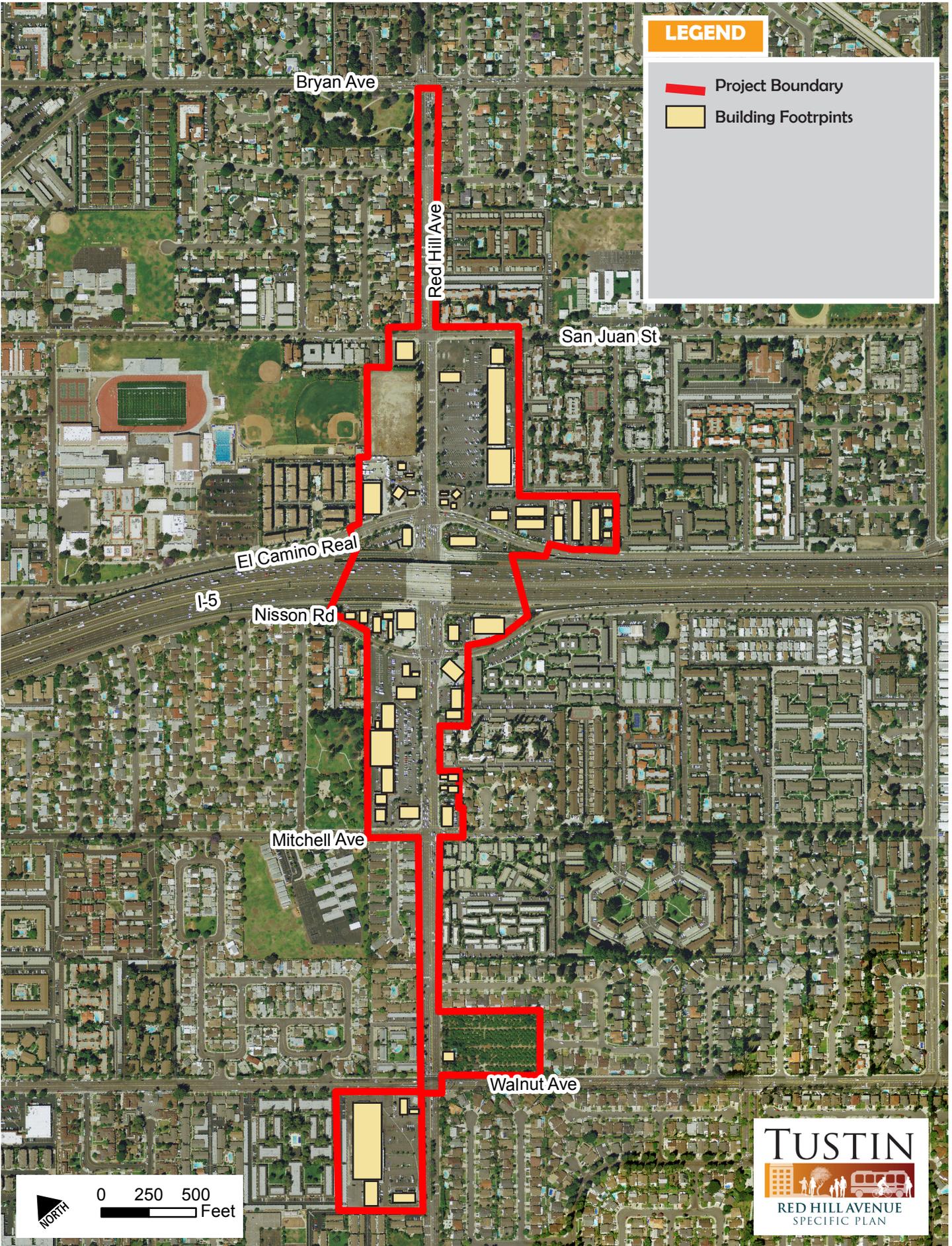


Figure 6.1: Building Footprints

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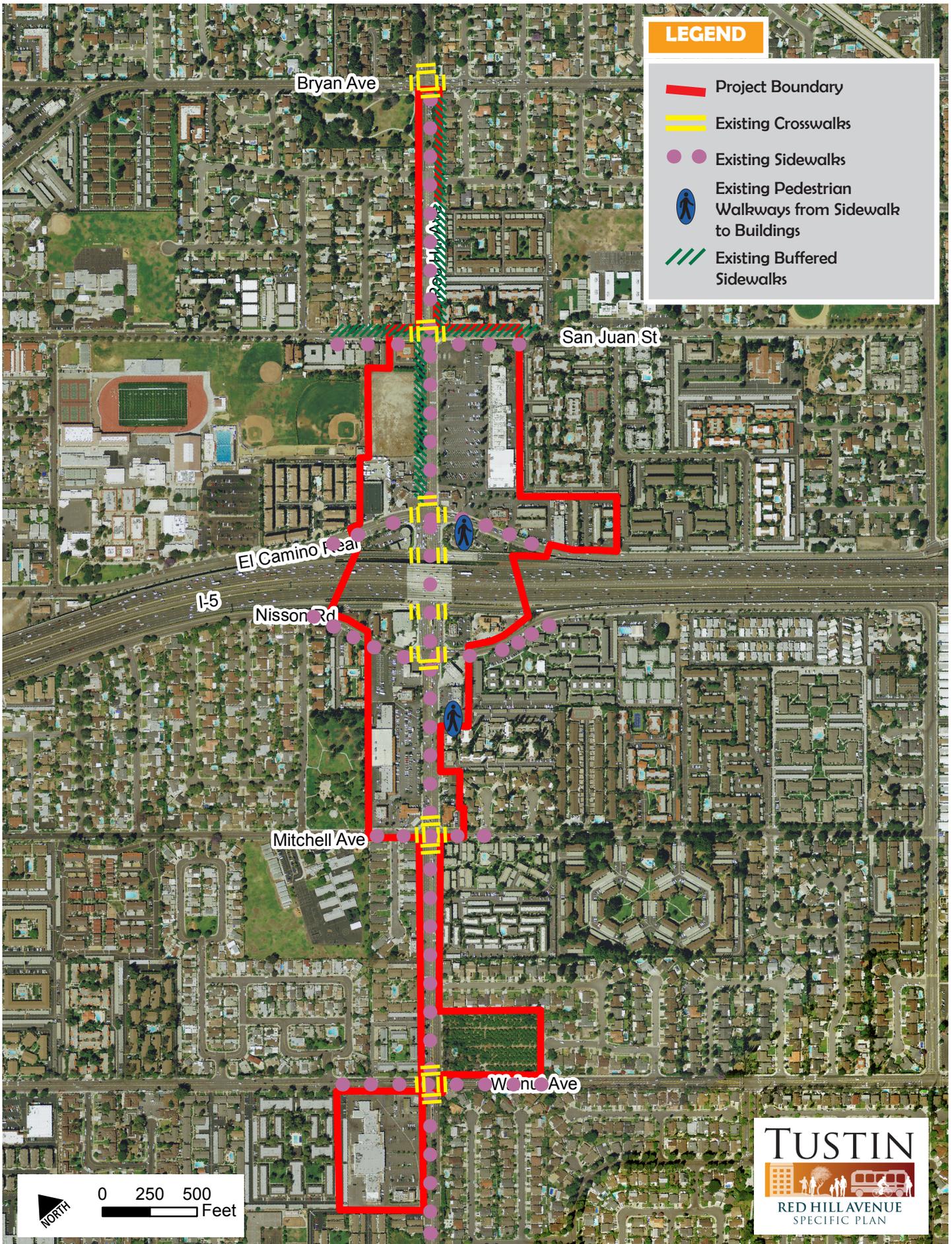


Figure 6.2: Pedestrian Connections

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### Streetscape Character

Streetscape character does not vary widely within the project area. Crosswalks are in overall good condition; however, some sidewalk segments are damaged and may need future improvements. The east side of Red Hill Avenue, north of San Juan Street, is landscaped with grass, plants, and trees adjacent to a multi-family residential building. The west side of Red Hill Avenue between San Juan Street and El Camino Real, directly adjacent to the vacant lot is also landscaped with street trees. The large surface parking lot on the east side of Red Hill Avenue, north of the I-5, contains trees and landscaping. Red Hill Avenue, south of the I-5, is well landscaped with plants and trees around the perimeter of surface parking lots and along the street frontage.

The project area lacks unified street furniture or other “placemaking” enhancements such as benches, trash receptacles, lighting, bicycle racks, planters, pavement markings, etc. There is an existing bus shelter on the west side of Red Hill Avenue, south of the I-5; and two wayfinding signs located on the west side of Red Hill Avenue, north of El Camino Real, and at the OCTA bus stop on El Camino Real indicating the direction of Old Town Tustin. Existing shopping centers with multiple tenants would benefit from a comprehensive sign program to integrate and unify the design of signs throughout the project area. There is also opportunity for gateway signage on the four triangular lots located at the freeway entrances and exits. These parcels provide access points to the northern and southern portions of the project area from a major regional freeway.

Generally, the project area does not have a cohesive streetscape character and is often defined by large open areas of surface parking lots. However, there are existing features scattered throughout the project area that provide useful examples of enhanced pedestrian-friendly streetscape character such as bus shelters, buffered sidewalks, pedestrian walkways from the sidewalk to building entrances, and wayfinding signs.



*Existing bus shelter*



*Existing wayfinding sign to Old Town Tustin*



## Summary of Existing Conditions

- Many buildings within the project area are located at a distance from the sidewalk often to accommodate large areas of surface parking.
- Many commercial and retail businesses are found along Red Hill Avenue with multiple driveways for access, which can pose points of conflict between vehicles, bicyclists, and pedestrians.
- The lack of bicycle lanes within the project area may force bicyclists to share a traffic lane with vehicles or utilize the sidewalk potentially restricting pedestrian movements.
- Pedestrian activity within the project area occurs within surface parking lots at the main retail centers and around the main points of access to public transportation, specifically the OCTA bus stops on El Camino Real.
- Site observations also indicate that people walk through surface parking lots to connect from the sidewalk to commercial and retail building entrances.
- The larger shopping and commercial centers along Red Hill Avenue lack dedicated pedestrian access from the sidewalk to building entrances, though a few restaurant uses along El Camino Real provide dedicated and landscaped pedestrian walkways and ADA access from the sidewalk.
- All marked crosswalks are “standard” painted parallel lines, which may not be as visible as “continental” or “ladder” style crosswalks that may be considered in future improvements.
- Mid-block crossings are a potential consideration along Red Hill Avenue subject to traffic engineering review, specifically along the blocks of San Juan Street to El Camino Real and Nisson Road to Mitchell Avenue. These two blocks are approximately 1,000 feet in length and the introduction of a mid-block crossing could improve pedestrian access and activity in the project area, if combined with a proposed landscaped median identified in the Red Hill Avenue Streetscape and Median Feasibility Study – El Camino Real to Bryan Avenue (Tait & Associates, Inc., September 10, 2012).
- The project area does not have a cohesive streetscape character and is often defined by large open areas of surface parking lot. However, there are existing features scattered throughout the project area that provide useful examples of enhanced pedestrian-friendly streetscape character such as bus shelters, buffered sidewalks, pedestrian walkways from the sidewalk to building entrances, and wayfinding signs.
- Existing shopping centers with multiple tenants would benefit from a comprehensive sign program to integrate and unify the design of signs throughout the project area.

- There is also opportunity for gateway signage on the four triangular lots located at the freeway entrances and exits.
- Zoning provisions and design guidelines for edges and boundaries should be aimed at mitigating any negative impacts that may arise from the presence of contrasting uses and building types.

## 7. Opportunities & Constraints Map

The following map illustrates opportunities and constraints within the Red Hill Avenue Specific Plan area.

- **Streetscape Improvements** – Opportunities for landscaping improvements along Red Hill Avenue, including a potential landscaped median and landscaped buffers along sidewalks to improve pedestrian access and travel along the roadway. Bicycle facility enhancements, such as bike lanes, bike route signs and markers, and accessible bicycle parking, may also be considered. There are existing wayfinding signs for “Old Town Tustin” in the project area. Similar wayfinding signs may be considered within the project area, highlighting important community locations.
- **Linkages with Adjacent Residential Areas** – The project area is surrounded by existing residential neighborhoods. Future development and improvements should create linkages to these residential areas, including pedestrian and bicycle access and improved circulation of vehicle traffic. Additional pedestrian amenities, such as sidewalks, trails, and street lighting should be encouraged in all new development to promote physical activity and decrease the reliance on automotive travel.
- **Intersection and Crosswalk Improvement** – Consider more highly visible “continental” or “ladder” style crosswalks at existing intersections. Decorative paving should also be considered to encourage a more cohesive streetscape in the project area.
- **Gateway Opportunities** – Gateway signage on the four triangular lots located at the Interstate 5 freeway entrances and exits should be considered. Gateway signs delineate a major entrance to a neighborhood, can be used as navigational and wayfinding tools, and promote the branding of the area.
- **Public Transit Stop Improvements** – Pedestrian activity in the project area occurs at existing public transit stops. Future improvements should make transit stops highly visible locations that pedestrians of all abilities can easily reach by means of accessible travel routes. Other treatments that increase the comfort of transit stops include sufficient lighting, sheltered seating and lean bars, trash receptacles, and transit route information.

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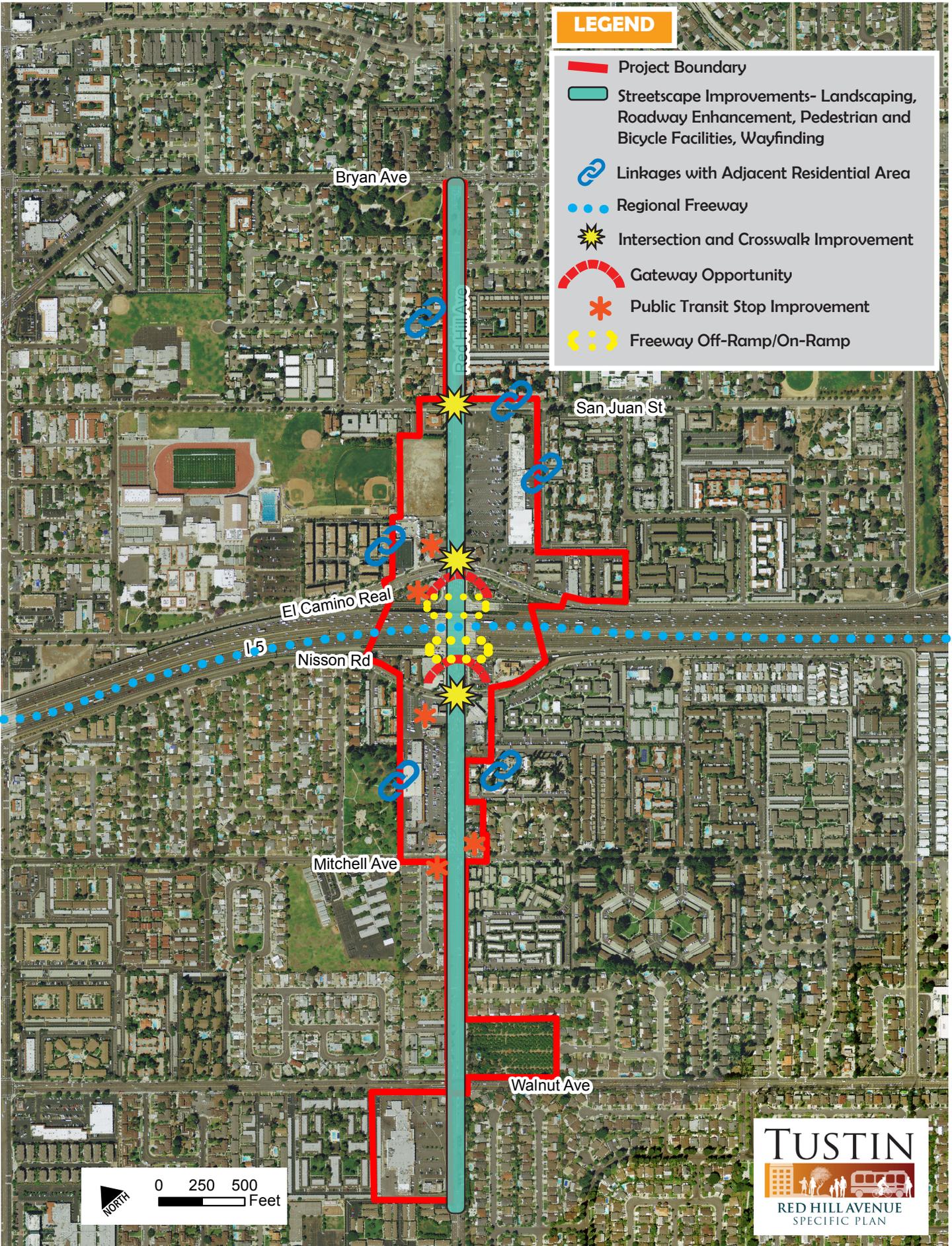


Figure 7.1: Opportunities and Constraints

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B

KOSMONT TUSTIN RED HILL CORRIDOR  
RETAIL PLAN



# Tustin Red Hill Corridor Retail Plan

*September 2017*

***DRAFT***

**Prepared By:  
Kosmont Companies**

# Project Background & Status

- Kosmont and Kimley-Horn were retained by the City of Tustin for the preparation of a market evaluation for the City's Red Hill Corridor as part of the Specific Plan for the region. Kosmont has prepared a Kosmont Retail NOW!<sup>®</sup> Retail Market Analysis, Strategy, and Implementation Plan ("Retail Plan") to support the market evaluation
- The purpose of the Retail Plan is to evaluate existing retail market conditions and provide recommended strategies to successfully promote economic growth within the City
- An overview of the Retail Plan is presented herein

# Kosmont Companies Overview

Kosmont Companies is a nationally-recognized real estate & economics advisory firm specializing in public-private transactions for 30 years.

- **Offers a full range of economics & real estate advisory services including:**
  - Market and Feasibility Analyses
  - Economic Development Strategies & Implementation
  - Identification of Funding Sources & Financing Strategies
  - Fiscal Impact & Economic Benefit Studies
  - Retailer/Developer & Business Recruitment
  - Public-Private Transaction Structuring & Negotiation
- **Winning track record of initiating and implementing projects for municipalities**
  - In-house team includes registered municipal advisors, financial analysts, lawyers, former bond underwriters, former city managers & department heads
  - Extensive network of brokers, investors and market data for real-time information, and retail industry leadership
- **Kosmont is not just a “study” company – we create strategies & make projects a reality**
  - Over \$12 billion in project negotiation and implementation since 1986
  - Completed hundreds of public and private financial transactions blending private investment with public funding sources to create win-win public-private financing structures

**Note:** If needed, Financial Advisory and Real Estate Brokerage services provided by Kosmont Realty Corporation (SEC / MSRB Registered Municipal Advisor and Licensed Real Estate Brokerage Firm)

# Kosmont Services Overview



## KOSMONT COMPANIES SERVICES OVERVIEW



## KRC SERVICES OVERVIEW

### Kosmont Companies Real Estate, Economic Development & Financial Advisory

#### Real Estate / Redevelopment Advisory

- Property Management Plans (PMPs)
- Project Strategies & Financial Evaluation
- Kosmont Retail NOW!<sup>®</sup>
- Entitlements / Due Diligence
- Developer Selection (RFP/Q)

#### Economic Development & Post RDA Strategies

- Economic Development / Strategies and Implementation
- Economic Analysis
- Fiscal Impact Analysis
- Economic Development / Public Purpose Asset Strategies
- Government Management Services

#### Financial Advisory

- Taxable / Tax Exempt Bonds and COPs
- Debt Restructuring / Refinancing
- Public Agency Financial Strategies / Post RDA Services
- Public / Private Transactions (P3)
- Continuing Disclosure Services

### Kosmont Realty Corporation\*\* Real Estate Brokerage Services Public-Private Transactions & Finance

#### Asset Valuation & Transaction Services

- RDA Dissolution Asset Sales
- Fair Market / Broker Opinion of Value
- Acquisitions / Dispositions
- Leases / Ground Leases
- Tenant Recruitment (Retail / Hotels / Employers)

#### Public-Private Project Financial Advisory

- Site-Specific Tax Revenue Agreements
- Lease / Leaseback
- Tax-exempt / Taxable Bond Financing
- Special District Financing
- Public Facilities / Pledged Assets

#### EB-5 Foreign Investment (California Golden Fund)

- Hotel
- Retail / Restaurant
- Office
- Medical / Hospital
- Infrastructure

\*Certified MBE and SBE

04/26/2013

\*\*SEC / MSRB Registered Municipal Advisor  
CA Licensed Real Estate Brokerage Firm

04/26/2013



# Key Personnel



**Larry J. Kosmont, CRE®**  
*President & CEO*

- Over 30 Years of Experience
- City Management (Santa Monica, Seal Beach, Bell Gardens, Burbank)
- Over \$10B in Transactions
- Economic Development
- Public Finance
- Public / Private Partnerships
- Gov't Funding Sources
- Post-Redevelopment Financing Strategies



**Ken K. Hira**  
*Executive Vice President*  
*ICSC Western Division*  
*P3 Retail Chair*

- Over 25 years of experience
- Retail Attraction and Development
- Real Estate Finance
- Transaction Structuring
- Negotiations
- Downtown Revitalization
- Acquisitions/Dispositions
- Market Analysis



**Joseph Dieguez**  
*Senior Vice President*

- Over 9 years of experience
- Market Supply/Demand Studies
- Financial & Feasibility Modeling
- Fiscal Impact / Economic Benefit Analysis
- Asset Valuation
- Financial Turnaround / Fiscal Stabilization Implementation
- Government Staffing and Operations Support

# Plan Outline

## **1. Analysis**

- a) Economic & Demographic Profile
- b) Market Demand Analysis

## **2. Strategy**

- a) Trade Area Retailer Voids
- b) Opportunity Site Assessment

## **3. Implementation**

- a) Summary of Findings
- b) Financing & Incentives

## 1. Analysis

- a) **Economic & Demographic Profile**
  - i. *Population & Household Demographics*
  - ii. *Unemployment & Employment by Industry*
- b) **Market Demand Analysis**
  - i. *Employment Growth by Industry*
  - ii. *Residential Supply, Vacancy & Lease Rates*
  - iii. *Retail Supply, Vacancy & Lease Rates*
  - iv. *Retail Sales Performance*
  - v. *Retail Sales Surplus / Leakage*

# 1. Analysis

## Economic & Demographic Profile

*Population & Household Demographics*

# Demographic Highlights

## Population & Households

- Population of ~80,800 and ~26,400 households within the City in 2015
- Population of ~584,300 and ~175,000 households within 5 miles from Site

## Income

- Avg. HH income ~\$102,400 in City and ~\$103,300 within 5 miles from Site
- 2.53% annual growth projected for HH income over next 5 years in City

## Other Demographic Characteristics

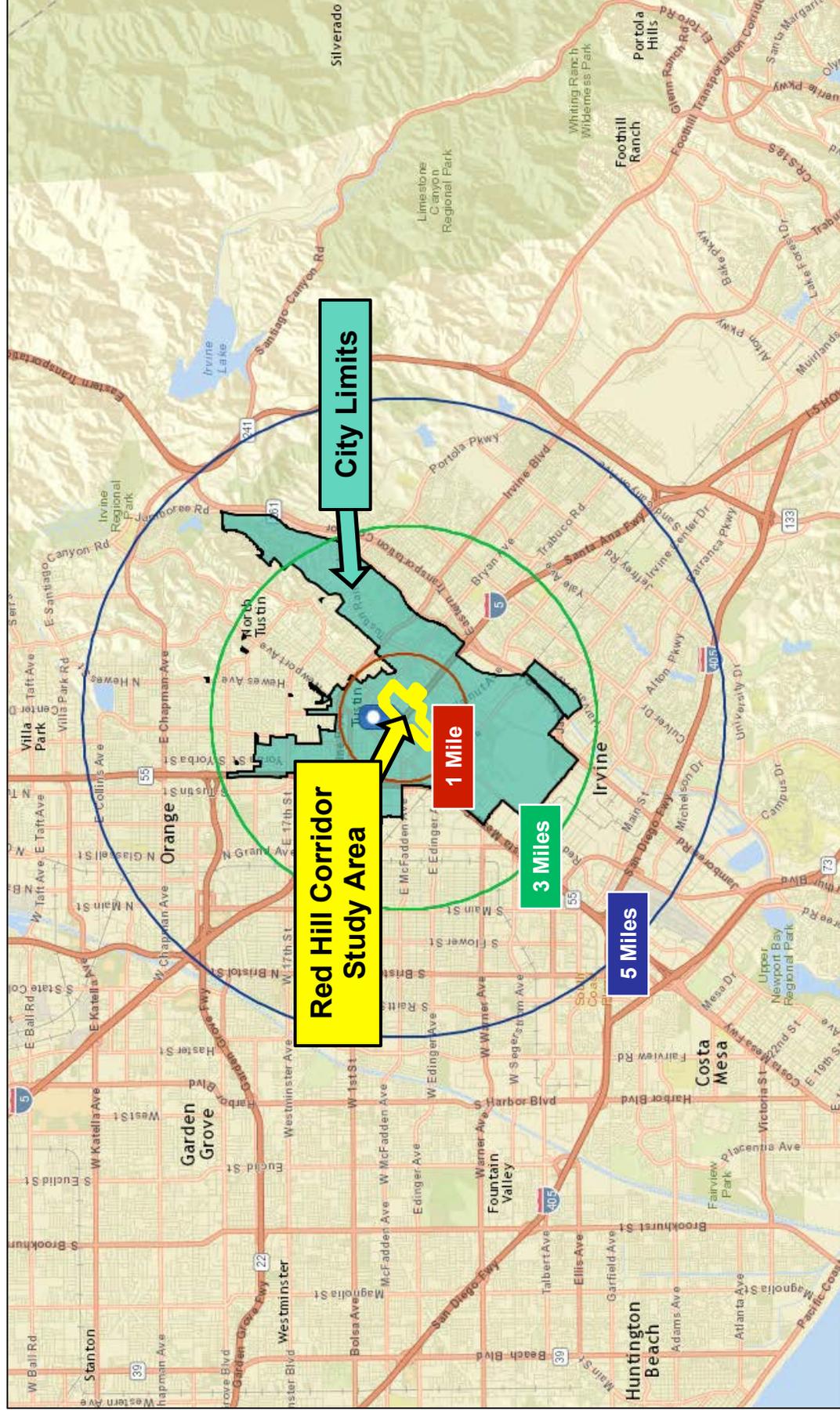
- Average household size of **3.0** in City (average for County)
- Median age of **33** in City (younger than County)
- ~**41%** Bachelor's Degree or higher (higher than County average)
- Race: ~54% White, ~20% Some Other Race, ~5% other / two or more races
- Ethnicity: ~**41%** Hispanic in City

Source: U.S. Census Bureau (2010); ESRI, Department of Finance (2015)

# Red Hill Corridor Study Area



# Tustin City Limits & Radii (Centered at Red Hill Ave. and the 5 Freeway)

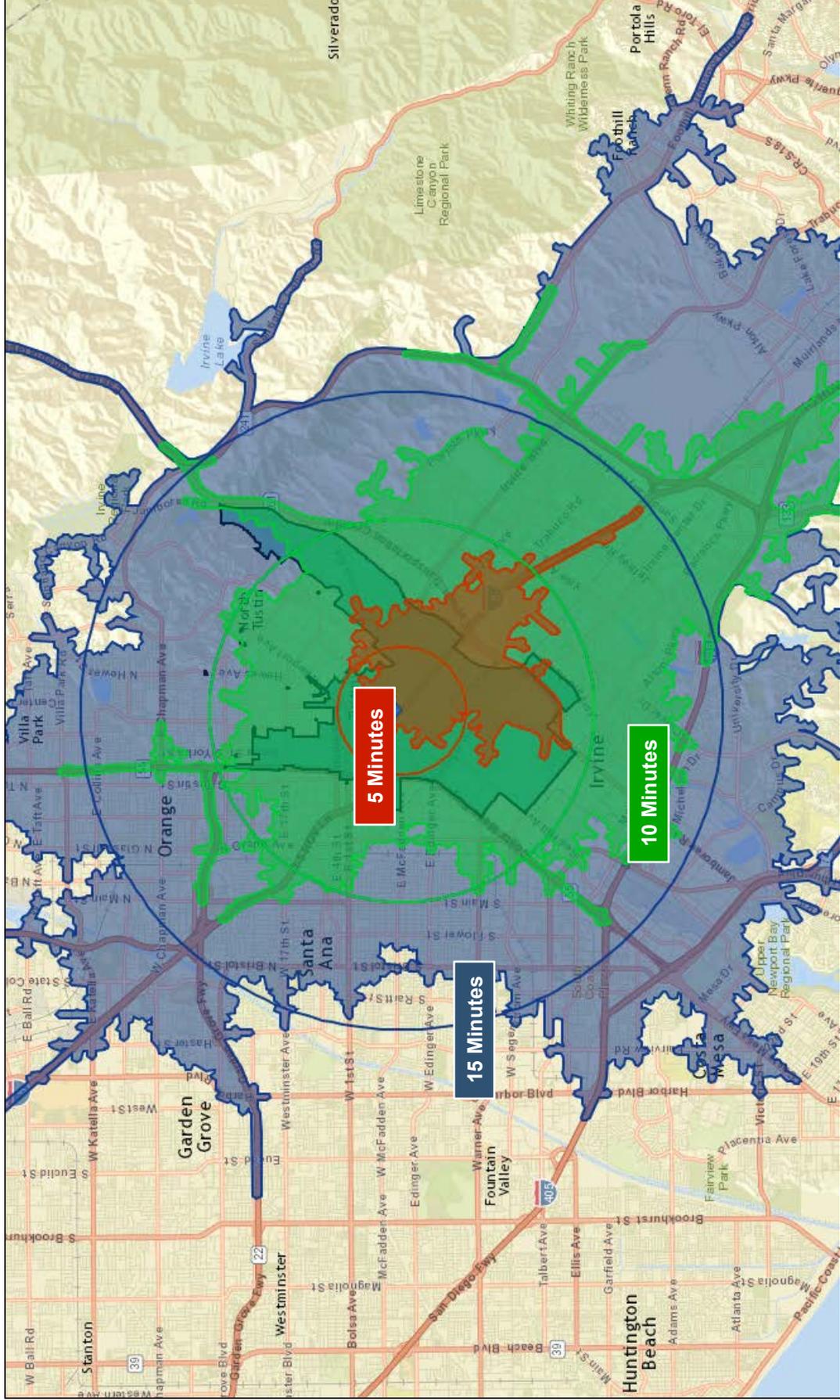


Source: ESRI (2015)

**Kosmont Retail NOW!**



# Drive Times (Centered at Red Hill Ave. and the 5 Freeway)



Source: ESRI (2015)

**Kosmont Retail NOW!**



# Population and Income

## City, County and State

<u>2015</u>	<u>City of Tustin</u>	<u>Orange County</u>	<u>State of California</u>
Population	80,796	3,124,130	38,371,836
Households	26,374	1,026,508	12,932,388
Average HH Size	<b>3.04</b>	3.00	2.90
Median Age	<b>33.4</b>	36.9	35.7
% Hispanic Origin	<b>40.6%</b>	34.8%	39.1%
Per Capita Income	\$33,672	<b>\$35,179</b>	\$29,788
Median HH Income	\$75,011	<b>\$77,676</b>	\$60,382
Average HH Income	\$102,356	<b>\$106,158</b>	\$87,152
<b>2015-2020 Annual Growth Rate</b>			
Population	<b>1.05%</b>	0.82%	0.73%
Median HH Income	2.53%	2.36%	3.36%

Source: ESRI, California Department of Finance (2015)

# Population and Income

## Radii from Red Hill Ave. and the 5 Freeway

	Radii		
	1 Mile	3 Miles	5 Miles
<b>2015</b>			
<b>Population</b>	36,260	<b>211,802</b>	<b>584,304</b>
<b>Households</b>	11,476	<b>63,709</b>	<b>174,959</b>
<b>Average HH Size</b>	3.14	<b>3.30</b>	3.29
<b>Median Age</b>	<b>33.5</b>	<b>33.5</b>	33.3
<b>% Hispanic Origin</b>	48.8%	45.9%	48.7%
<b>Per Capita Income</b>	\$27,102	<b>\$32,736</b>	\$31,238
<b>Median HH Income</b>	\$66,170	<b>\$77,551</b>	\$76,881
<b>Average HH Income</b>	\$85,078	<b>\$108,264</b>	\$103,255
<b>2015-2020 Annual Growth Rate</b>			
<b>Population</b>	0.79%	0.79%	0.83%
<b>Median HH Income</b>	2.77%	2.28%	2.20%

Source: ESRI (2015)

# Population and Income

## Drive Times from Red Hill Ave. and the 5 Freeway

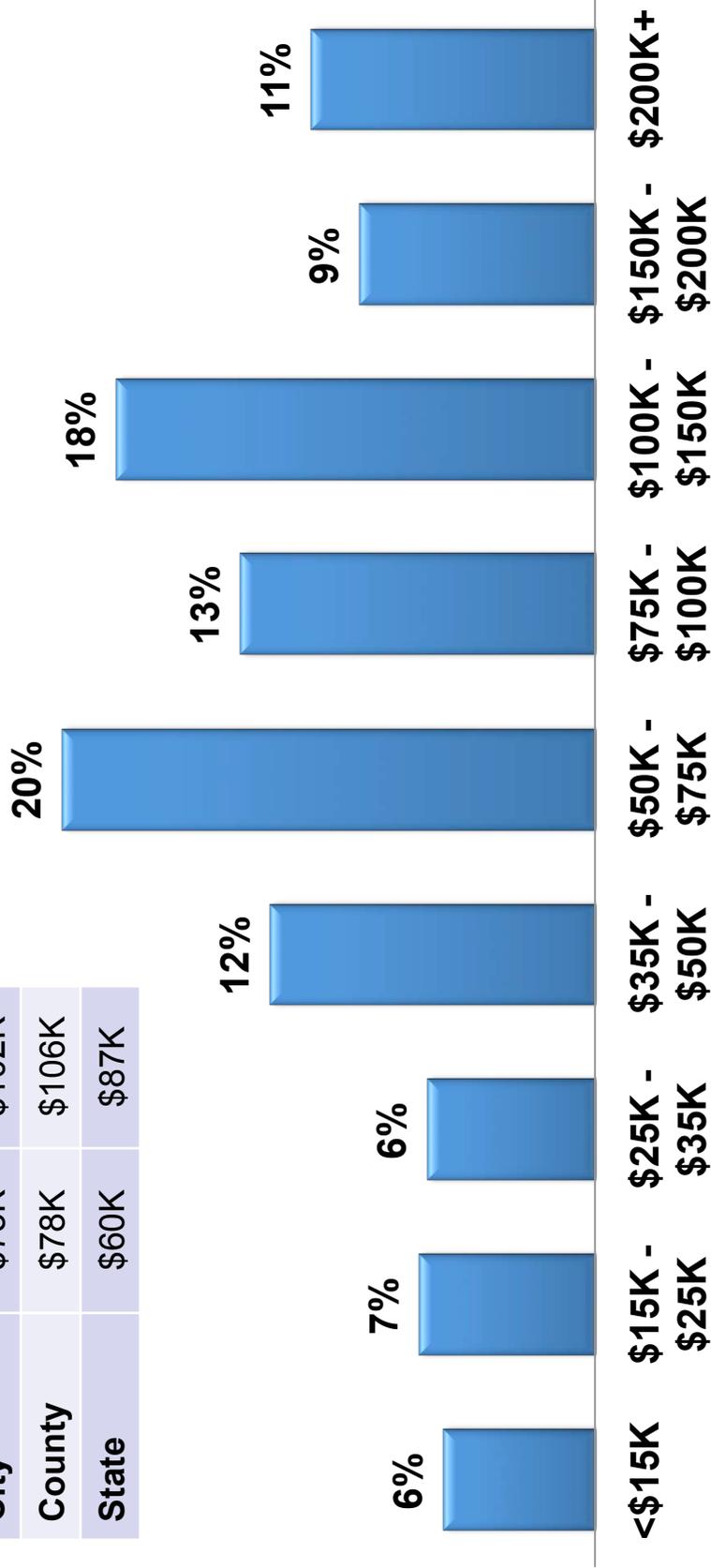
	Drive Times		
	5 Minutes	10 Minutes	15 Minutes
<b>2015</b>			
Population	42,808	<b>288,940</b>	<b>756,368</b>
Households	14,880	<b>99,705</b>	<b>242,811</b>
Average HH Size	2.87	2.88	<b>3.04</b>
Median Age	35.4	35.3	34.2
% Hispanic Origin	26.8%	28.3%	<b>41.5%</b>
Per Capita Income	<b>\$37,951</b>	<b>\$39,903</b>	\$33,506
Median HH Income	<b>\$86,765</b>	<b>\$86,215</b>	\$77,428
Average HH Income	<b>\$110,587</b>	<b>\$115,006</b>	\$102,976
<b>2015-2020 Annual Growth Rate</b>			
Population	1.04%	<b>1.16%</b>	1.12%
Median HH Income	<b>2.58%</b>	2.44%	2.26%

Source: ESRI (2015)

# Income Profile

City of Tustin – 2015 Households by Income Bracket

HH Income	Median	Avg.
City	\$75K	\$102K
County	\$78K	\$106K
State	\$60K	\$87K

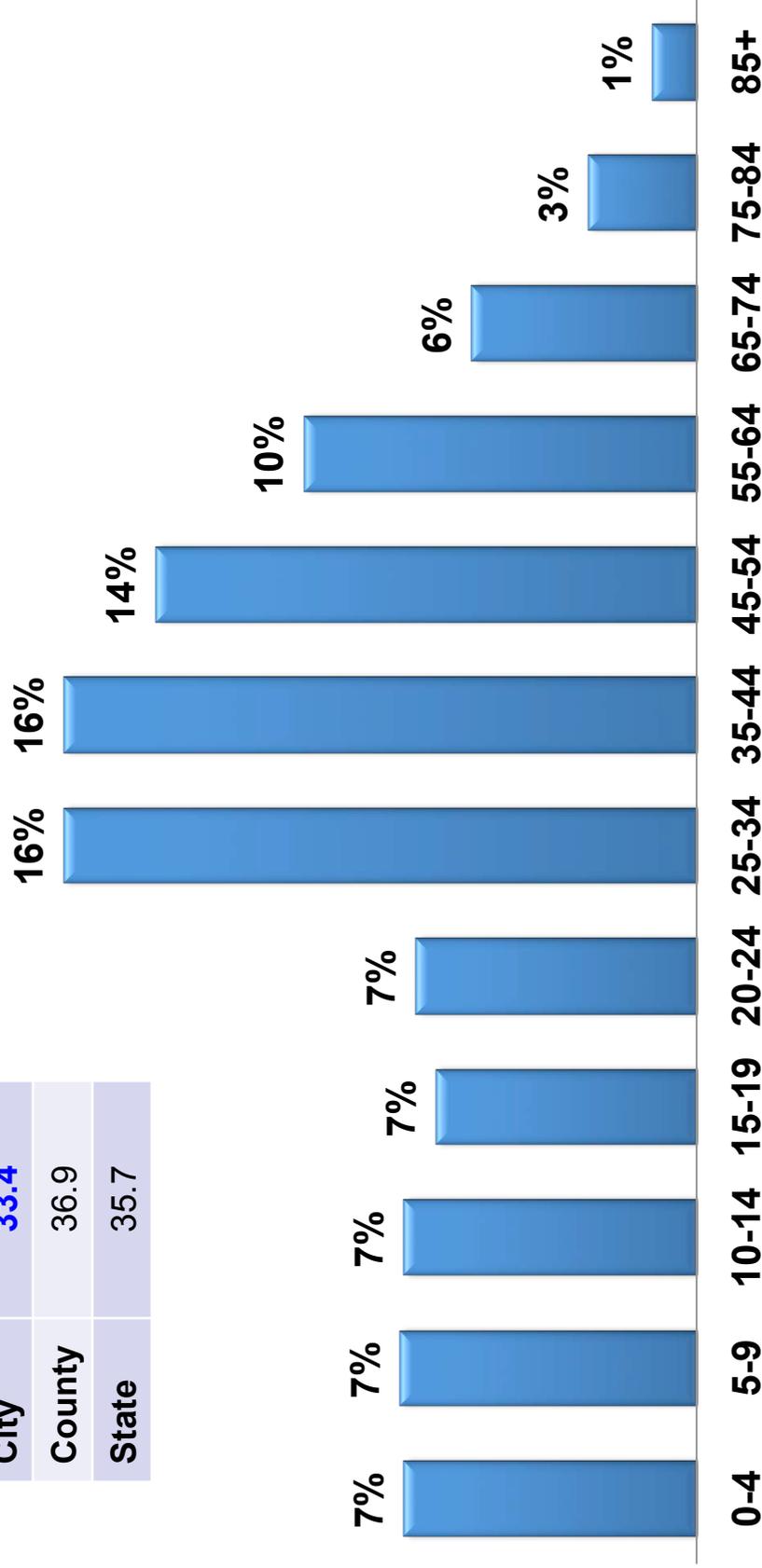


Source: U.S. Census Bureau (2010); ESRI (2015)

# Age Profile

City Population by Age Bracket in 2015

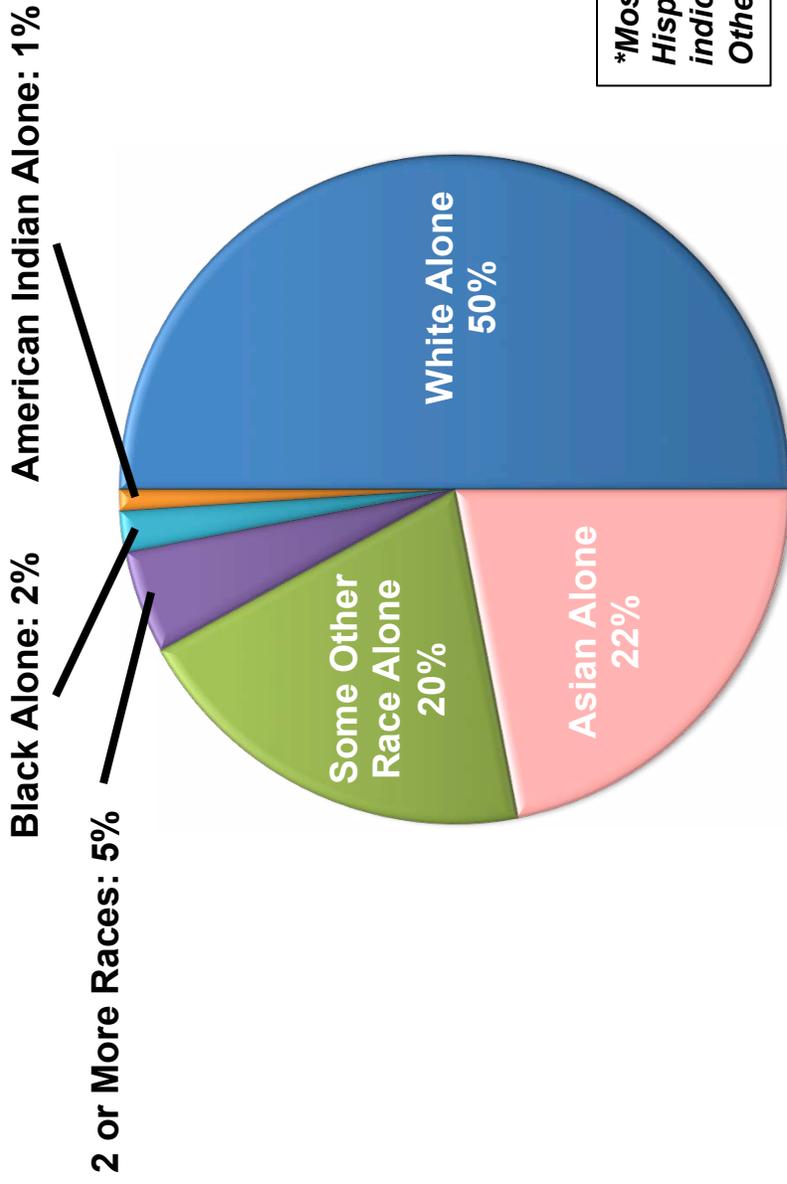
	Median Age
City	33.4
County	36.9
State	35.7



Source: U.S. Census Bureau (2010); ESRI (2015)

# Race & Ethnicity

## City Population by Race & Ethnicity in 2015



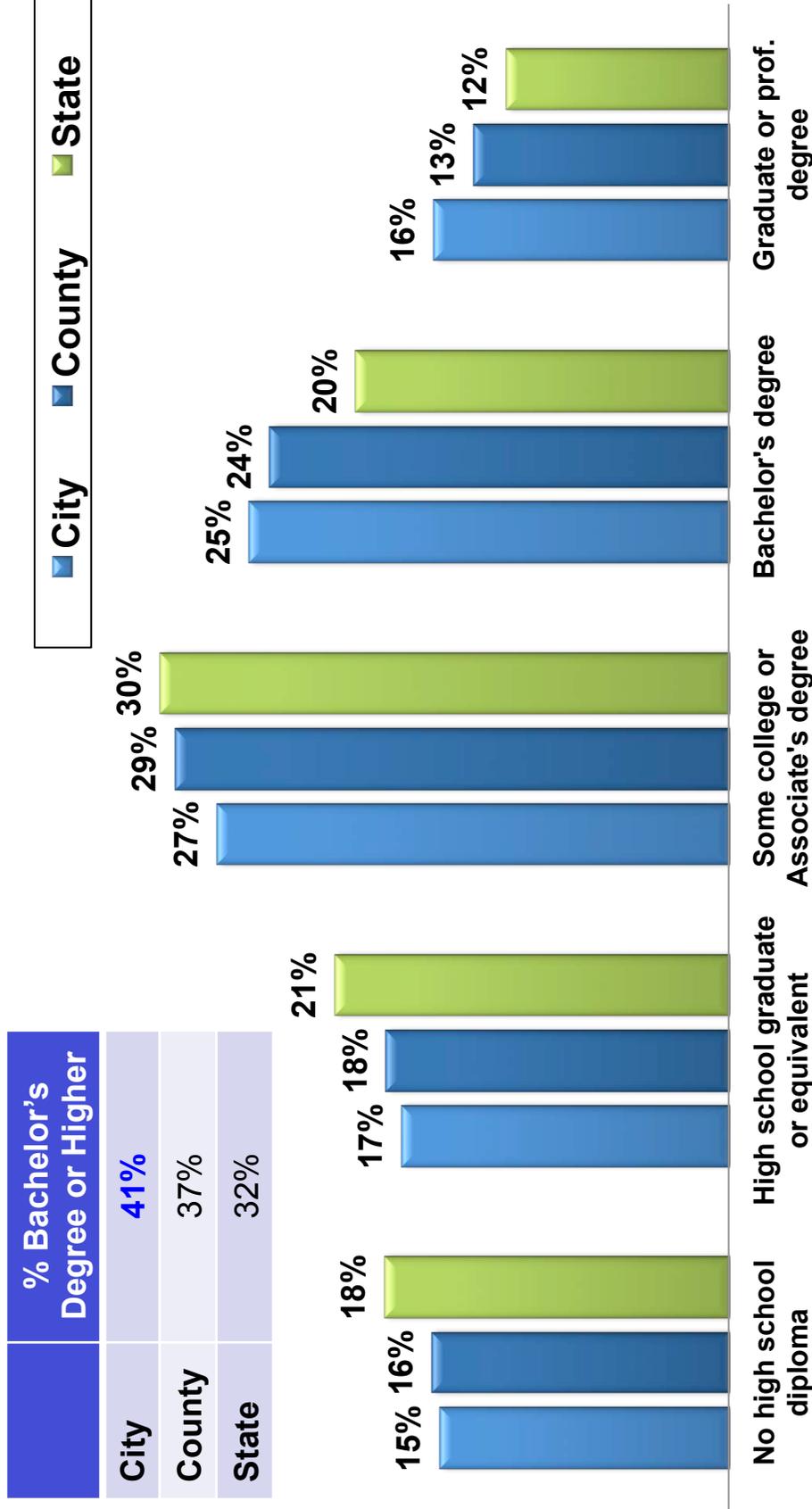
## **Hispanic Origin of Any Race: 41%**

**Note:** U.S. Census Bureau defines race and ethnicity as two separate and distinct identities. One Census question asks respondents which socio-political race (of categories in pie chart above) they associate most closely with, and a separate question asks whether they associate with "Hispanic, Latino, or Spanish origin" or not (defined as ethnicity).

