

DOWNTOWN COMMERCIAL CORE SPECIFIC PLAN



ADOPTED JULY 3, 2018 BY ORDINANCE NO. 1497

CITY OF TUSTIN, CA

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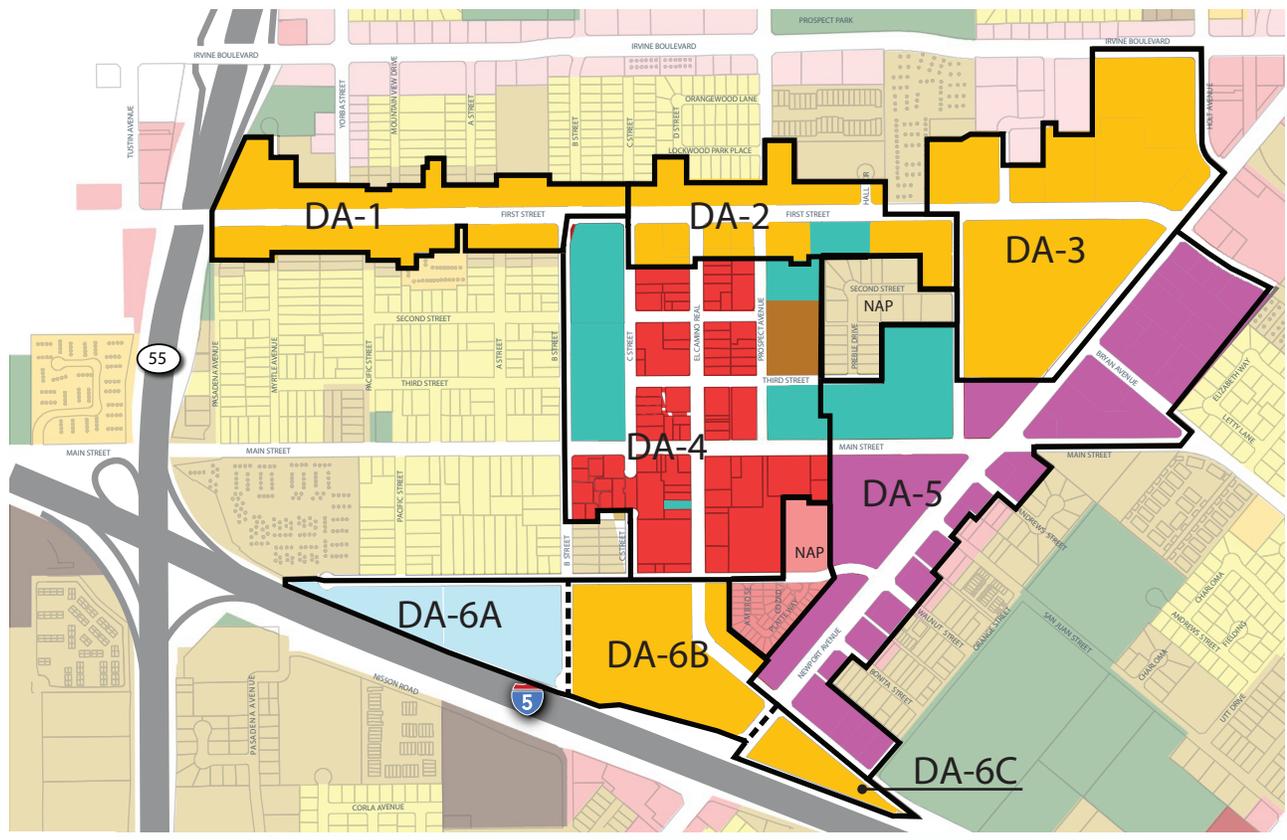


EXECUTIVE SUMMARY

Executive Summary

The Downtown Commercial Core Specific Plan (DCCSP) sets a planning and regulatory framework to preserve and enhance the planning area as a vital, pedestrian-friendly, and attractive commercial core in Tustin. The DCCSP seeks to attract more patrons to support and strengthen businesses in Old Town through shopping, dining, and entertainment opportunities to foster community interaction and pedestrian activity. To bring additional patrons to the planning area, the DCCSP also introduces the opportunity for mixed use residential development in select areas, which requires a discretionary approval process to ensure projects are high quality. The following pages provide a brief Executive Summary of the detailed topics addressed within Chapters 1 through 6 of the DCCSP. Figure i.1, Development Areas and Land Use Designations, provides an overview of the Downtown Commercial Core (DCC), including the six Development Areas (DAs).

Figure i.1 Development Areas (DAs) and Land Use Designations



- Legend**
- Downtown Mixed Use (DM)
 - Downtown Commercial (DC)
 - Mobile Home (MH)
 - Multi-Family (MF)
 - Old Town (OT)
 - Civic/Institutional (CI)
- NAP = Not a part



Vision Key Components

The vision for the DCCSP is to create a vibrant, cohesive, connected, livable, and memorable city core. The DCCSP vision and goals address complex needs within the DCC through a multi-faceted approach. The vision and goals have been distilled into a few key components that serve as the foundation for achieving the vision and goals. The key components are listed below.

- Promoting pedestrian-oriented commercial first floor development to invigorate the area and expand walkability;
- Transforming streets through future streetscape, roadway, pedestrian and bicycle-oriented improvements;
- Drawing more patrons to Old Town by embracing, preserving and promoting its unique historic character;
- Maintaining a commercial emphasis for the project area; and
- Introducing the opportunity for high-quality integrated residential mixed use and focused multi-family development.

Land Use Designations and Permitted Uses

The Land Use Plan delineates six land use designations to promote shopping, dining, entertainment, employment and housing in a pedestrian-friendly setting focused around the historic Old Town. The designations correspond with permitted and conditionally permitted uses, which are largely consistent with the uses allowed under the previous zoning districts prior to the adoption of the DCCSP. However, integrated commercial-residential mixed use (vertical and/or horizontal format) and multi-family residential are optional land uses new to the DCC and require approval of a discretionary entitlement.

There are two basic types of mixed use development: vertical and horizontal. Vertical mixed use occurs when the residential use is located above the ground floor commercial use. Horizontal mixed use occurs when a residential use is located on the first floor in a separate building from commercial use, but on the same parcel.

The six land use designations of the Land Use Plan are described below and illustrated in Figure i.1.

Downtown Mixed Use (DM) Land Use Designation - The DM designation is divided into five subcategories, DM(1) through DM(5), based on characteristics including location, proximity to existing uses/zones such as single-family residential, and parcel size, which impact allowable uses. The DM designation provides for the following uses:

- Retail, service, office
- Food service, medical, hospitality

- Auto service (in some subcategories)
- Residential mixed use (requires discretionary entitlement)

Old Town (OT) Land Use Designation - The primary objective of the OT designation is to promote preservation and increase the vibrancy of Old Town, while also introducing the opportunity for high-quality residential mixed use through a discretionary review process. The OT designation provides for the following uses:

- Retail, service, office
- Food service, medical, hospitality
- Residential mixed use (requires discretionary entitlement)
- Auto service prohibited

Downtown Commercial (DC) Land Use Designation - The primary intent for this designation located along Newport Avenue is to enhance and preserve the existing commercial thoroughfare. The DC land use designation provides for the following uses:

- Retail, service, office
- Food service, medical, hospitality
- Auto service
- Residential prohibited

Civic/Institutional (CI) Land Use Designation - The CI designation provides for the following uses:

- Civic, public, and institutional uses
- Residential prohibited

Multi-Family (MF) Land Use Designation - The MF designation provides for the following use:

- Multi-family residential use (requires discretionary entitlement)

Mobile Home (MH) Land Use Designation - The intent is for the existing mobile home park to remain as currently developed. The MH designation provides for the following use:

- Mobile homes within a mobile home park



Urban Design Plan

The Urban Design Plan, which is based on the community vision for the DCC, is summarized below according to each Development Area (DA).

- DA-1 and DA-2 -
 - Northern gateway to Old Town
 - Pedestrian and bicycle street transformation
 - Vehicular circulation changes and additional parking
 - Ground floor commercial
 - Residential mixed use (typically vertical format due to parcel size)
 - Small parcel size
- DA-3
 - Mixed use village
 - Residential mixed use in vertical format and shopping emphasis on principal streets
 - Horizontal residential mixed use on non-principal streets
 - Maintain larger parcels to unify development potential rather than splitting parcels and piecemealing development
 - Maximize relationship to transit and bicycle facilities
- DA-4
 - Preserve Old Town character with mixed use focus
 - Develop vacant parcels
 - Expand range of businesses
 - Extend walkability
 - Boost patronage by increasing residents through mixed use development
 - Create additional parking opportunities
 - Create parklets on El Camino Real to encourage gathering
 - Strengthen visual appeal and branding

- DA-5
 - Maintain commercial emphasis
 - Balance auto-centric nature with increased pedestrian and bicycle amenities on Main Street
 - Main Street improvements highlight Old Town
 - Vacate alley segments located within commercial parcels on Newport Avenue to consolidate parcels when feasible

- DA-6A
 - Continue transition to multi-family use
 - Design sensitivity to adjacent Cultural Resources District
 - Relationship to and compatibility with approved 140-unit condominium project

- DA-6B
 - Shopping and entertainment destination
 - Vertical residential mixed use on principal streets and horizontal mixed use on non-principal streets
 - Lead patrons from Newport Avenue to Old Town
 - Higher density urban form near freeway
 - Coordinated look on east and west sides of El Camino Real
 - Emphasize public gathering

- DA-6C
 - Multiple development options including small shopping center, hospitality and residential mixed use
 - Mixed use encouraged along El Camino Real due to lower traffic volumes
 - Higher density urban form adjacent to freeway
 - Design circulation to avoid impacts on Newport Avenue



Vehicular Circulation

The DCCSP includes conceptual street improvements to create “complete streets” in which the roadway design gives pedestrians and bicyclists greater emphasis and vehicles less dominance. The following provides a brief summary of the conceptual improvements.

Main Street - The conceptual Main Street improvements are key to economic development in Old Town because they will strengthen pedestrian connections between nearby businesses, the Tustin Branch Library and Tustin Civic Center, and Old Town. The conceptual plan provides reduced vehicular lanes, pedestrian and bicycle improvements, on-street parallel and diagonal parking, pedestrian bulb-outs, enhanced pedestrian crossings, and landscaping to visually support the roadway transformation.

First Street - The conceptual First Street improvements promote a pedestrian friendly corridor by reducing the number of traffic lanes and lane widths. The improvements also provide an on-street buffered bicycle lane, diagonal parking on both sides of the street, a landscaped median, and wider sidewalks.

Second and Third Streets - Pedestrian orientation in Old Town is enhanced by transforming Second and Third Streets from two-way to one-way streets flowing in opposite directions to form a couplet. Conceptual improvements include diagonal parking, wider sidewalks, and landscaped bulb-outs on one side.

Parking

The DCCSP establishes parking standards, but also provides that parcels located within the DCC may take advantage of alternative parking requirements through various provisions. The DCCSP:

- Establishes new provisions unique to the DCC;
- Incorporates existing provisions from the Tustin City Code (TCC) ; and
- Allows for innovative parking alternatives currently provided in the TCC.