**Source:** U.S. Census Bureau (2010); ESRI (2015)

# Educational Attainment

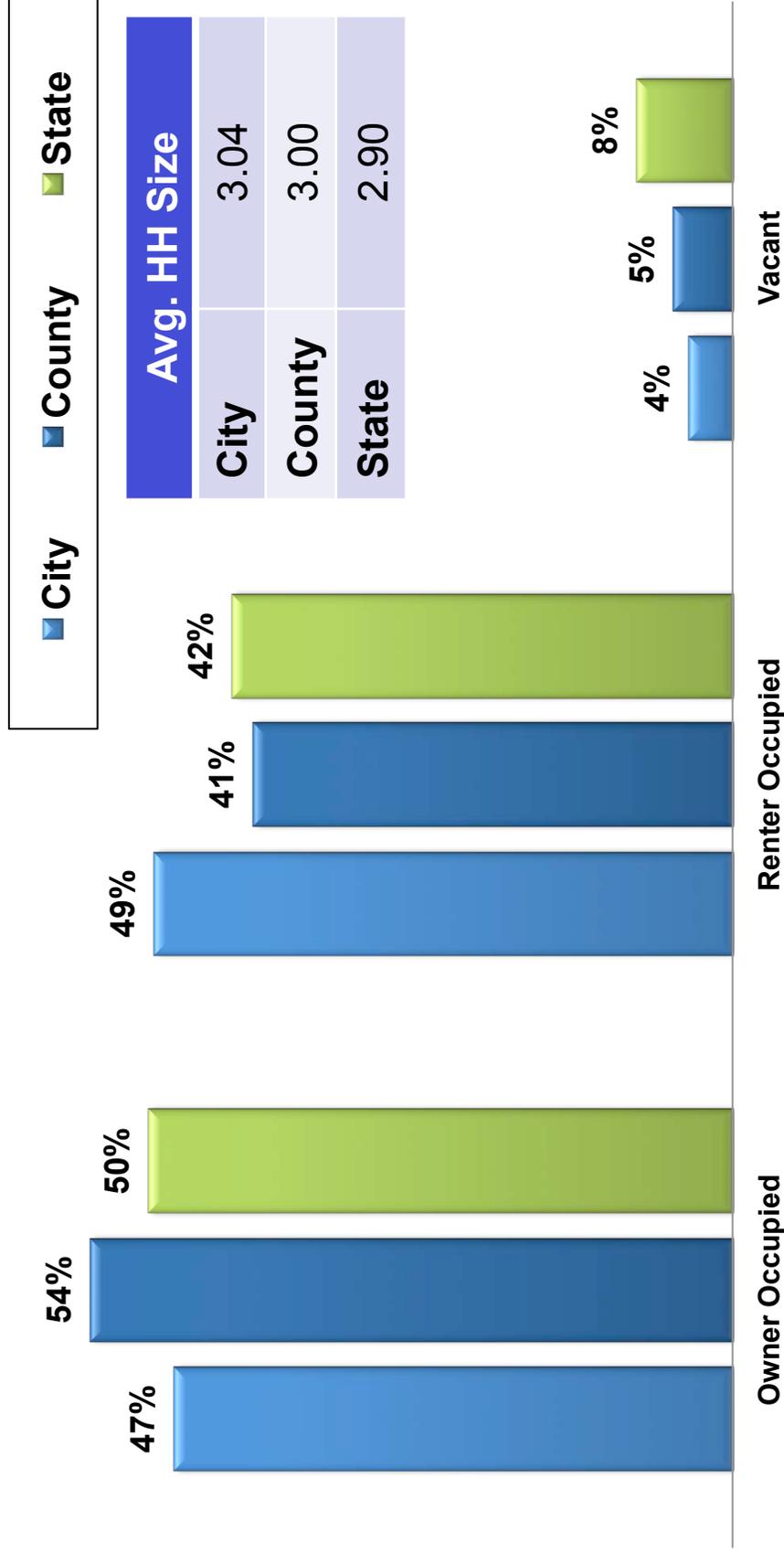
**Population Aged 25+ by Educational Attainment**



Source: U.S. Census Bureau (2010); ESRI (2015)

# Housing & Household Size

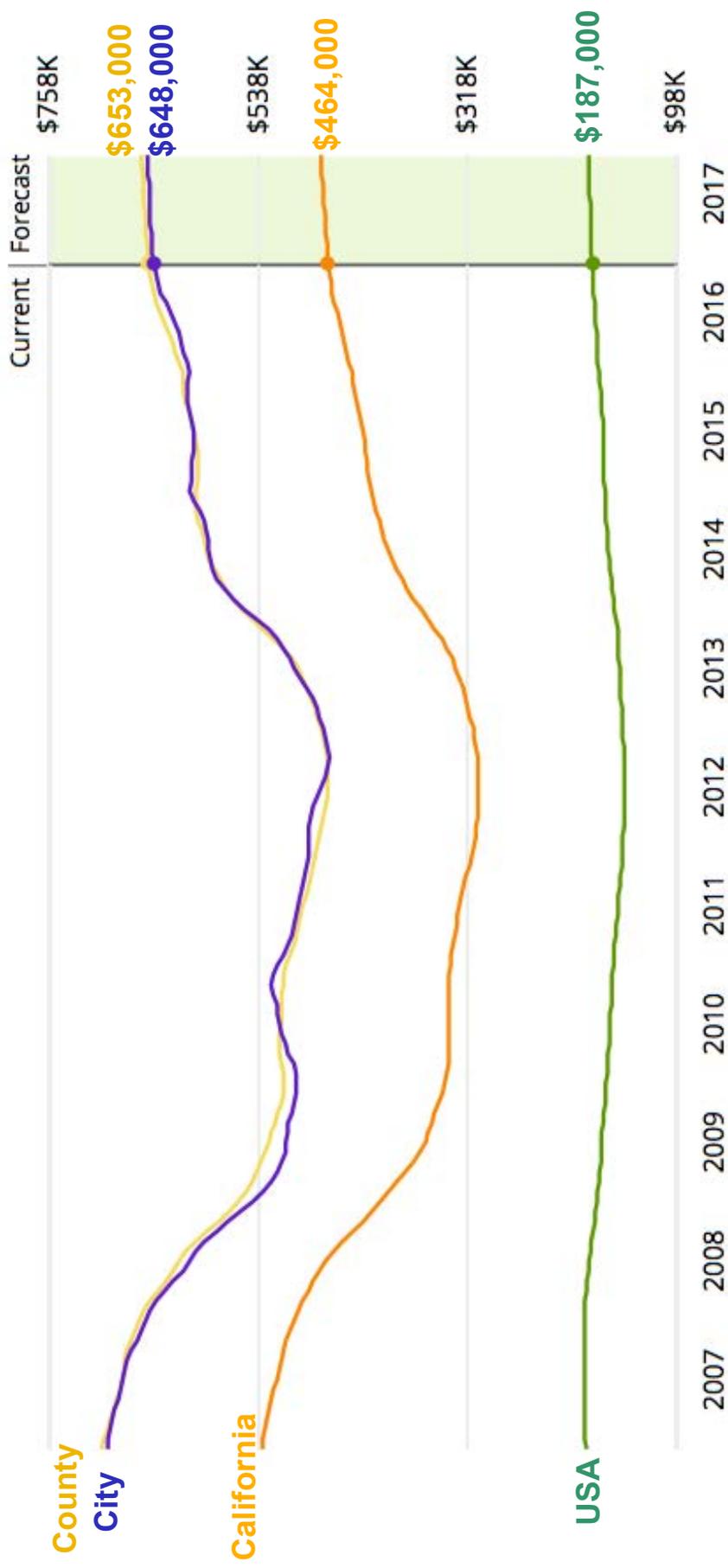
## Housing Breakdown (2015)



Source: U.S. Census Bureau (2010); ESRI (2015)

# Home Value History

## Zillow Home Value Index



Source: Zillow.com (May 2016)

# Population Segmentation Profile

“Tapestries” in City	Percent	Sample Characteristics
<p><b>1. International Marketplace</b></p>	<p>22%</p>	<ul style="list-style-type: none"> <li>• Young, many foreign-born</li> <li>• Highly diverse</li> <li>• Shop at warehouse/club stores and specialty markets</li> <li>• Consumers are attentive to personal style and purchases reflect their youth and their children</li> <li>• Spanish language media influence (music, television, websites)</li> </ul>
<p><b>2. Enterprising Professionals</b></p>	<p>18%</p>	<ul style="list-style-type: none"> <li>• Well educated, wealthier population</li> <li>• Larger than average Asian population</li> <li>• Entertainment includes gambling, trips to the beach, and watching movies on demand or streaming videos online</li> <li>• Convenience is key - many shop at Amazon.com, pick up drugs at the Target pharmacy, and frequent dry cleaners</li> <li>• Food preferences include: The Cheesecake Factory, Chick-fil-A, and Starbucks</li> </ul>

Source: ESRI (2015)

# Jobs / Housing Balance

<b>2015</b>	<b>City of Tustin</b>	<b>Orange County</b>	<b>State of California</b>
Employment	42,107	1,687,209	16,840,429
Households	26,374	1,026,508	12,932,388
<b>Jobs/Housing Ratio</b>	<b>1.60</b>	<b>1.64</b>	<b>1.30</b>

Source: ESRI (2015)

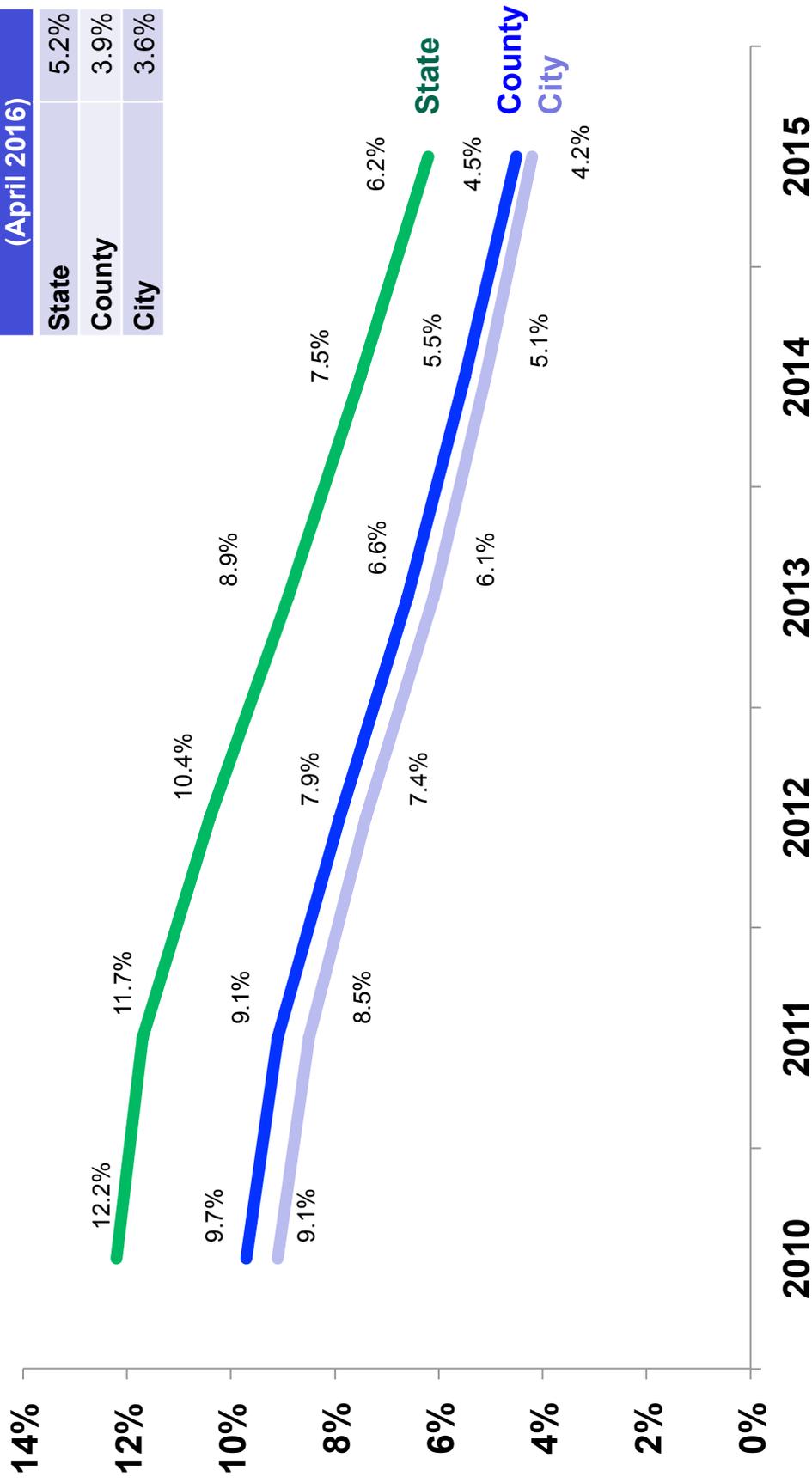
# 1. Analysis

## Economic & Demographic Profile

*Unemployment & Employment by Industry*

# Unemployment

Unemployment Rate (April 2016)	
State	5.2%
County	3.9%
City	3.6%

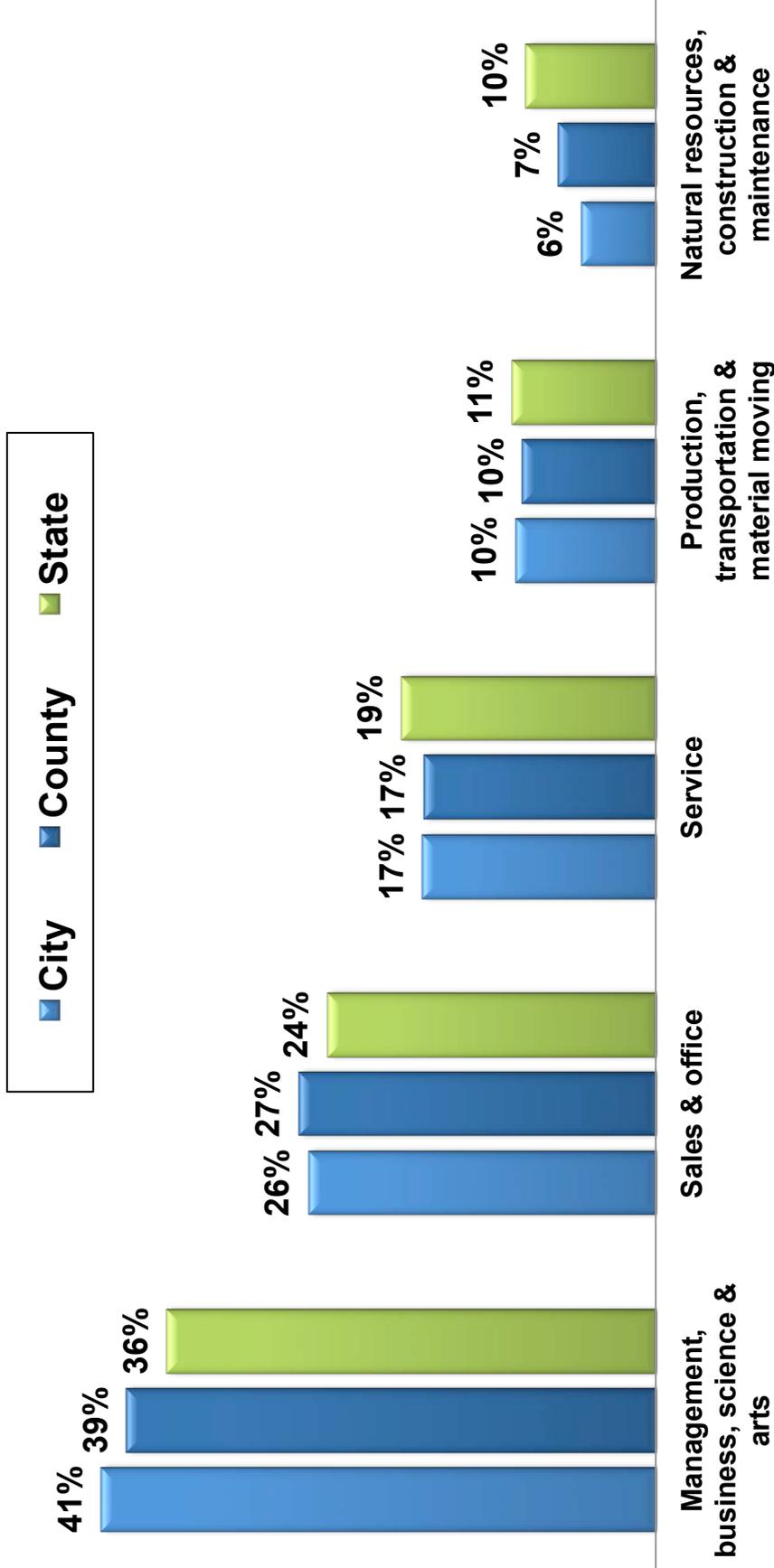


Note: Not seasonally adjusted; annual averages

Source: California Employment Development Department (2015)

# Resident Employment by Occupation

Civilian Employed Population Age 16+ by Occupation



Source: U.S. Census Bureau (2010); ESRI (2015)

# Employment by Industry

City Resident Employed Population (Age 16+)	
Health Care and Social Assistance	11.3%
Retail Trade	9.9%
Professional, Scientific, and Tech. Services	9.8%
Manufacturing	9.6%
Accommodation and Food Services	9.2%
Administration and Support, Waste Mgmt.	7.6%
Educational Services	7.5%
Wholesale Trade	6.1%
Finance and Insurance	5.5%
Construction	4.3%
Public Administration	3.5%
Other Services (excl. Public Admin.)	3.0%
Information	2.7%
Real Estate and Rental/Leasing	2.5%
Transportation and Warehousing	2.3%
Mgmt. of Companies and Enterprises	2.1%
Arts, Entertainment, and Recreation	2.0%
Agriculture, Forestry, Fishing, and Hunting	0.7%
Utilities	0.5%
Mining, Quarrying, Oil and Gas Extraction	0.1%

*“Industries in which City residents work”*

Workers Employed Within City	
Retail Trade	15.6%
Professional, Scientific, and Tech. Services	12.0%
Accommodation and Food Services	10.3%
Administration and Support, Waste Mgmt.	9.9%
Manufacturing	8.7%
Health Care and Social Assistance	8.0%
Finance and Insurance	7.3%
Educational Services	6.1%
Wholesale Trade	5.9%
Information	4.1%
Other Services (excl. Public Admin.)	3.6%
Construction	3.0%
Real Estate and Rental/Leasing	2.8%
Transportation and Warehousing	0.8%
Public Administration	0.7%
Mgmt. of Companies and Enterprises	0.7%
Arts, Entertainment, and Recreation	0.3%
Mining, Quarrying, Oil and Gas Extraction	0.0%
Agriculture, Forestry, Fishing, and Hunting	0.0%
Utilities	0.0%

*“Jobs in the City”*

Source: U.S. Census Bureau Center for Economic Studies (2013)

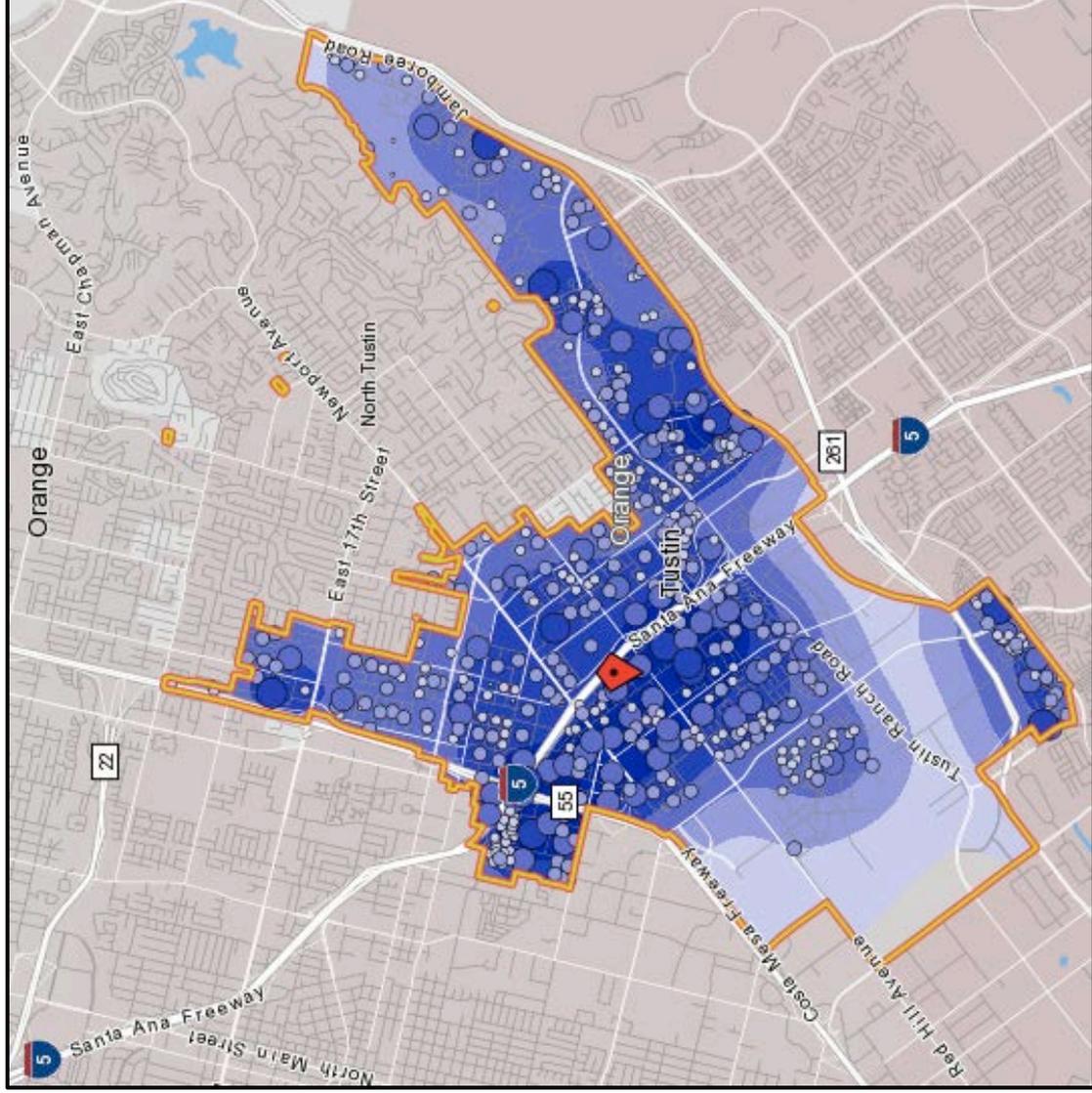
# Select Major Employers within the City

Major Employers	No. of Employees
Tustin Unified School District	1,449
Rockwell Collins Inc.	600
Ricoh Electronics Inc.	500
Costco	450
City of Tustin	372
Newport Specialty Hospital	300
Tustin Hospital Medical Center	300
Toshiba America Medical Systems	300
Micro Vention Inc.	300
Balboa Water Group	253

**Note:** Top 10 listed by number of employees (high to low)

**Source:** City of Tustin (CAFR 2014-2015)

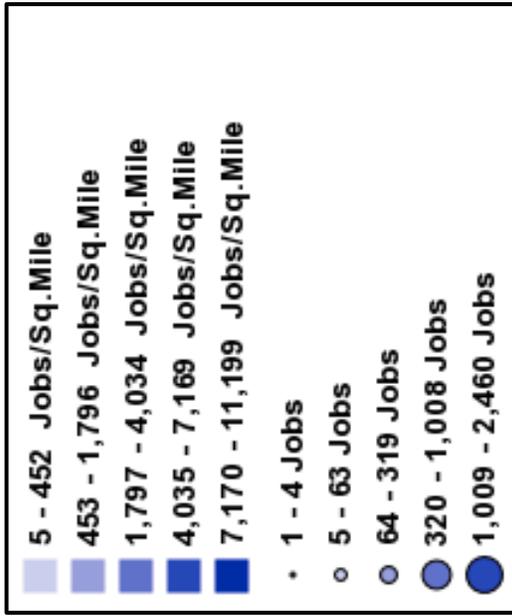
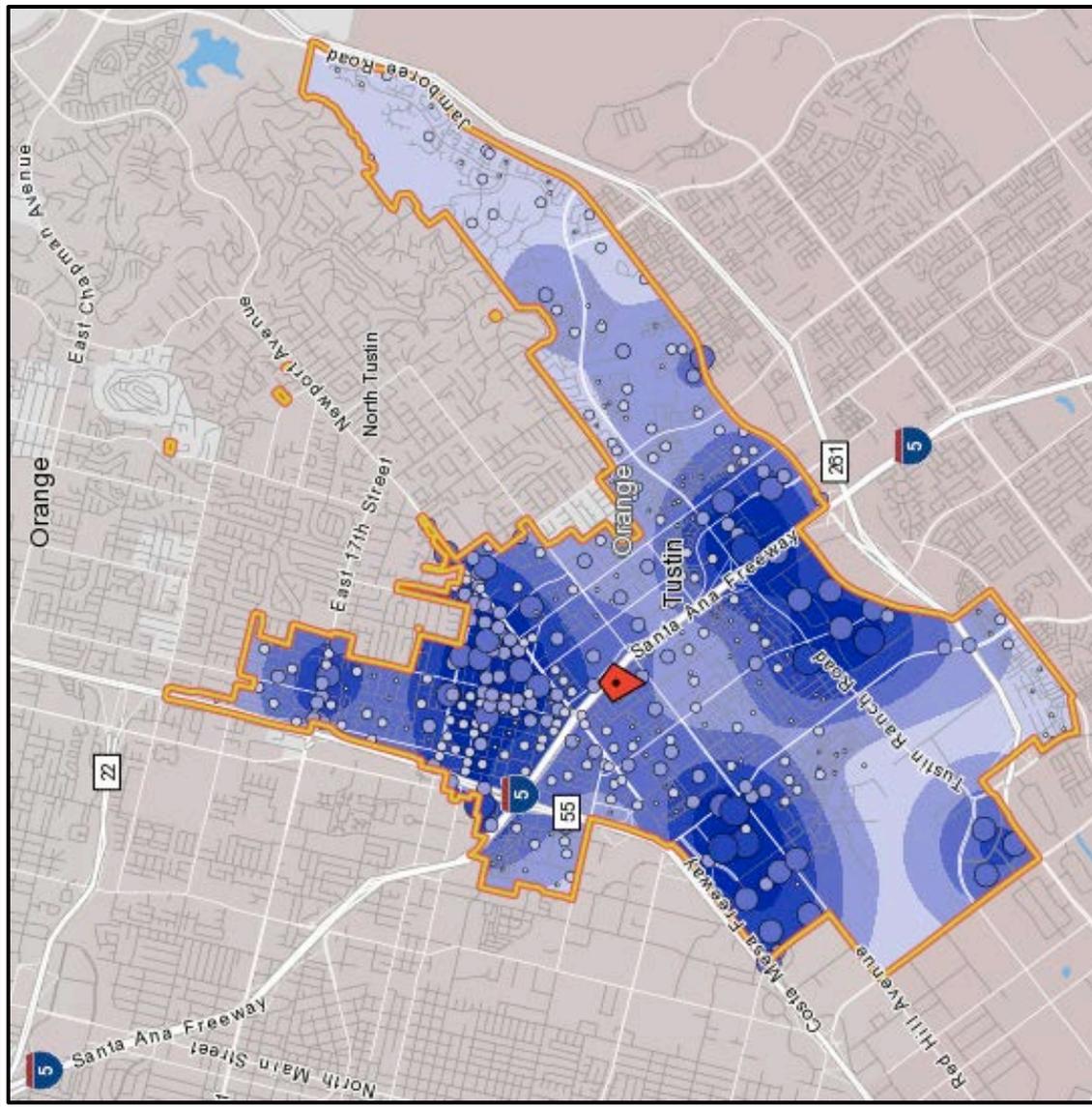
# Resident Concentration Within City



5 - 359	Employed residents/Sq. Mile
360 - 1,421	Employed residents/Sq. Mile
1,422 - 3,193	Employed residents/Sq. Mile
3,194 - 5,672	Employed residents/Sq. Mile
5,673 - 8,861	Employed residents/Sq. Mile
•	1 - 2 Employed residents
○	3 - 27 Employed residents
○	28 - 135 Employed residents
●	136 - 427 Employed residents
●	428 - 1,042 Employed residents

Source: U.S. Census Bureau Center for Economic Studies (2013)

# Employment Concentration Within City



Source: U.S. Census Bureau Center for Economic Studies (2013)

# Resident and Employee Commute

Employed Residents Place of Work	
Irvine	15.7%
Santa Ana	9.8%
Tustin	6.0%
Anaheim	5.1%
Los Angeles	5.0%
Orange	4.9%
Newport Beach	4.0%
Costa Mesa	3.9%
San Diego	2.0%
Lake Forest	1.9%
Huntington Beach	1.8%
Garden Grove	1.4%
Fullerton	1.3%
Mission Viejo	1.2%
Long Beach	1.2%
Fountain Valley	1.1%
Brea	1.1%
Riverside	0.9%
Laguna Hills	0.9%
Corona	0.8%
San Francisco	0.7%
Aliso Viejo	0.7%
El Segundo	0.7%
Ontario	0.7%
Laguna Niguel	0.6%
Other	26.7%

City Employee Origin	
Santa Ana	8.1%
Anaheim	5.8%
Irvine	5.5%
Tustin	5.4%
Orange	4.0%
Los Angeles	3.9%
Garden Grove	2.9%
Huntington Beach	2.9%
Costa Mesa	2.3%
North Tustin CDP	2.0%
Mission Viejo	2.0%
Lake Forest	2.0%
San Diego	1.7%
Fullerton	1.7%
Long Beach	1.6%
Corona	1.3%
Westminster	1.3%
Newport Beach	1.3%
Aliso Viejo	1.1%
Rancho Santa Margarita	1.1%
Yorba Linda	1.1%
Riverside	1.0%
Fountain Valley	1.0%
Buena Park	0.9%
Laguna Niguel	0.9%
Other	37.4%

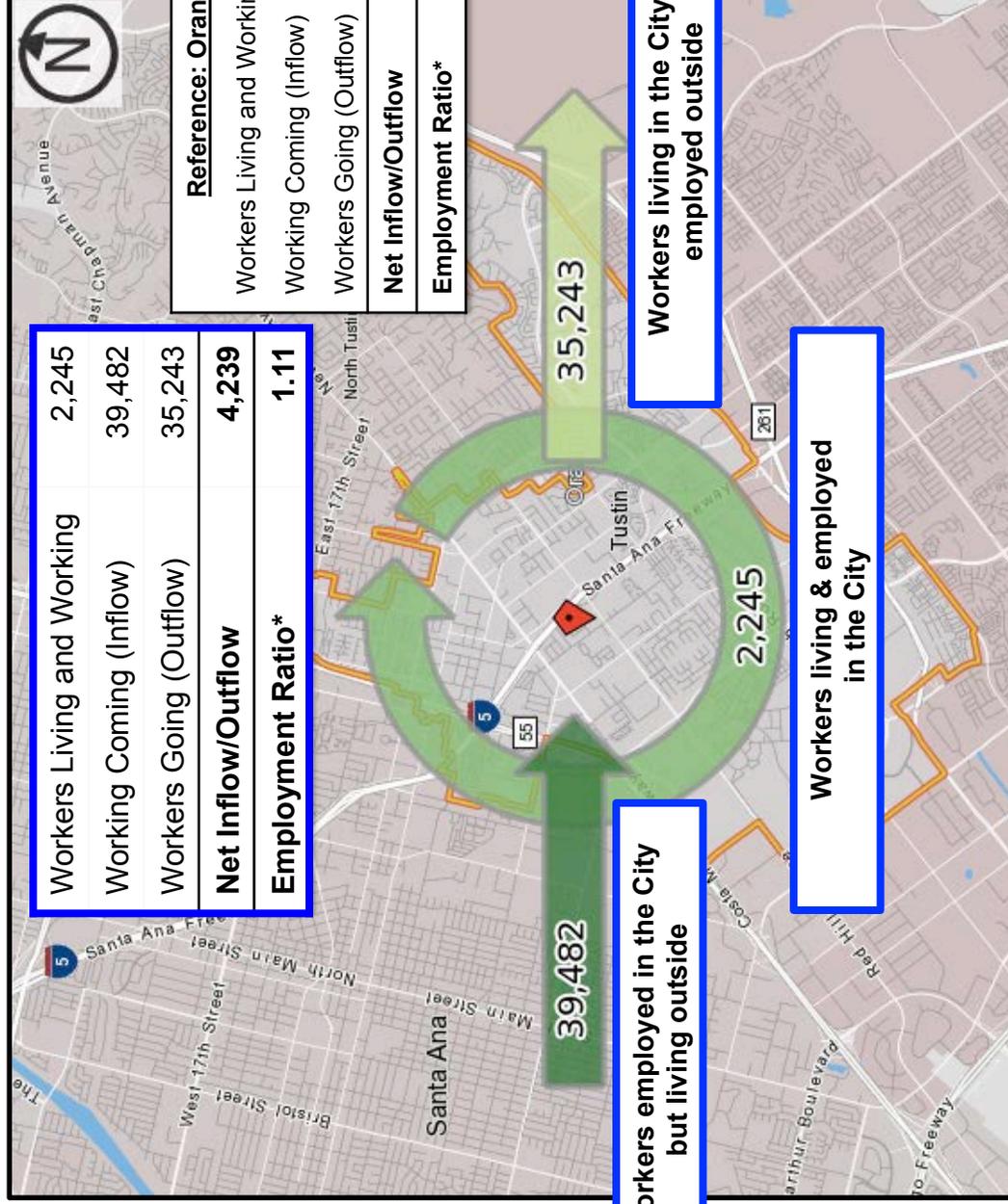
*“Where City residents work”*

Source: U.S. Census Bureau Center for Economic Studies (2013)

*“Where people who work in the City come from”*

# Worker Inflow / Outflow

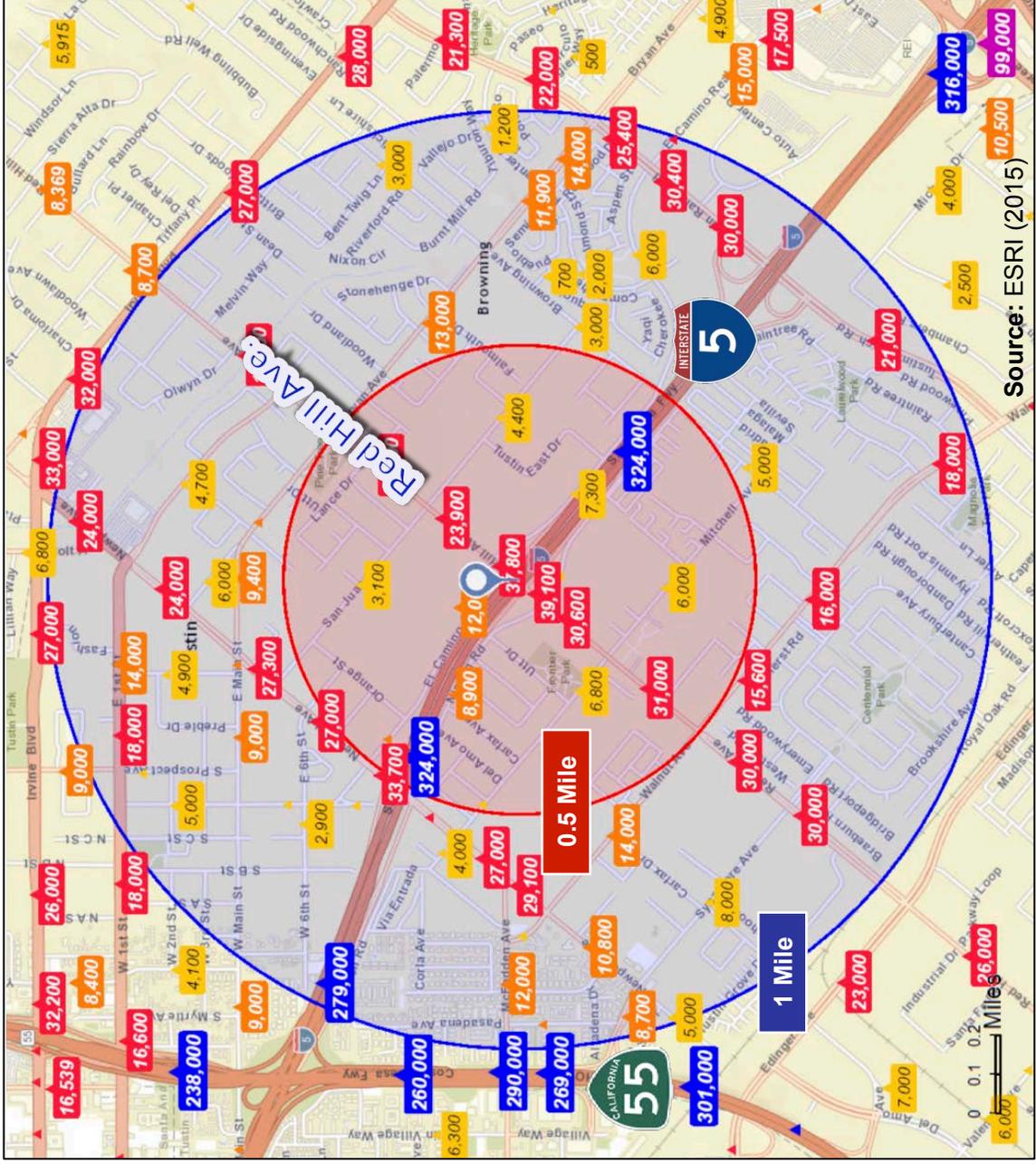
## “Are jobs coming or going”



\***Employment Ratio** = People employed within City (living and working in City + those who come into the City for work) / Employed population of City (living and working in City + workers who live in the City, but work outside of the City)  
**Source:** U.S. Census Bureau Center for Economic Studies (2013)

# Traffic Map

## Radii from Red Hill Ave. and the 5 Freeway



Source: ESRI (2015)

# Summary: Demographics and Employment

- Relatively young, diverse population; HH size on par with Orange County, slightly higher than the State average
- Lower unemployment compared to State and slightly less than Orange County, with most workers in the city employed in retail, professional services, accommodation and food services, administration and support, manufacturing, and healthcare
- Most employees who work in the City come from Irvine, Santa Ana, Tustin, Anaheim, and Los Angeles, yielding an overall net **inflow** of jobs

# 1. Analysis

## Market Demand Analysis

*Employment Growth by Industry*

# Employment Projections by Industry

## Orange County

Industry	2012	2022	Annual Growth 2012-22	Total Growth 2012-22	Total Change 2012-22
Professional and Business Services	259,900	335,000	2.9%	28.9%	75,100
Health Care and Social Assistance	149,100	185,900	2.5%	24.7%	36,800
Accommodation and Food Services	141,300	174,700	2.4%	23.6%	33,400
Retail Trade	143,900	168,400	1.7%	17.0%	24,500
Construction	71,400	95,700	3.4%	34.0%	24,300
Financial Activities	108,200	132,400	2.2%	22.4%	24,200
Wholesale Trade	76,900	96,000	2.5%	24.8%	19,100
Educational Services (Private)	24,700	30,400	2.3%	23.1%	5,700
Government	147,900	153,500	0.4%	3.8%	5,600
Arts, Entertainment, and Recreation	39,300	44,600	1.3%	13.5%	5,300
Information	24,300	27,300	1.2%	12.3%	3,000
Transportation and Warehousing	24,000	25,100	0.5%	4.6%	1,100
Utilities	4,000	5,000	2.5%	25.0%	1,000
Other Services	44,600	44,600	0.0%	0.0%	0
Mining and Logging	500	400	-2.0%	-20.0%	(100)
Manufacturing	158,200	150,900	-0.5%	-4.6%	(7,300)
Total Nonfarm	1,148,100	1,669,900	1.8%	17.8%	251,800
Total Farm	2,800	3,300	1.8%	17.9%	500
Other	103,000	116,100	1.3%	12.7%	13,100
<b>Total Employment</b>	<b>1,523,900</b>	<b>1,789,300</b>	<b>1.7%</b>	<b>17.4%</b>	<b>265,400</b>

Source: California Employment Development Department, U.S. Bureau of Labor Statistics (2012)

# 1. Analysis

## Market Demand Analysis

*Residential Supply, Vacancy & Lease Rates*

# Multifamily Housing

## Radii from Red Hill Ave. and the 5 Freeway

	Radii		
	1 Mile	3 Miles	5 Miles
<b>June 2016</b>			
<b>Existing Housing Units</b>	5,120	23,023	62,955
<b>Vacant Units</b>	155	774	2,012
<b>Vacancy Rate</b>	3.6%	3.9%	3.8%
<b>Average Sale Price Per Unit</b>	\$216,000	\$216,606	\$229,896
<b>Average Rents:</b>			
<b>Studio</b>	\$1,379	\$1,071	\$1,315
<b>1 BR</b>	\$1,487	\$1,534	\$1,593
<b>2 BR</b>	\$1,756	\$1,928	\$2,067
<b>3+ BR</b>	\$2,265	\$2,324	\$2,493
<b># of Multifamily Buildings For Sale</b>	2	4	17
<b>Cap Rate</b>	5.2%	4.7%	4.7%

Source: CoStar (2016)

# 1. Analysis

## Market Demand Analysis

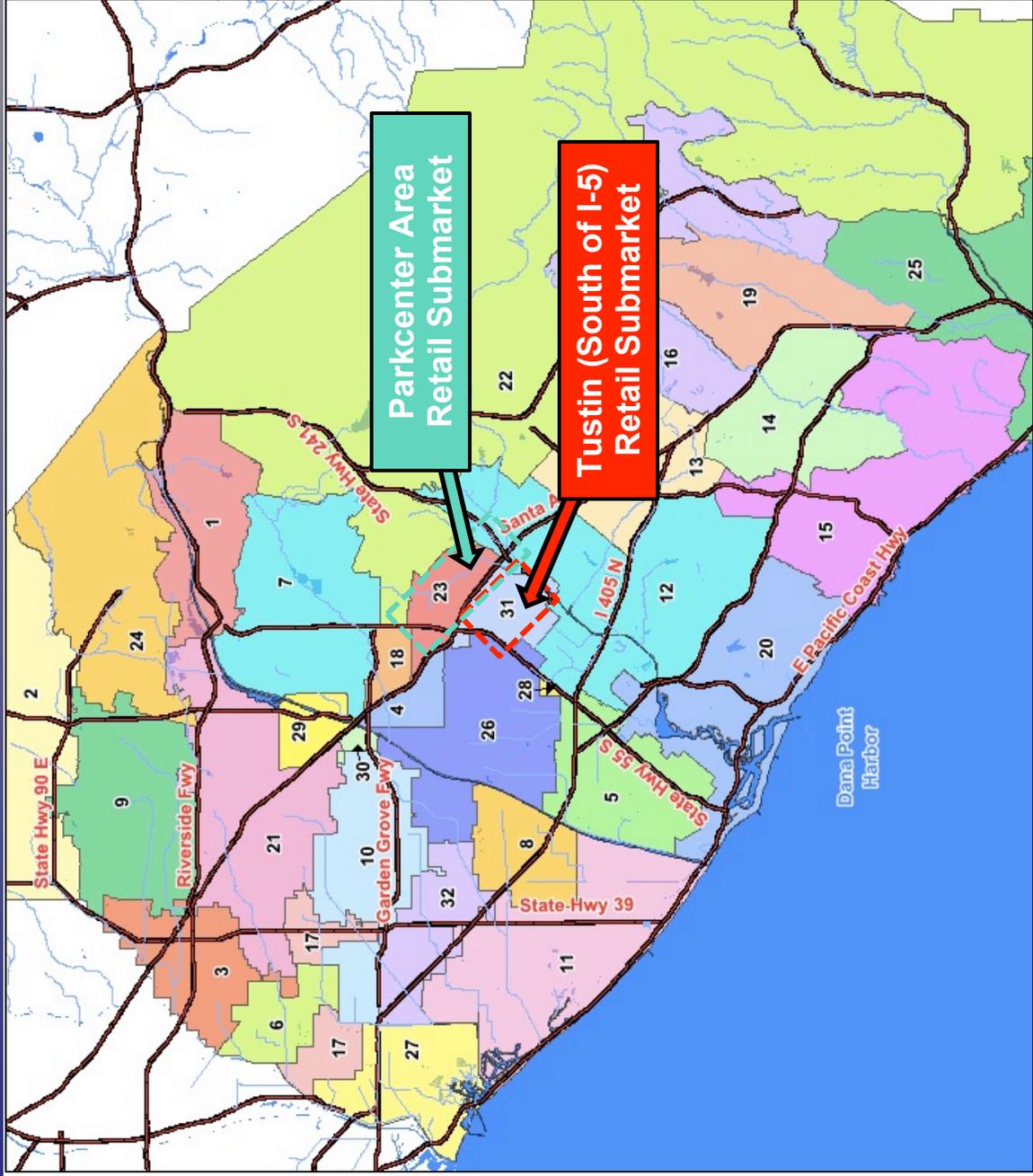
*Retail Supply, Vacancy & Lease Rates*

# Retail Supply, Vacancy & Lease Rates

- Supply, vacancy, and lease rates for retail uses are compared between submarkets within the Orange County retail market area
- Tustin falls within the Parkcenter Area and the Tustin (South of I-5) submarkets (as defined by CoStar)
- Tustin retail vacancy is slightly **above** Parkcenter Area and Tustin (South of I-5) submarkets and overall Orange County market average
- City of Tustin retail lease rates slightly **higher** than the Parkcenter Area and Tustin (South of I-5) submarket average and the overall Orange County market average

Source: CoStar Property (Q1 2016)

# Orange County Retail Submarket Overview



- 1 Anaheim Hills
- 2 Brea/La Habra
- 3 Buena Park/La Palma
- 4 Civic Center Area
- 5 Costa Mesa
- 6 Cypress
- 7 East Orange
- 8 Fountain Valley
- 9 Fullerton
- 10 Garden Grove
- 11 Huntington Beach
- 12 Irvine
- 13 Irvine Spectrum
- 14 Laguna Hills/Aliso Viejo
- 15 Laguna Niguel/Laguna Beach
- 16 Lake For/Foothill Rch/RSM
- 17 Los Alamitos/Stanton
- 18 Main Place Area
- 19 Mission Viejo
- 20 Newport Beach
- 21 North/East Anaheim
- 22 Outlying Orange County
- 23 Parkcenter Area
- 24 Placentia/Yorba Linda
- 25 S J Capistrano/S Clemente/Dana Pt
- 26 Santa Ana
- 27 Seal Beach
- 28 South Santa Ana
- 29 Stadium Area
- 30 The City Area
- 31 Tustin (South of I-5)
- 32 Westminster

Source: CoStar Property (2016)

**Kosmont Retail NOW!**



# Parkcenter Area and Tustin (South of I-5) Retail Submarket Overviews



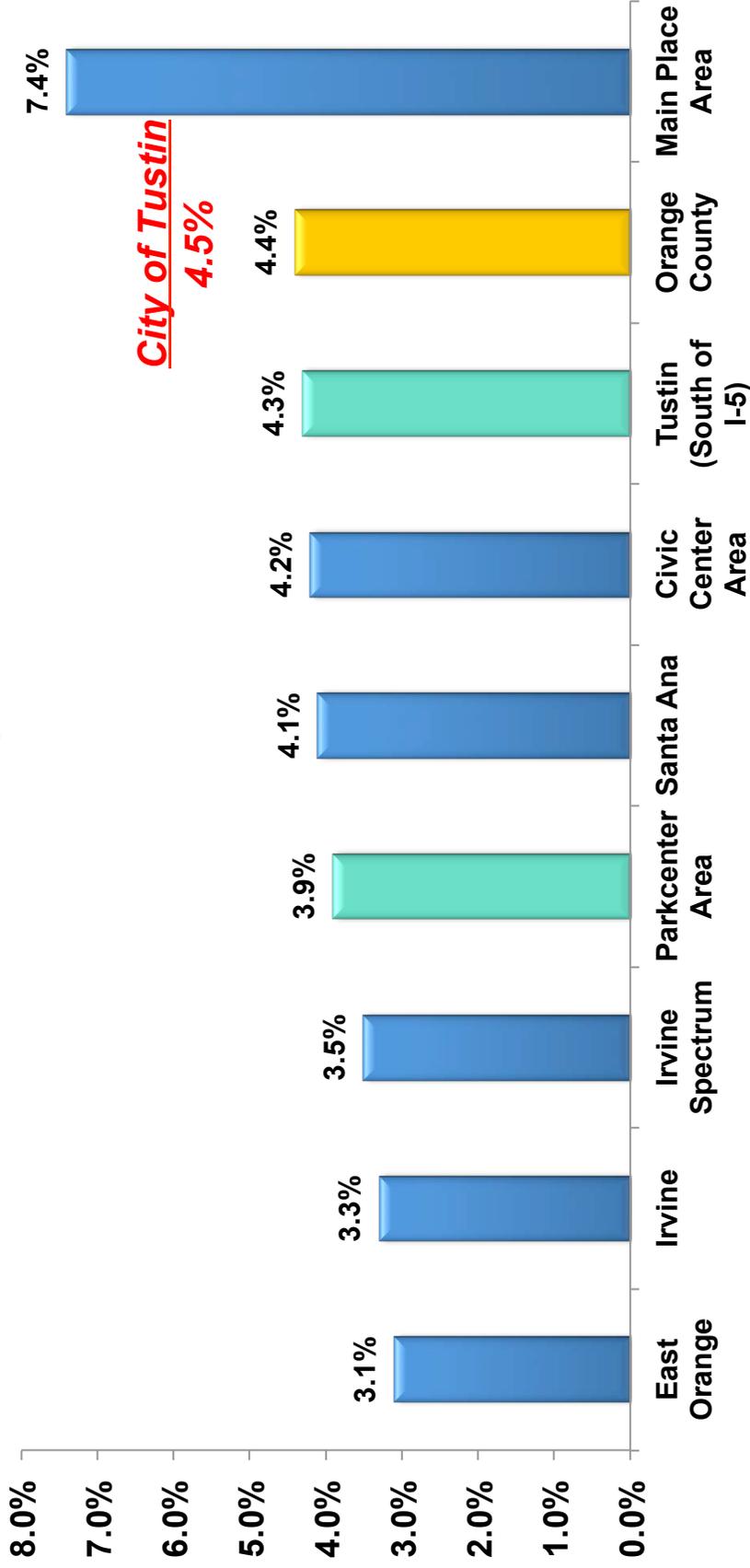
Source: CoStar Property (2016)

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# Retail Vacancy by Orange County Submarket\*

**Retail Vacancy (Q1 2016)**



Total G.L.A.	6.5M	5.2M	3.3M	4.2M	6.9M	1.6M	1.5M	143.2M	2.5M
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Source: CoStar Property (Q1 2016); \*Submarkets within 5 miles of Site

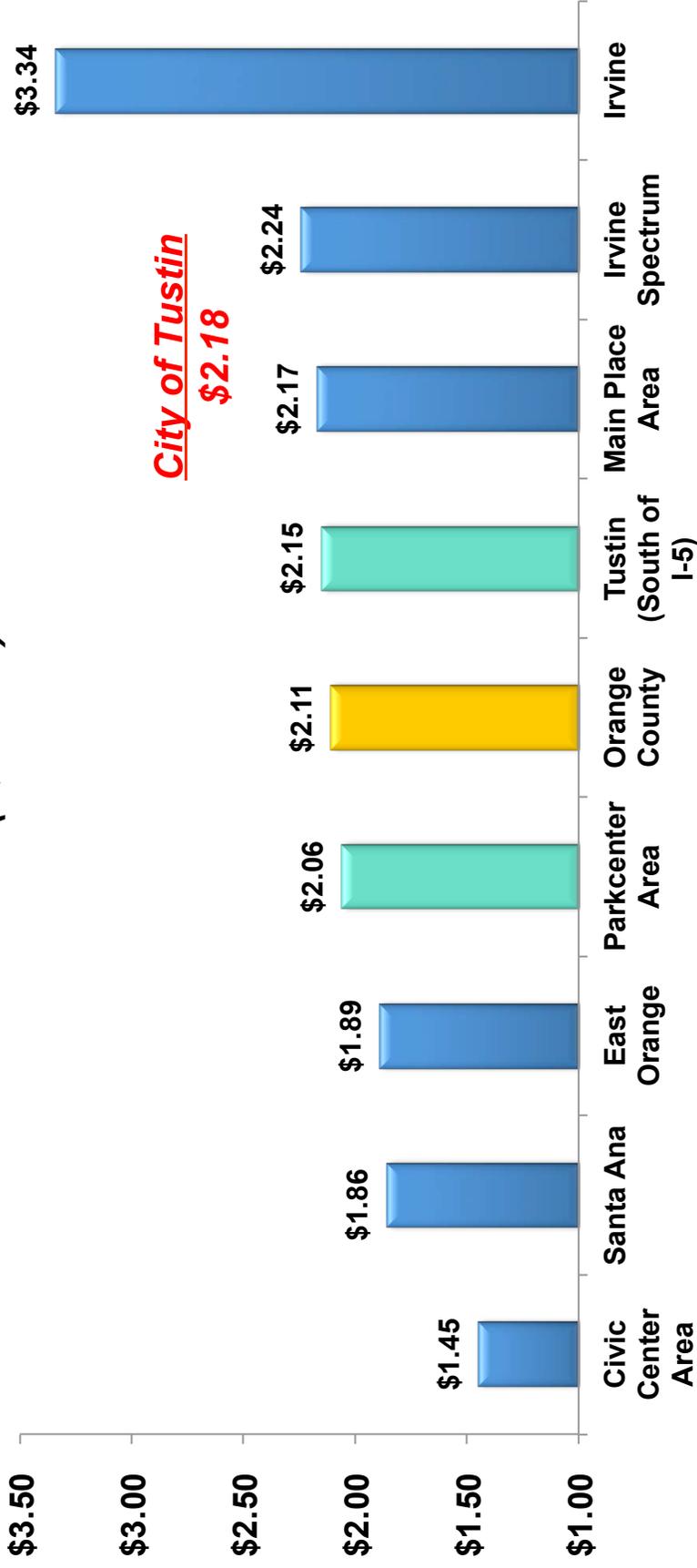
G.L.A. = Gross Leasable Area



# Retail Lease Rates

## by Orange County Submarket\*

**Average Asking Retail Lease Rates – \$PSF / Month**  
(Q1 2016)



Total G.L.A.	1.6M	6.9M	6.5M	4.2M	143.2M	1.5M	2.5M	3.3M	5.2M
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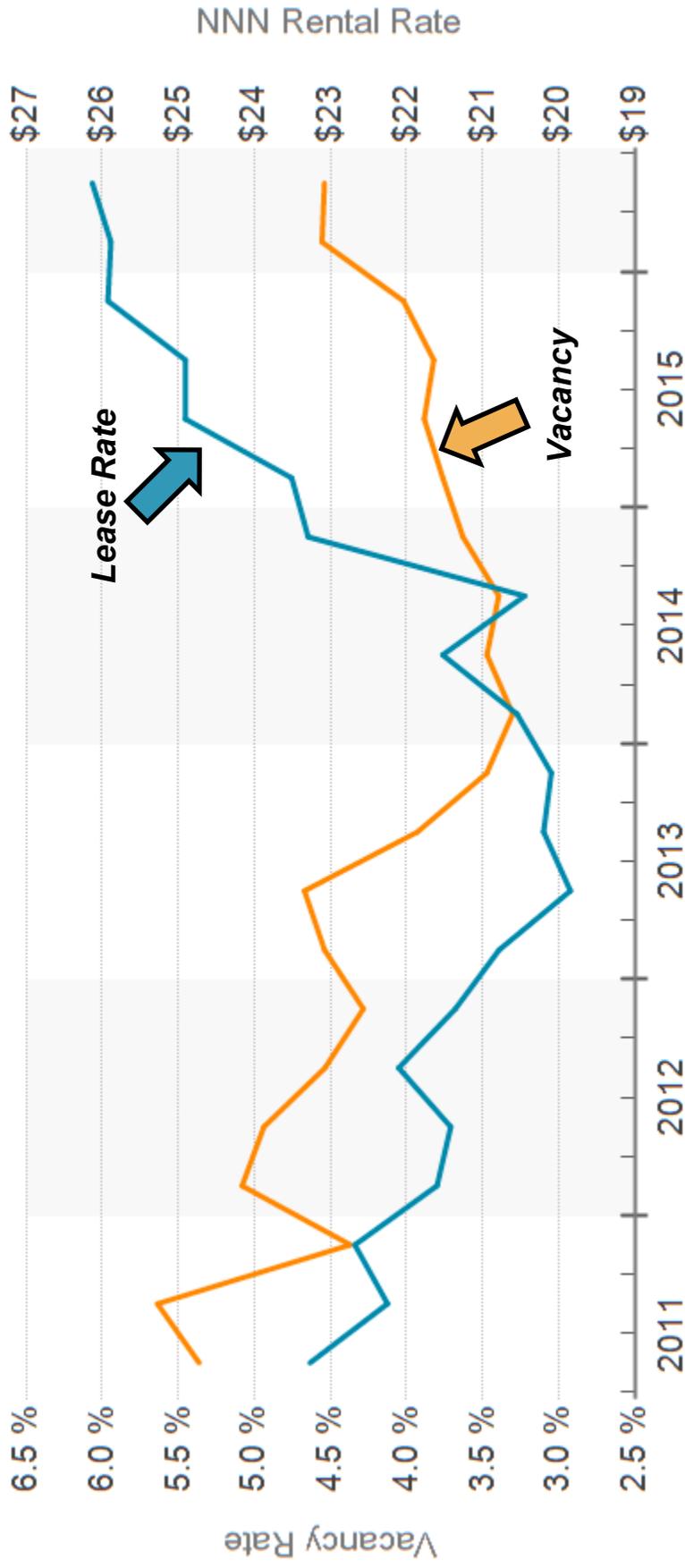
Source: CoStar Property (Q1 2016); \*Submarkets within 5 miles of Site

G.L.A. = Gross Leasable Area



# City Vacancy & Lease Rate Trends

## City of Tustin



Source: CoStar Property (Q1 2016)

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# Parkcenter Area Submarket Detail

## By Retail Subcategory

Retail Type	# of Centers	G.L.A.	Vacancy	Asking NNN Rates
General Retail / Free Standing	114 Bldgs	0.77M SF	1.9%	\$2.84
Mall	1	0.50M SF	0.0%	\$0.00
Power Center	2	0.90M SF	0.0%	\$0.00
Shopping Center	41	2.03M SF	7.3%	\$1.95
<b>Total Retail Market</b>		<b>4.19M SF</b>	<b>3.9%</b>	<b>\$2.06</b>

Source: CoStar Property (Q1 2016)

G.L.A. = Gross Leasable Area

# Tustin (South of I-5) Submarket Detail

## By Retail Subcategory

Retail Type	# of Centers	G.L.A.	Vacancy	Asking NNN Rates
General Retail / Free Standing	31 Bldgs	0.12M SF	4.0%	\$2.19
Mall	0	0.00M SF	0.0%	\$0.00
Power Center	1	1.01M SF	2.0%	\$2.88
Shopping Center	17	0.35M SF	11.2%	\$1.93
<b>Total Retail Market</b>		<b>1.49M SF</b>	<b>4.3%</b>	<b>\$2.15</b>

Source: CoStar Property (Q1 2016)

G.L.A. = Gross Leasable Area

# 1. Analysis

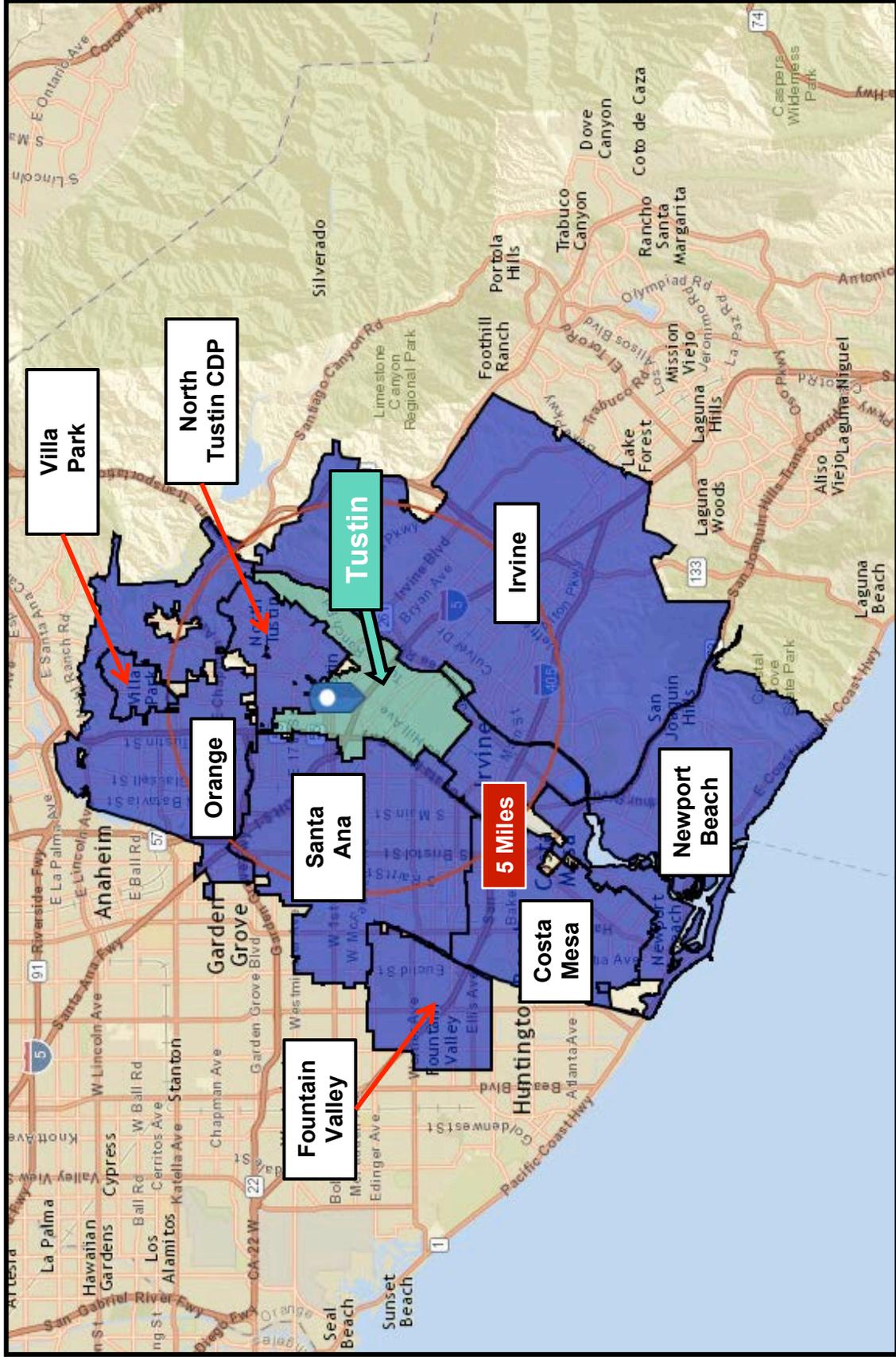
# Market Demand Analysis

*Retail Sales Performance*

# Retail Sales Performance

- Consumer spending across retail categories is totaled and normalized for population within the City and comparison regions for the purpose of evaluating potential sales leakage / capture across jurisdictions
- Retail sales per capita for the City (~\$29,500) is above average compared to other nearby regions and with Orange County (~\$17,900)
- Higher performing sales categories include those in heavy commercial, such as, **building materials, auto dealers and supplies, and gasoline stations**, in addition to **general merchandise stores, sporting goods, electronics stores, food and beverage stores, and restaurants**
- Lower performing retail categories include **health and personal care, miscellaneous store retailers, and non-store retailers**

# Tustin & Comparison Cities



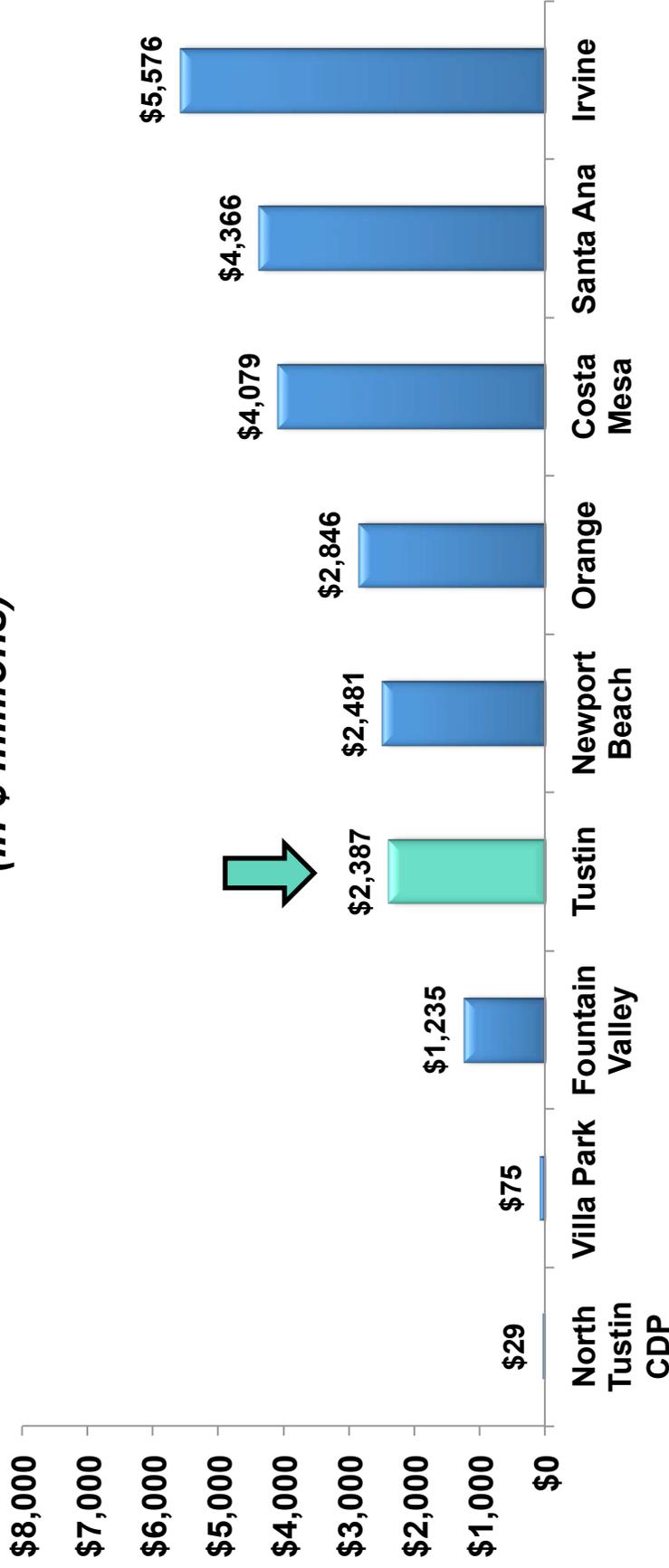
Source: ESRI (2015)

**Kosmont Retail NOW!**



# Retail Sales Comparison Tustin & Comparison Cities

**Retail Sales in 2015**  
*(in \$ millions)*



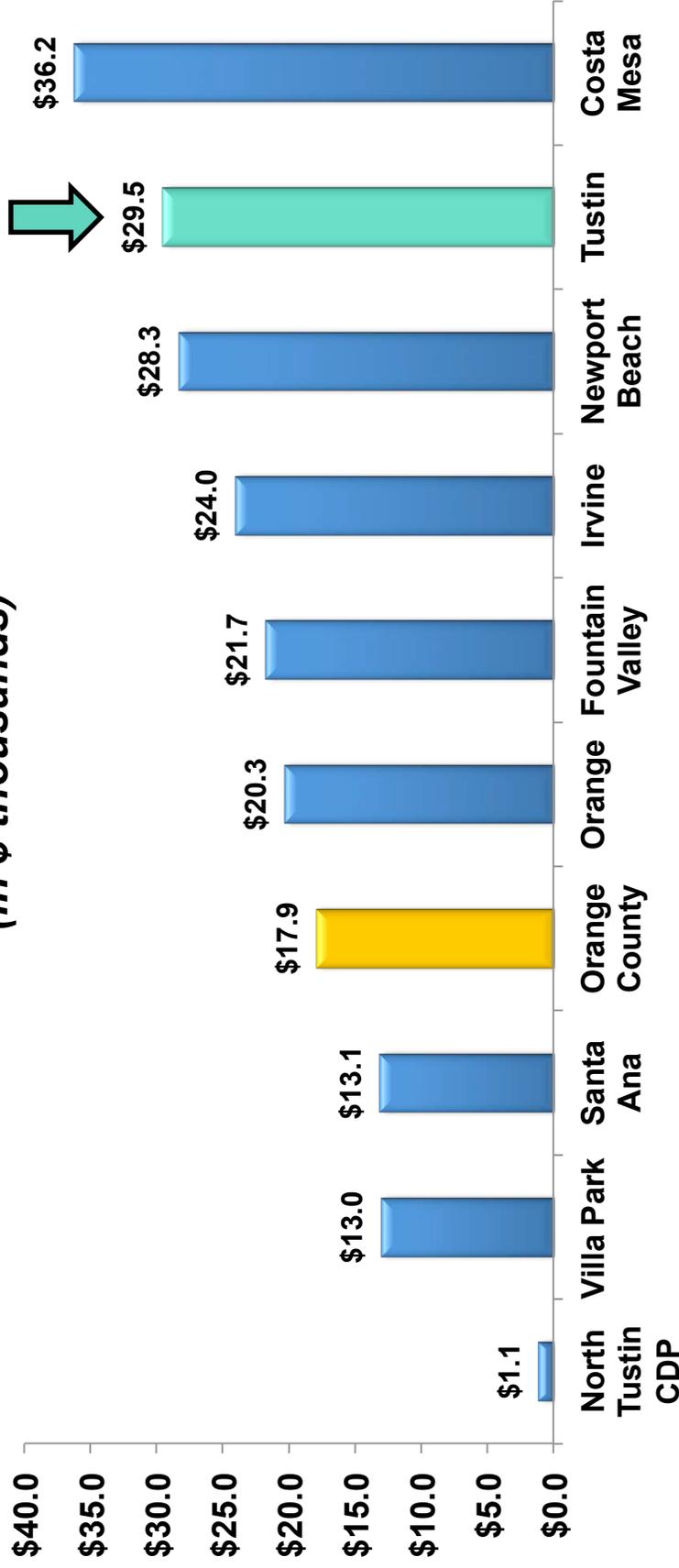
**2015 Population (000s):**

26	6	57	81	88	140	113	333	233
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Source: ESRI, Dun & Bradstreet (2015)

# Per Capita Retail Sales Tustin & Comparison Regions

Per Capita Retail Sales in 2015  
(in \$ thousands)



2015 Population (000s):

26	6	333	3,124	140	57	233	88	81	113
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Source: ESRI, Dun & Bradstreet (2015)

# Retail Category Definitions

- **Shopper Goods / GAFO** *(General Merchandise, Apparel & Accessories, Furniture & Other Sales)*
  - Clothing & Clothing Accessories Stores
  - General Merchandise Stores
  - Furniture & Home Furnishings Stores
  - Health & Personal Care Stores
  - Sporting Goods, Hobby, Book & Music Stores
  - Electronics & Appliance Stores
  - Miscellaneous Store Retailers (incl. Office Supply)
- **Convenience Goods**
  - Food and Beverage (Grocery Stores)
  - Food Service and Drinking Places (Restaurants & Bars)
- **Heavy Commercial Goods**
  - Building Materials (Home Improvement)
  - Auto Dealers & Supplies
  - Gasoline / Service Stations
- **Non-Store Retailers (e.g., Online Shopping & Mail-Order)**

**Note:** Retail Categories delineated by NAICS / California Board of Equalization

# Per Capita Retail Sales by Category City & Comparison Regions

Per Capita Retail Sales	Tustin	Costa Mesa	Fountain Valley	Irvine	Newport Beach	North Tustin CDP	Orange	Santa Ana	Villa Park	Orange County
<b>Shopper Goods (GAFO):</b>										
Clothing & Accessories Stores	\$1,278	\$3,976	\$582	\$1,165	\$2,866	\$102	\$1,392	\$934	\$185	\$1,062
General Merchandise Stores	\$5,579	\$5,253	\$5,253	\$2,316	\$2,489	\$46	\$1,349	\$1,681	\$2,682	\$2,546
Furniture & Home Stores	\$832	\$2,554	\$725	\$1,108	\$800	\$9	\$567	\$377	\$0	\$579
Health & Personal Care Stores	\$742	\$1,650	\$651	\$1,226	\$1,321	\$104	\$839	\$968	\$617	\$911
Sporting Goods, Hobby, Book Stores	\$1,246	\$910	\$688	\$655	\$881	\$47	\$568	\$414	\$0	\$544
Electronics & Appliance Stores	\$3,195	\$2,235	\$6,129	\$4,555	\$1,690	\$255	\$1,447	\$922	\$244	\$1,652
Miscellaneous Store Retailers	\$765	\$1,509	\$875	\$922	\$1,560	\$126	\$1,557	\$920	\$69	\$885
<b>Total GAFO</b>	<b>\$13,637</b>	<b>\$18,087</b>	<b>\$14,903</b>	<b>\$11,947</b>	<b>\$11,607</b>	<b>\$689</b>	<b>\$7,719</b>	<b>\$6,216</b>	<b>\$3,797</b>	<b>\$8,179</b>
<b>Convenience Goods:</b>										
Food & Beverage Stores	\$3,056	\$3,180	\$2,148	\$2,152	\$3,904	\$148	\$3,055	\$1,981	\$4,409	\$2,336
Food Services & Drinking Places (Restaurants)	\$2,269	\$3,185	\$1,968	\$2,221	\$4,390	\$180	\$2,487	\$1,201	\$879	\$1,883
<b>Total Convenience</b>	<b>\$5,325</b>	<b>\$6,365</b>	<b>\$4,116</b>	<b>\$4,373</b>	<b>\$8,294</b>	<b>\$328</b>	<b>\$5,542</b>	<b>\$3,182</b>	<b>\$5,288</b>	<b>\$4,219</b>
<b>Heavy Commercial:</b>										
Bldg Materials, Garden Equip. Stores	\$1,142	\$751	\$287	\$911	\$620	\$63	\$1,386	\$625	\$199	\$746
Motor Vehicle & Parts Dealers	\$8,453	\$9,709	\$1,108	\$5,219	\$6,644	\$0	\$4,649	\$2,389	\$3,635	\$3,888
Gasoline Stations	\$791	\$773	\$942	\$621	\$507	\$0	\$706	\$500	\$0	\$617
<b>Total Heavy Commercial</b>	<b>\$10,386</b>	<b>\$11,233</b>	<b>\$2,337</b>	<b>\$6,751</b>	<b>\$7,771</b>	<b>\$63</b>	<b>\$6,741</b>	<b>\$3,514</b>	<b>\$3,834</b>	<b>\$5,251</b>
Non-store Retailers	\$199	\$477	\$354	\$897	\$583	\$35	\$291	\$193	\$53	\$273
<b>Total Retail</b>	<b>\$29,547</b>	<b>\$36,162</b>	<b>\$21,710</b>	<b>\$23,968</b>	<b>\$28,255</b>	<b>\$1,115</b>	<b>\$20,293</b>	<b>\$13,105</b>	<b>\$12,972</b>	<b>\$17,922</b>

Key:  Indicates higher value for Tustin

Indicates lower value for Tustin

Source: ESRI, Dun & Bradstreet (2015)

# Top Sales Tax Producers

AutoNation Ford	Target
AutoNation Infiniti	TJ Maxx
Best Buy	Total Wine & More
Chevron	Toyota Lease Trust
Costco	Toys R Us
Home Depot	Tustin Acura
Jewelry Exchange	Tustin Buick/GMC
Lowe's	Tustin Chrysler/Jeep/Dodge
McLean Cadillac	Tustin Hyundai/Mazda
Micro Center	Tustin Lexus
Ralphs	Tustin Nissan
REI	Tustin Toyota
Shell	

Source: HdL Companies (Q4 2015)

# 1. Analysis

## Market Demand Analysis

*Retail Sales Surplus / Leakage*

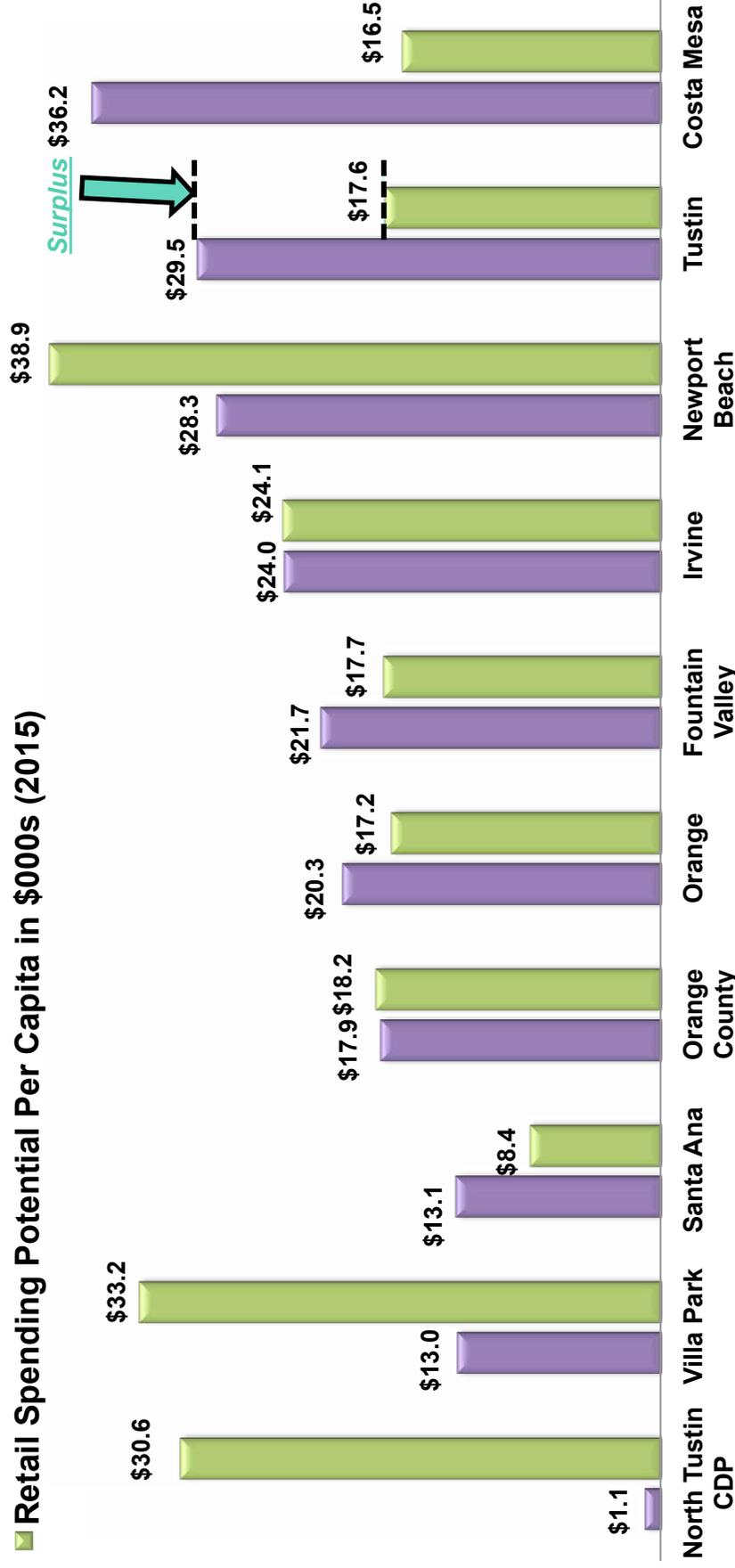
# Retail Sales Surplus / Leakage

- Overall retail sales in the City are **higher** than retail spending potential based on households and average household income, suggesting that, overall, the City is likely capturing a significant portion of Tustin resident retail purchases and additional retail spending by residents of other cities (i.e. sales **surplus**)
- Certain categories, however, are exhibiting a retail sales leakage, including:
  - Health & Personal Care Stores
  - Miscellaneous Store Retailers
  - Food & Beverage Stores (Grocery)
  - Gasoline Stations
  - Non-store Retailers

# Retail Sales Surplus / Leakage

## “Cash Registers vs. Wallets”

- Retail Sales Per Capita in \$000s (2015)
- Retail Spending Potential Per Capita in \$000s (2015)



Per Capita Sales Surplus/Leakage, Total Surplus/Leakage, and Percent Surplus/Leakage:

CDP	Per Capita Sales Surplus/Leakage (\$000s)	Total Surplus/Leakage (\$)	Percent Surplus/Leakage
North Tustin	(\$29.5K)	(\$757M)	(96%)
Villa Park	(\$20.2K)	(\$118M)	(61%)
Santa Ana	\$4.7K	\$1,566M	56%
Orange County	(\$0.2K)	(\$768M)	(1%)
Orange	\$3.1K	\$436M	18%
Fountain Valley	\$4.0K	\$226M	22%
Irvine	(\$0.1K)	(\$33M)	(1%)
Newport Beach	(\$10.6K)	(\$931M)	(27%)
Tustin	\$12.0K	\$968M	68%
Costa Mesa	\$19.6K	\$2,216M	119%