Pedestrian Circulation

The DCCSP includes conceptual improvements to enhance pedestrian orientation in the DCC. These conceptual pedestrian improvements include:

- Widened Sidewalks – Main Street, First Street, Second Street, Third Street
- Decorative Sidewalk Paving – Main Street, El Camino Real
- Bulb-Outs – Main Street, First Street, Second Street, Third Street

- Crosswalk with Enhanced Paving- Main Street
- Pedestrian Gathering Areas
- Increasing Sidewalk Widths
- Decorative Crosswalks
- Accessible Pedestrian Signals and Flashing Light Crosswalks (where appropriate)

Bicycle Improvements

Conceptual bicycle lane improvements include:

- Main Street (between Prospect Avenue and Newport Avenue) -
 - On-street bicycle lane (Class 2) on the north side
 - Off-road bicycle lane (Class 1) integrated with the sidewalk on the south side.
- First Street (between the 55 Freeway and Newport Avenue) -
 - Striped on-street bike lane with a striped buffer (Class 2) on both sides of the street
- “Sharrows” (identified by symbols painted on the roadway pavement to indicate that motor vehicles and bicycles are to share the same travel lane) (Class 3) -
 - Main Street from B Street to Prospect Avenue
 - El Camino Real from First Street to Newport Avenue
 - B Street between First Street and Sixth Street
 - Prospect Avenue between First Street and Main Street
 - Centennial Way between First Street and Main Street
 - Sixth Street between B Street and Newport Avenue
- Bike racks are encouraged at regular intervals within the public right-of-way and within private development



Street Trees

Much of the street tree canopy in the DCC is provided by the Indian Laurel Fig (*Ficus microcarpa*); however, Ficus trees are characterized by invasive roots, which due to the large size of the trees in the DCC, sometimes negatively affect public and private infrastructure and require increased maintenance. The DCCSP includes a program to gradually replace the existing Ficus trees in the DCC with one tree species. Suggestions for an appropriate tree species are included in the Street Tree Palette. The aim of the Ficus replacement is to provide an attractive streetscape in the DCC, require less tree maintenance, and conserve water through a drought-tolerant species.

Ficus are the only street trees along many street segments in the DCC. Therefore, it is important that the replacement be done in a manner that is least disruptive to the character of the street. The DCCSP stipulates replacement of every-other Ficus tree with the selected tree species following a systematic, phased schedule that cycles back to replace the remaining trees. The DCCSP requires replacement of Ficus with 48-inch box sized trees or larger.

The conceptual street improvements provided within the DCCSP include street tree and landscape improvements to further beautify the streetscape within the DCC.

Monumentation, Public Art, and Street Furniture

The conceptual Main Street improvements include installation of a street arch at the eastern entry into Old Town from Newport Avenue to draw attention to Old Town. Similarly, an entry arch or other major additional Old Town gateway signage is envisioned at the northern entry into Old Town at El Camino Real/First Street and the southern entry on El Camino Real at Newport Avenue and/or Sixth Street with new development when it occurs. In addition, a coordinated program of directional signs is needed to assist in branding the area.

The DCCSP encourages public art, especially the implementation of the Tustin Pioneers Recognition Program, and installation of coordinated street furniture that reflects the desired character for the DCC.

Parklets

To facilitate community interaction, generate activity, and create a memorable amenity that enhances the Old Town street scene, the City has identified conceptual locations within the public right-of-way along El Camino Real for public parklets, which consist of bulb-outs with enhanced paving, landscaping and low walls to create public gathering areas adjacent to the sidewalk and to provide outdoor sidewalk seating areas for businesses such as restaurants. Two additional parklets are conceptually located within bulb-outs on Main Street. Further expansion of the network of parklets to other areas within the DCC is encouraged.

Residential Entitlement Process

Residential development within the project area is not a permitted or conditionally permitted use. Residential mixed use and multi-family residential use require approval of a discretionary Residential Allocation Reservation (RAR). The RAR review process consists of two phases, with preliminary distribution of units in the first phase and final allocation of units upon approval of the proposed project in the second phase. The City's Residential Allocation Bank establishes a maximum number of 887 new dwelling units that may be developed within the DCC area, which are allocated among the DAs by the DCCSP. Only high-quality mixed use or multi-family residential projects will receive an allocation of units and approval of a RAR.

The DCCSP allows for transfer of allocated residential units from one DA to another through approval of an RAR. The Community Development Director may approve a transfer of units up to 25 percent of the original DA unit allocation and shall determine the donating DA(s). Transfers greater than 25 percent shall be reviewed and acted upon by the Planning Commission, which will take into consideration the donating DA(s) recommended by the Community Development Director. Transferred units shall be deducted from the donating DA(s) so that the maximum number of new dwelling units within the DCC shall not exceed 887 units.

Development Standards

The DCCSP Development Standards for the individual DAs are generally similar to the standards under the current respective zoning designations in the TCC, including building heights, parking space requirements, and the amount of required landscape. Required building setbacks under the DCCSP are more flexible to encourage siting of buildings near the street edge to increase pedestrian orientation. New development along Newport Avenue is encouraged to establish buildings near the street to create a defined street edge, while screening parking from the street through building placement.

Design Criteria

The Commercial Design Criteria and Residential Design Criteria address building and site design using text and photographic examples to ensure high quality design. The Design Criteria address topics including architectural style, building design, provisions for specific DAs, design of mixed use/multi-family residential, signage, parking, landscaping, and lighting. The Design Criteria draw from the infill provisions of the Cultural Resources District Design Guidelines.

The criteria for architectural style require historic styles to be used within Old Town. The DAs located further from Old Town, including DA-5 along Newport Avenue, allow for contemporary interpretations of historic styles and/or the mixing of contemporary materials such as glass with historic materials including brick.



Mixed Use Design Criteria and Development Standards are new and unique to the DCC. Therefore, a summary of the major mixed use provisions is provided below.

- a. The format of mixed use may be vertical, horizontal, or a combination of both.
- b. Mixed use shall require a significant commercial component integrated with the residential component. Mixed use commercial space shall span the building width of principal street frontages and be a minimum of 45 feet deep.
- c. For mixed use adjacent to principal streets, the ground floor shall be limited to commercial uses only. On principal streets, office uses are encouraged to be located on upper floors. On non-principal streets, office uses may be located on the ground floor. New development may include office uses at street level if the buildings or tenant spaces are designed with display windows and other architectural details similar to non-office commercial uses.
- d. For mixed use commercial components adjacent to principal streets, the ground floor shall feature prominent display windows and pedestrian oriented architecture.
- e. In vertical mixed use the residential component shall be permitted only on upper floors.
- f. In horizontal mixed use the residences shall be allowed on the ground floor, provided they do not front on a principal street. Horizontal mixed use may feature ground floor entrances to individual units, such as stoops.
- g. Public or semi-public spaces and landscaping are encouraged to interact with the streetscape. On-site parking shall not be located between the street and the front elevation of a mixed use building.
- h. Vertical and horizontal changes in plane are encouraged and detailed architecture is required on all four elevations.
- i. The residential component of mixed use shall provide the required number of parking spaces entirely onsite, or on an immediately adjacent site if an Off-Site Parking Exception is approved in conjunction with the project entitlement.
- j. The residential component of mixed use shall comply with the private and common open space requirements per unit, including enclosed storage space.
- k. Residential landscape requirements shall be as specified in the Residential Design Criteria and the TCC.
- l. Signage shall be as provided in the Commercial Design Criteria and the TCC. DA-4 (Old Town) also permits pedestrian sidewalk signs.

Administration and Implementation

Administration provisions within the DCCSP address development review processes including RAR entitlements. The limitations and procedures associated with Specific Plan adjustments, modifications and amendments are also discussed. The Required Findings applicable to all projects within the DCC are presented, including additional Required Findings for residential mixed use or multi-family residential, residential unit transfers, and Exceptions. Tables at the end of the chapter identify implementation actions and potential funding sources.

Conclusion

This Executive Summary provides a general overview of the DCCSP. See Chapter 1, Section 1.7, How to Use this Plan, for guidance focused to particular readers, such as those with an interest in developing land or expanding/starting a business in the DCC.



1

INTRODUCTION

Ch. 1 Introduction

1.1 Overview

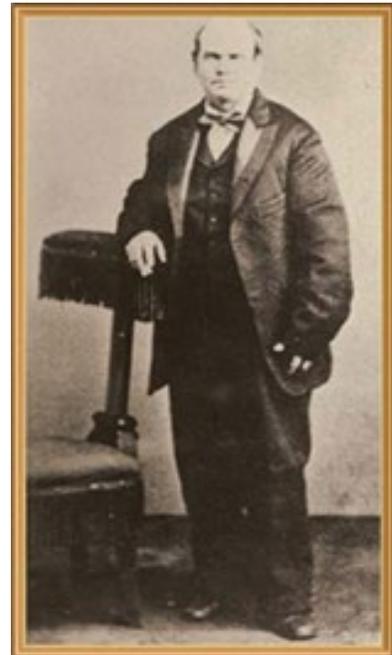
The Downtown Commercial Core Specific Plan (DCCSP) sets a planning and regulatory framework to preserve and enhance the planning area as a vital, pedestrian-friendly, and attractive commercial core in Tustin. The City of Tustin was originally founded in the 1870s by Columbus Tustin in what is today the heart of the planning area, “Old Town”. The DCCSP seeks to attract more patrons to support and strengthen businesses in Old Town through shopping, dining, and entertainment opportunities to foster community interaction and pedestrian activity. The DCCSP includes strategies to transform the auto-centric streets and development patterns encircling the historic core by narrowing select streets to allow space for integration of pedestrian and bicycle improvements.

The DCCSP brings back a historic building pattern that mixes living options with commercial use by providing a discretionary process for consideration of integrated mixed use and limited multi-family residential development.

The DCCSP is the culmination of a comprehensive community outreach, planning, and design effort. It reflects the vision and goals of various stakeholders, including residents, businesses and property owners, the technical advisory group, the Planning Commission, and City Council. The DCCSP contains implementation mechanisms, including development regulations, design criteria, and strategies to preserve the historic charm of Old Town and spur transformation of surrounding areas into an attractive, lively, and economically healthy commercial and mixed use core where people can live, work, shop, dine, and relax in a unique pedestrian friendly atmosphere.

INSIDE THIS CHAPTER

- 1.1 Overview
- 1.2 Planning Area and Context
- 1.3 Vision and Goals
- 1.4 Specific Plan Purpose
- 1.5 Planning Process
- 1.6 Specific Plan Format
- 1.7 How to Use This Plan
- 1.8 Relationship to Other Documents



City Founder Columbus Tustin

The original Tustin Garage building, now The Black Marlin Restaurant, still stands at the corner of El Camino Real and Sixth Street.



1.2 Planning Area and Context

The city of Tustin is located in central Orange County, California, 12 miles inland from the Pacific Ocean. The DCCSP planning area consists of approximately 220 acres located in the northern portion of the city. The jurisdictions surrounding the planning area include the City of Tustin immediately to the north, south, east and west, unincorporated land within the County of Orange (North Tustin) further to the northeast, and the City of Santa Ana further to the northwest and southwest, as shown in Figure 1.1, Specific Plan Context.

As illustrated in Figure 1.2, the planning area, referred to as the Downtown Commercial Core (DCC), is centered around the intersection of Main Street and El Camino Real in Old Town. The DCC boundaries extend to the parcels on the north side of First Street and the east side of Newport Avenue, south to Interstate 5 (I-5), and west along First Street to State Route 55 (55 Freeway).