Note: Spending potential based on number of households, average household income, and estimated percentage of income spent on retail goods and services Source: ESRI, Dun & Bradstreet (2015)

# Retail Sales Surplus / Leakage by Category

## City of Tustin

Retail Category	Retail Spending Potential	Retail Sales	Retail Surplus/ (Leakage)	Percent Surplus/ (Leakage)
<b><u>Shopper Goods (GAFO):</u></b>				
Clothing & Clothing Accessories Stores	\$90,902,898	\$103,294,211	\$12,391,313	13.6%
General Merchandise Stores	\$200,655,983	\$450,793,450	\$250,137,467	124.7%
Furniture & Home Furnishings Stores	\$43,453,352	\$67,201,946	\$23,748,594	54.7%
Health & Personal Care Stores	\$81,538,544	\$59,954,576	<b>(\$21,583,968)</b>	<b>(26.5%)</b>
Sporting Goods, Hobby, Book & Music Stores	\$38,239,253	\$100,711,548	\$62,472,295	163.4%
Electronics & Appliance Stores	\$69,697,793	\$258,180,821	\$188,483,028	270.4%
Miscellaneous Store Retailers	\$63,439,617	\$61,773,563	<b>(\$1,666,054)</b>	<b>(2.6%)</b>
<b>Subtotal – GAFO</b>	<b>\$587,927,440</b>	<b>\$1,101,910,115</b>	<b>\$513,982,675</b>	<b>87.4%</b>
<b><u>Convenience Goods:</u></b>				
Food & Beverage Stores (Grocery)	\$247,388,533	\$246,926,674	<b>(\$461,859)</b>	<b>(0.2%)</b>
Food Services & Drinking Places (Restaurants)	\$141,756,349	\$183,321,559	\$41,565,210	29.3%
<b>Subtotal – Convenience</b>	<b>\$389,144,882</b>	<b>\$430,248,233</b>	<b>\$41,103,351</b>	<b>10.6%</b>
<b><u>Heavy Commercial Goods:</u></b>				
Bldg Materials, Garden Equip. & Supply Stores	\$58,068,170	\$92,261,881	\$34,193,711	58.9%
Motor Vehicle & Parts Dealers	\$276,981,325	\$682,982,290	\$406,000,965	146.6%
Gasoline Stations	\$75,231,946	\$63,924,118	<b>(\$11,307,828)</b>	<b>(15.0%)</b>
<b>Subtotal – Heavy Commercial</b>	<b>\$410,281,441</b>	<b>\$839,168,289</b>	<b>\$428,886,848</b>	<b>104.5%</b>
Non-store Retailers	\$32,124,622	\$16,045,325	<b>(\$16,079,297)</b>	<b>(50.1%)</b>
<b>Total Retail</b>	<b>\$1,419,478,385</b>	<b>\$2,387,371,962</b>	<b>\$967,893,577</b>	<b>68.2%</b>

Source: ESRI, Dun & Bradstreet (2015)

# Retail Sales Leakage Categories and Supportable SF

## City of Tustin

Retail Sales Leakage Categories	Retail Sales Leakage	Estimated Sales PSF	Estimated Supportable SF
Health & Personal Care Stores	(\$21,583,968)	\$300	71,946 SF
Miscellaneous Store Retailers	(\$1,666,054)	\$400	4,165 SF
Food & Beverage Stores (Grocery)	(\$461,859)	\$400	1,154 SF
Gasoline Stations	(\$11,307,828)	\$800	14,134 SF
<b>Total Sales Leakage Categories</b>	<b>(\$35,019,709)</b>		<b>91,399 SF</b>

- ~91,399 SF of retail supported by existing sales leakage, does not account for existing retail vacancy within the City (estimated by CoStar to be approximately 250,000 SF)

Source: ESRI, Dun & Bradstreet, ULI & ICSC Dollars and Cents of Shopping Centers, HdL Companies, Kosmont Companies (2015)

# Retail Sales Surplus / Leakage by Category

## 3 Mile Radius from Red Hill Ave. and the 5 Freeway

Retail Category	Retail Spending Potential	Retail Sales	Retail Surplus/ (Leakage)	Percent Surplus/ (Leakage)
<b><u>Shopper Goods (GAFO):</u></b>				
Clothing & Clothing Accessories Stores	\$229,964,910	\$205,609,960	<b>(\$24,354,950)</b>	<b>(10.6%)</b>
General Merchandise Stores	\$507,890,610	\$619,415,412	\$111,524,802	22.0%
Furniture & Home Furnishings Stores	\$111,632,930	\$136,296,756	\$24,663,826	22.1%
Health & Personal Care Stores	\$208,676,878	\$176,481,431	<b>(\$32,195,447)</b>	<b>(15.4%)</b>
Sporting Goods, Hobby, Book & Music Stores	\$98,205,590	\$185,342,176	\$87,136,586	88.7%
Electronics & Appliance Stores	\$178,548,571	\$503,233,744	\$324,685,173	181.8%
Miscellaneous Store Retailers	\$162,411,975	\$200,102,065	\$37,690,090	23.2%
<b>Subtotal – GAFO</b>	<b>\$1,497,331,464</b>	<b>\$2,026,481,544</b>	<b>\$529,150,080</b>	<b>35.3%</b>
<b><u>Convenience Goods:</u></b>				
Food & Beverage Stores (Grocery)	\$621,889,425	\$524,599,095	<b>(\$97,290,330)</b>	<b>(15.6%)</b>
Food Services & Drinking Places (Restaurants)	\$359,010,986	\$386,303,339	\$27,292,353	7.6%
<b>Subtotal – Convenience</b>	<b>\$980,900,411</b>	<b>\$910,902,434</b>	<b>(\$69,997,977)</b>	<b>(7.1%)</b>
<b><u>Heavy Commercial Goods:</u></b>				
Bldg Materials, Garden Equip. & Supply Stores	\$155,125,135	\$213,194,651	\$58,069,516	37.4%
Motor Vehicle & Parts Dealers	\$713,383,126	\$1,219,424,941	\$506,041,815	70.9%
Gasoline Stations	\$190,852,956	\$139,888,069	<b>(\$50,964,887)</b>	<b>(26.7%)</b>
<b>Subtotal – Heavy Commercial</b>	<b>\$1,059,361,217</b>	<b>\$1,572,507,661</b>	<b>\$513,146,444</b>	<b>48.4%</b>
Non-store Retailers	\$81,786,568	\$48,485,437	<b>(\$33,301,131)</b>	<b>(40.7%)</b>
<b>Total Retail</b>	<b>\$3,619,379,660</b>	<b>\$4,558,377,076</b>	<b>\$938,997,416</b>	<b>25.9%</b>

Source: ESRI, Dun & Bradstreet (2015)

# Retail Sales Leakage Categories and Supportable SF 3 Mile Radius from Red Hill Ave. and the 5 Freeway

Retail Sales Leakage Categories	Retail Sales Leakage	Estimated Sales PSF	Estimated Supportable SF
Clothing & Clothing Accessories Stores	(\$24,354,950)	\$300	81,183 SF
Health & Personal Care Stores	(\$32,195,447)	\$300	107,318 SF
Food & Beverage Stores (Grocery)	(\$97,290,330)	\$400	243,225 SF
Gasoline Stations	(\$50,964,887)	\$800	63,706 SF
<b>Total Sales Leakage Categories</b>	<b>(\$204,805,614)</b>		<b>495,432 SF</b>

- ~495,432 SF of retail supported by existing sales leakage, does not account for existing retail vacancy within a 3 mile radius from Red Hill Ave. and the 5 Freeway (estimated by CoStar to be approximately 464,494 SF)

Source: ESRI, Dun & Bradstreet, ULI & ICSC Dollars and Cents of Shopping Centers, HdL Companies, Kosmont Companies (2015)

# Surplus/Leakage Summary by Category

## Surplus Retail Categories

- Clothing & Clothing Accessories Stores
- General Merchandise Stores
- Furniture & Home Furnishings Stores
- Sporting Goods, Hobby, Book & Music Stores
- Electronics & Appliance Stores
- Food Services & Drinking Places (Restaurants)
- Bldg. Materials, Garden Equip. & Supply Stores
- Motor Vehicle & Parts Dealers

## Leakage Retail Categories

- Health & Personal Care Stores
- Miscellaneous Store Retailers
- Food & Beverage Stores (Grocery)
- Gasoline Stations
- Non-store Retailers

## 2. Strategy

- a) Trade Area Retailer Voids
- b) Opportunity Site Assessment

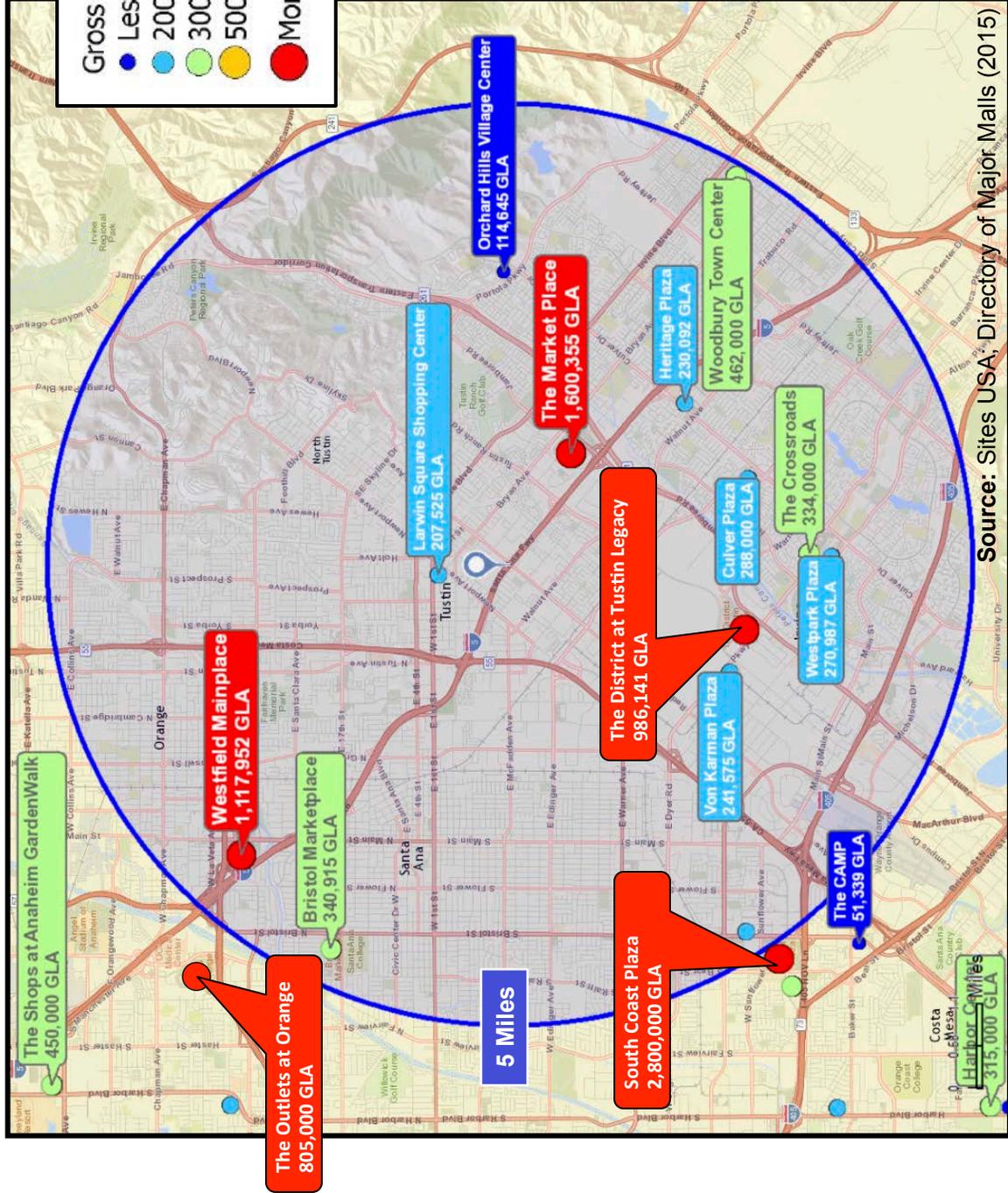
## 2. Strategy

# Trade Area Retailer Voids

# Summary: Retailer Voids

- Retailer voids were evaluated for all major retail categories within the City and larger trade area
- Does not account for potential compatibility between trade area characteristics and retailer demographic preferences, as well as current retailer expansion activity evaluated later
- Potential voids include office supply stores, groceries, and fitness/gyms

# Major Shopping Center Map Tustin Trade Area (5 miles)



# National Retailer Voids (within City Limits)

## Auto Parts Tires

AAMCO  
America's Tire  
CARQUEST  
Firestone  
Goodyear  
Honest 1  
Midas  
NAPA  
Pep Boys  
Quick Lane

## Banks

AmericanWest Bank  
Compass Bank  
East West Bank  
First-Citizens Bank & Trust Company  
First Bank  
HSBC  
M&T Bank  
Umpqua Bank

## Clothing Apparel

Abercrombie & Fitch  
Aeropostale  
American Eagle Outfitters  
Ann Taylor  
Ann Taylor Factory  
Ann Taylor Loft Outlet  
Anthropologie  
Avenue  
Banana Republic  
BCBG Max Azria

bebe  
Buckle  
Catherines  
Charlotte Russe  
Chico's  
Citi Trends  
dd's Discounts  
Dress Barn  
Express  
Fallas Paredes  
Forever 21  
Fossil  
Gap  
H&M  
Hollister Co.  
J. Crew  
J. Jill  
Lane Bryant  
Last Call  
Lucky Brand Jeans  
Men's Warehouse  
New York & Company  
Nordstrom Rack  
PacSun  
Rainbow  
Rue21  
Saks OFF 5<sup>TH</sup>  
Talbots  
The Childrens Place  
The Limited  
Urban Outfitters  
Victoria's Secret  
Wet Seal

## Computers Electronic

Apple Store  
Fry's Electronics

## Convenience Stores

Exxon  
Sinclair  
Sunoco  
Texaco

## Craft Fabric Stores

Hobby Lobby  
Jo-Ann

## Department Stores

Barneys New York  
Bloomingdale's  
Dillard's  
JCPenney  
Macy's  
Neiman Marcus  
Nordstrom  
Saks Fifth Avenue

Source: Sites USA, Kosmont Companies (2015)

# National Retailer Voids (cont.) (within City Limits)

## Discount Department Stores

Babies R Us  
Burlington Coat Factory  
David's Bridal  
Kmart  
Sears  
Stein Mart  
SuperTarget  
Toys R Us  
Tuesday Morning  
Wal-Mart  
Wal-Mart Supercenter

## Dollar Stores

Dollar General  
Family Dollar  
Just-A-Buck

## Fitness

24 Hour Fitness  
Anytime Fitness  
Bay Club  
Equinox Fitness  
Gold's Gym  
In-Shape  
LA Fitness  
Lifetime Fitness  
Planet Fitness  
Powerhouse Gym  
World Gym

## Furniture Household

Anna's Linens  
Ashley Furniture  
Bassett  
Bed Bath & Beyond  
Cost Plus  
Crate and Barrel  
IKEA  
Jennifer Convertibles  
La-Z-Boy  
LAMPS PLUS  
Pottery Barn  
Relax the Back  
Sur La Table  
The Container Store  
Williams-Sonoma  
Z Gallerie

## Grocery Stores

Bristol Farms  
Cardenas  
El Super  
Food 4 Less  
Gelsons  
Grocery Outlet  
H Mart  
Jons Marketplace  
Neighborhood Market  
Northgate Market  
Pavillons  
Safeway  
Superior Grocers  
Top Valu Market

## Vallarta Supermarkets

Vons  
WinCo Foods

## Home Improvement

Ace Hardware  
Kelly-Moore  
Orchard  
Restoration Hardware  
Sherwin-Williams  
True Value

## Office Supply

Office Depot  
Office Max  
Staples

## Pet Stores

Petco  
PetsMart

## Sporting Goods

Champs Sports  
Golfsmith  
Play It Again Sports  
Roger Dunn Golf Shops  
Sport Chalet  
Sports Authority

## Wholesale

Sam's Club  
Smart & Final

Source: Sites USA, Kosmont Companies (2015)

# National Retailer Voids – Restaurants (within City Limits)

## Restaurants Bakery Bagels

Bruegger's  
Corner Bakery  
Einstein Bros  
Manhattan Bagel  
Noah's

## Restaurants Casual

Another Broken Egg  
Applebee's  
Baja Fresh  
BJ's Restaurant & Brewery  
Brio  
Buca di Beppo  
Buffalo's Southwest Cafe  
Buffalo Wild Wings  
Capital Grille  
Carino's  
Chart House  
Chevys  
Chili's  
Claim Jumper  
Elephant Bar  
Fleming's  
Fuddruggers  
Golden Corral  
Habit Burger Grill  
Home Town Buffet  
Hooters  
IHOP  
Joe's Crab Shack

Johnny Rockets  
Macaroni Grill  
Maggiano's  
Marie Callender's  
McCormick & Schmick's  
Morton's  
Noodles & Company  
Olive Garden  
Outback Steakhouse  
P.F. Chang's  
Qdoba  
Rainforest Café  
Red Lobster  
Ruth's Chris  
Samurai Sam's  
Sizzler  
Smashburger  
T.G.I. Friday's  
The Cheesecake Factory

## Restaurants Fast Food Major

Sonic

## Restaurants Fast Food Minor

A&W  
Church's Chicken  
Long John Silver's  
Panda Express  
Popeyes  
Raising Canes  
Rally's

Steak n Shake  
Wing Stop

## Restaurants Ice Cream Smoothie

Bruster's  
Carvel

Cold Stone Creamery

Menchie's

NRgize

Orange Julius

Planet Smoothie

Red Mango

Rita's

Robeks

Smoothie King

Surf City Squeeze

TCBY

## Restaurants Pizza

Blaze Pizza  
Chuck E. Cheese's  
Hungry Howie's  
Live Basil Pizza  
Marco's Pizza  
MOD Pizza  
Mountain Mike's Pizza  
Papa Murphy's  
Pieology  
Pizza Factory  
Pizza Patron  
Pizza Studio

PizzaREV  
Project Pie  
Sbarro  
Shakeys  
Villa Pizza

## Restaurants Sandwich

Blimpie  
Charley's Grilled Subs  
Firehouse Subs  
Great Steak  
Jimmy John's  
Quiznos  
Sandella's Flatbread  
Schlotzsky's Deli

Source: Sites USA, Kosmont Companies (2015)

## 2. Strategy

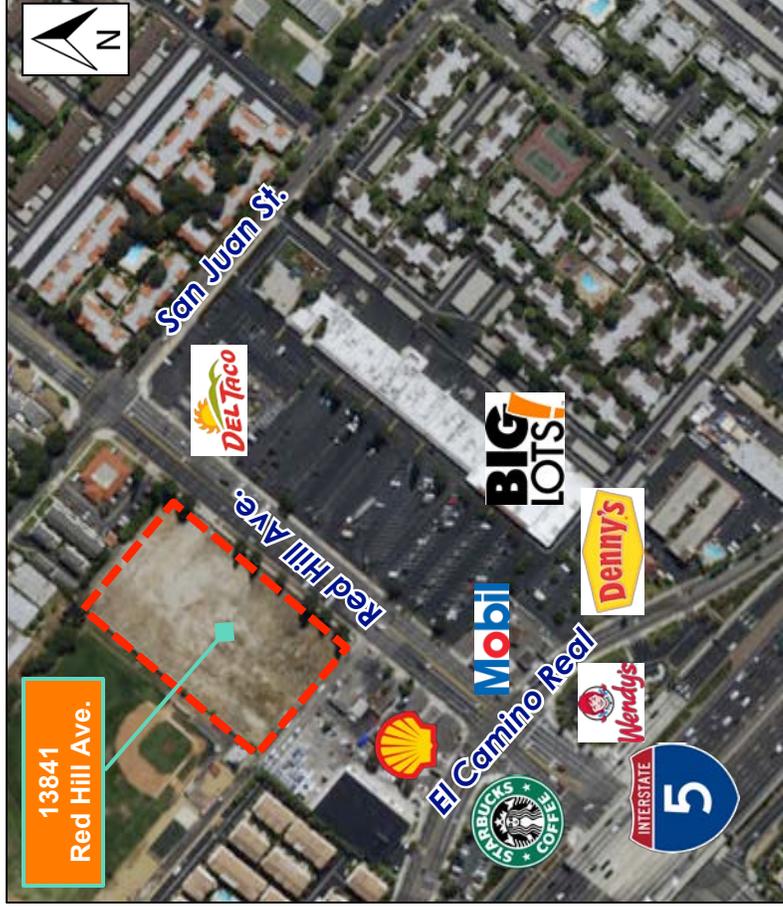
# Opportunity Site Assessment

# Opportunity Site Area Overview



# Opportunity Site Area #1 North of I-5; Vacant Parcel

Address/ Location	Size (acres)	Ownership	Zoning
13841 Red Hill Ave.	2.86	WASL Tustin Investors V LLC	C-2 Central Commercial



Strengths	Challenges	Opportunities
<ul style="list-style-type: none"> <li>Large vacant parcel located along major road (Red Hill Ave.) and close proximity to I-5</li> <li>Good visibility</li> </ul>	<ul style="list-style-type: none"> <li>Mid-block site</li> </ul>	<ul style="list-style-type: none"> <li>Potential retail/commercial development</li> <li>Strong corridor development opportunity</li> <li>Total land area: ~2.86 acres</li> </ul>

# Opportunity Site Area #2 North of I-5; Big Lots Anchored Center

Address/Location	Size (acres)	Ownership	Zoning
13742 Red Hill Ave.	0.51	Mayflower Properties	C-1 Retail Commercial
13802 Red Hill Ave.	0.35	Howard L. Abel Trust	
13822 Red Hill Ave.	4.79	Howard L. Abel Trust	
13852 Red Hill Ave.	1.96	Lingo Associates LLC	
1571 El Camino Real	0.88	Howard L. Abel Trust	C-2 Central Commercial
Unmarked Parcel	0.28	Lingo Associates LLC	



## Strengths

- Several parcels located along major road (Red Hill Ave.) and close proximity to I-5
- Excellent visibility

## Challenges

- Multiple property owners
- Slightly different zoning requirements between C-1 and C-2

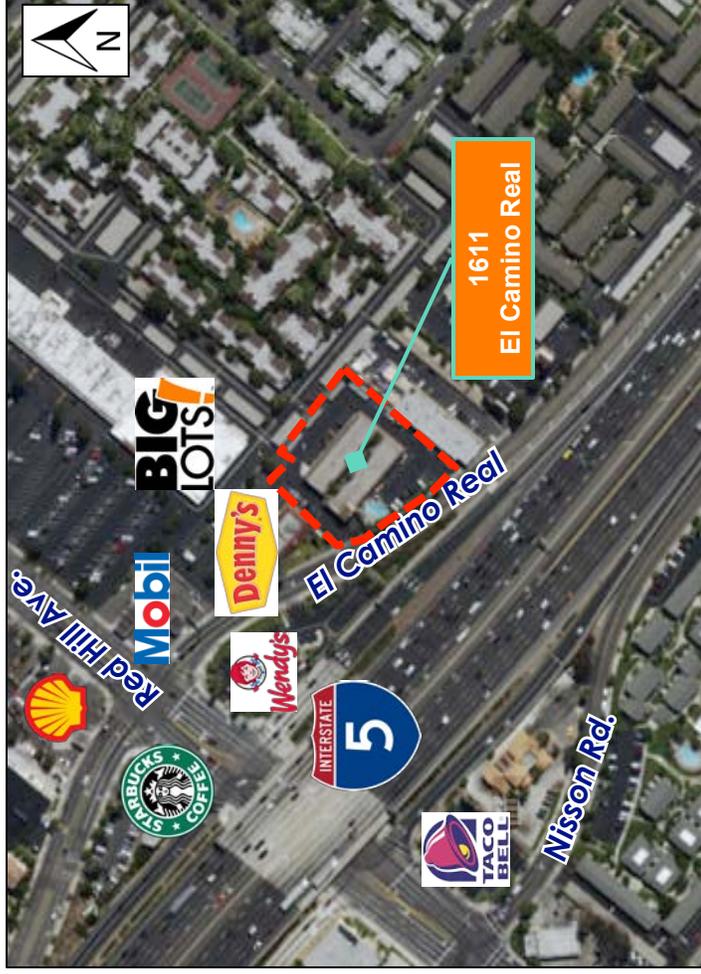
## Opportunities

- Large retail/commercial development opportunity
- Total land area: ~8.77 acres

# Opportunity Site Area #3

## North of I-5; Key Inn and Suites Site

Address/ Location	Size (acres)	Ownership	Zoning
1611 El Camino Real	1.20	Sixpence Inn of Tustin	C-2 Central Commercial



### Strengths

- Located close to I-5 along El Camino Real (freeway off-ramp runs west of Site)
- Good visibility from I-5

### Challenges

- Parcel does not front Red Hill Ave.
- Challenges for future retail/commercial development given existing hotel use

### Opportunities

- Potential commercial/ destination redevelopment
- Total land area: ~1.20 acres

# Opportunity Site Area #4

## South of I-5; Furniture Store

Address/ Location	Size (acres)	Ownership	Zoning
1471 Nisson Rd.	0.48	Billy F. Mathis Inc.	C-1 Retail Commercial



Strengths	Challenges	Opportunities
<ul style="list-style-type: none"> <li>• Located directly next to I-5 and along Red Hill Ave. (freeway off-ramp runs directly north of Site)</li> <li>• Excellent visibility</li> </ul>	<ul style="list-style-type: none"> <li>• Parcel fronts Nisson Rd. (limited access to Site)</li> <li>• Small parcel</li> </ul>	<ul style="list-style-type: none"> <li>• Pad retail/restaurant development</li> <li>• Total land area: ~0.48 acres</li> </ul>

# Opportunity Site Area #5

## South of I-5; Vacant Gas Station

Address/ Location	Size (acres)	Ownership	Zoning
14081 Red Hill Ave.	0.54	Thomas H. Wilson & Marilyn L. Grandon	C-1 Retail Commercial



Strengths	Challenges	Opportunities
<ul style="list-style-type: none"> <li>Parcel located along major road (Red Hill Ave.) and close proximity to I-5</li> <li>Great visibility</li> </ul>	<ul style="list-style-type: none"> <li>Small parcel size</li> </ul>	<ul style="list-style-type: none"> <li>Pad retail/restaurant development</li> <li>Total land area: ~0.54 acres</li> </ul>

# Opportunity Site Area #6 South of I-5; Stater Bros. Anchored Center

Address/Location	Size (acres)	Ownership	Zoning
14131 Red Hill Ave.	0.16	Huber G. Wilson; Old Town Inc.	C-1 Retail Commercial
14171 Red Hill Ave.	2.30	Huber G. Wilson; Armand Pedicini Trust	
14171 Red Hill Ave. (Stater Bros.)	0.83	Huber G. Wilson	
14201 Red Hill Ave.	1.51	Huber G. Wilson; Armand Pedicini Trust	



## Strengths

- Several parcels located along major road (Red Hill Ave.) and close proximity to I-5
- Excellent visibility

## Challenges

- Two Sites feature same address, would likely require merging parcels
- Two parcels do not front Red Hill Ave.

## Opportunities

- Large retail/commercial development
- Total land area: ~4.80 acres

# Opportunity Site Area #7

## South of I-5; Redhill Shopping Village

Address/Location	Size (acres)	Ownership	Zoning
14501 Red Hill Ave.	0.51	John Yegenian Trust	C-2 Central Commercial
14511 Red Hill Ave.	4.77	Jill Richter Trust	
14551 Red Hill Ave.	0.57	Jill Richter Trust	
14601 Red Hill Ave.	0.55	Jill Richter Trust; Batla Food Group, Inc.	



Strengths	Challenges	Opportunities
<ul style="list-style-type: none"> <li>Several parcels located at major intersection (Red Hill Ave. &amp; Walnut Ave.)</li> <li>Excellent visibility</li> </ul>	<ul style="list-style-type: none"> <li>One parcel (14551 Red Hill Ave.) does not front Red Hill Ave.</li> <li>Multiple property owners</li> </ul>	<ul style="list-style-type: none"> <li>Large retail/commercial development</li> <li>Total land area: ~6.40 acres</li> </ul>

## 3. Implementation

- a) Summary of Findings
- b) Financing & Incentives

# Summary of Findings

## Demographics & Employment

- Relatively young, diverse population; similar HH size to Orange County
- Employment concentrated within retail, professional services, accommodation and food services, administration and support, manufacturing, and healthcare

## Retail Retention & Recruitment

- City performs **above average** relative to neighboring jurisdictions in terms of retail sales per capita and resident and non-resident spending (i.e. surplus)
- Higher performing sales categories include **building materials, motor vehicle and parts, gasoline stations, general merchandise, sporting goods, electronics, food and beverage, and restaurants**, while lower performing retail categories include **health and personal care, miscellaneous, and non-store** retailers

## Economic Development without Redevelopment

- The world of economic development for California cities has evolved following the dissolution of Redevelopment Agencies
- Alternative economic tools (such as EIFDs and CRIAs) and new legislation should be explored for Tustin to retain and improve general fund tax base and facilitate potential public-private transactions

# Overview of Financing, Incentives & Other Economic Development Tools

- City may consider evaluation of potential economic development tools & strategies on case-by-case / transactional basis:

## Local Level

- Enhanced Infrastructure Financing Districts (“EIFD”) / Community Revitalization Investment Area (“CRIA”)
- Site-specific tax revenue (“SSTR”) pledges
- Impact fee reductions / waivers / deferrals
- Development opportunity reserve (“DOR”)
- Tax-exempt revenue & utility bonds
- Lease-leaseback financing
- Ground leases
- Operating covenants

## State & Federal Level

- Affordable Housing and Sustainable Communities (AHSC) Cap-and-Trade Funds
- Small Business Administration (SBA) loans
- U.S. Economic Development Administration (EDA) grants
- New Market Tax Credits (NMTCs)
- CA Infrastructure Bank (I-Bank) loans
- EB-5 Immigrant Investment
- Community Development Block Grants (CDBG)

# Post-RDA Economic Development Tools

Cities have 9 BASIC TOOLS for Public/Private Projects



*These tools often work best when used together*

# Questions & Discussion

Thank you



Kosmont Companies | Kosmont Realty Corporation | California Golden Fund (EB-5)  
1601 N. Sepulveda Blvd., #382 | Manhattan Beach, CA 90266  
(424) 456-3088 | [www.kosmont.com](http://www.kosmont.com) | CA Broker #01182660

# C COMMUNITY OUTREACH SUMMARY

## Workshop #1 Summary Notes

### Tustin Red Hill Avenue Specific Plan Workshop #1: Establishing the Vision for Tustin Red Hill Avenue July 21, 2016 Community Workshop

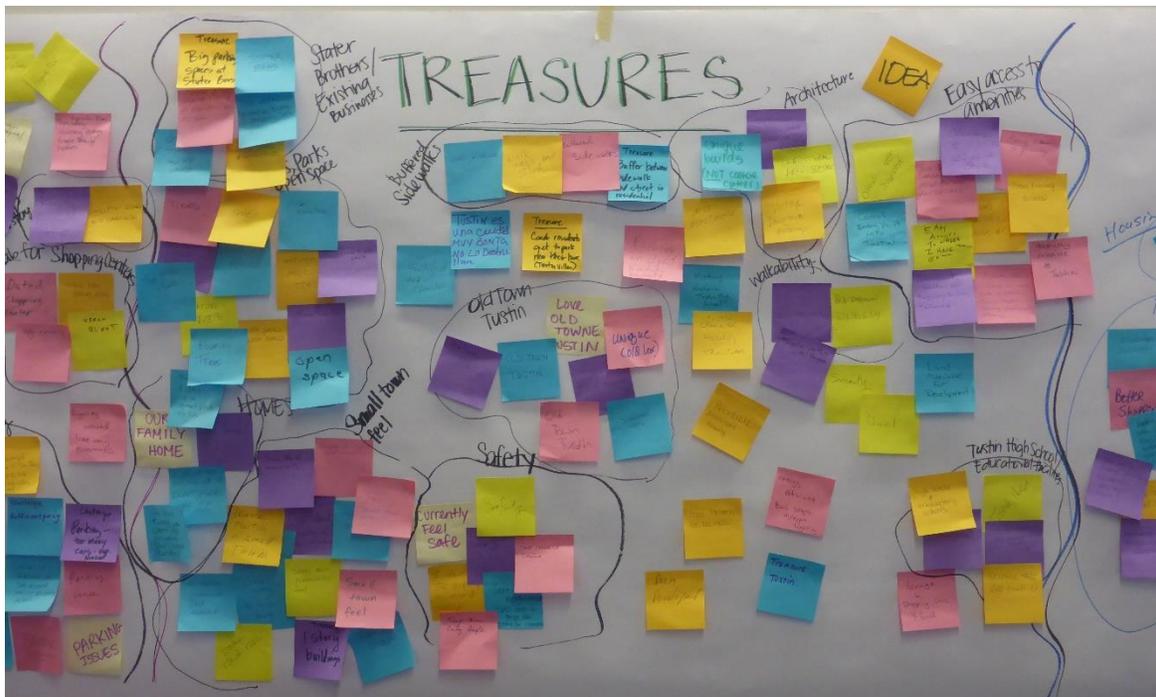
On July 21, 2016, the City of Tustin held the first Community Workshop for the Tustin Red Hill Avenue Specific Plan. Attendees learned about the planning process, project goals and objectives, and the overall project overview. Workshop attendees also participated in an interactive Post-It note exercise to identify treasures, challenges, and visions for the study area. Participants had the opportunity to provide their thoughts, concerns, and hopes for the future of their neighborhood. Approximately 90 participants attended the workshop.

The following Treasures, Challenges, and Visions were offered by workshop attendees and are provided verbatim in this summary.

***Repetitive listing of items/ideas represent their multiple occurrences on individual Post-It notes. These items/ideas are listed repeatedly to fully represent all workshop participants' views and input.***



Workshop participants were provided a map of the Specific Plan area.



Post-It Notes were used during an interactive exercise to identify Treasures, Challenges, and Visions for the Red Hill Corridor

## Treasures

Workshop participants were asked to provide their ideas and thoughts on what they value most in the Red Hill Avenue study area. Participants also shared their favorite things in the study area.

### Easy Access to Amenities

- Signage to shopping, gas, and food
- Convenient location to shopping, dining, and schools
- Easy freeway access
- Shops and eateries are near-by
- Open – not congested
- Easy access to where I have to go
- Central entry point into Tustin
- I like the three lanes on each side of the street – keeps traffic moving morning and evening
- Less traffic compared to neighboring arterials Easy access to freeways
- Gateway entrance to Tustin
- Closer to stores and bank

### Walkability

- Serenity
- Quiet
- Walkability

## Workshop #1 Summary Notes

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- Walkable
- Bicycle Paths on Bryan

### Architecture

- Traditional Architecture
- Consistent Architecture (Spanish?)
- Vintage downtown buildings
- Unique buildings (Not cookie cutter)



### Trees, Parks, Open Space

- Mature hardwood trees
- Pine tree park
- Open space
- The parks trees
- Trees
- Trees
- Trees
- Mature trees
- Parks – pine tree park
- Green mature trees
- Little parks near homes
- Blooming trees
- Nature trees



### Stater Brothers/Existing Businesses

- Nearby stores and restaurants
- Stater Brothers
- Stater Brothers shopping center at Redhill and Nisson
- Big parking spaces at Stater Brothers structure
- Second time thrift store
- Love workplace – Stater Brothers shopping center

### Safety

- No more crime
- Currently feel safe
- Redhill and San Juan traffic at intersection, accident ridden
- Safety
- Safety
- Redhill/San Juan St left turn light (too many crashes this year)
- Keep our City Safe
- I value a sense of security and safety

- Safe neighborhood – TPD does a great job protecting the community
- Lighting around freeway entrances

### Old Town Tustin

- Old Town Tustin
- Old Town
- Downtown Area
- Old Town
- Old Town Tustin
- Love Old Town Tustin
- Unique (Old hx)

### Buffered Sidewalks

- Walkways and Streets
- Buffered Sidewalks
- Buffer on Sidewalk
- Buffer between sidewalk and street in residential

### Small Town Feel

- Small Town Feel
- One story building
- Low density Housing
- Single Family, one-story, no HOA neighborhood with front and back yard
- Small town community feel
- Small town feel
- Rancho San Juan condos
- Leave Tustin a small town
- I love this small town feel Small town feel but modern
- One story building
- Neighborhood, no HOA
- Sense of community
- No high-rise commercial retail residential
- Low density housing new this corridor
- No apartments

### Homes

- Our family home
- I value my house as is don't f\*\*k with it!!
- My home!
- We love Tustin, we want to preserve our home (condos)
- The Tustin Villas is a great place to live

## Workshop #1 Summary Notes

- Condo residents get to park near their home (Tustin Villas)

### History/Historic Structures

- Tustin Character History, Structures
- History – Historic Tustin High School

### Other Treasures

- Affordable Investment Property
- Good location for business
- Parking on the street needed, necesito
- View of Foothills at end of corridor
- Tustin es una ciudad muy bonita no la destrollan (Tustin is a beautiful city, don't destroy it)



Post-It Notes were used during an interactive exercise to identify Treasures, Challenges, and Visions for Tustin Redhill Corridor

## Challenges

Workshop participants were asked to provide their ideas and thoughts on what the biggest challenges are in the Red Hill Avenue study area. Participants also identified the existing problems within the study area.

### Businesses

- Relocating existing business and residence
- Lack of stores/businesses I want to see

### Rehabilitation/Maintenance

- Cleanliness (lack of)
- Businesses looks run down
- Mantener nuestras casas protegidas (Keep our homes protected/safe)
- Mantener nuestro ciudad protesido (Keep our city protected/safe)
- Mantener nuestras casas; que con sacrificio, las hemos, mantenido (Keep our homes that with sacrifice, we have kept/maintained)

### Traffic and Noise

- Lots of Traffic
- Traffic Noise
- High Traffic Volume
- Only one lane to enter North and South bound on the 5 freeway
- Traffic congestion increase in cut through traffic on Lance – Diamond head to avoid signal
- Traffic is horrible on Redhill
- Morning traffic at Redhill/ECR
- Speed of traffic too high North of San Juan
- No quere mos mover nos (We do not want to move)
- Redhill traffic has increased noise and accidents
- Traffic flow problem
- Roads are bad
- Abysmal freeway access
- Traffic and noise from traffic on Redhill
- Challenge signal timing so people quit being killed or hurt
- Traffic, cars, people
- Quality of life
- Noise
- Congestion and noise

### History

- Keeping History
- Tustin History and character

## Workshop #1 Summary Notes

### Parking

- Parking
- Parking issues
- Pine tree park parking
- Parking issue
- Parking – too many cars especially on Nissan
- Not enough parking on San Juan St. next to High School and Elementary School
- Parking
- Parking
- Lack of street parking between San Juan and Bryan
- Challenges finding parking at Stater Brothers
- Parking
- Parking
- Parking is difficult along this corridor except at Big Lots
- Lack of parking in the higher density residential areas
- Street parking out of control
- Lack of street parking
- Challenge sufficient parking

### Bicycle Lanes

- Proper bike lanes
- Lack of bicycle lanes
- No bike lanes

### Crime/Safety

- Crime near hotel
- Safety
- Get rid of Key Inn
- Key Inn Clientele
- Safety at all freeway areas

### Property Taxes/Property Value

- Property Tax
- Decrease in property values
- How much is this going to cost me in property tax?
- Property taxes

### Landscaping

- Not enough landscaping/trees
- Lack of landscaping
- Less trees now



### Rehab for Shopping Center

- Looks rundown, older
- Ugly retail
- Urban blight
- Dated shopping center

### Type of Housing

- No high density residential housing
- Too many multi + units
- Finding a place to live after you take mine
- Affordable housing
- Apartments with no control re number of occupants\

### Community Character

- Taking away from old town feeling (example: old town feeling Tustin looks better/homey feeling)
- Density – do not encourage more people to live here in pursuit of revenue

### Project Area Boundaries

- How were boundaries chosen?
- Specific plan boundary cutting through single family homes

### Other Challenges

- Better maintained walking paths
- Quality of life
- New City council members not aware of property owner's needs
- Lack of public transportation
- Air quality
- Displaced school children
- I'm not agreed with this project
- Leave it alone
- No charge facility for electric vehicles
- Replacing current livelihood (income)
- Water

## Workshop #1 Summary Notes



Post-It Notes were used during an interactive exercise to identify Treasures, Challenges, and Visions for Tustin Redhill Corridor

### Visions

Workshop participants were asked to provide their ideas and thoughts on the future of the Red Hill Avenue study area. Participants identified concepts that can help improve the study area.

#### Sustainable

- Solar Power
- Irrigation use non potable water
- Upscale shopping and green building
- Accommodating to future (tele) communication needs and data demand related to transportation
- Energy self sufficient
- Green Recording
- Energy Efficient bus stops with night lighting
- A sustainable community that promotes education and art. That values individuals as well as the environment

#### Parking

- More available parking
- City parking
- Lots of off street parking
- Parking, parking, parking
- Landlords provide apartment parking structures

#### Greenery

- More parks

- More trees
- Trees
- Green belt
- Updated parks with plenty of parking
- More green areas
- More community gardens
- Fix water feature at Frontier Park
- Water Frontier Park
- Make 1000' wide each side of Redhill and continue to Irvine Blvd
- Lots of green/succulent space
- More greenery
- Another park in the area
- Nicer landscaping
- Green park areas
- Low density housing with lots of trees

### **Housing**

- No old folks retirement homes/center
- Affordable Housing for new buyers/renter
- Low income homes built so others have opportunities to afford their own home
- Housing for working people

### **Non-Vehicular Mobility**

- Nice sized bicycle lanes
- Walkabout type of town
- Bike lanes and bike rentals
- Vibrant walkable retail
- Pedestrian friendly (safer for kids to walk/bike to/from school)

### **Business**

- Better shopping
- Economically viable uses
- Remodel existing shopping center
- Get rid of gas tank on corner of Redhill and San Juan. Use the lot across of as Restaurants and outdoor shops
- Nicer Businesses
- Family friendly business
- Better use of Big Lots area, no one goes to that center
- Enhance existing shopping strip malls
- Update look to retail centers
- Improve shopping mall appearance

## Workshop #1 Summary Notes

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- Good looking shopping (grocery, dry cleaners, mall boxes) areas
- No vacant lots
- Buildings with shopping and housing and parking all in one

### Tustin High School/Educational Facilities

- Tustin High
- Schools that are near-by
- The High School, the Lambert Elementary School
- Education Facilities
- High School and elementary schools
- New/more preschool, elementary, and schools

### Safety

- Increased police presence
- Redhill/San Juan St left turn light!
- Safety
- The corner of Utt & San Juan & G.V. & " " becomes family friendly
- Increased police presence
- Be particular about the types of stores and what type of people you will attract to this area

### Improve Traffic

- Left hand turn signal at Redhill & San Juan
- Improve Traffic
- Improve Traffic Flow
- No Heavy Traffic
- Just want Redhill paved. Don't ruin this city!
- Increase freeway entrance number of lanes both north and south bound of the 5 freeway
- No parking allowed on Nisson between Redhill and Newport
- Change in street lights to decrease accidents – ex: San Juan/Redhill St.
- Reduce speed or add stop signs instead of red lights

### Old Town Feel

- Small town feel – community
- Keep the quaint homey community feel there is already enough multi-family living
- Keep buildings in a low profile
- Old town feel
- Not a repeat of Irvine/Downtown Anaheim
- Treat lower Tustin as well as new development (fairness)
- Low density
- Keep uniqueness
- Traditional old town architecture

- No multistory buildings
- Do not destroy Tustin! It's a beautiful City
- New, modern, more jobs
- To match the rest of Tustin/Irvine
- No urban style high density housing
- Build to community needs not developer whims
- No hotels, no apartments
- Don't make everything look the same – keep old construction

### Uniqueness

- History
- Architecture
- Community Center
- More upscale Businesses
- Tax Base for City

### Development

- No mas proyectos por favor (no more projects please)
- No more projects (no more projects)
- No queremos mas proyectos (don't want more projects)
- No mas proyectos (no more projects)
- No mas proyectos (no more projects)
- Land Available for Development

### Other Visions

- No additional residential so close to freeway
- Properly building zoning is complied with –
- Specific Plan boundary should stop at San Juan
- Hope proposed specific plan holds better than the N Tustin specific plan!
- Conscience
- Community Gym that is accessible to everyone in the community – activities for youth

### Questions from Participants:

- Who is on the project team?
- Has the City Council discussed and/or decided on future development plans within the Specific Plan area?
- Does the City have the power of eminent domain to acquire all or part of properties within the Specific Plan area?



## Workshop #1 Summary Notes



### Vision Statements

Following the Post-It note exercise, workshop attendees separated into small groups and developed group vision statements for the Red Hill Corridor. The vision statement describes the community's hopes, dreams, and aspirations; and serves as a tool to help guide short-term and long-term actions. The following vision statements were drafted by workshop participants and are provided verbatim in this summary:

#### Group 1

Maintain the current small town feel

Maintain the current residences

Bring in more attractive businesses that serve the local community

Improve traffic flow and safety

Provide more parking

Add more street greenery

#### Group 2

Small community

Natural Environment

Family Living

#### Group 3

Preserve small town feel while updating architecture and landscaping, keeping it low-density and non-car friendly.

### **Group 4**

Single family residence (not cheap)

Green grass in our parks

As residents of this beautiful city of Tustin, we like to see single family residences as opposed to more apartment buildings that usually do not provide enough parking for their tenants. We as proud residents of Tustin want to see green grass in our parks and mature trees. We like to see more upscale single family residences with plenty of parking.

### **Group 5**

Limit development to commercial area (we don't want people to lose their home)

Do not put residential in commercial area.

No additional strip malls and no "bog box" (like Target) stores

No buildings more than 2 story!!

No high density residential

More green/succulent spaces

More pedestrian and bike friendly

Keep a grocery store with big parking spaces in this corridor

Need another park with adequate parking

## Workshop #1 Summary Notes

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### Group 6

A sustainable community. One that is sustainable economically, environmentally, culturally, and socially. Also, adaptable to future technological changes.

Energy self sufficient

Adaptable to aging population

Lots of trees along streets (City of Trees)

### Group 7

Small town feel, environment friendly, safety, good education with affordable housing.

Affordability – Apartment and reasonably priced homes

Safety – Red Hill/San Juan Red Light and crime

Education – Higher scores and better schools

Cleanliness – Areas, businesses, parks, and streets

Small City – Individuality, differences, residential

Environment – Trees (Mature hard wood) and grass

### Group 8

Plan to improve our communities, environment, and nature but be sure to keep our towns current uniqueness and history. And be able to keep family homes to be able to pass down to our generations.

- More office buildings
- Indoor swapmeet
- High rise affordable housing
- More sit-down/family restaurants
- Maintain current residency and families
- More sidewalks
- Improve Pine Tree Park – parking and equipment
- Synchronized light signals
- Small lake or lagoon and fountains
- Better street lighting
- More bike lanes
- Continued uniqueness
- Small town feel
- Small child/baby child care
- Improve apt/condo parking - # of spaces/per unit
- Improve public parking (structure?)
- Tustin History
- Youth community center with pool – free or affordable
- Skate park
- Amphitheater
- More trees/gardens/nature

## Workshop #1 Summary Notes

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### Group 9

Redo Big Lots parking lot. Reface/remodel, more lighting

Empty lot for more dining/small businesses/health food restaurants – create jobs and income for the City

No multistory buildings/living quarters. Too much traffic/congestion/air pollution.

Maintain historic character of the City by design. More trees.

Concerned about eminent domain – taking our property.

### Group 10

Need food and entertainment

A livable, walkable a safe community that reflects spirit of small Tustin character

Improved traffic flow

Environmental considerations. Conservation, green construction, sustainability

No big Box retail, fewer franchises

### Comment Cards Received

Some workshop participants also provided additional comments and concerns about the project. Comments are provided verbatim, below.

**Comment 1**

*How were boundaries determined? We are concerned about eminent domain. Can or will properties be taken away or resized?*

**Comment 2**

*What is the purpose of the Corridor?*

**Comment 3**

*What will happen to our homes?*

**Comment 4**

*Will eminent domain take place here?*

**Comment 5**

*If more business is going to be brought in this area, it will bring more traffic.*

**Comment 6**

*Where is the City going to make space for all this improvement?*

**Comment 7**

*I'm concerned about traffic, congestion, and air pollution. By adding lanes to Red Hill my house is becoming damaged by vibration in the street. Will houses be taken by eminent domain by the City? Apparently my front land/yard was taken by this in the past.*

**Comment 8**

*The Red Hill Corridor plan entails a small area. Tustin needs to preserve Tustin's small town feel. No high density residences or businesses. Also, since they added #3 N/B lane on Red Hill Ave., Lance St. absorbs the overflow parking from the adjacent apartments. There has been an increase in traffic from Red Hill to Lance and up Diamond Head Dr. to Bryan Ave. (through residential area) in effort to avoid the signal.*

**Comment 9**

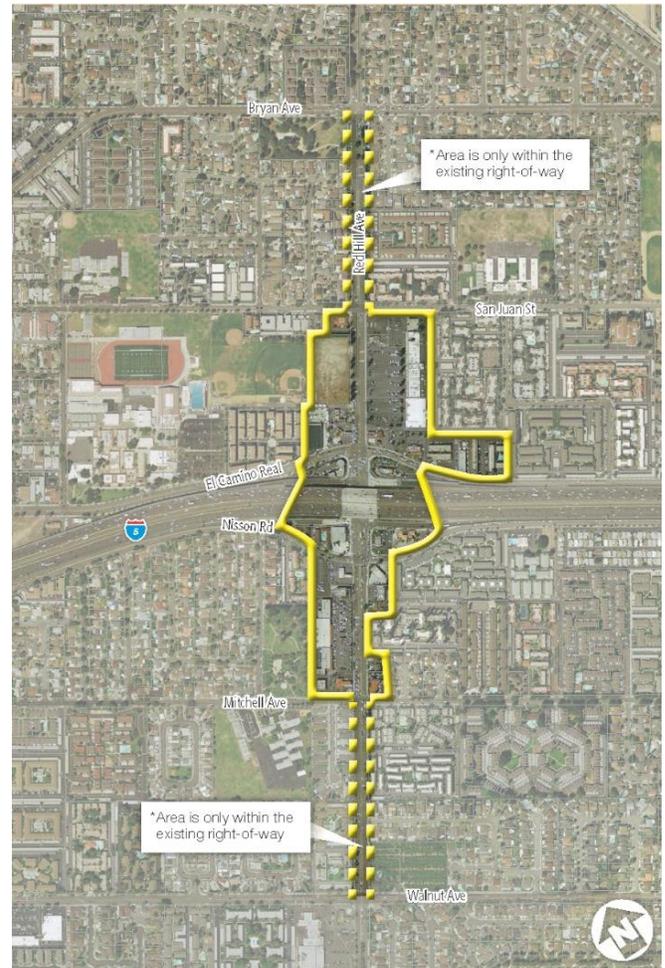
*Provide speakers with microphones.*

## Workshop #2 Notes

### Tustin Red Hill Avenue Specific Plan Workshop #2 December 1, 2016 Community Workshop

On December 1, 2016, the City of Tustin held the second Community Workshop for the Red Hill Avenue Specific Plan. Attendees learned about the planning process, project goals and objectives, and the overall project overview. Workshop attendees also participated in an interactive Post-It note exercise to identify land uses, opportunity areas and concerns, transportation, streetscape, and big ideas. Participants had the opportunity to provide their thoughts, concerns, and hopes for the future of this area. Approximately 45 participants attended the workshop.

***Comments are provided verbatim. Please refer to workshop maps and boards for specific project area location references and comments on images of potential future development in the project area.***



### Station 1: Project Overview

Workshop participants were asked to provide their ideas and thoughts on what they value most in the Red Hill Avenue study area. Participants also shared their favorite aspects of the study area. Community comments from the first workshop were provided to remind participants of what was said.

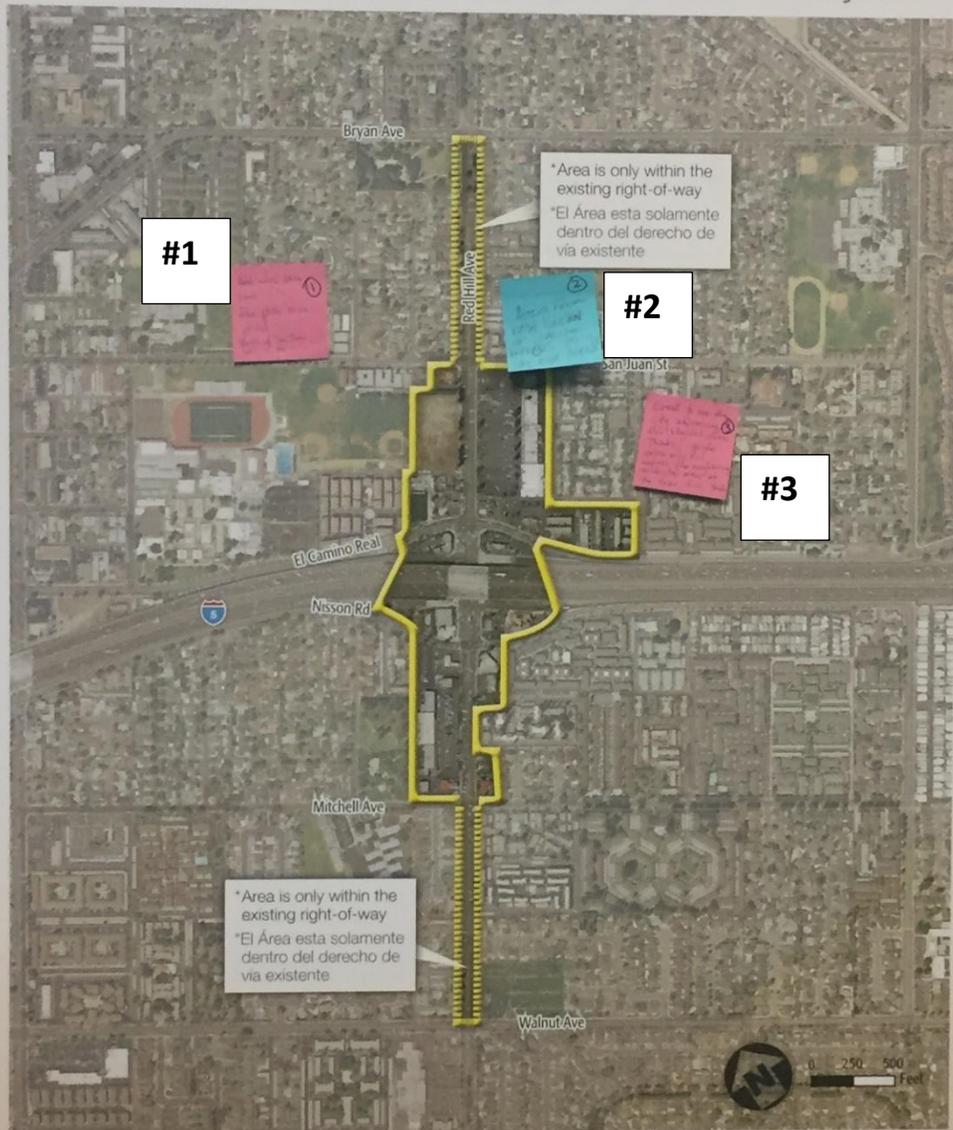
**BOARD: Proposed Revised Project Area Map (from the first to the second workshop, the residential area to the north and west of San Juan was removed from the project area)**

1. *Add this area back into the plan area please, North of San Juan West of Red Hill*
2. *Audio from new section of stadium [from the Tustin High School] affects us way over here!*
3. *Great to see the city addressing this blighted area. Thanks... go the extra mile and address the neighboring residential areas at the same time. Thanks*

**Workshop #2 Notes**

**S Station #1: Project Overview**  
**E Estación #1: Resumen del Proyecto**

**Proposed Revised Project Area Map**  
**Propuesta con Revisiones del Mapa del Área del Proyecto**



\*Existing "Right-of-Way" includes property dedicated to the City through an easement for roadway and sidewalk purposes.  
 \*El "Derecho de Via" incluye la propiedad dedicada a la Ciudad a través de una derecho de paso para proposito de la carretera o la acera\*

*Workshop participants proposed other areas to include in the Specific Plan project area.*

**Station #1: Community Outreach Vision Statements**

**Estación #1: Difusión Comunitaria Declaraciones de la Visión**



Visions for Redhill Avenue  
 Plan Específico de la Avenida Red Hill

**GROUP 1**

- Maintain the current small town feel
- Maintain the current residences
- Bring in more attractive businesses that serve the local community
- Improve traffic flow and safety
- Provide more parking
- Add more street greenery

**GRUPO 1**

- Conservar las viviendas existentes
- Abrir más negocios atractivos que sirvan a la comunidad local
- Mejorar el flujo vial y la seguridad
- Proveer más estacionamiento
- Aumentar la vegetación en las calles

**GROUP 2**

- Small community
- Natural Environment
- Family Living

**GRUPO 2**

- Comunidad pequeña
- Ambiente Natural
- Vida Familiar

**GROUP 3**

- Preserve small town feel while updating architecture and landscaping, keeping it low-density and non-car friendly.

**GRUPO 3**

- Conservar la atmósfera de ciudad pequeña al mismo tiempo de actualizar la arquitectura y el paisajismo, manteniendo un ambiente de baja densidad y que no sea atractivo para andar en coche.

**GROUP 4**

- Single family residence (not cheap)
- Green grass in our parks
- As residents of this beautiful city of Tustin, we like to see single family residences as opposed to more apartment buildings that usually do not provide enough parking for their tenants. We as proud residents of Tustin want to see green grass in our parks and mature trees. We like to see more upscale single family residences with plenty of parking.

**GRUPO 4**

- Residencia unifamiliar (no de bajo precio)
- Césped verde en nuestros parques
- Como residentes de esta hermosa ciudad de Tustin, queremos ver residencias familiares en lugar de edificios de apartamentos que por lo general no proveen suficiente estacionamiento para sus inquilinos. Como residentes orgullosos de la ciudad de Tustin, queremos ver más césped verde y árboles crecidos en nuestros parques. Nos gustaría ver más residencias familiares exclusivas con suficiente estacionamiento.

*Community Vision Statements from Workshop #1*

**Station #1: Community Outreach Vision Statements**

**Estación #1: Difusión Comunitaria Declaraciones de la Visión**



Visions for Redhill Avenue

Plan Específico de la Avenida Red Hill

<p><b>GROUP 5</b></p> <ul style="list-style-type: none"> <li>• Limit development to commercial area (we don't want people to lose their home)</li> <li>• Do not put residential in commercial area</li> <li>• No additional strip malls and no "big box" (like Target) stores</li> <li>• No buildings more than 2 story!!</li> <li>• No high density residential</li> <li>• More green/succulent spaces</li> <li>• More pedestrian and bike friendly</li> <li>• Keep a grocery store with big parking spaces in this corridor</li> <li>• Need another park with adequate parking</li> </ul>	<p><b>GRUPO 5</b></p> <ul style="list-style-type: none"> <li>• Limitar el desarrollo a las zonas comerciales (no queremos que la gente pierda sus viviendas)</li> <li>• No pongan zonas residenciales en zonas comerciales</li> <li>• No más centros comerciales de tipo "gran almacén" (como Target)</li> <li>• ¡No edificios que tengan más de dos pisos!</li> <li>• No áreas residenciales de alta densidad</li> <li>• Más espacios verdes/atractivos</li> <li>• Más áreas atractivas para andar a pie o en bicicleta</li> <li>• Mantener una tienda de abarrotes con espacios grandes de estacionamiento en este corredor</li> <li>• Se necesita otro parque con estacionamiento adecuado</li> </ul>
<p><b>GROUP 6</b></p> <ul style="list-style-type: none"> <li>• A sustainable community. One that is sustainable economically, environmentally, culturally, and socially. Also, adaptable to future technological changes.</li> <li>• Energy self sufficient</li> <li>• Adaptable to aging population</li> <li>• Lots of trees along streets (City of Trees)</li> </ul>	<p><b>GRUPO 6</b></p> <ul style="list-style-type: none"> <li>• Una comunidad sostenible</li> <li>• Una comunidad que sea sostenible económicamente, ambientalmente, culturalmente y socialmente. También adaptable a futuros cambios tecnológicos.</li> <li>• Autosuficiente energéticamente</li> <li>• Adaptable a una población de edad madura</li> <li>• Muchos árboles en las calles (una Ciudad de Árboles)</li> </ul>
<p><b>GROUP 7</b></p> <ul style="list-style-type: none"> <li>• Small town feel, environment friendly, safety, good education with affordable housing.</li> <li>• Affordability – Apartment and reasonably priced homes</li> <li>• Safety – Red Hill/San Juan Red Light and crime</li> <li>• Education – Higher scores and better schools</li> <li>• Cleanliness – Areas, businesses, parks, and streets</li> <li>• Small City – Individuality, differences, residential</li> <li>• Environment – Trees (Mature hard wood) and grass</li> </ul>	<p><b>GRUPO 7</b></p> <ul style="list-style-type: none"> <li>• Una atmósfera de ciudad pequeña, medio ambiente amigable, seguridad, buena educación con viviendas de costo accesible</li> <li>• Asequibilidad - apartamentos y casas a precios razonables</li> <li>• Seguridad – Red Hill/San Juan, Luz Roja y Crimen</li> <li>• Educación – Calificaciones más altas y mejores escuelas</li> <li>• Limpieza - Áreas, empresas, parques y calles</li> <li>• Ciudad pequeña - Individualidad, diferencias, residencial</li> <li>• Medio ambiente – Árboles (crecidos de madera dura) y césped</li> </ul>

Community Vision Statements from Workshop #1

**Station #1: Community Outreach Vision Statements**  
**Estación #1: Difusión Comunitaria Declaraciones de la Visión**



**Visions for Redhill Avenue**  
**Plan Específico de la Avenida Red Hill**

**GROUP 8**

Plan to improve our communities, environment, and nature but be sure to keep our towns current uniqueness and history. And be able to keep family homes to be able to pass down to our generations.

- More office buildings
- Indoor swap meet
- High rise affordable housing
- More sit-down/family restaurants
- Maintain current residency and families
- More sidewalks
- Improve Pine Tree Park – parking and equipment  
Synchronized light signals
- Small lake or lagoon and fountains
- Better street lighting
- More bike lanes
- Continued uniqueness
- Small town feel
- Small child/baby child care
- Improve apt/condo parking - # of spaces/per unit  
Improve public parking (structure?)
- Tustin History
- Youth community center with pool – free or affordable

**GRUPO 8**

Un plan para mejorar nuestras comunidades, el medio ambiente, y la naturaleza pero asegurándose de mantener la historia y originalidad actual de nuestras ciudades. Y también poder conservar las casas familiares para poderlas heredar a nuestras generaciones.

- Más edificios de oficinas
- Un Mercado Pulga interior
- Viviendas en Edificios Altos a precios razonables
- Más restaurantes familiares/con mesas
- Mantener las residencias actuales y las familias
- Más aceras
- Mejorar el Parque Pine Tree – el estacionamiento y el equipo
- Semáforos sincronizados
- Un lago pequeño o laguna y fuentes
- Mejor alumbrado en las calles
- Más carriles para bicicletas
- Continuidad en la singularidad
- Un ambiente de pueblo pequeño
- Cuidado de niños y bebés
- Mejorar el estacionamiento de apartamentos/condominios - # espacios/unidad
- Mejorar el estacionamiento público (¿Estructura?)
- Historia de Tustin
- Centro de comunidad juvenil con piscina – gratis o de bajo costo
- Parque de patinaje
- Anfiteatro
- Más árboles/jardines/naturaleza

*Community Vision Statements from Workshop #1*

**Station #1: Community Outreach Vision Statements**  
**Estación #1: Difusión Comunitaria Declaraciones de la Visión**

TUSTIN  
RED HILL AVENUE  
SPECIFIC PLAN

Visions for Redhill Avenue  
Plan Específico de la Avenida Red Hill

**GROUP 9**

- Redo Big Lots parking lot. Reface/remodel, more lighting
- Empty lot for more dining/small businesses/health food restaurants – create jobs and income for the City
- No multistory buildings/living quarters. Too much traffic/congestion/air pollution.
- Maintain historic character of the City by design. More trees.
- Concerned about eminent domain – taking our property.

**GRUPO 9**

- Reconstruir el estacionamiento de Big Lots. Restaurar/remodelar, más alumbrado
- Lote vacante para más lugares para cenar/negocios pequeños/restaurantes de comida saludable – crear fuentes de trabajo e ingresos para la ciudad
- No edificios de pisos múltiples/viviendas. Demasiado tráfico/congestionamiento/contaminación ambiental.
- Mantener el carácter histórico de la Ciudad por diseño. Más árboles.
- Preocupados por el dominio eminente – la toma de nuestras propiedades.

**GROUP 10**

- Need food and entertainment
- A livable, walkable a safe community that reflects spirit of small Tustin character
- Improved traffic flow
- Environmental considerations. Conservation, green construction, sustainability
- No big Box retail, fewer franchises

**GRUPO 10**

- Necesitamos comida y entretenimiento
- Una comunidad segura, con vida, para pasear a pie, que refleje el espíritu del carácter del pequeño Tustin
- Mejor flujo vial
- Consideraciones ambientales. Conservación, construcción verde, sostenibilidad
- No almacenes grandes, menos franquicias

Community Vision Statements from Workshop #1

**Station #1: Project Overview**  
**Estación #1: Resumen del Proyecto**



Existing conditions in the Tustin Red Hill Avenue project area  
Condiciones existentes en el área del proyecto  
en la Avenida Red Hill en Tustin



*Existing conditions in the project area*

## Workshop #2 Notes

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### Station 2: Land Use and Activities

#### **BOARD: Where would you like to see MIXED-USE?**

Workshop attendees identified areas where they would like to see mixed-use development using blue sticky dots. Identified areas are located between San Juan Street and El Camino Real; between Nissan Road and Mitchell Avenue; and the commercial center at the southwest corner of Red Hill Avenue and Walnut Avenue. *(See map on Page 10)*

1. *Add Parking @ old '76 station. Even if it is just temporary. Stater Bros. no parking @ peak hours*

#### **BOARD: Examples of Mixed-Use Developments**

1. *Picture 1: way too modern! Looks like Irvine*
2. *Picture 2: nice*
3. *Picture 3: ugly!*
4. *Picture 4: nice*
5. *Picture 5:*
  - *Nice, but with parking*
  - *3-story ok, 4<sup>th</sup> floors only if setback from streets and existing residential*
  - *Not more than 3 stories*
  - *Nice if parking structure abuts this location*

**Station #2: Land Use and Activities**  
**Estación #2: Actividades y Uso del Terreno**

**TUSTIN**  
 RED HILL AVENUE  
 SPECIFIC PLAN

Where would you like to see MIXED-USE?  
 Use the BLUE dots to identify locations in the project area

¿En dónde le gustaría ver terreno de Uso MIXTO?  
 Utilice los puntos adhesivos AZULES para identificar las ubicaciones en el área del proyecto

**#1**

\*Area is only within the existing right-of-way  
 \*El Área esta solamente dentro del derecho de via existente

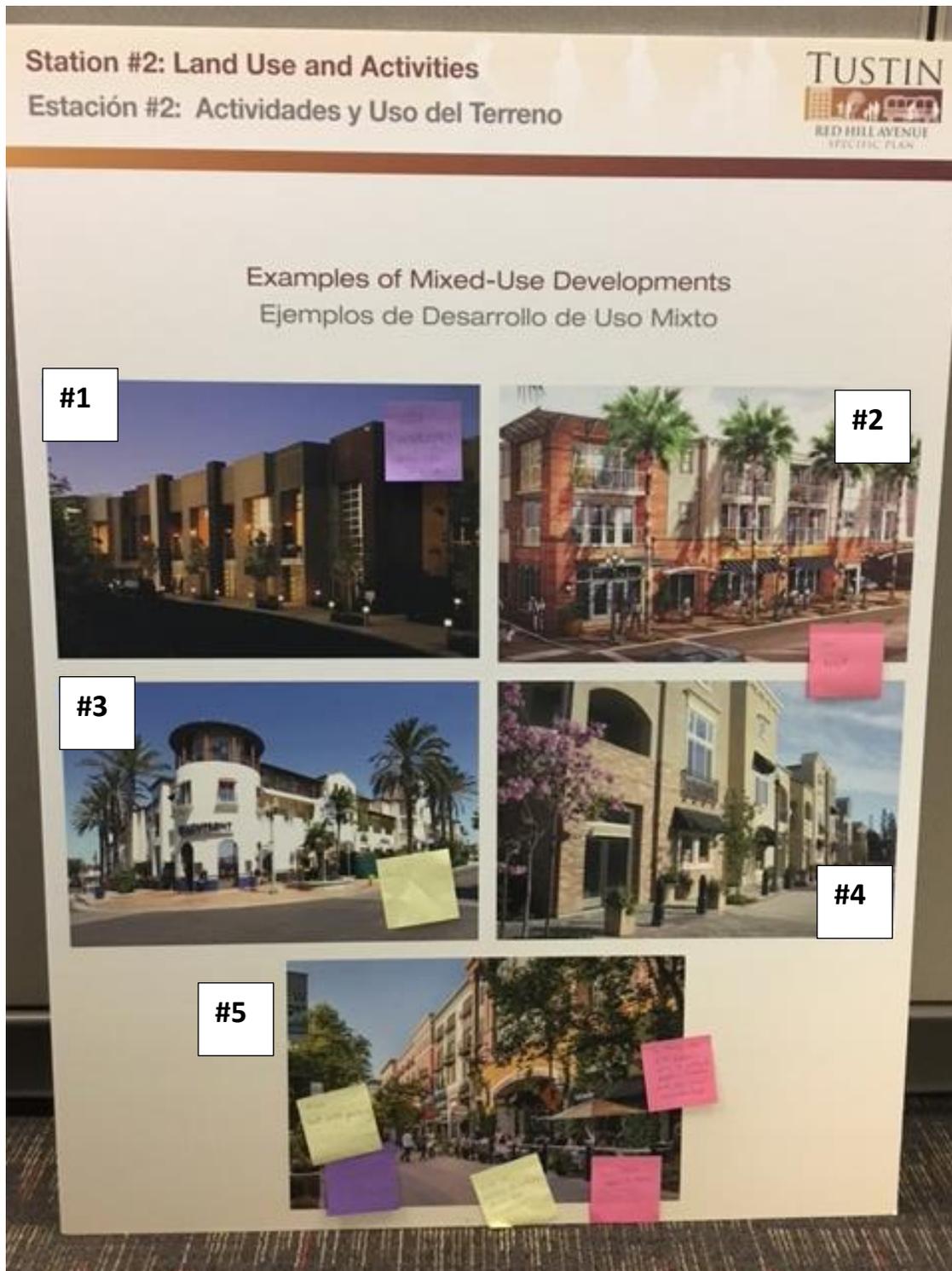
\*Area is only within the existing right-of-way  
 \*El Área esta solamente dentro del derecho de via existente

0 250 500 Feet

*\*Existing "Right-of-Way" includes property dedicated to the City through an easement for roadway and sidewalk purposes.  
 \*El "Derecho de Via" incluye la propiedad dedicada a la Ciudad a través de una derecho de paso para propósito de la carretera o la acera*

*Workshop attendees identified areas where they would like to see mixed-use*

## Workshop #2 Notes



*Workshop attendees provided comments on the type of mixed-use development that they prefer*

**BOARD: Where would you like to see COMMERCIAL uses?**

Workshop attendees identified areas where they would like to see commercial uses using red sticky dots. Identified areas are located between San Juan Street and Mitchell Avenue. (See map on Page 13)

1. *How about mixed use parking (day commercial and night residential) where apartments back up to commercial*
2. *Don't add residential units in this area. Focus of commercial. Move parking off Red Hill behind new commercial*
3. *Incorporate park into shopping center design*
4. *Yes! (to #3 comment)*
5. *Can we include this? (Commercial area on the southwest corner of Red Hill Avenue and Walnut Avenue)*

**BOARD: Examples of Commercial Uses**

1. Picture 1: No comments
2. Picture 2: No comments
3. Picture 3: *This style is too modern doesn't fit "old town" feel*
4. Picture 4: *Nice*
5. Picture 5: No comments
6. Picture 6: No comments

**Workshop #2 Notes**

**Station #2: Land Use and Activities**  
**Estación #2: Actividades y Uso del Terreno**

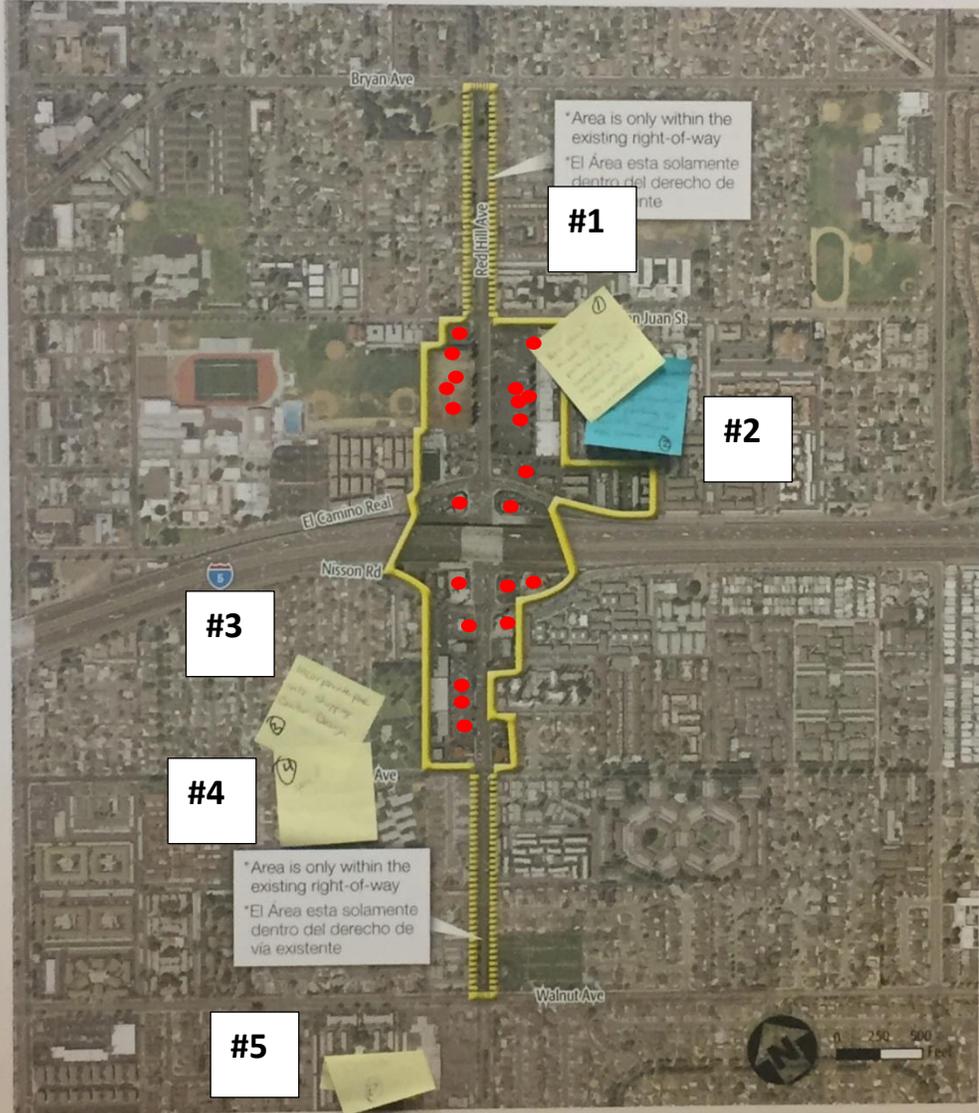


Where would you like to see COMMERCIAL uses?

Use the RED dots to identify locations in the project area

¿En dónde le gustaría ver terreno de uso COMERCIAL?

Utilice los puntos adhesivos ROJOS para identificar las ubicaciones en el área del proyecto



\*Existing "Right-of-Way" includes property dedicated to the City through an easement for roadway and sidewalk purposes.

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*Workshop attendees identified areas where they would like to see commercial uses*

Station #2: Land Use and Activities  
Estación #2: Actividades y Uso del Terreno



Examples of Commercial Uses  
Ejemplos de Terreno de Uso Comercial



Workshop attendees provided comments on the type of commercial development that they prefer

## Workshop #2 Notes

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### BOARD: Where would you like to see RESIDENTIAL uses?

Workshop attendees identified areas where they would like to see residential uses using yellow sticky dots. Workshop attendees identified the vacant parcel south of San Juan Street, on the west side of Red Hill Avenue; the parking lot on the southeast corner of Red Hill Avenue and San Juan Street; and the commercial center on the southwest corner of Red Hill Avenue and Walnut Avenue. (See map on Page 16)

### BOARD: Examples of Residential Uses

1. Picture 1:
  - *Like Bryan and El Camino. I like this*
2. Picture 2:
  - *Ugly*
  - *Ugly*
  - *Architecture needs canopies, patios, overhangs for shade and attractiveness/ Blank, flat walls even with windows are energy inefficient and ugly!*
3. Picture 3:
  - *Fits Tustin character*
  - *Nice*
  - *Underground parking for customers of mixed use*
  - *Nice. No higher with parking*
4. Picture 4:
  - *Modern architecture ok don't compete with old town*
    - *Agree*
    - *Agreed!*
5. Picture 5:
  - *Old town character*
6. Picture 6:
  - *Looks too much like Irvine!*

**Station #2: Land Use and Activities**  
**Estación #2: Actividades y Uso del Terreno**

**TUSTIN**  
 RED HILL AVENUE  
 SPECIFIC PLAN

Where would you like to see RESIDENTIAL uses?  
 Use the YELLOW dots to identify locations in the project area

¿En donde le gustaría ver terreno RESIDENCIAL?  
 Utilice los puntos adhesivos amarillos para identificar los lugares en el área del proyecto

\*Area is only within the existing right-of-way  
 \*El Área esta solamente dentro del derecho de via existente

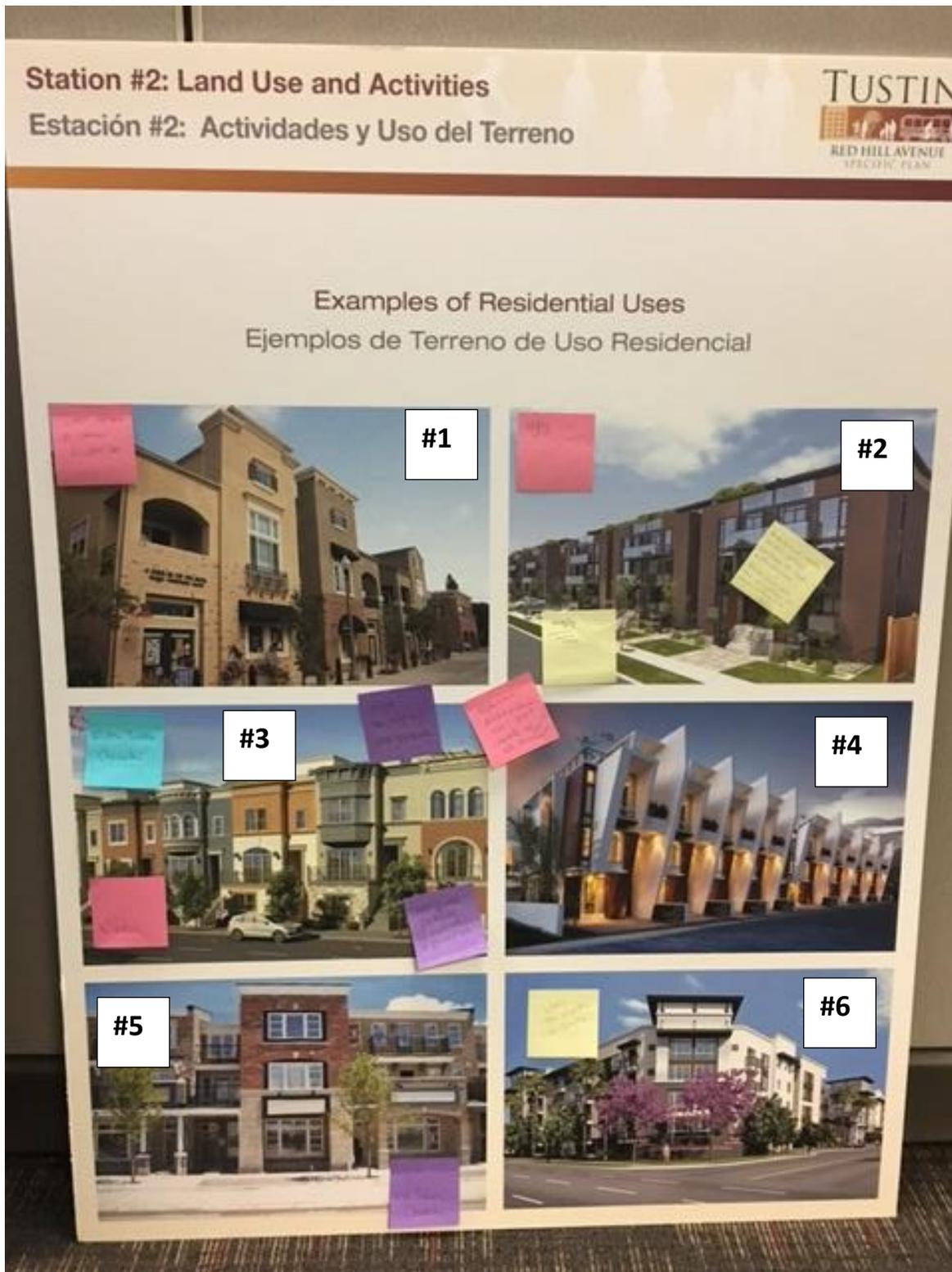
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0 250 500 Feet

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 \*El "Derecho de Via" incluye la propiedad dedicada a la Ciudad a través de una derecho de paso para propósito de la carretera o la acera*

*Workshop attendees identified areas where they would like to see residential uses*

## Workshop #2 Notes



*Workshop attendees provided comments on the type of residential development that they prefer*

**BOARD: Existing Land uses in the Project Area**

1. *Don't Access control*
2. *Shared use parking strategy*
3. *Crosswalks*
4. *Keep open space? Parking lot*
5. *Two right turn lanes on to Highway*
6. *Grocery important*
7. *Provide park access*
8. *Two right turn lanes onto highway*
9. *Build enough parking*
10. *Redesign ingress/egress*
11. *Add more parking! District-wide*

**What types of land uses and activities would you like to see in the project area?**

- *I can't imagine carrying groceries, children, pets, etc... through a garage up flights of stairs while sick/incapacitated just to make the area more aesthetically pleasing*
- *Having the tenants pay for parking or walking blocks to be able park a vehicle also reduces quality of life*
- *High Density Housing reduces quality of life for those who are not also provided adequate parking*
- *Commercial with parking but no additional residential*
- *Focus more on commercial use along Red Hill*
- *Residential 20-25 du/ac @ 99 cents center. SW Red Hill/Walnut*
- *Mixed uses that activate the street*
- *Big Lots site: mixed use that incorporates parking structure with wrap around retail with option for residential long-term parking*
- *Use the current vacant land to build a multi-level parking structure to serve a mini-mall built in the current Big Lots strip mall. Build a pedestrian bridge over Red Hill to connect parking to the mall*
- *Streetscape plan*
- *Density is not a 4-letter word but no more than 30 du/acre*
- *Please do not make our city look like a dessert*
- *Groceries and shopping area*
- *Places to hang out*
- *Affordable housing*
- *Mixed use yes, 20-22 du/ac*
- *Residential mixed-use @ Big Lots center, 25-30 du/acre*
- *Residential use should not be considered. Commercial use exists and should be maintained*
  - *YES!*
- *Family friendly activity area*

**Workshop #2 Notes**

**Station #2: Land Use and Activities**  
**Estación #2: Actividades y Uso del Terreno**



Existing land uses in the project area  
 Tipo de Terreno existente en el área del proyecto



*Workshop attendees identified land uses in the project area*



## Workshop #2 Notes

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### Station 3: Opportunity Areas/ Areas of Concern

#### **BOARD: Please Identify “Opportunity Areas” and “Areas of Concern”**

Workshop attendees identified “opportunity areas” using green sticky dots and “areas of concern” using red sticky dots. (See map on Page 22)

1. *Incorporate park into shopping center design*
2. *This shopping center needs to go residential (SW Corner Red Hill/Walnut)*
3. *This shopping center needs to be revamped*

#### **BOARD: Please describe the concerns and opportunities within the project area.**

- *Too much additional residential- not a good idea*
- *Reduce driveways*
- *Bike lanes similar to Newport Ave would be great*
- *Provide more side-walks*
- *The bike lane transition at Red Hill/ El Camino is incredibly dangerous*
- *Make walking along Red Hill a more pleasant experience*
- *Correcting the ‘timing of traffic lights’ is not a real fix*
- *Bike lanes, sharrows, green paint – on the bike lane (“Sharrows” are shared, use arrows used to identify bike routes along a roadway)*
- *Bottleneck traffic under freeway- big concern*
- *Key Inn area is a crime area- graffiti, etc., trash, dumped oil*
- *Provide good street lightings*
- *I’m concerned that the residential areas north of San Juan have been removed from the plan area*
- *Traffic concerns during construction*
- *Homeless camps*
- *More open spaces*
- *Include shopping center at southwest corner of Red Hill and Walnut in the study area*
- *Old gas station on corner of Red Hill Nissan eye sore and too many homeless people in area*
- *Bike lanes*
- *Parking: Our neighborhood has become a parking lot for condo residents. Need more parking*
- *Underground parking a possibility in new areas? (New opportunity areas/areas of concern)*
- *Parking and traffic*
- *Shopping center on Walnut/Red Hill needs to be addressed → “ditto” post-it as well*
- *Lance Street is clogged every night with parked cars of people in nearby apts. Our guests can’t find parking!*
- *Future traffic congestion in addition to what exists currently 2. Parking issues*

**Station #3: Opportunity Areas/Areas of Concern**  
**Estación #3: Áreas de Oportunidad/Áreas de Preocupación**

**TUSTIN**  
 RED HILL AVENUE  
 SPECIFIC PLAN

Please identify "Opportunity Areas" and "Areas of Concern"  
 Use the GREEN dots to identify Opportunity Areas  
 Use the RED dots to identify Areas of Concern

Por favor identifique las "Áreas de Oportunidad" y las "Áreas de Preocupación"  
 Utilice los puntos adhesivos verdes para identificar las Áreas de Oportunidad y utilice los puntos adhesivos rojos para identificar las Áreas de Preocupación

Labels on map: Bryan Ave, Red Hill Ave, San Juan St, El Camino Real, Nisson Rd, Michener Ave, Walnut Ave.