El Camino Real, the famous route marked by recognizable historic bells, forms the north-south backbone of Old Town. Many historic residential and commercial properties dating from the late 1800s through the post-World War II period are clustered in this area. On the perimeters of Old Town, the DCC includes the civic heart of Tustin defined by the Tustin Civic Center and the Tustin Branch Library on the east and Peppertree Park, the Tustin Area Senior Center, and Tustin Unified School District administration offices on the west.

The western and central portions of First Street feature an eclectic mix of commercial and office buildings situated relatively close to the street on small lots. In contrast, the auto-oriented commercial development along Newport Avenue, the eastern segment of First Street, and the southern portion of the DCC is primarily characterized by buildings oriented around large parking lots, with little direct relationship to the street.

Within the interior of the DCC boundaries, the planning area excludes two existing residential neighborhoods: the first located along Prebble Drive/E. Second Street and the second located along Ambrose Lane/Platt Way.

Residential neighborhoods surround the DCC planning area to the north, east, and south beyond I-5. An historic residential neighborhood lies west of B Street between the planning area and the 55 Freeway.



Old Town Tustin forms the heart of the Specific Plan's character and identity.

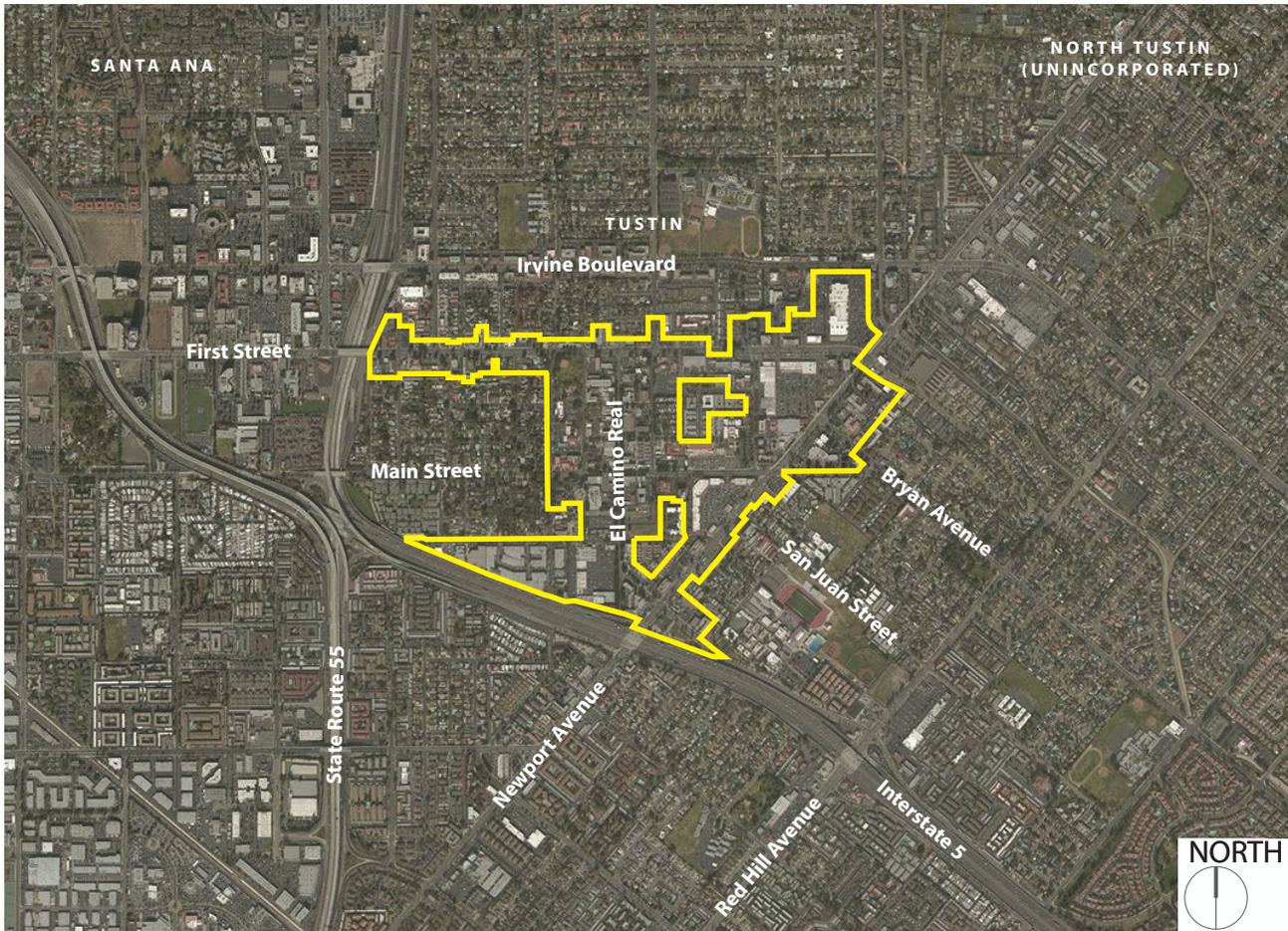


Main Street serves as the east-west axis of the planning area.



The historic El Camino Real traverses from north to south through the planning area.

Figure 1.1 Specific Plan Context



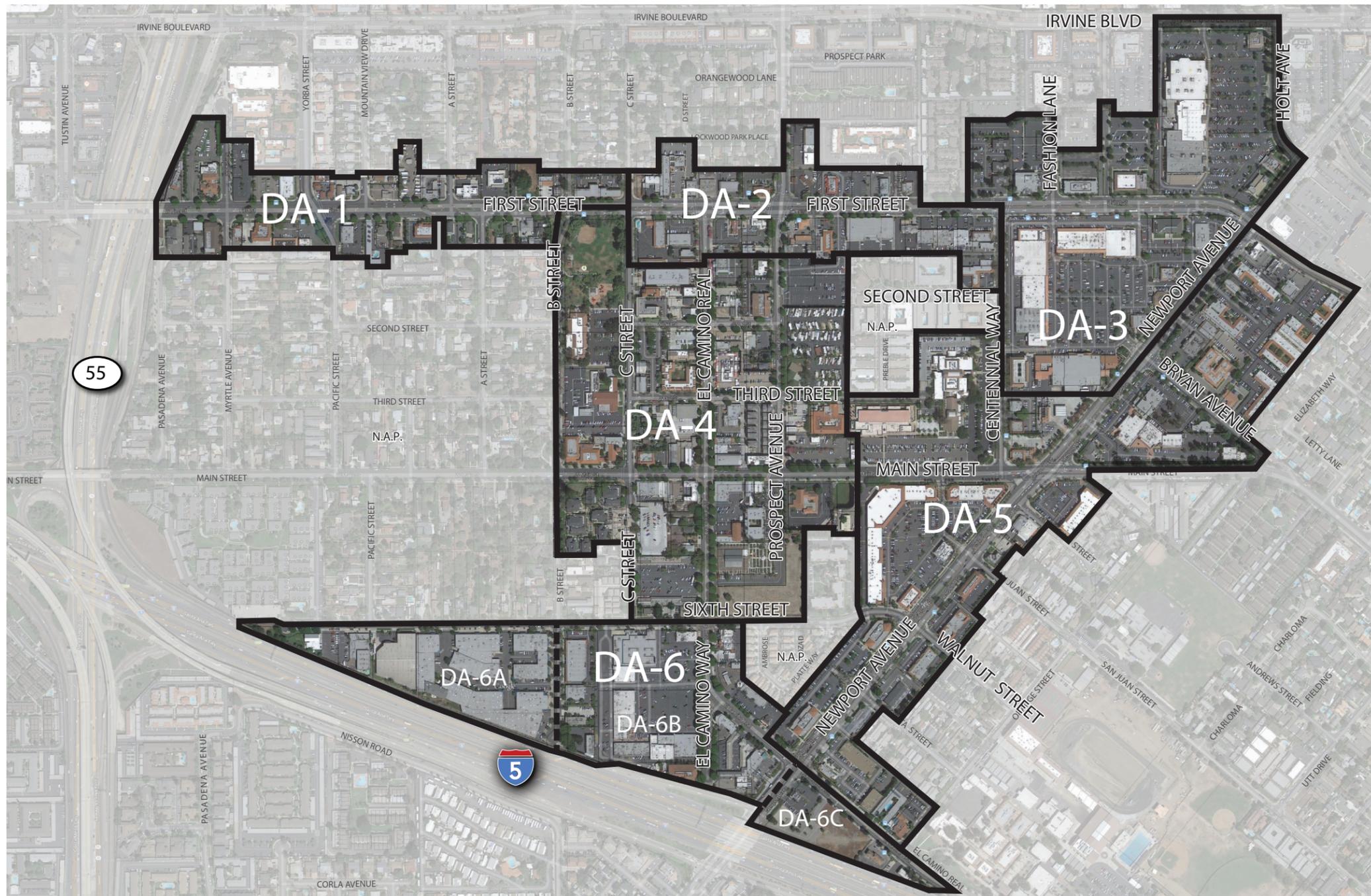
Today the Artz building on Main Street is home to a popular restaurant.



Tustin High shows its community spirit.



The original Tustin Food Center building located at the southeast corner of Main Street and El Camino Real is now known as Cox's Market Plaza.



Legend

DA-1	Development Area 1	DA-4	Development Area 4	NAP	Not A Part
DA-2	Development Area 2	DA-5	Development Area 5		
DA-3	Development Area 3	DA-6	Development Area 6		

Figure 1.2 Downtown Commercial Core

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For additional details regarding the existing conditions within the planning area, refer to the Existing Conditions Report in Appendix B.

1.3 Vision and Goals

Vision

The following vision was developed through a comprehensive community outreach process and expresses the community's aspirations for the DCC, as discussed in Section 1.5.

The DCC of the future is characterized by the following:

- 1. Enhanced Old Town Character.** Old Town charm has been preserved and cultivated to create a cohesive design character that enhances the community's sense of place.
- 2. Economic Development.** An appropriate mix, intensity, and orientation of land uses improves the business environment, reinvigorates the area, and makes the downtown core a destination for new residents, the larger community, workers, and visitors.
- 3. Economic Diversity.** Commercial areas (particularly along El Camino Real, Main Street, and First Street) have been revitalized and the economic vitality of the planning area strengthened with lively businesses that provide the essential array of amenities needed and desired by residents, the larger community, workers, and visitors.
- 4. Complementary Development Areas.** The design of each Development Area reflects its distinct character, while complementing Old Town.
- 5. Convenient Access.** Connections between the DCC, adjacent residential neighborhoods, existing and new employment, and commercial centers are improved via a more inclusive circulation network for pedestrians, bicyclists, transit users, and drivers.
- 6. Community Interaction.** Integrated public spaces provide opportunities for social events, interaction, and strengthening the area's sense of community.

Community Visioning Quotes

"A successful downtown means...a place where people want to establish their business and become a part of the community."

"A successful downtown means... community, economy, and culture."

"What I like most about downtown... the community feel and small-town vibe - tranquil, quaint, and historic".

"What I like most about downtown... events including Farmer's Market, Tiller Days, Art Walk, Chili Cook-off, and light shows."

7. Aesthetic Enhancements. Attractive streetscape treatments, building design, gateway elements, and wayfinding signage contribute to the retail vibrancy, social vitality, and distinct character.