Text boxes on map:

- #1: Corporate Park into Shopping Center Design
- #2: [Handwritten note]
- #3: This Shopping Center needs to be reworked

Callouts:

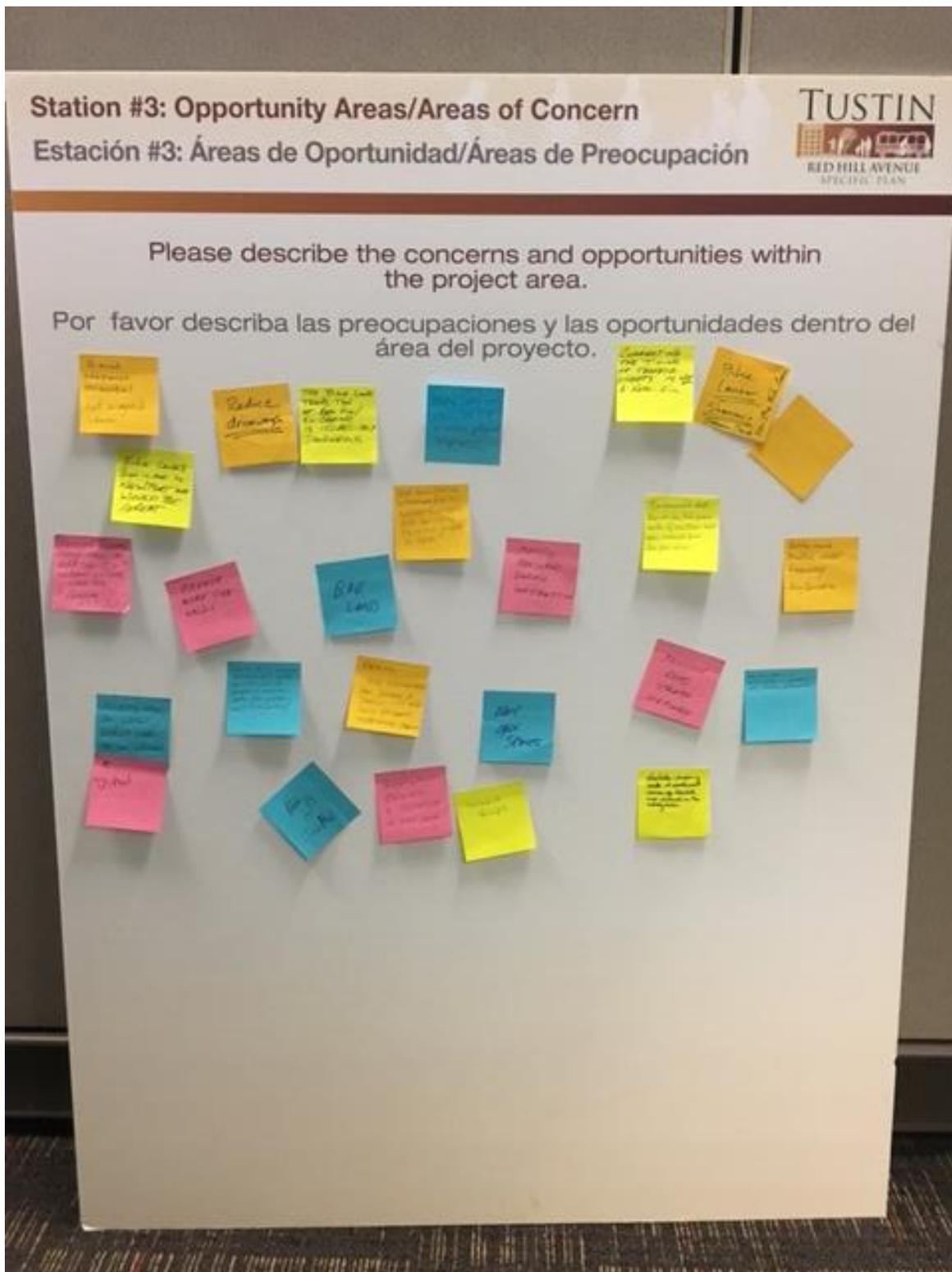
- \*Area is only within the existing right-of-way
- \*El Área esta solamente dentro del derecho de via existente

Scale: 0 250 500 Feet

\*Existing "Right-of-Way" includes... the City through an easement for roadway and sidewalk purposes.  
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Workshop attendees identified opportunity areas and areas of concern in the project area

## Workshop #2 Notes



*Workshop attendees identified opportunities and concerns in the project area*

## Station 4: Transportation, Circulation and Mobility

### BOARD: What are your IDEAS for improving transportation, circulation and mobility in the project area?

- No roundabouts!
- Facilitate student access across Red Hill
- Address the parking issues in the neighboring residential areas. Existing parking problems will overflow into the Red Hill corridor area and corridor parking issues will flow into the residential area making these areas worse.
- Bottleneck under the freeway- What's the solution, I don't see one
- Don't like circle intersection (round about)
- Reduce driveways
- Parking
- Install medians limit left turns
- Parking, parking! Need a parking structure for overflow condo residents. Our neighborhood is now a parking lot for them.
- Freeway/Redhill circulation
  - Better light sync, land direction could change between am/pm (but not easily coordinate) widen lanes (but not favored)
- Need to redesign entrances to the Big Lots parking area. Currently, traffic turning into the parking area comes to almost a complete stop causing a backup that often extends across El Camino Real and under the Freeway. Also cars exiting the parking lot are entering Red Hill at a very slow speed
- At the San Juan/ Red Hill intersection change the Red Hill signals to include left-hand turn arrows
- Need to expand the San Juan/ Red Hill intersection to allow for left turn lanes on San Juan. Currently, if the lead car on San Juan intends to turn left, cars behind squeeze through to the right and enter the intersection without a clear view at cross traffic leading to many accidents
- At the San Juan/ Red Hill intersection many times drivers run through the intersection right after the light turns red. Change the timing so that the cross-traffic light does not turn green for a second or two after the other light turns red.
- We need neighborhoods to not be overflow parking for condos
- Bridge over Red Hill for students to walk from school to commercial area
- Change start time of high school so it's after work rush hour
  - Synchronize signals do not add more traffic obstruction
- Move commercial to street and relocate parking to structures behind the commercial
- Signal synchronization
- Add more parking
- Red Hill and freeway area (between El Camino and Nissan) is a scary dangerous place to drive. Make it safe!
- Red Hill is seven lanes wide- Focus on vehicular and bike traffic
- No circular roundabouts what-so-ever. Not feasible with current traffic conditions

## Workshop #2 Notes

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- Slow traffic on Red Hill by activating the street building street-scape



## Workshop #2 Notes

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### Station 5: Streetscape, Amenities and Urban Design

#### BOARD: Examples of Streetscape, Amenities and Urban Design Improvements

*\*One workshop participant provided input on the types of streetscape and amenities that they preferred*

1. Streetscape Picture 1: *NO*
2. Streetscape Picture 2: *NO*
3. Streetscape Picture 3: *YES*
4. Streetscape Picture 4: *NO*
5. Benches Picture 5: *YES*
6. Benches Picture 6: *NO*
7. Benches Picture 7: *YES*
8. Benches Picture 8: *YES*
9. Bicycle Racks Picture 9: *YES*
10. Bicycle Racks Picture 10: *NO*
11. Bicycle Racks Picture 11: *NO*
12. Bicycle Racks Picture 12: *NO*
13. Litter Receptacles Picture 13: *NO*
14. Litter Receptacles Picture 14: *YES*
15. Litter Receptacles Picture 15: *YES*
16. Litter Receptacles Picture 16: *NO*

#### BOARD: What are your IDEAS for improving the streetscape and urban design features in the project area?

- *Environmentally friendly*
- *Build a streetscape that improves aesthetics of Red Hill*
- *Wayfinding is silly*
- *Any gateway should honor wishing well gateways- We are not East Tustin/Irvine*
- *Need pedestrian separation from traffic through the use of parkways*
- *Create textural changes in paving for traffic calming and character/interest (painting/ stamped concrete)*
- *Sidewalks need to have some character to them not the typical/straight concrete slabs... with landscaping*
- *Murals along underpass and interesting lighting*
- *Wider sidewalks that two people can actually use (yes! Comment next to it)*
- *More landscaping*
- *Wider sidewalks – more walking space*
- *Streetscape- landscape*
  - *Median, parkway, historic public art on underpass walls, light posts and sign posts in the style of old town*
- *Signage and wayfinding (AMEN comment next to it)*

- *Should be timeless, representative, compliments identity of Tustin*
- *Lighting improvements that allows for change, mood and seasons*
- *Walkable streets*
- *The current project area needs to include high density housing on San Juan so that density (w/o parking) will decrease*

**BOARD: Potential Locations and Types of Signage, Wayfinding, and Identity Elements**

1. *Ensure that streetlight design coordinates with street furniture and that light standards will be designed in a way that will accommodate small cell antennas for wireless services*
2. *Make sure signage is tall enough to be seen from freeway*

Gateway A

3. *Pillars- Use less space and can be on both sides of Freeway*

Gateway B

No comments

Gateway C

4. *Nice one!*
5. *Great opportunity. Hope to see more of this type of thinking*

Secondary Gateway Signage

No comments

Directional Wayfinding

6. *Directional signs to "old town" would be good!*

Identity: Placemaking

7. *Paint? T u s t i n in underpass so car can see/at angle. High School mural project, have competition*

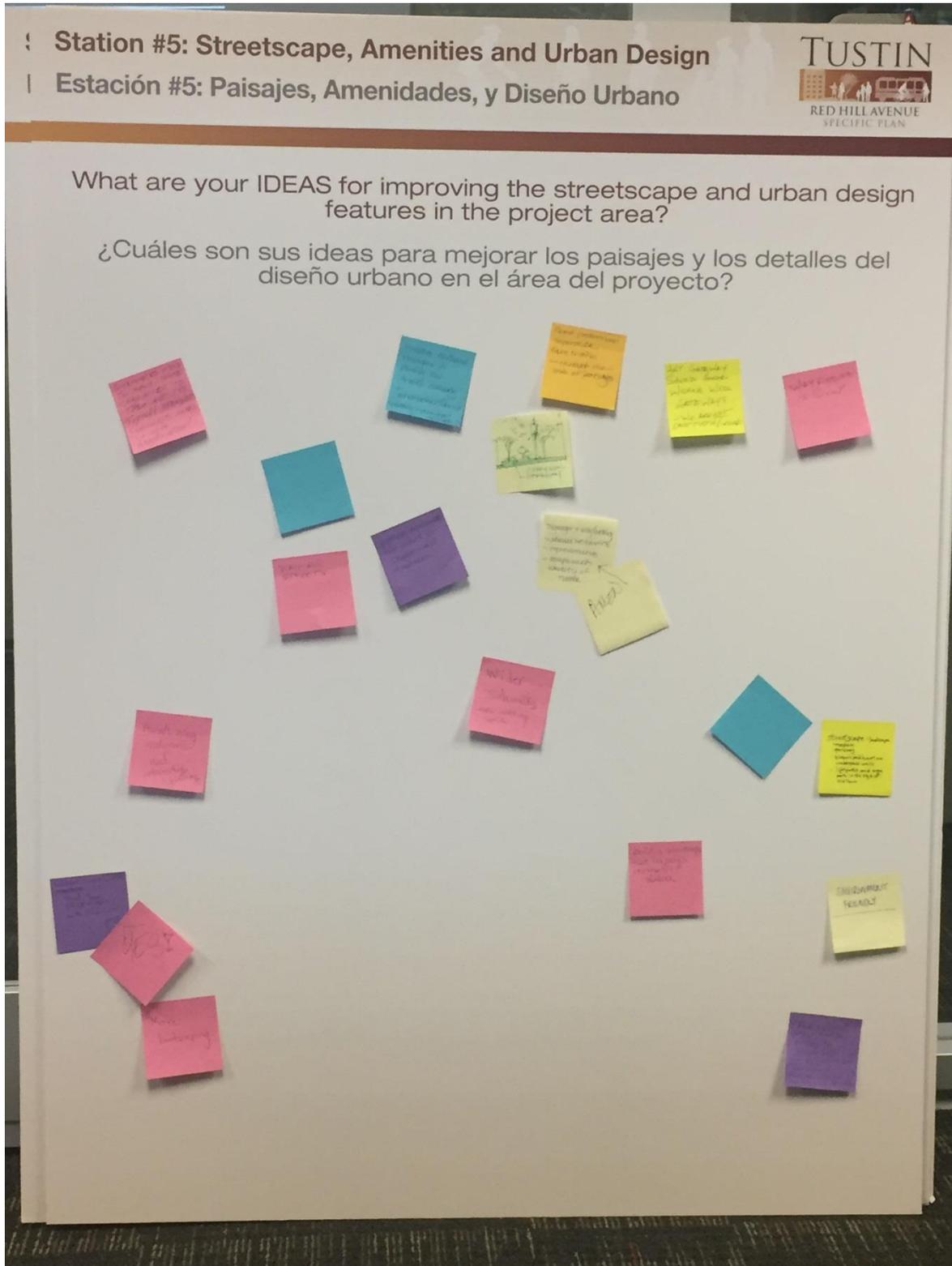
Identity: Banners

8. *Support using Banners, funding by sponsorship*

**Workshop #2 Notes**



*One workshop participant provided input on the types of streetscape and amenities that they preferred*



*Workshop attendees shared their ideas for improving streetscape and urban design features in the project area*

**Workshop #2 Notes**

**Station #5: Wayfinding and Identity**

**Estación #5: Señalamientos viales y marcadores de identidad**



Potential Locations and Types of Signage, Wayfinding, and Identity Elements

Ubicaciones Probables y tipos de letreros, señalamientos viales y elementos de identidad

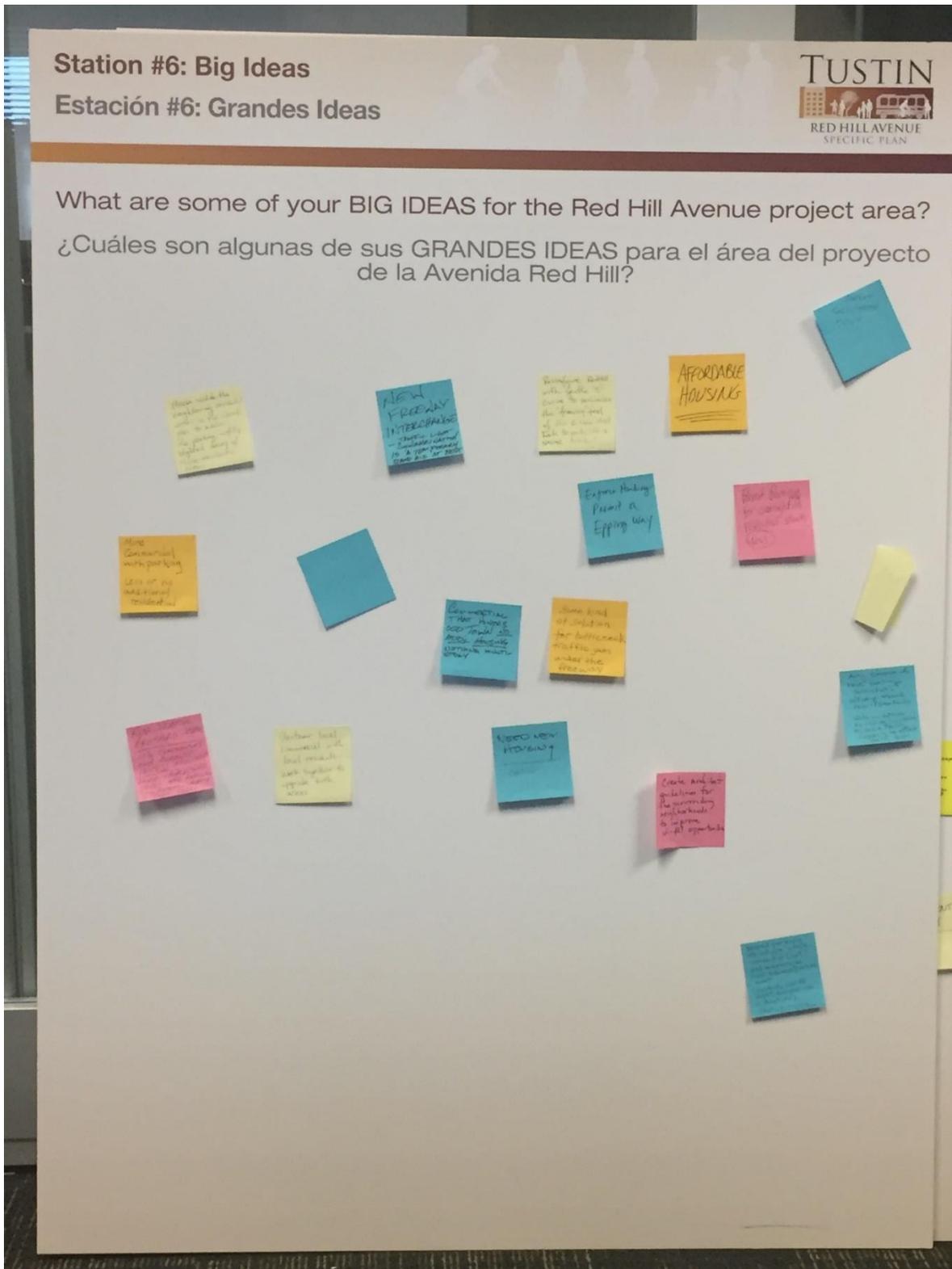
*Workshop attendees shared their ideas for locations and types of signage, wayfinding, and identity elements in the project area*

## Station 6: Big Ideas

### BOARD: Big Ideas

- *Pedestrian bridges*
- *Permit parking only for overnight on residential streets (Yes!)*
- *Any commercial have parking structures with wrap-around retail/store fronts. With an option to include long-term residential parking capability to offset parking issues*
- *Shared parking structure where residential (apt) and commercial (small business) areas meet. (residents use @ night, business use in daytime) checkout San Luis Obispo*
- *Create architect guidelines for the surrounding neighborhoods to improve in-fill opportunities*
- *Affordable housing*
- *Reconfigure Red Hill with gentle "S" curve to minimize the "freeway" feel of this 6 lane street. Feels too much like a NASCAR Track!*
- *Enforce parking permit on Epping Way*
- *Some kind of solution for bottleneck traffic jam under the freeway*
- *Need new housing (consider)*
- *Commercial that honors old town no additional housing nothing multi-story*
- *New freeway interchange – traffic light synchronizations a temporary band aid at best*
- *Partner local commercial with local residents- work together to upgrade both areas*
- *More commercial with parking, less or no additional residential*
- *Please include the neighboring residential areas in the overall plan to address the parking, infill, blighted decay of these residential areas*
- *Keep Stater Brothers store, it's convenient and lowest priced around, Ralphs and Albertsons prices are ridiculous only bargain during a freak sale*
- *Improve Red Hill landscape project*

**Workshop #2 Notes**



*Workshop attendees shared their BIG ideas for the project area*

## Comment Cards Received

Some workshop participants also provided additional comments and concerns about the project. Comments are provided verbatim, below.

### **Comment 1:**

#### Station #1: Project Overview

I was out of the country when the first workshop was held. I'm very disappointed to see the residential area in the northwest area being deleted from the plan. The area north of San Juan and west of Red Hill is becoming a blighted area. Please consider a way to address whatever concerns voiced in the first workshop while still being able to solve some of the massive problems in this area.

### **Comment 2:**

Attractive commercial with sufficient parking- nice ☺  
Additional residential development not a good idea re: traffic (all ready bad)  
BIGGEST PROBLEM!!!  
Bottleneck traffic under the freeway

### **Comment 3:**

Please use this project to address the economic segregation of Tustin. Between Red Hill & Newport, we have a Goodwill store, a Big Lot and 2- 99cent stores. District-0, Tustin Market Place- 0, Old Town-0. We have a problem here!

### **Comment 4:**

Apts, businesses don't have self-closing trash bins. Tops frequently left open and they are attracting raccoons and rats. These are urban raccoons- a much smarter and dexterous variety than rural cousins (due to natural selection in an urban area). They are very destructive to any landscaping improvements property owners or the city might do. Cleaning up trash, emptying city cans, citing owners of open trash containers, educating the property owners will help.  
We've seen a tremendous increase in problems around the Pine Tree Park area.

### **Comment 5:**

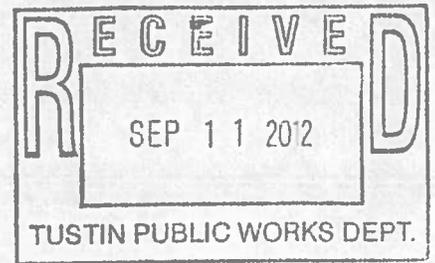
Modernizing and improving aesthetics is really great, but first address the parking issues spilling into residential areas! What is existing and the impact of future development. Also more pedestrian friendly!!  
No more big stores like grocery, Big Lots, etc. Tustin already has plenty!

### **Comment 6:**

Sidewalks are currently used by bike riders who almost run into strollers and elderly walkers. We need clear bike only and pedestrian only areas.

D

RED HILL AVENUE STREETScape FEASIBILITY STUDY



# **Red Hill Avenue Streetscape and Median Feasibility Study**

**El Camino Real to Bryan Avenue**

**CIP #70209**

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**Prepared For:  
City of Tustin  
300 Centennial Way  
Tustin, CA 92780**

**Prepared By:  
Tait & Associates, Inc.  
701 North Parkcenter Drive  
Tustin, CA 92705**

**September 10, 2012**

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## APPENDICIES

### Appendix 1

Existing Condition and Alternative Exhibits

### Appendix 2

Gateway Signage Location Exhibit

### Appendix 3

Preferred Concepts

### Appendix 4

Construction Cost Estimates for Preferred Concepts

### Appendix 5

Existing Overhead Utility Services Investigation and Cost Estimate--  
(Prepared by: Utility Specialists)

## INTRODUCTION

Tait & Associates, Inc. (TAIT) was retained by the City of Tustin (City) to undertake a feasibility study to look at possible ways to improve a section of Red Hill Avenue between El Camino Real and Bryan Avenue that would address the City's objectives identified in "The Neighborhoods of Tustin Town Center: A Strategic Guide for Development". This document is considered a strategic guide for the development within three areas or neighborhoods of the City known as the Center City, Southern Gateway and West Village. A portion of Red Hill Avenue Study Limits lies within the designated Center City area (El Camino Real to San Juan Street). In this guiding study several implementation action items were identified to help foster private investment and development within each city neighborhood. One of these implementation actions is to:

*Initiate short range infrastructure improvements, access and other programs within each neighborhood with the goal of improving the aesthetic appearance of streetscapes and pedestrian amenities, improving connectivity between neighborhoods and facilities, including provision of bike lanes where needed.*

For the Center City neighborhood, a priority was established to study gateway signage improvements at the intersection of Red Hill Avenue and El Camino Real and streetscape and median improvements along Red Hill Avenue.

TAIT scope was to undertake a feasibility study to investigate opportunity and constraints associated with undertaking the Center City's short range infrastructure improvements for Red Hill Avenue by investigating options to provide aesthetic improvements, enhance pedestrian and bicycle mobility and identify Gateway Signage locations. TAIT's project scope does not include the development of a landscape palette and hardscape program for the streetscape and median or the development of a Gateway Signage Program. The Project Area and Study Limits are depicted on the following Page.

## EXISTING CONDITIONS

Red Hill Avenue within the Study Limits is a six-lane major arterial street with a 14-foot wide stripped two-way left turn median. The existing street right of way is essentially 100 feet wide (50 feet on each side of the street center line), has 84 feet of pavement (with 42 feet on each side of the street centerline) and 8-foot wide parkways. There is a short section on the west side of Red Hill between El Camino Real north of the existing alley and extending to San Juan Street that has a half width right of way width of 60 feet. This section of Red Hill Avenue with a 60-foot half width is the ultimate half width right of way for Red Hill Avenue. The standard design for a major arterial street is to have a 120-foot right of way with 100 feet of pavement consisting of an 8-foot shoulder, six twelve-foot wide traffic lanes and a fourteen-foot median. Existing Red Hill Avenue traffic volumes are estimated at 21,000 ADT (both directions) from Interstate 5 to Bryan Avenue.

No parking is allowed along both sides of the street except for a recently added parking area adjacent to the City's Pine Tree Park. The street parkways include sidewalks ranging in width

from 4 feet, 8 feet and 9 feet. Portions of the parkway areas include turfed landscaped areas and street trees. The parkway on the east side of Red Hill includes overhead power, telephone, Cable TV and street lights on wooden poles. There are also several above grade utility structures and traffic signal cabinets within the Study Limits. At the northern end of the Study Limits the striped median becomes a raised landscaped median with turf and tree planters.



**PROJECT AREA MAP**



- **Study Limits – Red Hill Avenue from El Camino Real to Bryan Avenue**

The Project Area includes two elementary schools (CC Lambert and Marjorie Veeh), one middle school (CE Utt) and one high school (Tustin). A City park (Pine Tree Park) is located at the northern end of the Study Limits. Single family detached and multi-family apartment housing is located throughout the Project Area and along both sides of Red Hill in the northern half of the Study Limits (from San Juan Street to Bryan Avenue). Single family detached

housing with residential driveways exists on the western side of this section of Red Hill Avenue. For the multi-family housing some units face Red Hill and are setback from the street while others are located behind masonry block screen walls.

Development from El Camino Real to San Juan Street consists of commercial business along both sides of Red Hill with the exception of a large vacant parcel on the west side of Red Hill that has been entitled for a senior assisted living housing project known as "Monarch Village". Red Hill Avenue street frontage along the proposed Monarch Village is to be widened by 9 feet to accommodate the Master Plan of Arterial Highway half-width street section in this area. South of the Monarch Village there is an existing alley, a car wash and a Shell gas station. North of Monarch Village there is an existing real estate office. Existing street trees along this segment include tall evergreen trees located in a turf area that are to be removed to accommodate the widening of Red Hill along the frontage of Monarch Village.

The east side of Red Hill Avenue from El Camino Real to San Juan Street includes a commercial strip center, a fast food restaurant, a gas station (Mobil) and a vacant parcel. There are no street trees or parkway landscaping within this section of Red Hill Avenue right of way.

The Orange County Transportation Authority (OCTA) currently does not have any bus stops along Red Hill Avenue and does not have any plans to add bus stops along this segment.

An exhibit depicting the existing roadway condition with street striping and street cross sections is included in Appendix 1.

## **OPPORTUNITIES AND CONSTRAINTS**

Project constraints include the following elements:

- Existing Street was restriped to three lanes in each direction and removed all on-street parking and forces bicyclist to share a traffic lane or move to the sidewalk
- The existing power poles and utility structures within an eight-foot wide concrete sidewalk (from El Camino to San Juan) and a 4-foot wide concrete sidewalk (from San Juan to Bryan) restrict pedestrian and bicyclist movements.
- The existing Gas Station pump island located at the north east corner of Red Hill and El Camino Real is located within 18 feet of the existing curb face.
- The commercial development along the east side of Red Hill Avenue includes a circulation drive aisle abutting the street right of way.
- Existing residential driveways along the west side from San Juan to Lance Drive create potential conflict points with pedestrian and bicyclist that share the sidewalk.

Project Opportunities include the following elements:

- The existing median is 14 feet wide and could accommodate a landscaped median.
- The multiple access points to the commercial center could easily be reconfigured to accommodate a main entrance for full access while others provide restricted access.
- The proximity of San Juan and Lance Drive intersections would accommodate U-turn traffic for residential accessibility if a raised median is to be constructed.
- The existing roadway parkway on the east side of Red Hill from San Juan to Bryan contains existing street trees along the path of the overhead services.

- The eight-foot wide parkways provide some opportunity to add limited landscaping, if bicycle use can be eliminated from sharing the sidewalk with pedestrians.

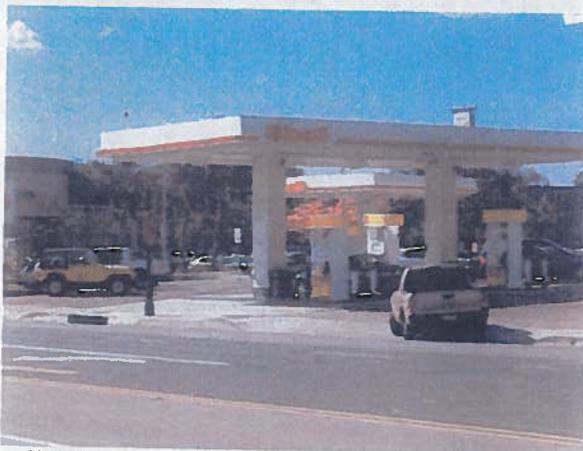
The following pages present site photographs taken along Red Hill Avenue Study Limits.



Looking north along Commercial Center north of El Camino Real



Looking south along Commercial Center



Shell Gas Station at NWC of Red Hill and El Camino Real



Existing parkway and evergreen trees along Future Monarch Village



Street View looking south from Bryan (south of Pine Tree Park)



Street View looking north from San Juan Street



Looking South on Red Hill along Pine Tree Park Frontage



Existing median street tree (Olive Tree) in masonry planter



Looking North toward Bryan at landscape median and parkway trees



Street View on west side of Red Hill looking north from San Juan



Street view on west side of Red Hill looking south

## UNDERGROUNDING OF EXISTING OVERHEAD SERVICES

As part of this study TAIT retained the consultant services of Utility Specialists to conduct a limited investigation of the existing overhead services along Red Hill within the limits of the study. The focus of this investigation was to identify the overhead services located on poles along the east side of Red Hill and develop an estimated cost to underground the existing services and related above ground structures. Utility Specialists' investigation is included in Appendix 5 and is summarized below.

The existing overhead services contains a single 4-wire 12 KV system for Southern California Edison, four to six communication lines (assumed to be AT&T, Time Warner and possibly Cox Communications) and the local street lights and their electrical distribution system. These services have been placed underground in Red Hill Avenue from the intersection of El Camino Real to the first driveway at the existing Commercial center. An Existing 66KV system begins on the overhead line north of Bryan Avenue.

The electrical services for the Commercial Center south of San Juan do not connect to this system, but the overhead system does provide services to the residential housing north of San Juan on both sides of the street.

The estimated cost to underground the existing overhead services in Red Hill Avenue including reconstruction of the individual residential services was estimated by Utility Specialist to be approximately \$2 million in today's dollars.

TAIT reviewed the City Ordinance No 76 issued in 1949 that granted Southern California Edison a franchise over the city's public streets for installing electrical services in accordance with the Franchise Act of 1937. This document did not address responsibility for relocations, but did address the fees to be paid to the city for the right to install services within public right of way.

After a review of the potential alternatives for satisfying the study goals with City Staff, it was determined that for the purposes of this feasibility study (to identify "*short range infrastructure improvements*") the incorporation of preferred option that required relocation or undergrounding of the overhead services did not meet the project intent. Therefore, the alternative that identified a proposed undergrounding did not get included in the final options.

## "SAFE ROUTE TO SCHOOL" CONSIDERATIONS

As mentioned earlier in this study, the Project Area include several schools, a city park, local commercial centers and residential housing that will require individuals to travel through the Study Limits along Red Hill Avenue to reach these areas. The recent restriping of Red Hill Avenue to add three lanes of traffic in each direction from Edinger Avenue (south of Interstate 5) to Bryan Avenue eliminated all shoulder and on-street parking areas on this section of roadway. This restriping was accomplish to meet the minimum requirements for Red Hill Avenue as a major arterial street in accordance with the County of Orange Master Plan of

Arterial Highways and meets the mobility needs of commuters using Red Hill Avenue for business and personal travel.

As a result of this restriping of Red Hill Avenue bicyclist on Red Hill had two options, either share the traffic lanes with commuter or share the sidewalk with pedestrians. Within the City of Tustin bicycle use of sidewalks is allowed per the City of Tustin Municipal Code 5345. Examples of this can be found at several areas within the City.

In developing the streetscape and median improvement alternatives presented later on in this study, TAIT developed a visual inventory of areas that would have to be improved to satisfy both pedestrian and bicyclist mobility along Red Hill Avenue.

The purpose of this visual inventory was to identify areas that would require improvements to be able to establish a "Safe Route to School" program. Elements such as intersection access ramps, driveway cross slopes, horizontal and vertical clear distances were investigated. Based on this investigation the following observations were made and are to be addressed in any construction associated with improvements to the Red Hill Avenue within defined the Study Limits as described herein:

- Current intersection ramps do not comply with latest standards for accessibility design and will require reconstruction.
- Current driveway configurations for all commercial and residential driveways do not provide a 4-foot wide section at 2% or less for an accessible path of travel across the driveways.
- Minimum clearance of 36-inches at point obstructions appeared to be satisfied; however, it is recommend that additional review be conducted during the next stage of design to confirm that the minimum horizontal clearance is achieved based on the final option selected for Red Hill Avenue Improvements.
- No areas where vertical obstructions of less than 80 inches (6 feet 8 inches) were observed; however, it is recommend that additional review be conducted during the next stage of design to confirm that the minimum horizontal clearance is achieved based on the final option selected for Red Hill Avenue Improvements.
- In our conducting of the visual observations a basic project assumption was that the city street scape improvements would include a full replacement of the street sidewalk so any deficiencies to the existing sidewalk surfaces and slope were not a consideration in our assessment of the existing street.

Items included in this bullet list will have to be addresses as part of a "Safe Route To School" improvement program and have been factored into the Preliminary Cost Estimates included in this report.

In addition during next design phase the pedestrian features at traffic signals will need to be reviewed to ensure that title 24 accessibility requirements are satisfied.

Alternatives to provide either a Class II (on-street) or a Class III (off-street) bike lanes have been included in this study to meet bicyclist mobility needs. However, this study did not look at bicycle use along Bryan Avenue, San Juan Street and El Camino Real that would be

necessary to extend the proposed bikeways from Red Hill Avenue to the schools, community facilities, and commercial centers from the residential clusters located throughout the Project Area. The same also applies to street sidewalks and intersection crossings outside of the Study Limits. A comprehensive Safe Route to School Program would require the expansion of the study area to include these facilities that are not located along Red Hill Avenue.

## **DEVELOPMENT OF ALTERNATIVES**

Based on the project scope to investigate possible pedestrian and bicycle mobility improvements that could be incorporated in to a streetscape and median improvement project TAIT developed four alternatives that were presented to the City for review and discussion. A detail drawing for each Alternative is included in Appendix 1 and a brief description of each is included herein.

In order to develop potential feasible alternatives that could potentially achieve the study goals, TAIT developed a set of basic project assumptions to help guide the development of potential alternatives. These basic assumptions included the following:

- The first basic assumption used in developing each of the four alternatives was that the proposed street widening along the Monarch Village street frontage was assumed to be an existing condition.
- A second basic assumption was that Red Hill Avenue's six traffic lanes (three in each direction) would have to remain, even though north of Bryan Avenue the existing street is reduced to four traffic lanes, two in each direction.
- A third basic assumption was that a roadway median would have to be provided in all alternatives. The width of the median could be reduced from 14 feet, but would still have to be provided to provide access to commercial and residential uses along Red Hill Avenue.
- A Fourth basic assumption was that each alternative must include some form of streetscape improvements.
- Finally, the fifth and final basic assumption was that street widening could be considered, but not when it would require the acquisition of additional right of way or when the widening would not satisfy the required roadway section for a major arterial street. However, the acquisition of public access easements dedications on private could be considered to achieve pedestrian and bicyclist mobility benefits.

In addition to the above basic assumptions, in reviewing the Study Limits it became apparent that the two roadways section from El Camino Real to San Juan Street and from San Juan Street to Bryan Avenue would have to be looked at separately since an approach used in one section may not apply to the other section.

The following is a description of the four Alternatives developed by TAIT and presented to City Staff.

### **Alternative No. 1**

In this Alternative the 14-foot median width between El Camino Real and San Juan is maintained, but the striped median is turned into a raised median that is to be landscaped and include median trees. Left Turn access into and out of the Commercial Center is provide via a new main entry into the center. For the proposed Monarch Village site left turn access would be provided into the site at the northern driveway and left turn access would be provided for traffic exiting from the existing City Alley to go north on Red Hill Avenue (it is important to note that the entitlements for Monarch Village do not require that these turn movement be provided for Monarch Village, and as an option for this Alternative they could be eliminated to maximize the median landscape area). Left Turn lanes would also be provided at El Camino Real and San Juan Street per the existing conditions.

New concrete improvements would be undertaken from El Camino Real to San Juan for the construction of new sidewalks, access ramps and driveway aprons on both sides of the street with provisions for a nine-foot combined bikeway and sidewalk on the west side of Red Hill and a 8-foot pedestrian sidewalk on the east side of Red Hill.

A proposed two-way bike lane from El Camino to San Juan would be provided on the west side of Red Hill Avenue. This two-way off-street Class III bike lane would be combined with the west side pedestrian sidewalk within a 9-foot wide concrete paved area. Signage would be placed to inform pedestrians and bicyclist of the joint use. Since the existing sidewalk at the Shell Gas station is only eight feet wide the street may need to be narrowed one foot or a one-foot wide access easement would have to be obtained across the Gas station frontage (note the existing gas pump island is located 10-foot outside of the street right of way at this location).

Street parkway landscape improvements on both sides of Red Hill from El Camino Real to San Juan would include placement of elongated street tree planters. These planters would be ten feet long and two-foot wide at their narrowest point and three-foot wide at their widest point to accommodate planting of a single street tree within each planter. The additional planter length would allow plants and shrubs to be placed in addition to the street tree. The three foot width limitation allows the combined two-way bike lane and sidewalk to be maintained at a minimum six-foot width for just a short distance. The elongated street tree planters would be spaced along both side of Red Hill Avenue with a maximum spacing of 50-feet. Negotiations with Southern California Edison would have to be undertaken to plant new street trees under the existing overhead services.

From San Juan to Bryan the existing 14-foot striped or landscape median would be reduced to 10 feet to accommodate on-street 5-foot wide Class II bike lane in each direction. These on-street bike lanes would terminate at San Juan and Bryan. In addition to the median width reduction, the existing traffic lanes would be restriped to provide three eleven-foot wide traffic lanes in each direction (reducing the outside traffic lane that currently is adjacent to the street curb from 13 feet to 11 feet, but this lane would be adjacent to the five-foot wide bike lane). No street widening is proposed in this section of Red Hill. Left turn and U-turn access would

be provide at, Lance Drive and the existing turn lanes at San Juan Street and Bryan Avenue would be maintained.

New concrete improvements would be undertaken from San Juan to Bryan to for the construction of new 4-foot wide sidewalks, access ramps and driveway aprons on both sides of the street.

From San Juan to Bryan, the 10-foot median would be landscaped with trees and plantings/shrubs. The 10-foot wide turn lanes would be separated from the opposing traffic by painted double solid yellow stripes. In the street parkways, it is assumed that the existing street trees would be retained where they are in good condition, replaced where they are not in good condition (subject the Southern California Edison approval). Also the total sidewalk width would be reduced to four-foot on both sides of the street and a 3½-foot wide landscape planter would separate the street curb from the sidewalk along this section of Red Hill.

This Alternative does not include any street widening nor any property acquisition, but would require a minor one-foot street narrowing along the Shell Gas Station property to obtain the nine-foot wide combined bikeway and pedestrian sidewalk. An option to the narrowing would be to widen the right of way by one-foot or obtain a one-foot wide public access easement along the Shell Gas station's Red Hill Avenue street frontage.

### **Alternative No. 2**

In this Alternative, the proposed roadway improvements to the section of Red Hill Avenue between San Juan Street and Bryan Avenue remains the same as described in Alternative No. 1 with a ten-foot wide landscaped median, five-foot wide on-street bike lanes in each direction, new driveway aprons, new access ramps, new 4-foot sidewalks with a 3.5-foot wide landscape planter separating the street curb and gutter from the sidewalk and street trees.

From El Camino Real to San Juan Street five-foot wide on-street Class II bike lanes are proposed. To accomplish this, the proposed existing 14-foot landscaped median is reduced to 12 feet. Some the street traffic lanes would be reduced in width and restriped as follows: a 5-foot bike lane, 11-foot outside traffic lane, 10-foot center traffic lane and 11-foot inside traffic lane next to the 12-foot wide landscaped median on both sides of Red Hill. This Alternative also requires a one-foot to two-foot street widening of Red Hill on the west side from El Camino to San Juan to accommodate the proposed on-street bike lanes. In the parkways north of the existing alley the elongated street tree planters are proposed on both side of red Hill from El Camino to San Juan.

Construction of new concrete sidewalks, access ramps and driveway aprons from El Camino to San Juan (and also from San Juan to Bryan) are proposed with a minimum of a 6 to 8-foot sidewalk on the west side of Red Hill and a 8-foot sidewalk on the eastside of Red Hill. The placement of the 10-foot street tree planters as described in Alternative No. 1 is proposed for Alternative No. 2 on both sides of Red Hill from El Camino Real to San Juan

This alternative does not require any property acquisition to widen the roadway right of way but as previously described would include some minor street widening on the west side of Red Hill from El Camino to San Juan.

### **Alternative No. 3**

In this Alternative, the proposed on-street bike lanes from San Juan Street and Bryan Avenue in Alternatives No. 1 and No. 2 are not included in Alternative No. 3. The proposed street median would then remain at its current width of 14 feet, but would become a landscaped median with median trees. The street parkways from San Juan to Bryan would remain as described in Alternatives No. 1 and No. 2 with a new concrete driveway aprons, access ramps, and four-foot sidewalk separated from the street curb with a 3.5-foot wide landscape planter area on both sides of Red Hill Avenue.

From El Camino Real to San Juan Street the proposed improvements would include the construction of new concrete driveway aprons, access ramps, and an 8-foot wide one-way off-street combined Class III bike lane and pedestrian sidewalk along both sides of the street. Signs would be posted to identify the shared use. On the eastside of Red Hill the combined use area would require the acquisition of 6-foot wide section along the eastside of Red Hill Avenue by means of a property acquisition or the dedication of a new public access easement. The easement would allow the existing overhead power lines and poles to remain in place.

The 10-foot street tree planters (as described in Alternative No. 1) would still be utilized on the west side of Red Hill. However, on the eastside of Red Hill Avenue a six-foot wide landscape planter area would be provided between the back of curb and the 8-foot wide shared use sidewalk and bike lane. The existing overhead service poles would remain in this six-foot landscaped area and other existing above ground utility structures would be relocated to maintain a clear 8-foot path for the combined sidewalk and bike lane.

One disadvantage of this option is that the offsite property to accommodate the northbound off-street bike lane will impact the traffic circulation within the existing commercial center and possibly the fast food restaurant. In addition the addition area along the Mobil Gas Station would reduce the offset distance to the gas pump island from its existing 14.5 feet to 8.5 feet, which may not be sufficient to maintain the drive aisle for the pumps adjacent to Red Hill.

As indicated this Alternative requires the acquisition of additional public right of way or a public access easement along the east side of Red Hill Avenue from El Camino Real to San Juan Street, but does not require any street widening.

Also this Alternative does not include bike lanes on Red Hill between San Juan Street and Bryan Avenue, and the proposed 4-foot sidewalk within the same section to add addition street landscaping makes a share use of sidewalk between pedestrian and bicyclist impossible.

### **Alternative No. 4**

In this Alternative the proposed improvements from San Juan Street to Bryan Avenue remain as proposed in Alternatives No. 1 and No. 2 with one exception.

The one exception is that this alternative proposes to underground the existing overhead services and remove the existing poles from the street parkways. This work is proposed to extend from El Camino Real to Bryan Avenue and as indicated previously, the estimated cost for this undergrounding is projected to cost approximately \$2 million.

The other notable change in this Alternative is included in the section of Red Hill between El Camino and San Juan. In this Alternative the off-street Class III bike lanes as proposed in Alternatives No. 1 and No. 3 are not included and the proposed 5-foot wide Class II on-street bike lanes are proposed for each side of Red Hill (which means that the on-street bike lanes will exist between El Camino Real and Bryan Avenue in Alternative No. 4). In order to accommodate the 5-foot wide on-street bike lanes and maintain a 12-foot wide landscape median, the street would have to be widened 2-foot to 3-foot on both sides of Red Hill from El Camino Real to San Juan. Also in order to accommodate the 10-foot street tree planters on the eastside of Red Hill from El Camino Real to San Juan a 2-foot property acquisition or public access easement dedication would be required along the existing commercial center's street frontage. If these street tree planters are not necessary, a minimum sidewalk width of 5 feet could be maintained from El Camino Real to San Juan Street within the existing street right of way. This option would also require the undergrounding of the existing overhead services and removal of the existing poles from El Camino to San Juan.

The construction of new concrete sidewalk, access ramps and driveway aprons, landscaped medians and street trees planters would also occur in Alternative 4.

In this Alternative, there would be a significant cost for undergrounding the overhead services, the widening of the existing street on both sides of Red Hill from El Camino Real to San Juan Street, and the acquisition of additional right of way or public access easement along the eastside of Red Hill from El Camino to San Juan.

## **OTHER ALTERNATIVES CONSIDERED BUT NOT ADVANCED**

Three additional alternatives were identified in the early study efforts, but were not advanced. These three additional alternatives included:

1. Street Widening to its Ultimate Major Arterial Highway Condition – this alternative would result in significant property acquisition to acquire 10-foot of additional roadway right of way on both sides of the street, would require the undergrounding of the overhead services, would require eight foot of street widening on both sides of the street and still would not provide the desired bike lanes.
2. Reduction in Traffic Lanes from six to four lanes (two in each direction) – This alternative was not consider further since it would not satisfy the Orange County Master Plan of Arterial Highways nor serve the commuting needs for the City of Tustin.
3. Combination of a Landscaped Median and Bikeway – This option would require roadway widening and addition property acquisition to enable bicyclist to leave the

median area an access the pedestrian crosswalks to leave the median at cross streets. In addition this alternative would require traffic signal modifications and improvements to accommodate the bicyclist through movements across the intersecting streets. The 14-foot median may also have to be widening to ensure bicyclist safety.

## **GATEWAY SIGNAGE LOCATION STUDY**

TAIT's analysis of Gateway Signage Locations initially identified five potential locations. However, in reviewing these five locations with the City and reviewing the City's goals for their Gateway Signage Program, all but two locations were eliminated. The two remaining proposed locations are depicted on the Exhibit in Appendix 2 and are described as follows:

Location No. 1 - In the southeast corner of Red Hill and El Camino on the north bound side of the street and outside of Caltrans Right of Way. This sign location would have to be coordinated with the existing Wendy's sign, the existing street signage and traffic signal poles and pedestrian access along the existing sidewalk.

Location No. 2 - In the proposed landscape median area north of the intersection of Red Hill and El Camino Real. In the exhibit included Appendix 2 the location of this Gateway sign is depicted within the existing stripped median layout but could be position anywhere within the proposed landscape median.

The purpose of the Gateway signage is to announce the arrival into Tustin for traffic leaving the freeway. It is not intended to be information or directional signage such as the existing Old Towne signs that is located on the north bound Interstate-5 off ramp or at the northwest corner of Red Hill and El Camino in front of the Shell Gas Station.

The Gateway Signage study by TAIT for this project did not include the development of a Gateway sign concept, so the final placement will depend on the type of signage to be used and the size of the proposed sign. For the purposes of this Gateway Signage Location Study TAIT considered the Gateway Signage to be more of a monument type sign like the examples depicted on the Gateway Signage Location exhibit included Appendix 2 which uses the City logo as an example of a monument type sign. Other signage options such as a bridge truss extending over and across Red Hill Avenue was not considered in this study.

The placement of a Gateway Sign on Caltrans' bridge or existing retaining walls did not appear to achieve the intended purpose for announcing a traveler's arrival into the City of Tustin. Because of this, no discussion where held with Caltrans to investigate placement within Caltrans right of way or existing structures.

## PREFERRED CONCEPTS

The four proposed Alternatives were then submitted for City review. A meeting was then held with City Staff and TAIT to discuss the City's comments and obtain additional information from TAIT on the four proposed Alternatives. Based on the City's review and the discussion that occurred the City identified two preferred concepts that incorporated various components and elements from the original four alternatives which became the studies "Preferred Concepts". In these Preferred Concepts The City requested that Tait consider a Red Hill avenue with and without the proposed Monarch Village street improvements to Red Hill Avenue.

The two Preferred Concepts developed based on City comments are depicted on the exhibits included in Appendix 3 and are described as follows:

### Concept No. 1 -

#### From El Camino Real to San Juan Street:

The improvements to this section of Red Hill can be described as follows:

- Reduction of the 14-foot striped median to a 12-foot raised landscaped median with 10-foot left turn lanes separated from opposing traffic with 2-foot raised curb or double-double yellow solid stripe traffic lanes
- 5-foot wide on-street Class II bike lanes in the north bound direction only
- Construction of a new combined 8-foot minimum concrete sidewalk (with new access ramps and new driveway aprons) and Class III off-street bike way in the south bound direction only.
- Three traffic lanes in each direction with the southbound traffic lanes remaining unchanged and the north bound traffic lanes being restriped as follows: an 11-foot outside traffic lane, 10-foot center traffic lane and 11-foot inside traffic lane next to the 12-foot landscaped median
- New concrete sidewalks, access ramps and driveway aprons will be provided with sidewalks widths remaining per their existing condition or be widened by a foot along the undeveloped Monarch Village street frontage.
- The proposed elongated street tree planters that have a maximum length of ten feet and a varying width from 2 feet to 3 feet are proposed along both sides of the street section.

#### From San Juan Street to Bryan Avenue:

This section of Red Hill is the same that was identified in Alternative No. 1 and No. 2. The proposed street improvements in this section can be described as follows:

- A 10-foot wide raised landscape median with 10-foot left turn lanes separated from opposing traffic with double yellow solid stripe traffic lanes
- 5-foot wide on-street Class II bike lanes in both directions
- Three lanes of traffic in each direction with an 11-foot outside traffic lane, 10-foot center traffic lane and 11-foot inside traffic lane next to the 10-foot landscaped median
- Construction of new 4-foot wide concrete sidewalk (with new access ramps and new driveway aprons) separated from the street curb by a 3.5-foot wide continuous

landscape planter running the entire length of the street section and on both sides of the street.

In this Concept the existing overhead utility services and power poles will remain in their current condition. Negotiations with Southern California Edison to place additional street trees under their existing overhead lines will be necessary to plant additional street streets or replace existing trees that are in a poor condition.

This Concept requires no street widening or the acquisition of additional right of way or public access easements.

### **Concept No. 2 -**

#### **From El Camino Real to San Juan Street:**

The improvements to this section of Red Hill can be described as follows:

- Reduction of the 14-foot striped median to a 10-foot raised landscaped median with 10-foot left turn lanes separated from opposing traffic with double yellow solid stripe traffic lanes
- 5-foot wide on-street Class II bike lanes in both directions
- Three traffic lanes in each direction with an 11-foot outside traffic lane, 10-foot center traffic lane and 11-foot inside traffic lane next to the 10-foot landscaped median
- Construction of new concrete sidewalks (with new access ramps and new driveway aprons) at a reduced minimum width of 4 feet and the addition of a 3½-foot wide continuous landscape planter running the entire length of the street section and on both sides of the street separating the sidewalk from the street curb.

#### **From San Juan Street to Bryan Avenue:**

This section of Red Hill is the same that was identified in Alternative No. 1 and No. 2 and in Concept No. 1. The proposed street improvements in this section can be described as follows:

- A 10-foot wide raised landscape median with 10-foot left turn lanes separated from opposing traffic with double yellow solid stripe traffic lanes
- 5-foot wide on-street Class II bike lanes in both directions
- Three lanes of traffic in each direction with an 11-foot outside traffic lane, 10-foot center traffic lane and 11-foot inside traffic lane next to the 10-foot landscaped median
- Construction of new 4-foot wide concrete sidewalk (with new access ramps and new driveway aprons) separated from the street curb by a 3.5-foot wide continuous landscape planter running the entire length of the street section and on both sides of the street.

In this concept the existing overhead utility services and power poles will remain in their current condition. Negotiations with Southern California Edison to place additional street trees under their existing overhead lines will be necessary to plant additional street streets or replace existing trees that are in a poor condition.

This Concept requires no street widening or the acquisition of additional right of way or public access easements.

## **ESTIMATED CONSTRUCTION COSTS FOR THE PREFERRED CONCEPTS**

Detailed preliminary construction cost estimates are included in Appendix 4 for the two Preferred Concepts.

The estimated construction cost for Concept No. 1 is \$1,000,000 in 2012 dollars.

The estimated construction cost for Concept No. 2 is \$915,000 in 2012 dollars.

Cost assumptions include:

- Removal and reconstruction of all sidewalks and access ramps.
- Removal of all existing landscaping except for street trees under the overhead power lines.
- Off-site private driveway improvements are limited to five feet beyond the street right of way, and only includes the removal and construction of concrete driveway.
- A five percent (5%) mobilization cost factor is included in the cost estimate.
- A of twenty percent (20%) contingency cost factor has been included in the estimated costs.
- The cost to furnish and install the Gateway Signage in not included in these preliminary construction cost estimates.

Please note that the cost analysis does not include soft costs for landscape planning efforts, preliminary and final designs, technical studies and any permitting costs.

### **CITY RECOMMENDED CONCEPT**

Based upon further review of the two "Preferred Concepts" the City has selected Concept No. 2 as their "Recommended Concept" to be advanced for this section of Red Hill Avenue from El Camino Real to Bryan Avenue.

This recommendation is based on the following features being provided:

- Provisions for a continuous 5-foot wide on-street bike lanes
- Construction of new concrete street sidewalks, access ramps and driveway aprons
- Establishment of new street landscape planters with street trees
- Creation of a landscaped median with median trees

The creation of the new concrete sidewalks and driveway apron and provisions to provide new on-street bike lanes will be accomplished to provide "Safe Route to School" improvements along this section of Red Hill Avenue.

**Red Hill Avenue  
Streetscape and Median Feasibility Study  
El Camino Real to Bryan Avenue  
CIP #70209**

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**Appendix 1**

**Existing Condition and Alternative Exhibits**

- Alternative No. 1**
- Alternative No. 2**
- Alternative No. 3**
- Alternative No. 4**

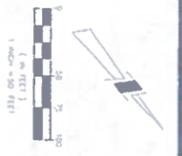


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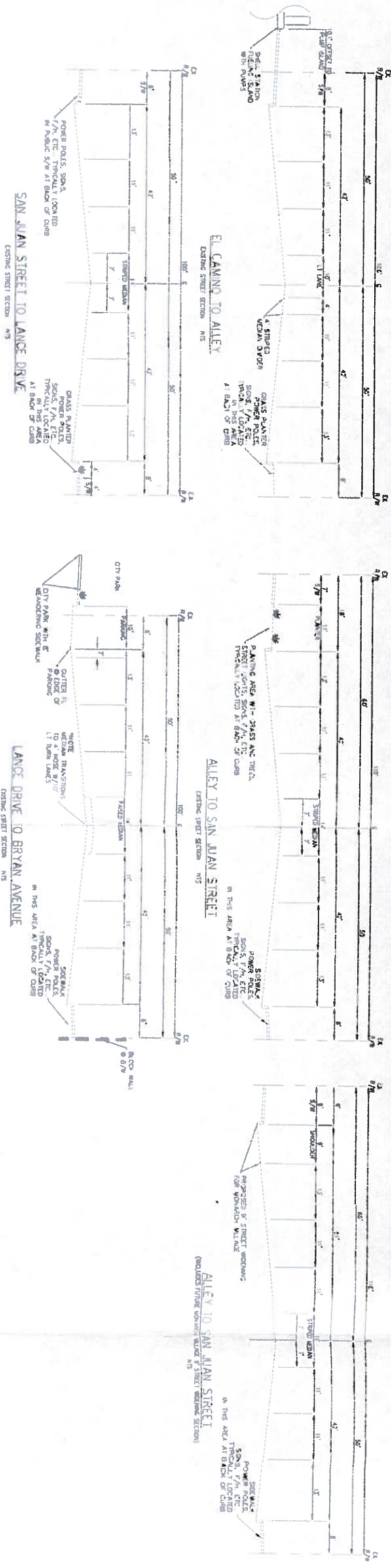
**MONARCH VILLAGE ASSUMPTIONS:**  
 1. CONCEPTUAL STUDY OF SENIOR HOUSING PROJECT. ALL BE CONSIDERED AS AN EXISTING CONDITION UNLESS NOTED OTHERWISE.  
 2. STREET SECTIONS FROM ALLEY TO SAN JUAN ARE SHOWN WITH 9' WIDENING. ALL OTHER WIDENINGS ARE NOTED.  
 3. STREET SECTIONS FROM ALLEY TO SAN JUAN ARE SHOWN WITH 9' WIDENING. ALL OTHER WIDENINGS ARE NOTED.

**LEGEND:**

- UTILITY LINE
- STREET
- SIDEWALK
- BIKE LANE
- PROPOSED STREET WIDENING
- PROPOSED MEDIAN WIDENING
- EXISTING MEDIAN WIDENING
- PROPOSED TREE PLANTING
- STREET LIGHTING



REFERENCE LINE A  
SEE ABOVE RIGHT



<p>DATE: 04.11.12          CHECKED BY: [Name]          DRAWN BY: [Name]          DATE: 04.11.12          SCALE: AS SHOWN</p>	<p><b>CONCEPTUAL FEASIBILITY STUDY EXISTING          RED HILL AVENUE STREETSCAPE &amp; MEDIAN STUDY</b></p>
--	---

**TAIT**  
 781 N. Park Center Drive  
 Santa Ana, CA 92705  
 P: 714.740.8300 F: 714.740.8321  
 www.tait.com

NO.	DESCRIPTION	BY	DATE	CHKD.	DATE



REFERENCE LINE A  
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**MONARCH VILLAGE ASSUMPTIONS:**

MONARCH VILLAGE ASSUMPTIONS ARE BASED ON THE MONARCH VILLAGE MASTER PLAN AND THE MONARCH VILLAGE PHASE 1 DEVELOPMENT PLAN. MONARCH VILLAGE ASSUMPTIONS ARE SUBJECT TO CHANGE WITHOUT NOTICE.

**ALTERNATIVE 1 DESCRIPTION:**

- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON WEST SIDE OF RED HILL AVENUE FROM EL CAMINO BLVD. TO SAN JUAN STREET.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON EAST SIDE OF RED HILL AVENUE FROM EL CAMINO BLVD. TO SAN JUAN STREET.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON WEST SIDE OF RED HILL AVENUE FROM SAN JUAN STREET TO BRYAN AVENUE.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON EAST SIDE OF RED HILL AVENUE FROM SAN JUAN STREET TO BRYAN AVENUE.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON WEST SIDE OF RED HILL AVENUE FROM BRYAN AVENUE TO LANCE DRIVE.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON EAST SIDE OF RED HILL AVENUE FROM BRYAN AVENUE TO LANCE DRIVE.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON WEST SIDE OF RED HILL AVENUE FROM LANCE DRIVE TO CITY PARK.
- PROPOSED 1' WIDE SIDEWALK WITH 2' WIDE MEDIAN ON EAST SIDE OF RED HILL AVENUE FROM LANCE DRIVE TO CITY PARK.

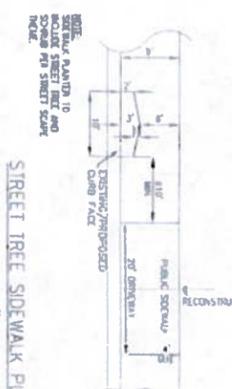
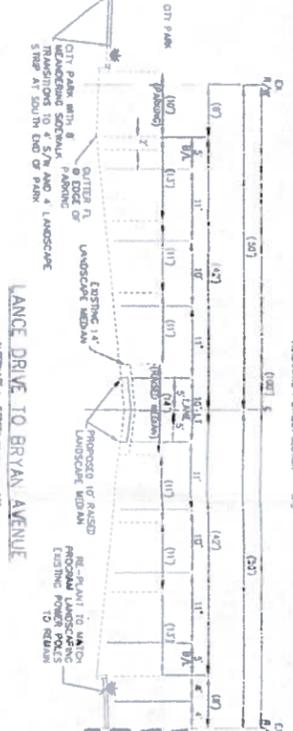
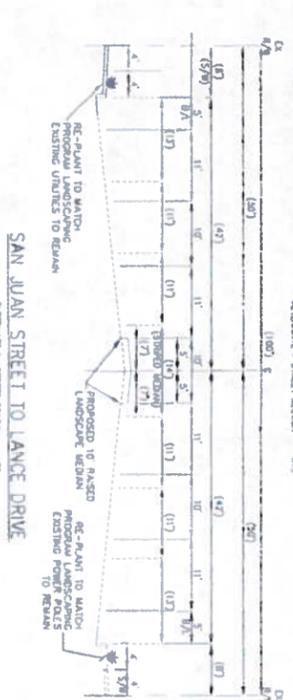
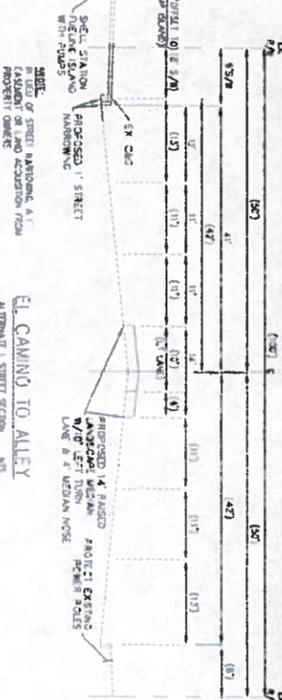
**STREET ISSUES:**

- WEST SIDE OF RED HILL AVENUE FROM EL CAMINO BLVD. TO SAN JUAN STREET.
- EAST SIDE OF RED HILL AVENUE FROM EL CAMINO BLVD. TO SAN JUAN STREET.
- WEST SIDE OF RED HILL AVENUE FROM SAN JUAN STREET TO BRYAN AVENUE.
- EAST SIDE OF RED HILL AVENUE FROM SAN JUAN STREET TO BRYAN AVENUE.
- WEST SIDE OF RED HILL AVENUE FROM BRYAN AVENUE TO LANCE DRIVE.
- EAST SIDE OF RED HILL AVENUE FROM BRYAN AVENUE TO LANCE DRIVE.
- WEST SIDE OF RED HILL AVENUE FROM LANCE DRIVE TO CITY PARK.
- EAST SIDE OF RED HILL AVENUE FROM LANCE DRIVE TO CITY PARK.

**GENERAL ASSUMPTIONS:**

- ALL PROPOSED AND RECONSTRUCTED DRIVEWAYS SHALL BE RECONSTRUCTED TO MEET THE REQUIREMENTS OF THE CALIFORNIA PUBLIC WORKS ACT AND THE CALIFORNIA PUBLIC WORKS REGULATIONS.
- ALL PROPOSED AND RECONSTRUCTED DRIVEWAYS SHALL BE RECONSTRUCTED TO MEET THE REQUIREMENTS OF THE CALIFORNIA PUBLIC WORKS ACT AND THE CALIFORNIA PUBLIC WORKS REGULATIONS.
- ALL PROPOSED AND RECONSTRUCTED DRIVEWAYS SHALL BE RECONSTRUCTED TO MEET THE REQUIREMENTS OF THE CALIFORNIA PUBLIC WORKS ACT AND THE CALIFORNIA PUBLIC WORKS REGULATIONS.

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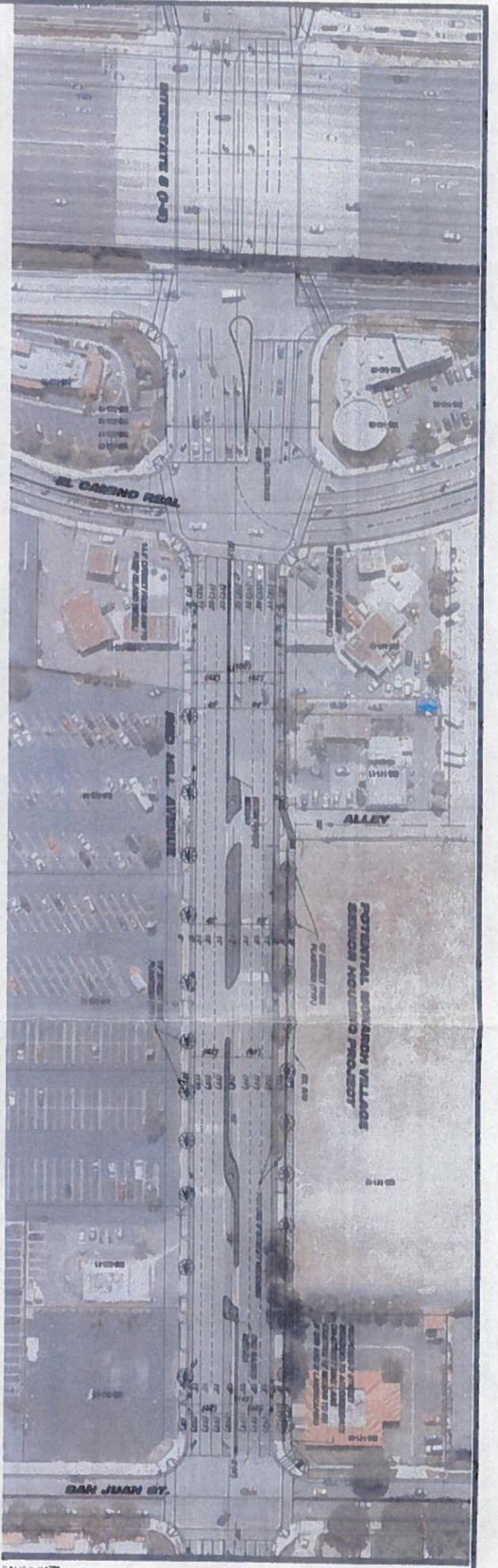
**CONCEPTUAL FEASIBILITY STUDY ALTERNATIVE 1**  
**RED HILL AVENUE STREETSCAPE & MEDIAN STUDY**



701 N. Fairbairn Drive  
Santa Ana, CA 92701  
Tel: 714.566.1000 Fax: 714.566.1011  
www.tait.com

NO.	DATE	BY	DESCRIPTION

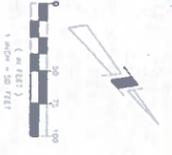
FOR REVIEW ONLY - NOT FOR CONSTRUCTION



REFERENCE LINE A  
SEE BELOW LEFT

**LEGEND:**

- TREE
- STREET
- PROPOSED STREET
- PROPOSED MEDIAN
- PROPOSED TREE LOCATION



**MONARD: VILLAGE ASSUMPTIONS:**

- 1. CONCEPT PROPOSAL IS A STUDY AND NOT A DESIGN. ALL ASSUMPTIONS ARE FOR CONCEPTUAL PURPOSES ONLY.
- 2. ALL ASSUMPTIONS ARE BASED ON THE INFORMATION PROVIDED AND ARE SUBJECT TO CHANGE.
- 3. STREET SECTIONS FROM ALTY TO SAN JUAN ARE SHOWN WITH 8' WIDENING. VILLAGES ARE SHOWN UNLESS NOTED OTHERWISE.

**ALTERNATIVE 2 DESCRIPTION:**

- PROPOSED ON STREETS 3' BUILT-UP FROM EL CAMINO REAL TO BRYAN
- PROPOSED 4' BUILT-UP FROM EL CAMINO REAL TO SAN JUAN
- PROPOSED 4' BUILT-UP FROM EL CAMINO REAL TO BRYAN
- PROPOSED 4' BUILT-UP FROM EL CAMINO REAL TO SAN JUAN
- PROPOSED 4' BUILT-UP FROM EL CAMINO REAL TO BRYAN
- PROPOSED 4' BUILT-UP FROM EL CAMINO REAL TO SAN JUAN

**STREET ISSUES:**

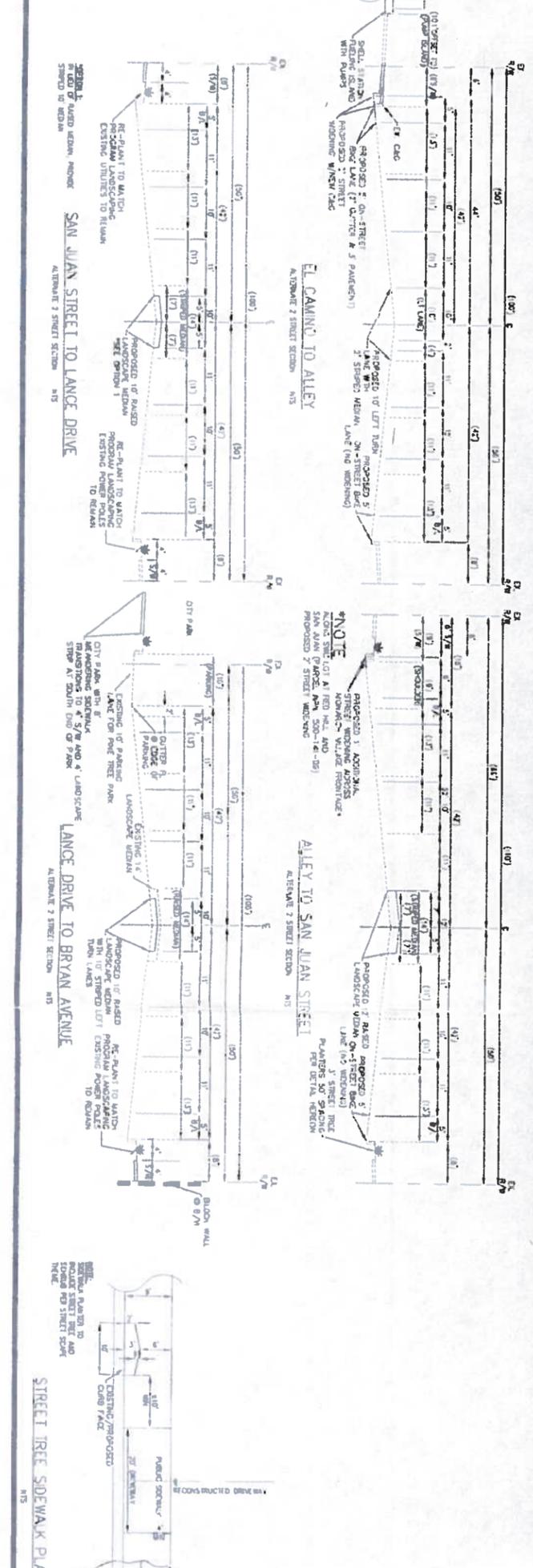
- WITH 500' OF 8' BUILT-UP FROM EL CAMINO REAL TO ALTY, REQUIREMENTS FOR 42" CURB, 42" SIDEWALK, AND 10' BUILT-UP FROM EL CAMINO REAL TO SAN JUAN, REQUIREMENTS FOR 42" CURB, 42" SIDEWALK, AND 10' BUILT-UP FROM EL CAMINO REAL TO BRYAN.
- WITH 500' OF 8' BUILT-UP FROM EL CAMINO REAL TO ALTY, REQUIREMENTS FOR 42" CURB, 42" SIDEWALK, AND 10' BUILT-UP FROM EL CAMINO REAL TO SAN JUAN, REQUIREMENTS FOR 42" CURB, 42" SIDEWALK, AND 10' BUILT-UP FROM EL CAMINO REAL TO BRYAN.

**GENERAL ASSUMPTIONS:**

- 1. ALL ASSUMPTIONS ARE BASED ON THE INFORMATION PROVIDED AND ARE SUBJECT TO CHANGE.
- 2. ALL ASSUMPTIONS ARE BASED ON THE INFORMATION PROVIDED AND ARE SUBJECT TO CHANGE.
- 3. ALL ASSUMPTIONS ARE BASED ON THE INFORMATION PROVIDED AND ARE SUBJECT TO CHANGE.



REFERENCE LINE A  
SEE ABOVE RIGHT



**CONCEPTUAL FEASIBILITY STUDY ALTERNATIVE 2**  
**RED HILL AVENUE STREETSCAPE & MEDIAN STUDY**

**TAIT**  
701 H Street, Suite 200, San Francisco, CA 94102  
Tel: 415.774.1111 | Fax: 415.774.1112 | www.tait.com

NO.	DESCRIPTION	BY	DATE	CHK.	NO.	DESCRIPTION	BY	DATE	CHK.





**Red Hill Avenue  
Streetscape and Median Feasibility Study  
El Camino Real to Bryan Avenue  
CIP #70209**

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**Appendix 2**

**Gateway Signage Location Exhibit**

REFERENCE LINE A  
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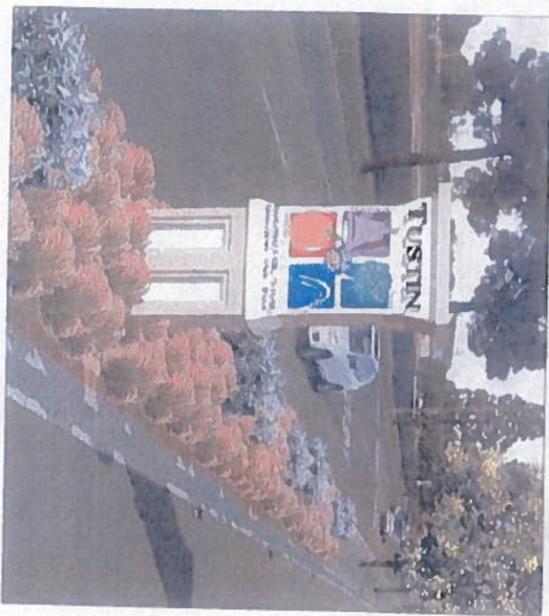
- OPTION #1:  
• SOUTHEAST CORNER RED HILL AND EL CAMINO REAL
- OPTION #2:  
• BACK TO BACK SIGNS LOCATED IN MEDIAN SOUTH OF ALLEY



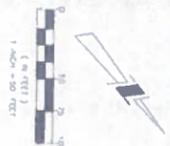
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TYPICAL GATEWAY SIGNAGE  
MADE COURTESY OF THE PLANNING CENTER



TYPICAL GATEWAY SIGNAGE  
MADE COURTESY OF THE PLANNING CENTER



1 1 1	DRAWN BY: DATE: 5-25-12 CHECKED BY: DATE: REVISED BY: DATE: 6-25-12 JOB NO: MFG328	<b>TYPICAL GATEWAY SIGN LOCATIONS</b> <b>RED HILL AVENUE STREETSCAPE &amp; MEDIAN</b> <b>IMPROVEMENT FEASIBILITY STUDY</b> <b>CITY OF TUSTIN</b> <b>CIP NO. 70209</b>	701 N. Parkcenter Drive Tustin, CA 92780 P: 714.840.8200 F: 714.840.8211 www.tait.com	NO.	DESCRIPTION	BY	DATE	CHK.	NO.	DESCRIPTION	BY	DATE	CHK.
				REVISIONS									

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**Red Hill Avenue  
Streetscape and Median Feasibility Study  
El Camino Real to Bryan Avenue  
CIP #70209**

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**Appendix 3**

**Preferred Concepts**

**Concept No. 1**

**Concept No. 2**



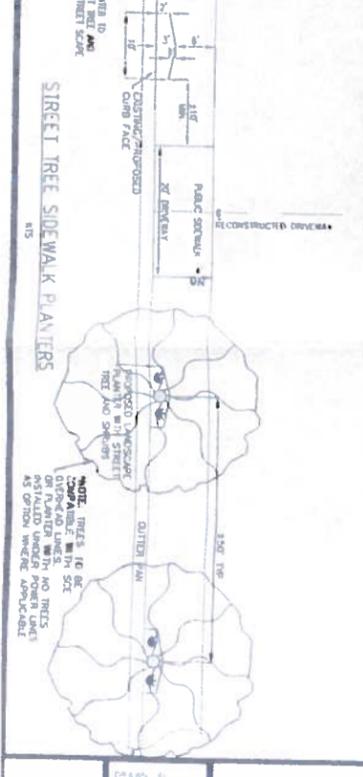
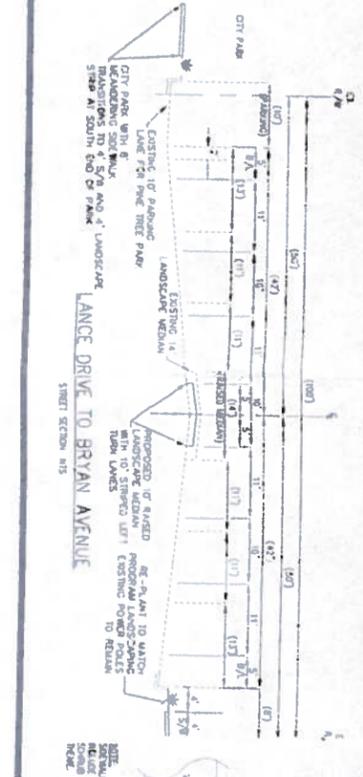
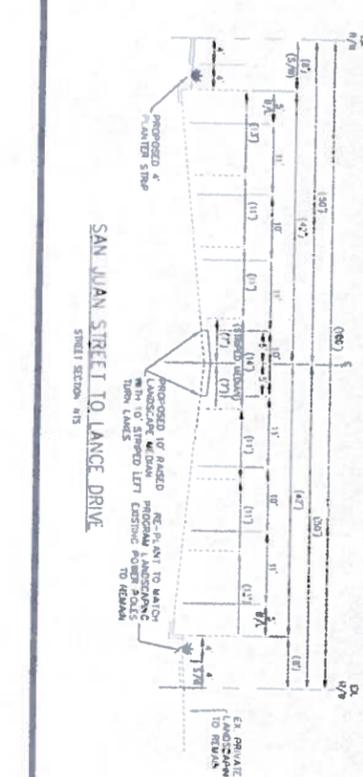
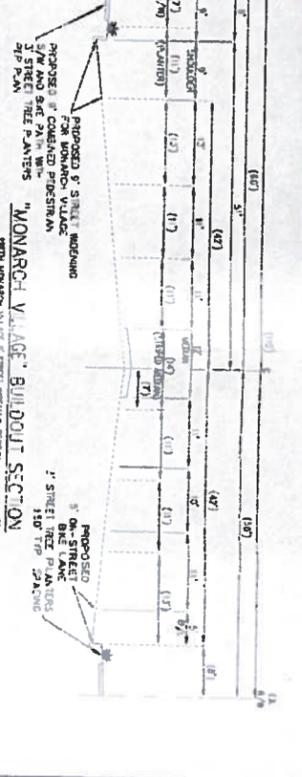
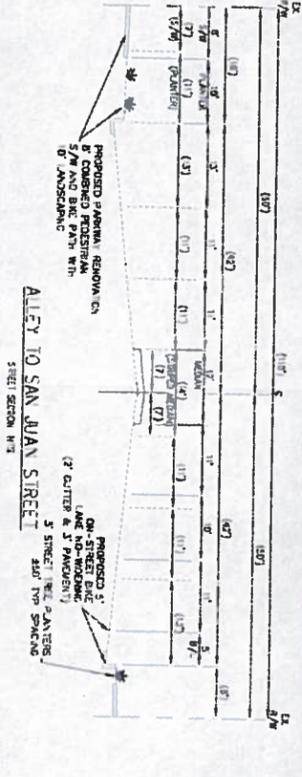
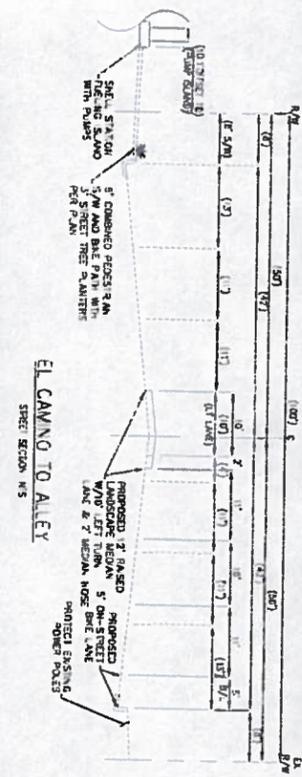
**POTENTIAL MONARCH VILLAGE SEASON HOUSING PROJECT**

REFERENCE LINE A SEE BELOW LEFT

**LEGEND**

- EXISTING
- PROPOSED
- PROPOSED 10' WIDE SIDEWALK
- PROPOSED 5' WIDE SIDEWALK
- PROPOSED 4' WIDE SIDEWALK
- PROPOSED 3' WIDE SIDEWALK
- PROPOSED 2' WIDE SIDEWALK
- PROPOSED 1' WIDE SIDEWALK
- PROPOSED 0' WIDE SIDEWALK
- PROPOSED 10' WIDE SIDEWALK
- PROPOSED 5' WIDE SIDEWALK
- PROPOSED 4' WIDE SIDEWALK
- PROPOSED 3' WIDE SIDEWALK
- PROPOSED 2' WIDE SIDEWALK
- PROPOSED 1' WIDE SIDEWALK
- PROPOSED 0' WIDE SIDEWALK

1" = 100' (1:100)



**OPTION 1 DESCRIPTION:**

- PROPOSED 10' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 5' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 4' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 3' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 2' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 1' WIDE SIDEWALK WITH PLANTERS
- PROPOSED 0' WIDE SIDEWALK WITH PLANTERS

**GENERAL ASSUMPTIONS:**

- ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED
- ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED
- ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED

**MONARCH VILLAGE ASSUMPTIONS:**

- STREET SECTION FROM ALLEY TO SAN JUAN STREET TO BE 5' WIDE SIDEWALK WITH PLANTERS
- STREET SECTION FROM ALLEY TO SAN JUAN STREET TO BE 5' WIDE SIDEWALK WITH PLANTERS

**CONCEPT NO. 1**

**RED HILL AVENUE STREETSCAPE & MEDIAN IMPROVEMENT FEASIBILITY STUDY**

**CITY OF TUSTIN**

**CIP NO. 70209**

**TAIT**

701 N. Parkcenter Drive  
Tustin, CA 92680  
Tel: 714.771.7000  
Fax: 714.771.7001  
www.tait.com

NO. DESCRIPTION REVISIONS BY DATE CHK. NO. DESCRIPTION REVISIONS BY DATE



**Red Hill Avenue  
Streetscape and Median Feasibility Study  
El Camino Real to Bryan Avenue  
CIP #70209**

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**Appendix 4**

**Construction Cost Estimates  
for  
Preferred Concepts**

ME0328  
 City of Tustin  
 Red Hill Avenue Streetscape & Median Feasibility Study  
 Preliminary Construction Cost Estimate (in 2012 dollars)

Date: July 29, 2012

	Concept 1	Concept 2 - Recommended
<b>El Camino to San Juan</b>		
Demolition	\$ 102,087.50	\$ 84,987.50
Sidewalk Improvements	\$ 194,360.00	\$ 178,530.00
Street Median Improvements	\$ 85,335.00	\$ 52,315.00
<b>San Juan to Bryan</b>		
Demolition	\$ 125,400.00	\$ 125,400.00
Sidewalk Improvements	\$ 168,400.00	\$ 168,400.00
Street Median Improvements	\$ 111,325.00	\$ 111,325.00
Subtotal - Demo & Const	\$ 786,907.50	\$ 720,957.50
Mobilization 5%	\$ 39,345.38	\$ 36,047.88
Contingency 20%	\$ 165,250.58	\$ 151,401.08
Const Erosion Control BMPs	\$ 7,000.00	\$ 7,000.00
<b>Total:</b>	<b>\$ 998,503.45</b>	<b>\$ 915,406.45</b>

**Red Hill Avenue  
Streetscape and Median Feasibility Study  
El Camino Real to Bryan Avenue  
CIP #70209**

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**Appendix 5**

**Existing Overhead Utility Services  
Investigation and Cost Estimate**

**Prepared By:  
Utility Specialists**

City of Tustin

Red Hill Avenue Conversion/Relocation

El Camino Real to Bryan Avenue

Prepared for Tait & Associates

By Utility Specialists

May 7, 2012

Red Hill Avenue has an existing overhead SCE pole line that consists of (1) 4-wire 12Kv system, (1) local street light distribution system, (4-6) communications attachments by ATT, Time Warner Cable and possibly Cox Communications or others under a standard pole sharing agreement. This pole line also ties in several perpendicular overhead systems, (1) at San Juan Street, (1) at Bryan Avenue and (2) systems between Lance Drive and San Juan Street that serve existing residential communities. This pole line also provides service points to several residences (northwest) and/or apartment communities (southeast) along Red Hill.

At the intersection of Red Hill and El Camino Real these facilities are underground. They rise to an overhead position on Pole # 4366594E (Figure 1.1) approximately 260' northeast of the intersection.

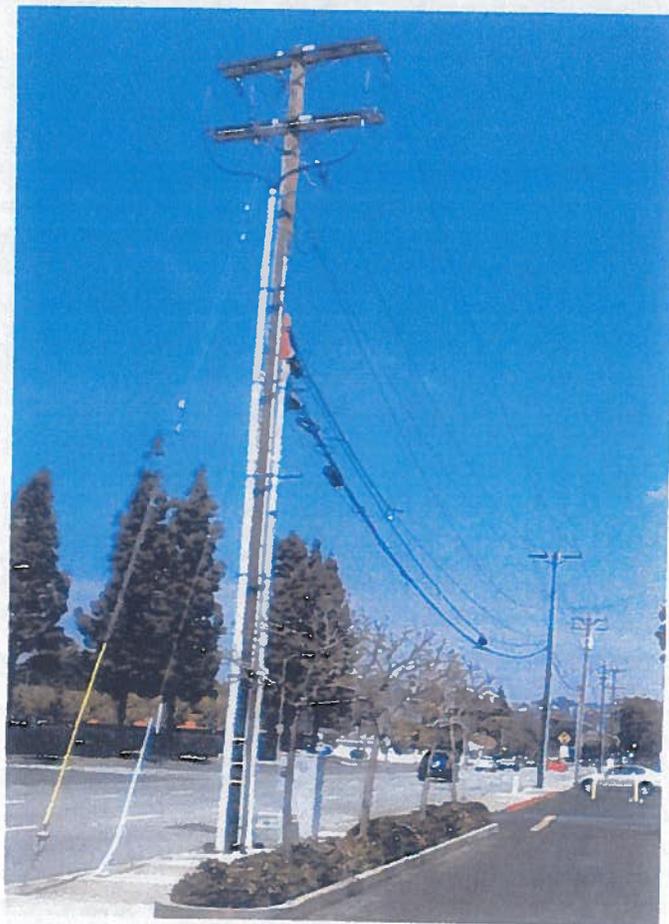


Figure 1.1

At the intersection of Red Hill and Bryan, this pole line joins an existing SCE 66Kv Transmission pole line (Figure 1.2) that comes from Bryan and joins this pole line and continues to the northeast along Red Hill.