Goals

Building on the fundamental ideas gathered during the visioning process, the following goal statements were developed for the DCC. The DCCSP goals consist of specific statements of what the Tustin stakeholders hope to achieve to fulfill the identified vision.

- 1. Market-Supported Development.** Encourage new development by providing for land uses that are economically viable for developers and serve residents, workers, and visitors.
- 2. Increased Patronage.** Provide for and encourage land uses and development that attracts and sustains patronage to support DCC businesses, particularly in Old Town.
- 3. Mixed Use.** Introduce high quality mixed use development with pedestrian oriented first floor commercial use to create a synergistic, desirable, livable, walkable, and attractive area.
- 4. Promotion of Old Town.** Enhance the planning area's visibility in the region through monumentation signage and area branding that embraces its unique historic character.
- 5. Quality Development.** Require quality development that brings integrated residential mixed use, focused multi-family development, and a revitalized Newport Avenue that maintains its commercial focus.
- 6. Feasible Implementation.** Create an action-oriented, implementable plan that directs tangible change, including City-initiated street improvements that foster pedestrian orientation.

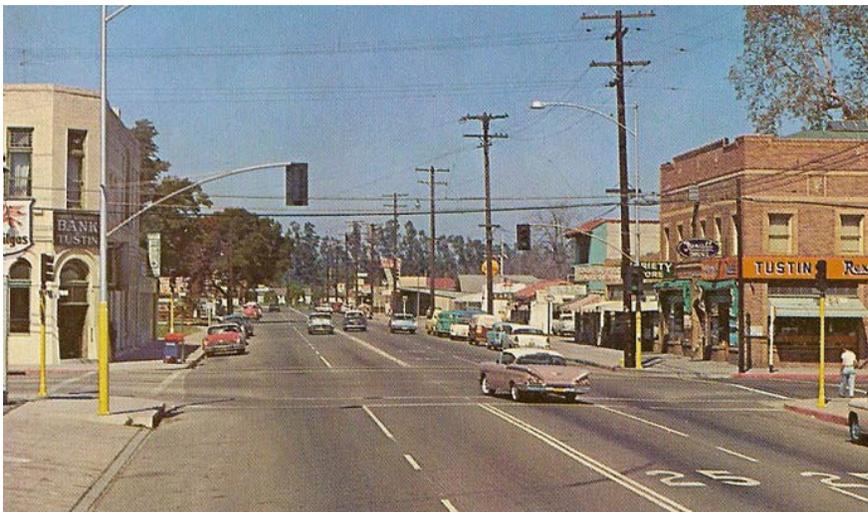
The DCCSP vision and goals address complex needs within the DCC through a multi-faceted approach. The vision and goals have been distilled into a few key components that serve as the foundation for achieving the vision and goals. Where text within the DCCSP promotes implementation of these key components, it is identified by green boxes placed in the document margin. The key components are listed below.

- Promoting pedestrian-oriented commercial first floor development to expand walkability;
- Introducing high-quality residential mixed use and focused multi-family development;
- Transforming streets through pedestrian-oriented improvements;
- Drawing more patrons to Old Town by embracing and preserving its unique historic character; and
- Maintaining a commercial focus for the planning area.

1.4 Specific Plan Purpose

Specific Plans are utilized to plan and guide the development of special areas such as the DCC that are characterized by unique location, features, and land uses. The DCCSP is a policy and regulatory document guiding future projects to ensure sensitivity to Old Town and spearhead long-term success of the DCC as a walkable and bikeable commercial core with integrated mixed use and focused multi-family development. It is to be used by City staff, developers, design professionals, businesses, property owners, and residents to ensure high-quality, context-sensitive development.

California Government Code Section 65450 establishes the authority for cities to adopt specific plans either by resolution or ordinance. The



A mid-century view of El Camino Real at Main Street.



Engaging the public in the creation of the Specific Plan ensures that the people of Tustin have direct input into the future of their community.

City of Tustin adopted the DCCSP by ordinance after public hearings before the Planning Commission and City Council. New development and improvements within the DCC, including development plans, site plans, conditional use permits, residential allocation reservations, and building plans, must be consistent with the DCCSP.

California law requires that a specific plan be consistent with the general plan of the adopting locality. A General Plan Amendment was processed concurrently with the DCCSP to ensure consistency. Appendix A contains the General Plan Consistency Analysis.

Pursuant to California Environmental Quality Act (CEQA) guidelines, the City of Tustin prepared a Program Environmental Impact Report (EIR), available as a separate document. As part of the approval process for the DCCSP and the General Plan Amendment, the EIR was certified by the City Council concurrently. A key objective of the DCCSP and EIR is to reduce the need for future detailed planning and environmental review procedures for development within the planning area. The DCCSP and accompanying EIR provide the necessary regulations, criteria and environmental documentation so that future renovation and development projects in conformance with the DCCSP may proceed without requiring new or additional environmental documentation.

1.5 Planning Process

The DCCSP is the culmination of a robust community-based process. This process actively engaged local stakeholders, business and property owners, neighborhood representatives, elected and appointed officials, and other members of the public.

The planning process began in fall 2014 and included three public community workshops at City Hall. To ensure a comprehensive approach, the City initiated an iterative process with opportunities for stakeholders to review key information, share opinions, and refine emerging DCCSP work products.

Throughout the planning process, a broad range of stakeholders participated in presentations, mapping exercises, and discussions about land use, design, mobility, and economic development. Participants voiced expectations for the future of the DCC; discussed the challenges,



issues, and opportunities of the area; and expressed ideas for enhancing the planning area. This information was documented through a variety of methods and tools to engage and inform the community, including workshops, stakeholder interviews, a technical advisory committee, and public meetings and hearings, as well as press releases.

Early in the process, an existing conditions assessment was prepared to inform the community and stakeholder groups about economic and market conditions, land use and urban design, circulation and parking, and infrastructure. Drawing from the existing conditions research, the first community workshop included a SWOT (strengths, weaknesses, opportunities, and threats) analysis to generate transformation strategies and define a vision for the future of the DCC. This work formed the basis for the DCCSP and can be found in Appendix B, Existing Conditions Report.

The feedback from the community engagement process ultimately created the vision and goals for the DCC. Detailed information from the community meetings is included in Appendix C, Summary of Community Workshops.

Various studies and surveys guided the development of the DCCSP, including a Catalytic Sites Study (Appendix D) and an Economic Analysis (Appendix E). The DCCSP also draws upon previous planning processes and documents prepared by the City over the years to help build the foundation for this document, including the Cultural Resources District Commercial Design Guidelines (2014) (Appendix F), the Cultural Resources District Residential Design Guidelines (2012), and the First Street Specific Plan (2012).



Tustin's early days centered around growing citrus.



One of the earliest Tustin buildings still stands.

1.6 Specific Plan Format

The DCCSP is organized into the following six chapters and appendices:

Chapter 1: Introduction

This chapter provides an overview of the planning area and context, presents the vision and goals for the DCC, describes the planning process, outlines the format and how to use the DCCSP, and discusses its relationship to other documents.

Chapter 2: Development Plan

This chapter presents the DCCSP land use plan and land use designations,



The original Tustin Hardware building is now home to Mrs. B's Consignments.

describes the urban design vision for the six Development Areas (DAs), and addresses public street and streetscape improvements that contribute to implementation of the DCCSP.

Chapter 3: Commercial Permitted Uses and Development Standards

This chapter identifies allowable non-residential land uses under each of the land use designations. It also provides commercial development regulations for private property within each DA, including building heights, setbacks, parking standards, and special use restrictions.

Chapter 4: Commercial Design Criteria

This chapter provides commercial design criteria to promote high-quality projects within the DCC. The provisions address both general criteria including architectural style, building mass and articulation, architectural details, and colors/materials as well as design criteria specific to each DA. The design criteria are supported by numerous photographic examples.

Chapter 5: Residential Provisions

This chapter presents regulations for the integration of mixed use and multi-family residential use into the DCC and provides residential development standards and design criteria.

Chapter 6: Administration and Implementation Plan

This chapter specifies the procedures for administering and amending the DCCSP, development/entitlement review, and required findings necessary to approve future projects. The chapter also outlines the major actions necessary to incrementally implement the vision, strategies, and concepts of the DCCSP through improvements and programs. It includes matrices identifying specific actions and potential funding sources.

Appendices

The appendices contain the following documents supporting the DCCSP:

Appendix A, General Plan Consistency Analysis

Appendix B, Existing Conditions Report

Appendix C, Summary of Community Workshops



Appendix D, Catalytic Sites Study

Appendix E, Economic Analysis

Appendix F, Cultural Resources District Commercial Design Guidelines

1.7 How to Use This Plan

The DCCSP is written to inform a wide variety of users: residents, property owners, merchants, architects, designers, building contractors, City staff, decision-makers, investors, developers, and other interested organizations and persons in the community.

The following sections identify and group key chapters within the DCCSP document based on the interest and need of the reader:

Quick Summary of the DCCSP:

Review the Executive Summary to become familiar with the overall goals and provisions of the DCCSP.

Overview of Conceptual Area-Wide Improvements:

For descriptions of the conceptual physical enhancements to the DCC built environment, including the urban design plan and conceptual improvements to streets, bicycle lanes, sidewalks, and streetscape, see Chapter 2.

Interest in Site-Specific Commercial Development:

If you are interested in improving a commercial building or developing a site with commercial use, or are a professional assisting with this endeavor, review Chapters 3 and 4, which provide allowable non-residential uses, commercial site development standards and commercial design criteria. Next, schedule a pre-application consultation with the City Community Development Department staff.

Developing Mixed Use or Multi-Family Residential Use:

If you would like to develop integrated commercial-residential mixed use on a vacant lot or renovate an existing commercial building to include mixed use, see Chapter 5 for an overview of mixed use within the DCC, residential development standards, and residential design criteria. If you would like to

develop multi-family residential use (within DA-6A), also refer to Chapter 5. In addition, the required discretionary residential entitlement process is described in Chapter 6.

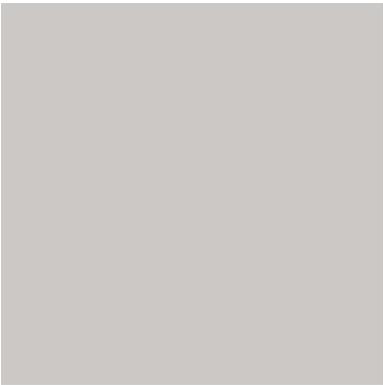
Desire to Expand or Start a Business:

If you are interested in enhancing an existing business or starting a new business within an existing building in the DCC, review Chapter 3, which specifies permitted non-residential land uses under the DCCSP. Then, visit the City Community Development Department to discuss the next steps.

1.8 Relationship to Other Documents

The DCCSP serves as the zoning ordinance for the DCC. The DCCSP is related to other City documents as follows:

- General Plan (GP) – The DCCSP implements the GP land use designation of Specific Plan and the relevant policies of the GP.
- Tustin City Code (TCC) – The DCCSP supersedes the TCC within the DCC. Where the DCCSP is silent or a section reference is provided, the provisions of the TCC shall apply.
- Cultural Resources District Commercial or Residential Design Guidelines (CRD-CDG or CRD- RDG) – The DCCSP is consistent with the provisions of the CRD-CDG for infill development. For projects located in Old Town (DA-4) or involving identified historic resources more than 50 years old in any DA, including preservation and rehabilitation, additions, or adaptive reuse, the CRD-CDG and CRD-RDG remain in force and apply.
- First Street Specific Plan (FSSP) – The DCCSP incorporates the provisions of the FSSP that are relevant to the First Street corridor, recognizing that some goals and circumstances have changed since the FSSP's original adoption, making portions no longer applicable. (Original FSSP adopted by City Council Resolution No. 85-126 and Council Ordinance No. 961 on December 16, 1985. Specific Plan Amendment 2012-001 adopted by City Council Ordinance No. 1414 on November 20, 2012.) The FSSP is repealed upon adoption of the DCCSP.