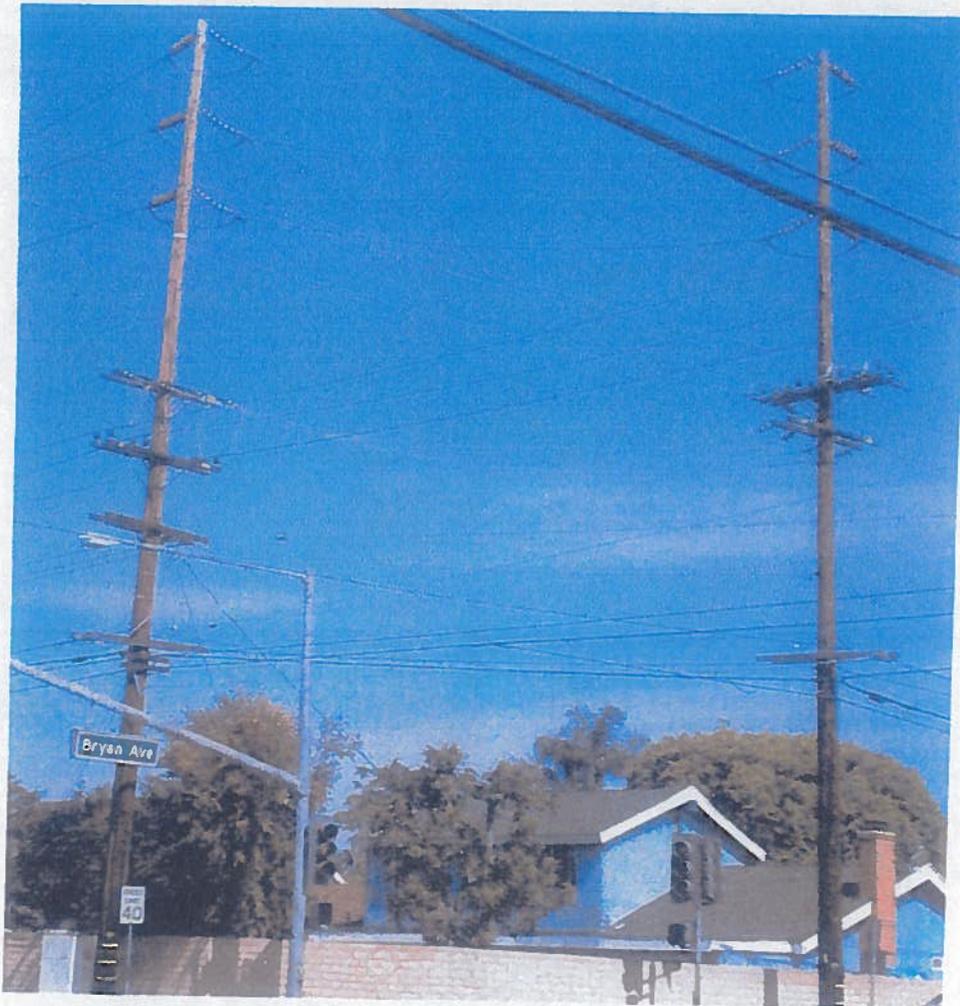


Figure 1.2

This overhead pole line has (2) remote SCE switches attached (Figure 1.3) that will need to be placed on above ground pad locations. There is also (1) manual switch at the corner of Red Hill and Bryan that will also need to be placed.



Figure 1.3

At several locations there are SCE, ATT and CATV risers (Figure 1.4) that go underground to serve various developments and/or services to different facilities. Along with these, there are several transformers that are providing service voltage power that we will need to find space for locating the pad mounted transformers so we can maintain service to all existing uses.

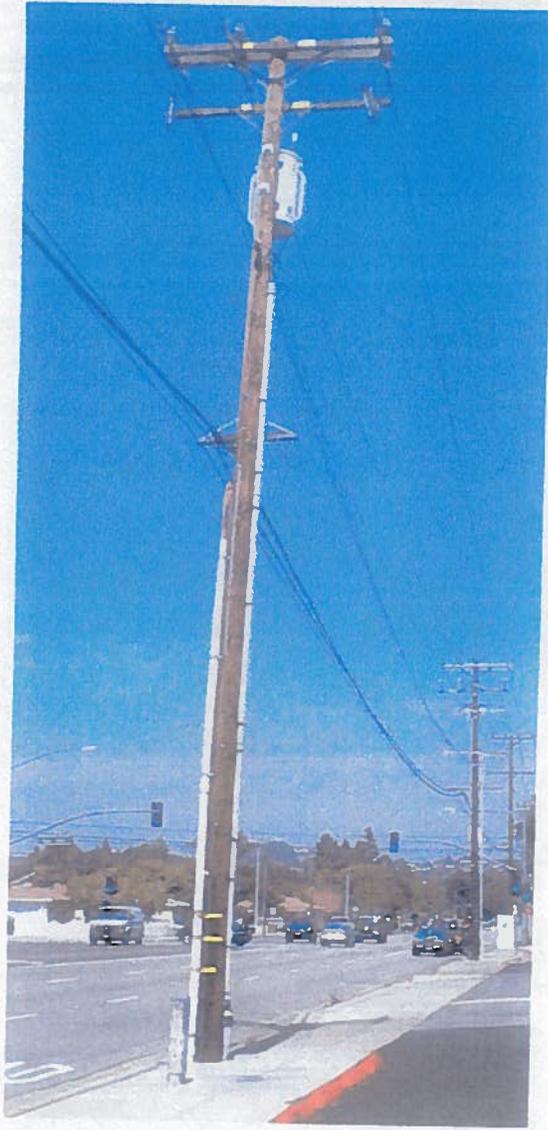


Figure 1.4

## Constraints

Relocations will need to take place at several locations in order to take advantage of some tariff applications. Specifically when dealing with services to the opposite side of the street to some residents.

Communications facilities (ATT and CATV) will be converted to underground as well along this route in a similar manner. It is expected that all of these facilities go together in a joint trench. Space will need to be considered for any and all underground and pad mounted structures per the SCE underground initiative.

SCE's Underground Initiative that has been in place for the last year or so requires all structure to be place above ground where possible. There are some exceptions where space is not adequate to provide for clearances and safety. The tie in with the SCE transmission overhead pole at Bryan may create a scheduling delay if any of the transmission facilities require relocation (this is not taken into consideration with respect to potential schedule or costs below). With the system feeding overhead services to adjacent residents, their will need to be some negotiations with the affected residents to discuss how to handle converting or relocating these facilities to keep service in place with the least amount of interruption or inconvenience to residents.

This possible conversion/relocation is fairly standard and should easily be managed to a successful outcome with the utility agencies involved.

## Potential Schedule

<b>Item</b>		<b>Duration</b>	<b>Predecessor</b>
	<b>SCE</b>		
1	Submit to Dry Utilities (Street Improvement Plans at 65% or better with conceptual conversion/relocation)	<b>0 wks</b>	
2	SCE Prelim Design	<b>12 wks</b>	1
3	Review and approve SCE Prelim Design	<b>2 wks</b>	2
4	SCE final design	<b>6 wks</b>	3
5	SCE final contracts	<b>3 wks</b>	4
6	Applicant sign SCE Contracts and send Payment	<b>2 wks</b>	5
	<b>Phone</b>		
7	Phone Design	<b>8 wks</b>	2
8	Execute Agreements	<b>2 wks</b>	7
	<b>Cable</b>		
9	Cable Design	<b>8 wks</b>	2
10	Developer execute Agreements	<b>2 wks</b>	9

# Utility Contacts

## **SCE**

Randy Williams – Planning Supervisor

(949) 458-4674

randall.williams@sce.com

## **ATT**

Craig Akin

(714) 666-5698

Ca1818@att.com

## **Time Warner**

Chris Donnelly

(714) 903-8307

chris.donnelly@twcable.com

## PRELIMINARY COST REPORT

**Red Hill Avenue**  
 May 7, 2012

The following is an opinion of probable gas, electric, telephone, and CATV costs. Utility Specialists' opinion of probable costs is made on the basis of our experience and qualifications and represents our best judgement based on direction, plans, and information provided by the client. Utility Specialists, cannot and does not guarantee that proposals, bids or the construction costs will not vary from the opinions that we have prepared. This information is proprietary and not for distribution without the written consent of Utility Specialists.

	REFUNDABLE OPTION COST OPINIONS	NON- REFUNDABLE ELECTRIC OPTION COST OPINIONS
<b>A. ELECTRIC DISTRIBUTION SYSTEM (UTILITY CHARGES)</b>		
1. Electric Deposit	\$0	\$0
2. Tax on Value of Trench & Conduit	\$0	\$0
3. Tax on Value of Substructures	\$0	\$0
4. Electric Line Relocations & Removals	\$0	\$0
5. Electric Conversion	\$565,690	\$565,690
6. Right-of-Way Fees	\$0	\$0
7. Street Lights (SCE)	\$0	\$0
8. Excess Electric Service Cable/Service Delivery Point	\$0	\$0
<b>B. GAS DISTRIBUTION SYSTEM (UTILITY CHARGES)</b>		
1. Gas Deposit	\$0	\$0
2. Gas/Fuel Line Relocations	\$0	\$0
<b>C. TELEPHONE DISTRIBUTION SYSTEM (UTILITY CHARGES)</b>		
1. Telephone Deposit	\$0	\$0
2. Tax on Value of Customer Work	\$0	\$0
3. Telephone Relocations	\$41,440	\$41,440
4. Telephone Conversion	\$224,410	\$224,410
5. Secondary M.P.O.E.	\$0	\$0
<b>D. CATV DISTRIBUTION SYSTEM (UTILITY CHARGES)</b>		
1. CATV Deposit	\$0	\$0
2. CATV Relocations	\$2,010	\$2,010
3. CATV Conversions	\$51,520	\$51,520
<b>E. CONTRACTOR INSTALLATION COSTS</b>		
1. Trench, Backfill, Conduit & Substructure Installation		
-Mains	\$0	\$0
-Services	\$0	\$0
-Conversions	\$1,055,350	\$1,055,350
2. Street Lights & Installation Cost	\$0	\$0
3. Dry Utility Consulting Fees	\$0	\$0
<b>F. ESTIMATED REIMBURSEMENTS</b>		
1. Electric Facilities (given as a refund based on allowance)	\$0	
2. Gas Trench (given as refund based on allowance)	\$0	
3. Telephone Trench, Conduit & Substructures	\$0	
4. CATV Facilities	\$0	
<b>G. ESTIMATED REFUNDS</b>		
1. Electric Deposit (refundable option only)	\$0	
2. Gas Deposit (refundable option only)	\$0	
3. Telephone Deposit	\$0	
<b>H. TOTALS</b>	<b>\$1,940,420</b>	<b>\$1,940,420</b>
<b>I. NET COST-Total Cost Minus Refunds &amp; Reimbursements</b>	<b>\$1,940,420</b>	<b>\$1,940,420</b>

E

MITIGATION MONITORING PROGRAM (MMRP)

# MITIGATION MONITORING AND REPORTING PROGRAM

## RED HILL AVENUE SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT SCH NO. 2017041031

Prepared for

City of Tustin  
300 Centennial Way  
Tustin, California 92780

Prepared by

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Date

October 2018

## 1.1 PROJECT LOCATION AND DESCRIPTION

The Red Hill Avenue Specific Plan area extends along Red Hill Avenue to Bryan Avenue to the northeast, and Walnut Avenue to the southwest. Interstate 5 (I-5) bisects the Specific Plan area creating the northern and southern portions of the Specific Plan.

The Red Hill Avenue Specific Plan provides planning policies and regulations that connect General Plan policies with future project-level development within the Specific Plan area. The purpose of the Specific Plan is to guide future change, promote high-quality development, and implement the community's vision for the Specific Plan area. The Specific Plan provides goals and objectives, a land use plan, regulatory standards, Design Criteria, and administration and implementation programs to encourage high-quality development.

The Specific Plan would allow for an additional 325,000 square feet of non-residential development and 500 additional dwelling units. The total development in the Specific Plan area anticipated with the buildout potential of the Specific Plan is 521 dwelling units and 621,446 square feet of non-residential development, inclusive of existing and proposed uses. Red Hill Avenue would be restriped within the paved width of the street to include on-street bike lanes, reduced lane widths, turn pockets, and landscaped medians where feasible.

## 1.2 PURPOSE OF MITIGATION MONITORING AND REPORTING PROGRAM

The California Environmental Quality Act (CEQA) requires that all public agencies establish monitoring and/or reporting procedures for mitigation adopted as conditions of approval in order to mitigate or avoid significant environmental impacts. This Mitigation Monitoring and Reporting Program (MMRP) has been developed to provide a vehicle by which to monitor Standard Conditions (SCs) and mitigation measures (MMs) outlined in the Red Hill Specific Plan Final Environmental Impact Report (EIR), State Clearinghouse No. SCH NO. 2017041031. The Red Hill Specific Plan MMRP has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Tustin Monitoring Requirements. Specifically, Section 21081.6 states:

- (a) When making findings required by paragraph (1) of subdivision (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:
  - (1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead or responsible agency, prepare and submit a proposed reporting or monitoring program.

- (2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

CEQA Guidelines Section 15097 provides clarification of mitigation monitoring and reporting requirements and guidance to local lead agencies on implementing strategies. The reporting or monitoring program must be designed to ensure compliance during project implementation. The City of Tustin is the Lead Agency for the Red Hill Specific Plan Project and is therefore responsible for ensuring the implementation of the MMRP. The MMRP has been drafted to meet the requirements of Public Resources Code Section 21081.6 as a fully enforceable monitoring program.

The MMRP is comprised of the Mitigation Program and includes measures to implement and monitor the Mitigation Program. The MMRP defines the following for each SC and MM identified in Table 1, *Mitigation Monitoring Requirements*:

- **Definition of Mitigation (SC, MM).** The mitigation measure contains the criteria for mitigation, either in the form of adherence to certain adopted regulations or identification of the steps to be taken in mitigation.
- **Responsible Party or Designated Representative.** Unless otherwise indicated, an applicant would be the responsible party for implementing the mitigation, and the City of Tustin or a designated representative is responsible for monitoring the performance and implementation of the mitigation measures. To guarantee that the mitigation will not be inadvertently overlooked, a supervising public official acting as the Designated Representative is the official who grants the permit or authorization called for in the performance. Where more than one official is identified, permits or authorization from all officials shall be required.
- **Time Frame.** In each case, a time frame is provided for performance of the mitigation or the review of evidence that mitigation has taken place. The performance points selected are designed to ensure that impact-related components of project implementation do not proceed without establishing that the mitigation is implemented or ensured. All activities are subject to the approval of all required permits from agencies with permitting authority over the specific activity.

The numbering system in Table 1 corresponds with the numbering system used in the EIR. The last column of the MMRP table will be used by the parties responsible for documenting when implementation of the mitigation measure has been completed. The ongoing documentation and monitoring of mitigation compliance will be completed by the City of Tustin. The completed MMRP and supplemental documents will be kept on file at the City of Tustin Community Development Department, Planning & Zoning Division.

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<b>AIR QUALITY</b>				
<p><b>SC 4.2-1: Dust Control.</b> During construction of the future development with the Specific Plan area, project applicants shall require all construction contractors to comply with South Coast Air Quality Management District’s (SCAQMD’s) Rules 402 and 403 in order to minimize short-term emissions of dust and particulates. SCAQMD Rule 402 requires that air pollutant emissions not be a nuisance off site. SCAQMD Rule 403 requires that fugitive dust be controlled with Best Available Control Measures so that the presence of such dust does not remain visible in the atmosphere beyond the property line of the emission source. This requirement shall be included as notes on the contractor specifications. Table 1 of Rule 403 lists the Best Available Control Measures that are applicable to all construction projects. The measures include, but are not limited to, the following:</p> <ul style="list-style-type: none"> <li>▪ <b>Clearing and grubbing:</b> Apply water in sufficient quantity to prevent generation of dust plumes.</li> <li>▪ <b>Cut and fill:</b> Pre-water soils prior to cut and fill activities and stabilize soil during and after cut and fill activities.</li> <li>▪ <b>Earth-moving activities:</b> Pre-apply water to depth of proposed cuts; re-apply water as necessary to maintain soils in a damp condition and to ensure that visible emissions do not exceed 100 feet in any direction; and stabilize soils once earth-moving activities are complete.</li> <li>▪ <b>Importing/exporting of bulk materials:</b> Stabilize material while loading to reduce fugitive dust emissions; maintain at least six inches of freeboard on haul vehicles; and stabilize material while transporting to reduce fugitive dust emissions.</li> <li>▪ <b>Stockpiles/bulk material handling:</b> Stabilize stockpiled materials; stockpiles within 100 yards of off-site occupied buildings must not be greater than 8 feet in height, must have a road bladed to the top (refers to a road to the top of the pile) to allow water truck access, or must</li> </ul>	<p>Applicant  Construction Contractor</p>	<p>Prior to issuance of Grading and Building Permits  Monitor during grading and construction</p>	<p>Community Development Department – Building Division</p>	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p>have an operational water irrigation system that is capable of complete stockpile coverage.</p> <ul style="list-style-type: none"> <li>▪ <b>Traffic areas for construction activities:</b> Stabilize all off-road traffic and parking areas; stabilize all haul routes; and direct construction traffic over established haul routes.</li> </ul> <p>Rule 403 defines large operations as projects with 50 or more acres of grading or with a daily earth-moving volume of 5,000 cubic yards at least 3 times in 1 year. Future development within the Specific Plan would potentially be considered a large operation. Large operations are required to implement additional dust-control measures (as specified in Tables 2 and 3 of Rule 403); provide additional notifications, signage, and reporting; and appoint a Dust Control Supervisor. The Dust Control Supervisor is required to:</p> <ul style="list-style-type: none"> <li>▪ Be employed by or contracted with the Property Owner or Developer;</li> <li>▪ Be on the site or available on site within 30 minutes during working hours;</li> <li>▪ Have the authority to expeditiously employ sufficient dust mitigation measures to ensure compliance with all Rule 403 requirements; and</li> <li>▪ Have completed the AQMD Fugitive Dust Control Class and have been issued a valid Certificate of Completion for the class.</li> </ul>				
<p><b>SC 4.2-2: Architectural Coatings.</b> Architectural coatings shall be selected so that the VOC content of the coatings is compliant with SCAQMD Rule 1113. This requirement shall be included as notes on the contractor specifications.</p>	Applicant Construction Manager	Identify in contractor specifications  During building plan check and construction	Community Development Department – Building Division	
<p><b>MM 4.2-1: Electric Vehicle (EV) Charging Stations.</b> Prior to the issuance of building permits, the City’s Building Official shall confirm that project plans and specifications designate that vehicle parking spaces developed within the Specific Plan area shall be EV ready to encourage EV use and appropriately size electrical panels to accommodate future expanded EV use.</p>	Applicant	Prior to issuance of Building Permits	Community Development Department – Building Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p><b>MM 4.2-2: Vanpool/Rideshare Programs.</b> Prior to the issuance of occupancy permits, the City’s Building Official shall confirm that future commercial uses within the Specific Plan area include Codes, Covenants, and Restrictions (CC&amp;Rs) that provide for a voluntary vanpool/shuttle and employee ridesharing programs for which all employees shall be eligible to participate. The voluntary ride sharing program could be achieved through a multi-faceted approach, such as designating a certain percentage of parking spaces for ride-sharing vehicles, designating adequate passenger loading and unloading and waiting areas for ridesharing vehicles, and/or providing a web site or message board for coordinating rides. This measure is not applicable to residential uses.</p>	Applicant	Prior to the issuance of Occupancy Permits	Community Development Department – Building Division	
<p><b>MM 4.2-3: Operational Emissions Reductions.</b> Prior to the issuance of building permits, the City’s Planning Official shall confirm that project plans and specifications consider and mitigate the impacts on regional air quality and GHG emissions when reviewing proposals for new development. Impacts shall be evaluated in accordance with SCAQMD recommended methodologies and procedures. Recommended mitigation measure may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> <li>▪ Install heat transfer modules in all furnaces;</li> <li>▪ Install solar panels for water heating systems for residential and other facilities;</li> <li>▪ Incorporate renewable energy sources in the project design (e.g., solar photovoltaic panels).</li> <li>▪ Include passive solar cooling/heating design elements in building designs;</li> <li>▪ Include design elements that maximize use of natural lighting in new development;</li> <li>▪ Include provisions to install energy efficient appliances and lighting in new development.</li> <li>▪ Install higher efficacy public street and exterior lighting.</li> </ul>	Applicant	Prior to the issuance of Building Permits	Community Development Department – Planning & Zoning Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<ul style="list-style-type: none"> <li>▪ Increase project density.</li> <li>▪ Incorporate design measures that promote bicycle, pedestrian, and public transportation use.</li> <li>▪ Provide preferential parking spaces for alternatively-fueled vehicles.</li> <li>▪ Incorporate measures that reduce water use and waste generation.</li> <li>▪ Provide informational materials on low ROG/VOC consumer products, cleaners, paints, and other products, as well as the importance of recycling and purchasing recycled material. Informational materials shall be provided to residential and commercial occupants through CC&amp;R requirements.</li> <li>▪ Incorporate measures and design features that promote ride sharing and consistency with the commute-reduction requirements of SCAQMD Rule 2202 (On-Road Motor Vehicle Mitigation Options).</li> </ul>				
<p><b>MM 4.2-4: Toxic Air Contaminants/Health Risk Assessment.</b> A project-specific Health Risk Assessment shall be conducted for future residential development proposed within 500 feet of the Interstate 5 right-of-way, pursuant to the recommendations set forth in the CARB <i>Air Quality and Land Use Handbook</i>. The Health Risk Assessment shall evaluate a project per the following SCAQMD thresholds:</p> <ul style="list-style-type: none"> <li>▪ <i>Cancer Risk:</i> Emit carcinogenic or toxic contaminants that exceed the maximum individual cancer risk of 10 in one million.</li> <li>▪ <i>Non-Cancer Risk:</i> Emit toxic contaminants that exceed the maximum hazard quotient of one in one million.</li> </ul> <p>The SCAQMD has also established non-carcinogenic risk parameters for use in HRAs. Noncarcinogenic risks are quantified by calculating a “hazard index,” expressed as the ratio between the ambient pollutant concentration and its toxicity or Reference Exposure Level (REL). An REL is a concentration at or below which health effects are not likely to occur. A hazard index less than one (1.0) means that adverse health effects are not expected. If projects are found</p>	Applicant	Site Specific CEQA Phasing	Community Development Department – Planning & Zoning Division	

<b>Table 1-1. Mitigation Monitoring Requirements</b>				
<b>Standard Condition (SC) and Mitigation Measure (MM)</b>	<b>Responsibility for Implementation</b>	<b>Timing</b>	<b>Responsibility for Monitoring</b>	<b>Monitor (Signature; Date of Compliance)</b>
to exceed the SCAQMD’s Health Risk Assessment thresholds, mitigation shall be incorporated to reduce impacts to below SCAQMD thresholds.				
<b>CULTURAL RESOURCES AND TRIBAL CULTURAL RESOURCES</b>				
<b>SC 4.3-1:</b> California Health and Safety Code Section 7050.5, CEQA Section 15064.5, and Public Resources Code Section 5097.98 mandate the process to be followed in the event of an accidental discovery of any human remains in a location other than a dedicated cemetery. California Health and Safety Code Section 7050.5 requires that in the event that human remains are discovered within the Specific Plan area, disturbance of the site shall be halted until the coroner has conducted an investigation into the circumstances, manner and cause of death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, or to his or her authorized representative, in the manner provided in Section 5097.98 of the Public Resources Code. If the coroner determines that the remains are not subject to his or her authority and if the coroner recognizes or has reason to believe the human remains to be those of a Native American, he or she shall contact, by telephone within 24 hours, the Native American Heritage Commission.	Qualified Archeologist  Native American Heritage Commission (NAHC) (as necessary)	County Coroner: within 24 hours of a discovery	Community Development Department – Planning & Zoning Division  Community Development Department – Building Division	
<b>MM 4.3-1:</b> The State CEQA Guidelines (14 CCR §15126.4[b][3]) direct public agencies, wherever feasible, to avoid damaging historical resources of an archaeological nature, preferably by preserving the resource(s) in place. Preservation in place options suggested by the State CEQA Guidelines include (1) planning construction to avoid an archaeological site; (2) incorporating the site into open space; (3) capping the site with a chemically stable soil; and/or (4) deeding the site into a permanent conservation easement.  Prior to issuance of a grading permit for grading of 2 feet or more in depth below the natural or existing grade, the applicant/developer shall provide written evidence to the City Planning Division that a qualified archaeologist has been retained by the applicant/developer to respond on an as-needed basis to address unanticipated archaeological discoveries and any archaeological requirements (e.g., conditions of approval) that are applicable to the project. The applicant/developer is encouraged to conduct a field	Applicant  Qualified Archaeologist  Native American monitor, if needed	Retention of archaeologist: Prior to issuance of Grading Permit and/or action that would permit site disturbance (whichever occurs first)  Recommended attendance of archaeologist at pre-grade meeting	Community Development Department – Planning & Zoning Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p>meeting prior to the start of construction activity with all construction supervisors to train staff to identify potential archaeological resources. In the event that archaeological materials are encountered during ground-disturbing activities, work in the immediate vicinity of the resource shall cease until a qualified archaeologist has assessed the discovery and appropriate treatment pursuant to CEQA Guidelines Section 15064.5 is determined.</p> <p>If discovered archaeological resources are found to be significant, the archaeologist shall determine, in consultation with the City and any local Native American groups expressing interest following notification by the City, appropriate avoidance measures or other appropriate mitigation. Per CEQA Guidelines Section 15126.4(b)(3), preservation in place shall be the preferred means to avoid impacts to archaeological resources qualifying as historical resources. Consistent with CEQA Guidelines Section 15126.4(b)(3)(C), if it is demonstrated that confirmed resources cannot be avoided, the qualified archaeologist shall develop additional treatment measures, such as data recovery, reburial/ relocation, deposit at a local museum that accepts such resources or other appropriate measures, in consultation with the implementing agency and any local Native American representatives expressing interest in prehistoric or tribal resources. If an archaeological site does not qualify as an historical resource but meets the criteria for a unique archaeological resource as defined in Section 21083.2, then the site shall be treated in accordance with the provisions of Section 21083.2.</p>		Identify requirements in approved grading and construction plans		

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p><b>MM 4.3-2:</b> Prior to issuance of any grading or building permits for any development projects under the Red Hill Avenue Specific Plan, the applicant shall provide a letter to the City of Tustin Community Development Department, or designee, from a paleontologist selected from the roll of qualified paleontologists maintained by the County of Orange, stating that the applicant has retained this individual and that the paleontologist shall provide on-call services in the event resources are discovered. The paleontologist shall be present at the pre-grading conference to establish procedures for paleontological resource surveillance. If paleontological resources are discovered during any development project within the Red Hill Avenue Specific Plan area, ground-disturbing activity within 50 feet of the area of the discovery shall cease.</p> <p>If the find is determined by paleontologists to require further treatment, the area of discovery will be protected from disturbance while qualified paleontologists and appropriate officials, in consultation with a recognized museum repository (e.g., National History Museum of Los Angeles County), determine an appropriate treatment plan.</p>	Applicant Qualified Paleontologist	Retention of paleontologist: Prior to issuance of Grading Permits and/or action that would permit site disturbance  Identify requirements in approved grading and construction plans  Attendance of paleontologist at pre-grade meeting	Community Development Department – Planning & Zoning Division	
<b>GEOLOGY AND SOILS</b>				
<p><b>SC 4.4-1:</b> Projects are required to comply with Tustin City Code, Chapter 9, Grading and Excavation. Prior to the issuance of any grading permits, the grading plans shall be accompanied by geological and soils engineering reports and shall incorporate all information as required by the City. Grading plans shall indicate all areas of grading. Grading plans shall provide for temporary erosion control on all graded sites scheduled to remain unimproved for more than 30 days.</p>	Applicant Registered Geotechnical Engineer	Prior to the issuance of Grading Permits  Identified in approved grading and construction plans	Community Development Department – Building Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p><b>SC 4.4-2:</b> A specific geotechnical survey shall be prepared by a certified geotechnical engineer to confirm/refine engineering design parameters regarding site preparation, grading, and foundation design, to assure design criteria are responsive to specific development site soils and potential effects of differential settlements resulting from ground shaking, as well as effects of subsidence, lateral spreading, and collapse potential. All geotechnical recommendations shall be noted on individual site development plans and implemented prior to issuance of an occupancy permit.</p> <p>Project-specific geotechnical measures shall be developed, as needed, based on the design-level geotechnical report and depicted on plans prepared by the geotechnical engineer of record or on plan sheets included within final grading plans, and subject to the approval by the City of Tustin Building Division and/or the Public Works Department.</p>	<p>Applicant Registered Geotechnical Engineer</p>	<p>Prior to the issuance of Grading and Building Permits  Identified in approved grading and construction plans</p>	<p>Community Development Department – Building Division and/or the Public Works Department</p>	
<p><b>SC 4.4-3:</b> Future developments shall limit grading to the minimum area necessary for construction. Final grading plans shall include best management practices (BMPs) to limit on-site and off-site erosion and a water plan to treat disturbed areas during construction and reduce dust. The plans shall be submitted to the City of Tustin Building Division and/or the Public Works Department for review and approval prior to the issuance of a grading permit.</p>	<p>Applicant Registered Geotechnical Engineer</p>	<p>Prior to issuance of Grading and Building Permits</p>	<p>Community Development Department – Building Division and/or the Public Works Department</p>	
<b>HAZARDS AND HAZARDOUS MATERIALS</b>				
<p><b>MM 4.6-1:</b> Prior to issuance of grading permits, a human health risk evaluation shall be prepared by a qualified environmental professional in consultation with Orange County Health Care Agency, Environmental Health Division (OCHCA-EH) for any individual site application proposed on a site with a current or former hazardous materially regulated facility to determine if there is a contamination risk to the proposed land use. Remedial activities, if necessary, may be required, in consultation with OCHCA-EH.</p>	<p>Applicant Qualified Environmental Professional</p>	<p>Prior to issuance of Grading and Building Permits</p>	<p>Community Development Department – Building Division</p>	

<b>Table 1-1. Mitigation Monitoring Requirements</b>				
<b>Standard Condition (SC) and Mitigation Measure (MM)</b>	<b>Responsibility for Implementation</b>	<b>Timing</b>	<b>Responsibility for Monitoring</b>	<b>Monitor (Signature; Date of Compliance)</b>
<b>HYDROLOGY AND WATER QUALITY</b>				
<b>SC 4.7-1:</b> Prior to the issuance of grading permits for any development projects under the Red Hill Avenue Specific Plan that would disturb more than one acre, the project applicant shall submit to the Department of Public Works an approved copy of the Storm Water Pollution Prevention Plan (SWPPP) and Notice of Intent (NOI) to comply with the General Permit for Construction Activities, confirming to the Current National Pollutant Discharge Elimination System (NPDES) requirements. The SWPPP shall be made part of the construction program. This SWPPP shall detail measures and practices that would be in effect during construction to minimize the individual project’s impact on water quality and stormwater runoff volumes. The plan shall incorporate all necessary Best Management Practices (BMPs) and other City requirements to eliminate polluted runoff until all construction work for the future development is completed. The SWPPP shall include treatment and disposal of all dewatering operation flows and for nuisance flows during construction.	Applicant	Prior to issuance of Grading and Building Permits	Public Works Department	
<b>SC 4.7-2:</b> Prior to issuance of grading permits for any development projects under the Red Hill Avenue Specific Plan, the project applicant shall prepare and submit a Water Quality Management Plan (WQMP) for the project, subject to the approval of the Department of Public Works. The WQMP shall include appropriate BMPs and low impact development (LID) techniques to ensure project runoff is adequately treated.	Applicant	Prior to issuance of Grading and Building Permits	Public Works Department	
<b>SC 4.7-3:</b> Projects within the Specific Plan area would be subject to conditions imposed by the City of Tustin Community Development Department and the Public Works Department in accordance with Section 4902 (Control of Urban Runoff) of the Tustin City Code which requires the project applicant to provide all drainage facilities necessary for the removal of surface water from a site and to protect off-site properties from a project’s water runoff. The storm drain system must be designed in accordance with the standards of the Orange County Flood Division.	Applicant	During grading and construction	Community Development Department and the Public Works Department	

<b>Table 1-1. Mitigation Monitoring Requirements</b>				
<b>Standard Condition (SC) and Mitigation Measure (MM)</b>	<b>Responsibility for Implementation</b>	<b>Timing</b>	<b>Responsibility for Monitoring</b>	<b>Monitor (Signature; Date of Compliance)</b>
<b>MM 4.7-1:</b> Prior to issuance of any grading or building permits for any development projects under the Red Hill Avenue Specific Plan, the project applicant shall prepare and submit to the Department of Public Works a hydrology and hydraulics analysis demonstrating that the existing condition flow rates are not exceeded by the proposed project flow rates.	Applicant	Prior to issuance of Grading and Building Permits	Public Works Department	
<b>MM 4.7-2:</b> Prior to issuance of any grading or buildings permits for any development projects under the Red Hill Avenue Specific Plan that do not have a direct connection to the City’s existing storm drain system, shall provide to the Department of Public Works hydraulic analyses of the downstream storm drain system that demonstrate no significant impacts to the City storm drain infrastructure.	Applicant	Prior to issuance of Grading and Building Permits	Public Works Department	
<b>NOISE</b>				
<b>SC 4.9-1:</b> To ensure compliance with Tustin City Code, grading and construction plans shall include a note indicating that loud noise-generating project construction activities (as defined in Section 4616(2) and Section 4617(e) of the Tustin City Code) shall take place between the hours of 7:00 AM and 6:00 PM on weekdays and from 9:00 AM to 5:00 PM on Saturdays. Loud, noise-generating construction activities are prohibited outside of these hours and on Sundays and City observed Federal holidays.	Applicant Construction Manager	Condition of Grading and Building Permits Monitor during grading and construction	Community Development Department – Building Division	
<b>SC 4.9-2:</b> Development projects are required to meet or exceed the 65 dBA CNEL exterior noise level standard, as defined by Table N-3 of the City of Tustin General Plan Noise Element, and the 45 dBA CNEL interior noise level standard of the City of Tustin General Plan Noise Element, and by Title 24, Part 2, of the California Building Code.	Applicant Construction Manager	Prior to issuance of Grading and Building Permits	Community Development Department – Building Division	
<b>MM 4.9-1: Construction Noise.</b> Prior to approval of grading plans, the City of Tustin Building Division shall ensure that plans include Best Management Practices to minimize construction noise. Construction noise Best Management Practices may include the following:  <ul style="list-style-type: none"> <li>▪ Construction contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers’ standards, and all stationary construction</li> </ul>	Applicant Construction Manager	Prior to issuance of Grading and Building Permits  Identify in approved grading and construction plans	Community Development Department – Planning and Building Divisions	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p>equipment shall be placed so that emitted noise is directed away from the noise sensitive use nearest the construction activity.</p> <ul style="list-style-type: none"> <li>▪ The construction contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and noise-sensitive receiver nearest to the construction activity.</li> <li>▪ The construction contractor shall limit haul truck deliveries to the same hours specified for construction equipment by Tustin City Code Article 4, Chapter 6, Section 4617. The contractor shall design delivery routes to minimize the exposure of sensitive land uses to delivery truck noise.</li> <li>▪ Construction activity within 50 feet of occupied noise sensitive uses shall reduce construction noise levels exceeding 85 dBA Leq at nearby sensitive land uses by one or more of the following methods to reduce noise to below 85 dBA Leq:               <ol style="list-style-type: none"> <li>1. Install temporary construction noise barriers within the line of site of occupied sensitive uses for the duration of construction activities that could generate noise exceeding 85 dBA Leq. The noise control barrier(s) must provide a solid face from top to bottom and shall:                   <ol style="list-style-type: none"> <li>a. Provide a minimum transmission loss of 20 dBA and be constructed with an acoustical blanket (e.g. vinyl acoustic curtains or quilted blankets) attached to the construction site perimeter fence or equivalent temporary fence posts;</li> <li>b. Be maintained and any damage promptly repaired. Gaps, holes, or weaknesses in the barrier or openings between the barrier and the ground shall be promptly repaired; and</li> <li>c. Be removed and the site appropriately restored upon the conclusion of the construction activity.</li> </ol> </li> </ol> </li> </ul>		Monitor during grading and construction		

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Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p>2. Install sound dampening mats or blankets to the engine compartments of heavy mobile equipment (e.g. graders, dozers, heavy trucks). The dampening materials must be capable of a 5-dBA minimum noise reduction, must be installed prior to the use of heavy mobile construction equipment, and must remain installed for the duration of the equipment use.</p>				
<p><b>MM 4.9-2: Construction Vibration.</b> The following measures shall be implemented by applicants for development within the Red Hill Avenue Specific Plan area to reduce construction vibration at nearby receptors:</p> <p>a. Avoid impact pile-driving where possible.</p> <p>b. In areas where project construction is anticipated to include pile drivers in close proximity to schools or historic structures, conduct site-specific vibration studies to determine the area of impact and to present appropriate vibration reduction techniques that may include the following:</p> <ul style="list-style-type: none"> <li>▪ Develop a vibration monitoring and construction contingency plan to identify structures where monitoring should be conducted, set up a vibration monitoring schedule, define structure-specific vibration limits, and address the need to conduct photo, elevation, and crack surveys to document before and after construction conditions.</li> <li>▪ Identify construction contingencies for when vibration levels approach the standards.</li> <li>▪ At a minimum, conduct vibration monitoring during pile-driving activities. Monitoring results may indicate the need for more or less intensive measurements.</li> <li>▪ When vibration levels approach standards, suspend construction and implement contingencies to either lower vibration levels or secure the affected structures.</li> </ul>	<p>Applicant Construction Manager</p>	<p>Prior to issuance of Grading and Building Permits</p> <p>Identify in approved grading and construction plans</p> <p>Monitor during grading and construction</p>	<p>Community Development Department – Building Division</p> <p>Public Works – Traffic Engineer</p>	

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<b>Standard Condition (SC) and Mitigation Measure (MM)</b>	<b>Responsibility for Implementation</b>	<b>Timing</b>	<b>Responsibility for Monitoring</b>	<b>Monitor (Signature; Date of Compliance)</b>
<ul style="list-style-type: none"> <li>Conduct a post-survey on any structures where either monitoring has indicated high levels or complaints of damage has been made. Make appropriate repairs or compensation where damage has occurred as a result of vibration.</li> </ul>				
<b>PUBLIC SERVICES</b>				
<b>SC 4.11-1:</b> Prior to the issuance of any grading or building permits for any development project under the Red Hill Avenue Specific Plan, the applicant shall submit a Fire Master Plan to the Orange County Fire Authority for review. Payment of fees and Fire Master Plan approval shall be obtained prior to the issuance of grading or building permits.	Applicant	Prior to issuance of Grading and Building Permits	Community Development Department – Building Division	
<b>SC 4.11-2:</b> Pursuant to Section 65995 of the California Government Code, prior to the issuance of building permits for any development projects under the Red Hill Avenue Specific Plan, the applicant shall pay developer fees to the Tustin Unified School District; payment of the adopted fees would provide full and complete mitigation of school impacts.	Applicant	Prior to the issuance of Building Permits	Community Development Department – Planning & Zoning Division	
<b>SC 4.11-3:</b> New development under the Red Hill Avenue Specific Plan shall be subject to the same General Obligation bond tax rate as already applied to other properties within the Tustin Unified School District for Measure G (approved in 2008) based upon assessed value of the residential and commercial uses.	Applicant	Prior to the issuance of Building Permits	Community Development Department – Planning & Zoning Division	
<b>RECREATION</b>				
<b>SC 4.12-1:</b> Prior to the approval of the final map for subdivisions under the Red Hill Avenue Specific Plan, applicants shall comply with the City of Tustin Subdivision Code (Article 9, Chapter 3, Part 3, Section 9331 of the Tustin City Code). Developers may dedicate land or pay a fee in lieu or a combination of both. The value of the amount of such fee shall be based upon the fair market value of the amount of land which would otherwise be required for dedication. Dedication of land may be required by the City for a condominium, stock cooperative, or community apartment project which exceeds 50 dwelling units.	Applicant	Condition of approval of Tentative Tract Map  Land dedication at final map recordation  In lieu fees prior to issuance of Building Permits	Community Development Department – Planning & Zoning Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<p><b>MM 4.12-1:</b> For residential projects not subject to City of Tustin Subdivision Code (Article 9, Chapter 3, Section 9331 of the Tustin City Code), prior to the issuance of building permits, applicants shall dedicate parkland or pay a park fee, on a per unit basis, reflecting the value of land required for park purposes. The amount of land which would otherwise be required for dedication shall be computed by multiplying the number of proposed dwelling units by 0.003 acre per person and 2.24 persons per dwelling unit. The parkland in-lieu fee shall be computed by multiplying the amount of land required for dedication by \$2,500,000 per acre.</p>	Applicant	Prior to the issuance of Building Permits	Community Development Department – Planning & Zoning Division	
<b>TRAFFIC AND TRANSPORTATION</b>				
<p><b>MM 4.13-1:</b> Red Hill Avenue at Interstate 5 Southbound Ramps: Re-stripe the eastbound approach (the off-ramp) to convert from a shared left-through lane and one dedicated right-turn lane to one dedicated left-turn lane and a shared left-through-right lane. This improvement would provide additional capacity for the heavy eastbound left-turn volume. With this improvement, the intersection would operate at Level of Service D or better during both peak hours. The California Department of Transportation’ (Caltrans) approval and cooperation would be required to implement this improvement.</p>	Applicant	Prior to issuance of Grading and Building Permits	Public Works – Traffic Engineer	
<b>UTILITIES AND SERVICE SYSTEMS</b>				
<p><b>SC 4.14-1:</b> Future development within the Specific Plan area would comply with Article 4, Chapter 10, Section 4952 of the Tustin City Code which seeks to reduce water consumption through (1) permanent water conservation requirements during non-shortage conditions and (2) four levels of water supply shortage response actions to be implemented within the City during times of declared water shortage. The program would prevent waste or unreasonable use of water; maximize the efficient use of water; and ensure a reliable and sustainable minimum supply of water for public health, safety, and welfare.</p>	Applicant	Prior to the issuance of Building Permits	Community Development Department – Building Division	

**Table 1-1. Mitigation Monitoring Requirements**

Standard Condition (SC) and Mitigation Measure (MM)	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature; Date of Compliance)
<b>SC 4.14-2:</b> Future development within the Specific Plan area would comply with Article 9, Chapter 7, Section 9704 of the Tustin City Code which establishes procedures and standards for the design, installation, and maintenance of water-efficient landscapes in conjunction with new construction projects within the City to promote the conservation and efficient use of water and to prevent the waste of available water resources.	Applicant	Prior to the issuance of Building Permits	Community Development Department – Building Division	
<b>SC 4.14-3:</b> Applicants shall prepare and obtain approval of a Construction and Demolition Waste Management Plan (CDWMD) for a project. The CWMP shall list the types and weights or volumes of solid waste materials expected to be generated from construction. The CDWMP shall include options to divert from landfill disposal, nonhazardous materials for reuse or recycling by a minimum of 65 percent of total weight or volume (or requirements in place at the time of project entitlement).	Applicant	Prior to issuance of Grading and Building Permits	Community Development Department – Building Division	