2

DEVELOPMENT PLAN

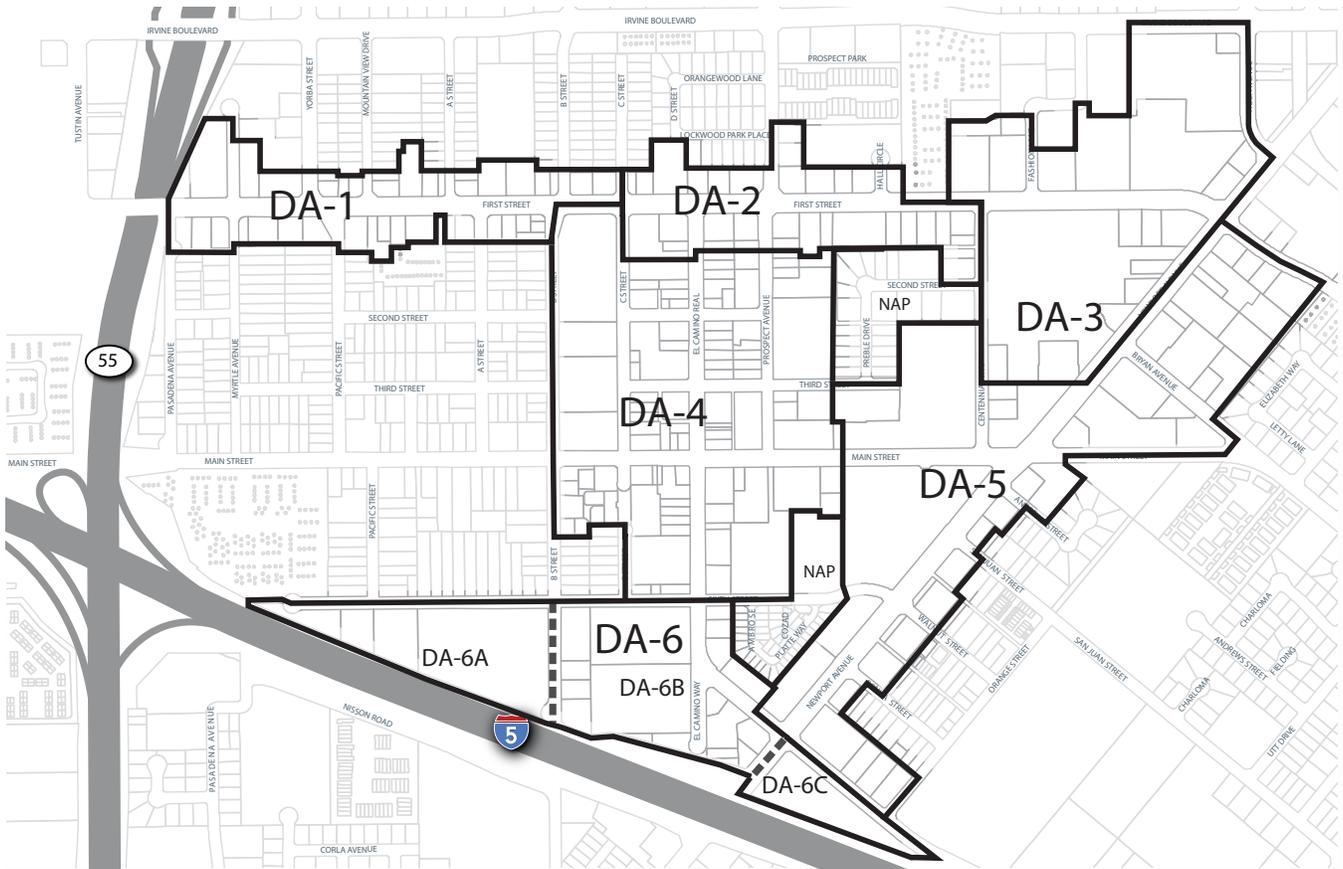
Ch. 2 Development Plan

The Development Plan directs key improvements within the DCC. Implementing the goals identified in Chapter 1, Introduction, this chapter establishes, defines, and describes the Land Use Plan, Urban Design Plan, Circulation and Parking Plan, Streetscape and Public Open Space Plan, and Infrastructure, Utilities and Servicing Plan for the DCC.

As illustrated in Figure 2.1, Development Areas, the DCC is divided into six Development Areas (DAs), which generally reflect differences in the character of the built environment. DA-6 is further divided into subareas A, B, and C. The DAs are used to present and describe various provisions and components of the DCCSP.

- INSIDE THIS CHAPTER**
- 2.1 Land Use Plan
 - 2.2 Urban Design Plan
 - 2.3 Circulation and Parking Plan
 - 2.4 Streetscape and Public Open Space Plan
 - 2.5 Infrastructure, Utilities and Servicing Plan

Figure 2.1 Development Areas



Legend

- Development Area
- Freeways
- Parcels

2.1 Land Use Plan

The vision for the DCCSP is to create a vibrant, cohesive, connected, livable, and memorable city core. The Land Use Plan delineates a range of land use designations promoting shopping, dining, entertainment, and employment in a pedestrian-friendly setting focused around the historic Old Town. Integrated residential mixed use and multi-family residential are optional land uses if approved through a discretionary entitlement process, as discussed in Chapter 6, Administration and Implementation Plan.

2.1.1 Land Use Designations

The six land use designations of the Land Use Plan are described below and illustrated in Figure 2.2 , Land Use Plan. For purposes of the DCCSP, “commercial” shall mean a non-residential use such as retail, service, restaurant, entertainment, and hospitality. Office use is generally considered a commercial use within the DCCSP, except where provisions aim to create pedestrian activity. In that case, office use shall be considered distinct from commercial use and will be noted as such in the text. “Mixed use” shall mean a building or development with residential use integrated with commercial use.

2.1.1.1 Downtown Mixed Use (DM) Land Use Designation

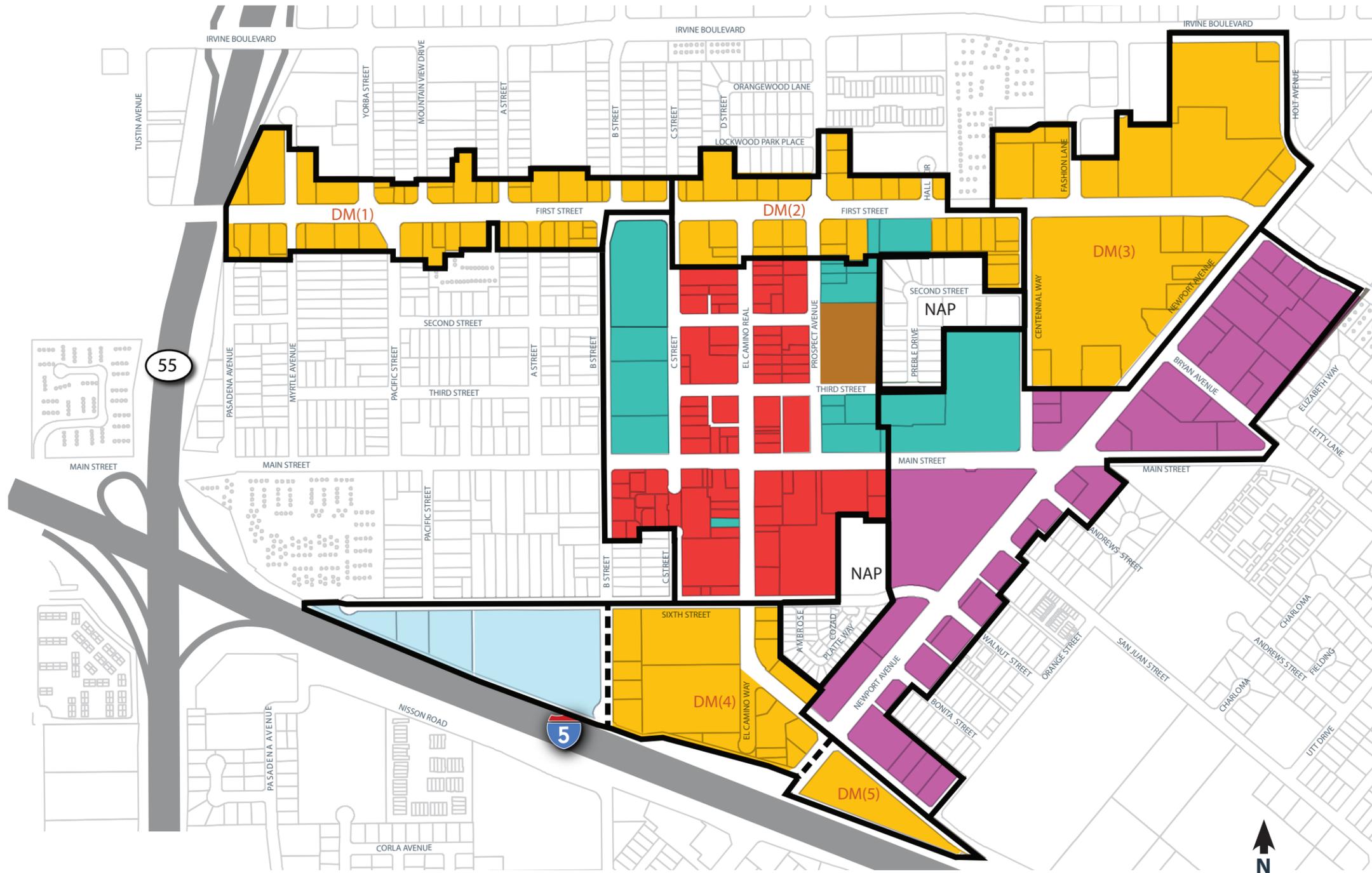
The DM land use designation applies to the parcels located on both sides of First Street from the 55 Freeway on the west to Newport Avenue on the east, generally on both sides of El Camino Real south of Sixth Street, and on the southwest side of El Camino Real east of Newport Avenue. The DM designation is divided into five subcategories, DM(1) through DM(5), based on characteristics including location, proximity to existing zones such as single-family residential, and parcel size, which impact allowable uses. The DM designation includes the greatest flexibility of uses, providing for retail, service, office, food service, medical, hospitality, and auto service uses (in some subcategories). Residential use in an integrated mixed use format (vertical or horizontal) is an option subject to the approval of a discretionary entitlement (refer to Section 2.1.2 for a description of vertical and horizontal formats, Chapter 5, Residential Provisions, and Chapter 6, Administration and Implementation Plan). Along principal commercial streets (refer to Figure 2.3 for location criteria), the first floor is typically reserved for commercial use to promote pedestrian activity. If office uses



KEY COMPONENT

Mixed Use

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Legend

- Downtown Mixed Use (DM) including subareas DM(1) to DM(5)
- Downtown Commercial (DC)
- Mobile Home (MH)
- Multi-Family (MF)
- Old Town (OT)
- Civic/Institutional (CI)
- NAP = Not a part

Figure 2.2 Land Use Plan

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are located on the first floor, the building must nevertheless maintain a pedestrian oriented design with display windows and architectural enhancements near the sidewalk, as further described in Chapter 4, Commercial Design Criteria.

2.1.1.2 Old Town (OT) Land Use Designation

The OT land use designation applies to the blocks located primarily between C Street on the west and Prospect Avenue on the east, from below the First Street frontage on the north to Sixth Street on the south. The OT designation provides for retail, service, office, food service, medical, and hospitality uses. Automotive uses are not allowed within Old Town to preserve its historic character. Integrated mixed use residential buildings are an option provided a discretionary residential entitlement is approved, as addressed in Chapter 6, Administration and Implementation Plan. For vertical mixed use within the OT designation, commercial uses are required on the first floor and residential use may only be located on upper floors. If office uses are located on the first floor, the building must nevertheless maintain a pedestrian oriented design with display windows and architectural detail near the sidewalk, as further described in Chapter 4, Commercial Design Criteria. Horizontal mixed use is limited to non-principal streets (refer to Figure 2.3).

KEY COMPONENT
**Preserve
Old Town**

KEY COMPONENT
Mixed Use

2.1.1.3 Downtown Commercial (DC) Land Use Designation

The DC land use designation applies to properties on both sides of Newport Avenue from First Street on the north to El Camino Real on the south (except for Larwin Square, which abuts First Street and is designated DM).

The DC land use designation provides for retail, service, office, food service, medical, hospitality, and auto service uses. Residential uses are not allowed within the DC land use designation. The primary intent for Newport Avenue is to remain a commercial thoroughfare under the DCCSP.

KEY COMPONENT
**Newport
Avenue
Commercial
Emphasis**

2.1.1.4 Civic/Institutional (CI) Land Use Designation

The CI land use designation applies to Peppertree Park, the Tustin Area Senior Center, the Tustin Unified School District administration offices, Tustin Presbyterian Church, and Tustin Community Preschool, which are located on the west side of C Street between First Street and Main Street. The CI designation also applies to the City of Tustin Main Street

Water Facility, Tustin Branch Library, and Tustin Civic Center, which are located on Main Street between Prospect Avenue and Centennial Way. This designation also applies to the United States Post Office facility that fronts on First Street with parking lot access from Prospect Avenue and the Tustin Hacienda Silverado Memory Care facility located on Third Street east of Prospect Avenue. Additionally, a small parcel located on the west side of El Camino Real between Main and Sixth Streets is designated CI. The CI designation provides for a range of civic, public, and institutional uses. Commercial uses (with some exceptions) and residential uses are not allowed under this land use designation.

KEY COMPONENT
Multi-Family Residential

2.1.1.5 Multi-Family (MF) Land Use Designation

The MF land use designation applies to the parcels located on the south side of Sixth Street west of B Street. The MF designation provides for multi-family residential use with approval of a discretionary entitlement, as described in Chapter 6, Administration and Implementation Plan. Residential projects also require Design Review as well as any other necessary entitlements such as subdivision maps. Parcels within the MF designation existing at the time of adoption of the DCCSP are permitted one residential unit by right subject to TCC Section 7262, Design Review, provided the unit is deducted from the residential bank (refer to Chapter 6).

KEY COMPONENT
Mobile Home

2.1.1.6 Mobile Home (MH) Land Use Designation

The MH land use designation applies to a portion of the east side of Prospect Avenue north of Third Street. The MH designation provides for mobile homes within a mobile home park. The intent of the DCCSP is for the existing mobile home park to remain as currently developed.

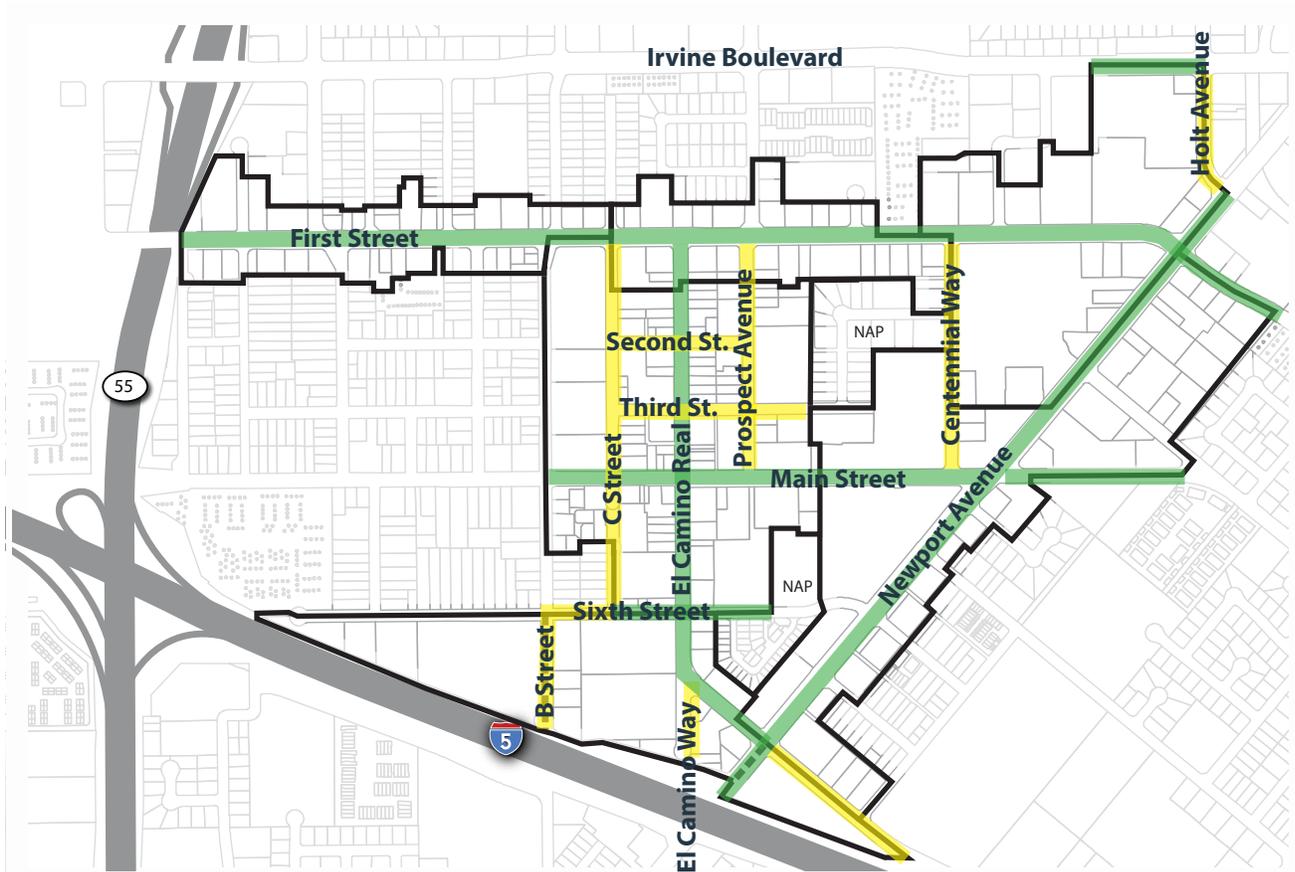
2.1.2 Location of Vertical and Horizontal Mixed Use

Figure 2.3 designates streets within the DCC as principal or non-principal, which specify where different formats of mixed use may be located. There are two basic types of mixed use development: vertical and horizontal. Vertical mixed use occurs when the residential use is located above the ground floor commercial use. Horizontal mixed use occurs when a residential use is located on the first floor in a separate building from commercial use, but on the same parcel.



Vertical mixed use may be located on either principal or non-principal streets. Horizontal mixed use is restricted to non-principal streets to maintain an active commercial focus on principal streets.

Figure 2.3 Principal or Non-Principal Streets



Principal Street – Where allowed within a DA and entitled as provided in Chapter 6, Administration and Implementation Plan, mixed use shall be designed as vertical format with first floor commercial use.

Non-Principal Street – Where allowed within a DA and entitled as provided in Chapter 6, Administration and Implementation Plan, mixed use may be designed as vertical format with first floor commercial or horizontal format in conjunction with commercial use in a separate building. Live/work units, where allowed and entitled, shall be limited to Non-Principal Streets.

2.2 Urban Design Plan

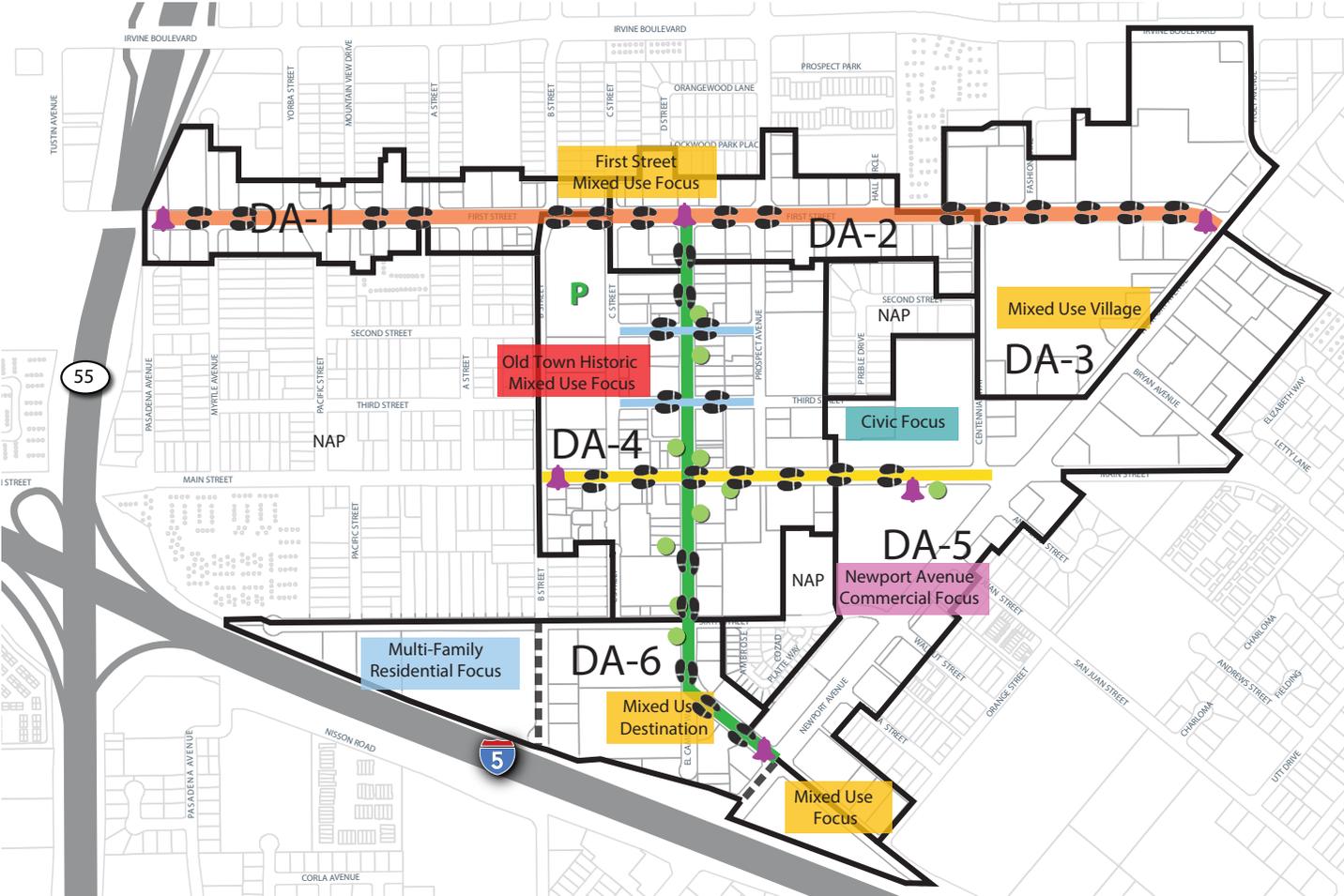
The overall design concept for the DCC is presented below in Figure 2.4, Urban Design Plan. To aid in visualizing the future DCC character, Figures 2.5 and 2.6 show conceptual streetscapes to further illustrate the vision for the DCC.

To implement the Urban Design Plan, which is based on the community vision for the DCC, the DCCSP specifies permitted uses, development standards, design criteria, and discretionary entitlements, which can be found within this document as listed below.

- Commercial Permitted Use Table (Table 3.1)
- Commercial Development Standards (Section 3.3)
- Commercial Design Criteria (Chapter 4)
- Residential Permitted Use Table (Table 5.1)
- Residential Development Standards (Section 5.3)
- Residential Design Criteria (Section 5.4)
- Discretionary Entitlements (Chapter 6)

On the following pages the boundaries of each DA are described and the key features for implementation of the Urban Design Plan are listed for each DA.

Figure 2.4 Urban Design Plan



Legend

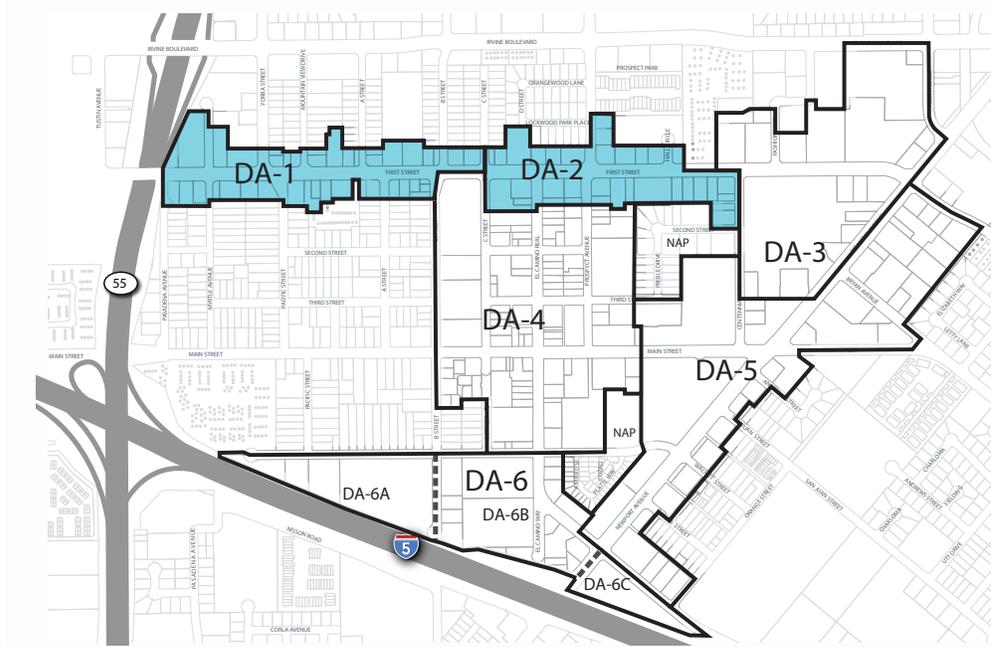
- P Peppertree Park ● Parklet (Conceptual Location) Pedestrian Orientation Gateway Monumentation
- Main Street Pedestrian Transformation First Street Pedestrian Transformation
- El Camino Real Pedestrian Transformation One-Way Street Transformation NAP - Not a Part



Figure 2.5 Conceptual Vision of New Development – Gateway to Old Town Area



Figure 2.6 Conceptual Vision of New Development – Gateway to Old Town Area



KEY COMPONENT
Pedestrian Orientation

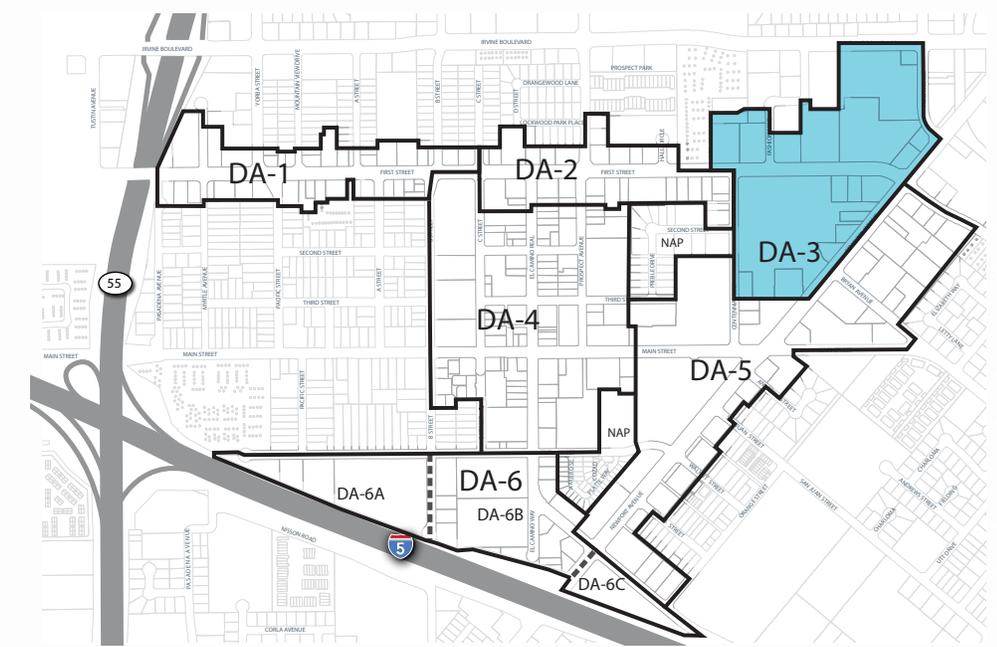
KEY COMPONENT
Mixed Use

2.2.1 DA-1 and DA-2

DA-1 and DA-2 comprise the westerly portion of First Street bounded by the historic residential neighborhoods to the north and south. The boundaries of DA-1 stretch along First Street from the 55 Freeway to C Street and DA-2 extends along First Street from C Street to Centennial Way.

Urban Design Plan focus:

- Northern gateway leading to Old Town
- Pedestrian and bicycle street transformation
- Vehicular circulation changes and additional parking
- Ground floor commercial
- Residential mixed use (vertical)
- Eclectic range of uses
- Small parcel size



2.2.2 DA-3

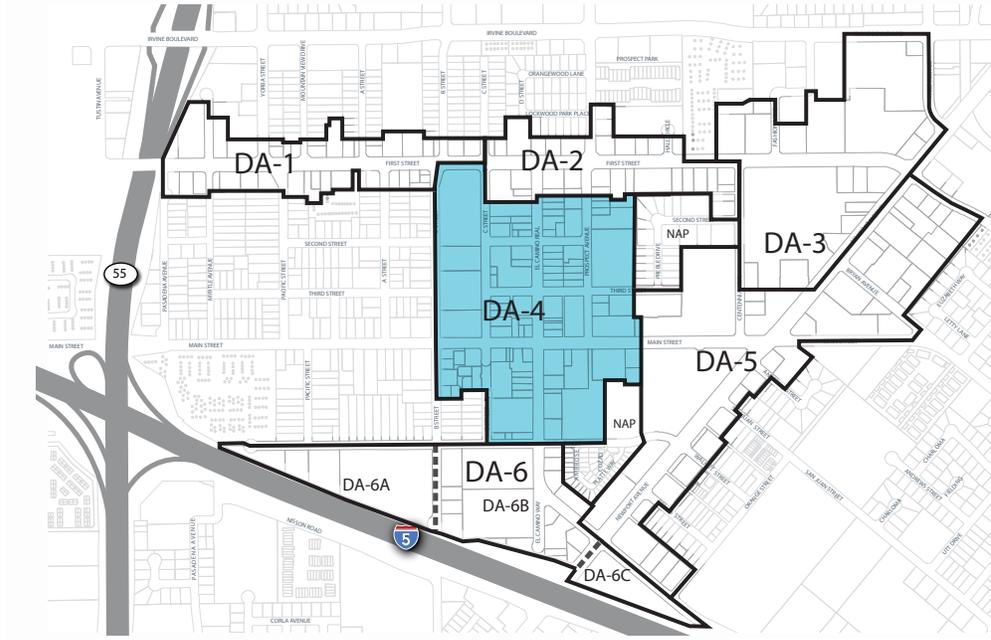
DA-3 is comprised of the eastern portion of First Street, extending from Centennial Way on the south side of First Street and Wellington Plaza on the north side of First Street to Newport Avenue.

Urban Design Plan focus:

- Mixed use village
- Commercial focus and shopping emphasis on principal streets
- Higher density vertical mixed use
- Horizontal residential mixed use on non-principal streets
- Maintain larger parcels to unify development projects rather than splitting parcels and piecemealing development
- Maximize relationship to transit and bicycle facilities

KEY COMPONENT
Newport Avenue Commercial Emphasis

KEY COMPONENT
Mixed Use



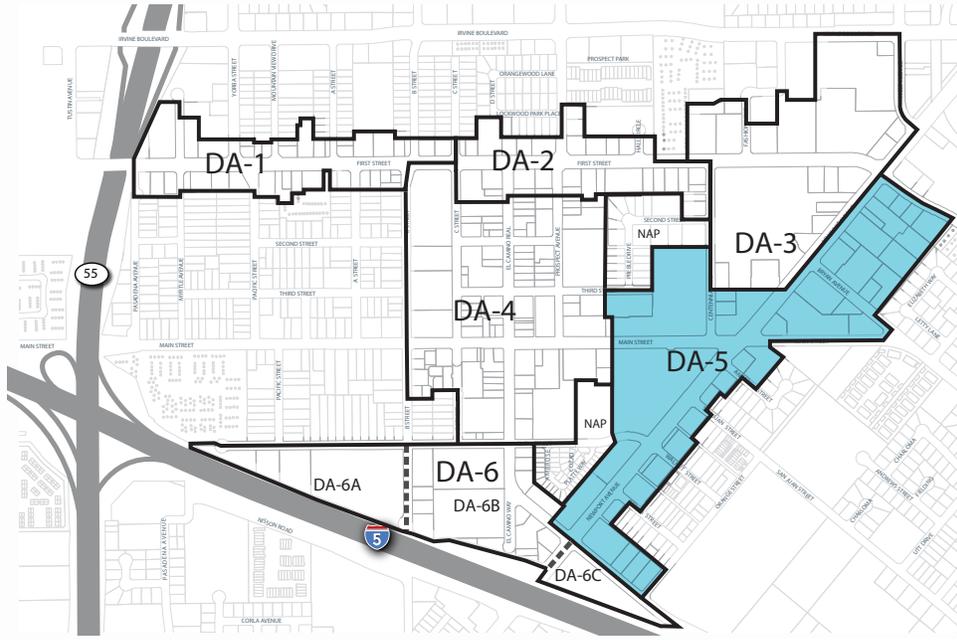
- KEY COMPONENT
Preserve Old Town
- KEY COMPONENT
Mixed Use
- KEY COMPONENT
Pedestrian Orientation

2.2.3 DA-4

DA-4 consists of the Old Town commercial district, located between B Street on the west, extending past Prospect Avenue to the eastern boundary of the Parque Santiago Mobile Home Park, from south of the First Street frontage to Sixth Street.

Urban Design Plan focus:

- Preserve Old Town character with mixed use focus
- Develop vacant parcels
- Expand range of businesses
- Extend walkability
- Boost patronage by increasing residents
- Create parklets on El Camino Real to encourage gathering
- Strengthen visual appeal and branding



2.2.4 DA-5

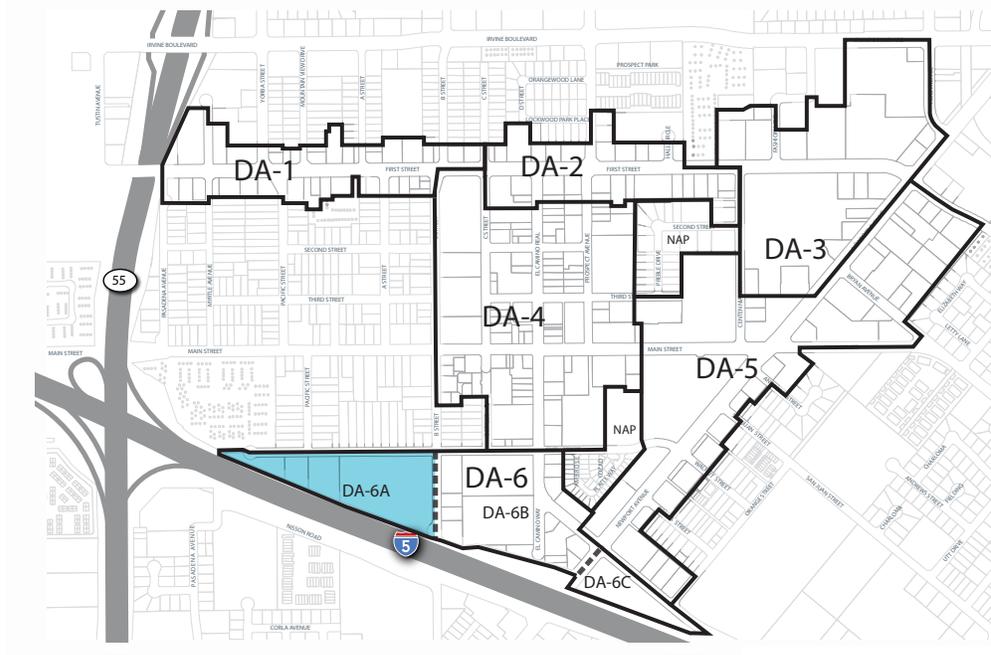
The borders of DA-5 are defined by the Tustin Branch Library on Main Street, the Tustin Civic Center on Centennial Way, and both sides of Newport Avenue from First Street to El Camino Real (excluding parcels located within the Larwin Square shopping center on the west side within DA-3).

Urban Design Plan focus:

- Maintain commercial
- Balance auto-centric nature with increased pedestrian and bicycle amenities on Main Street
- Main Street improvements highlight Old Town
- Vacate alley segments located within commercial parcels on Newport Avenue to consolidate parcels when feasible

KEY COMPONENT
**Preserve
Old Town**

KEY COMPONENT
**Newport
Avenue
Commercial
Emphasis**



KEY COMPONENT
Multi-Family Residential

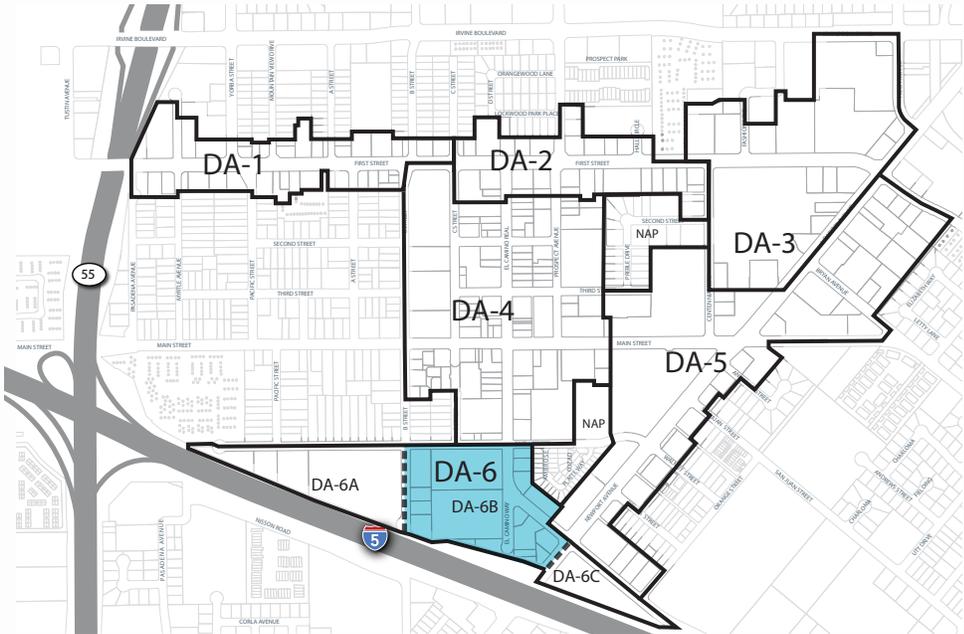
KEY COMPONENT
Preserve Old Town

2.2.5 DA-6A

DA-6A encompasses the blocks on the south side of Sixth Street from I-5 to B Street and abuts historic residential uses to the north.

Urban Design Plan focus:

- Continue transition to multi-family use
- Design sensitivity to adjacent Cultural Resources District
- Relationship to and compatibility with approved 140-unit condominium project



2.2.6 DA-6B

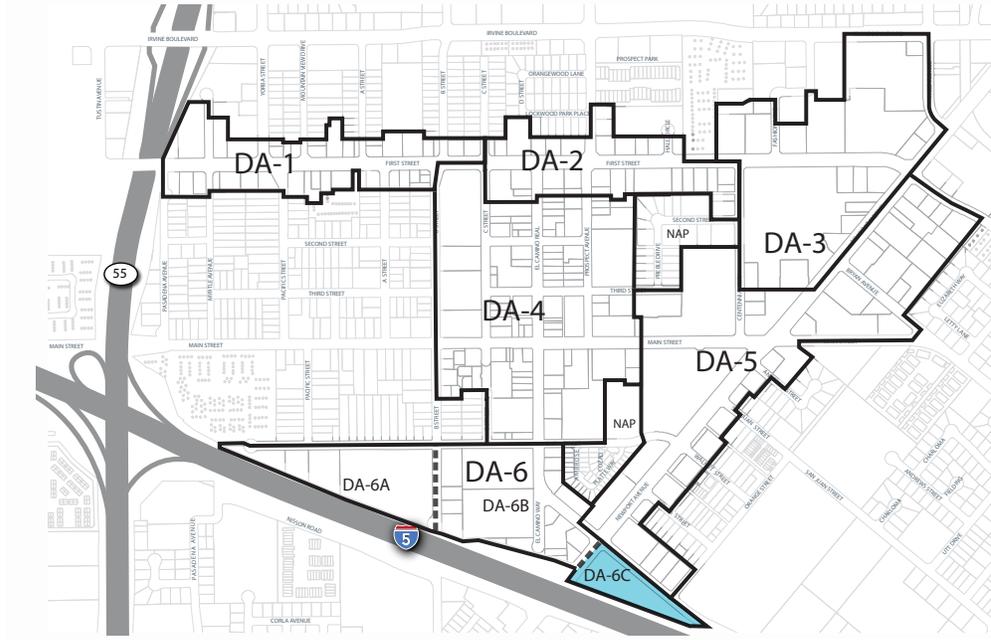
The boundaries of DA-6B include B Street on the west, Sixth Street on the north, the eastern frontage of El Camino Real, I-5 on the south, and Newport Avenue on the southeast. DA-6B forms a gateway into Old Town to the north.

Urban Design Plan focus:

- Shopping and entertainment destination
- Vertical mixed use on principal streets and horizontal mixed use on non-principal streets
- Lead patrons from Newport Avenue to Old Town
- Higher density near freeway
- Coordinated look on east and west sides of El Camino Real
- Emphasize public gathering

KEY COMPONENT
Mixed Use

KEY COMPONENT
Preserve Old Town



KEY COMPONENT
Mixed Use

2.2.7 DA-6C

DA-6C is bordered on the northeast by El Camino Real, on the northwest by Newport Avenue, and on the south by I-5.

Urban Design Plan focus:

- Multiple development options including small shopping center, hospitality and residential mixed use
- Mixed use encouraged along El Camino Real due to low traffic volumes
- Higher density adjacent to freeway
- Design circulation to avoid impacts on Newport Avenue

2.3 Circulation and Parking Plan

This section includes circulation plans and improvements for vehicles, pedestrians, bicyclists, and transit users. It also provides strategies for meeting the parking needs within the DCC.

2.3.1 Vehicular Circulation Plan

The DCC is served by a network of existing roadways, which includes several streets planned for improvements under the DCCSP to create “complete streets” in which the roadway design gives pedestrians and bicyclists greater emphasis. Refer to Figure 2.7, Vehicular Circulation Plan.

The DCCSP contains conceptual modifications to First and Main Streets, which are included in the Master Plan of Arterial Highways (MPAH) administered by the Orange County Transportation Authority (OCTA). To ascertain that the improvements under the DCCSP are agreeable to the OCTA, the City has sought and received preliminary approval from the OCTA to amend the MPAH as described below (contingent upon the OCTA receiving documentation that the City has amended its general plan and has complied with the requirements of CEQA):

- Reclassify First Street from just east of State Route 55 to Newport Avenue, from a primary (four-lane, divided) arterial to a divided collector (two-lane, divided) arterial; and
- Reclassify Main Street from the westernmost city limits to Newport Avenue, from a primary (four-lane, divided) arterial to a divided collector (two-lane, divided) arterial.

Figure 2.7 documents the existing roadway system within the DCC and highlights the roadways intended for improvements, which consist of First and Main Streets as well as Second and Third Streets. The following subsections discuss and illustrate the conceptual improvements.

2.3.1.1 Main Street

The conceptual Main Street improvements are key to economic development in Old Town because they will strengthen pedestrian connections between nearby businesses, the Tustin Branch Library and Tustin Civic Center, and Old Town. The City’s 2016-2017 Capital Improvement Program (CIP) includes conceptual planning efforts for Main Street specifically between B Street and Newport Avenues. The conceptual plans are consistent with the

KEY COMPONENT

**Transform
Streets**

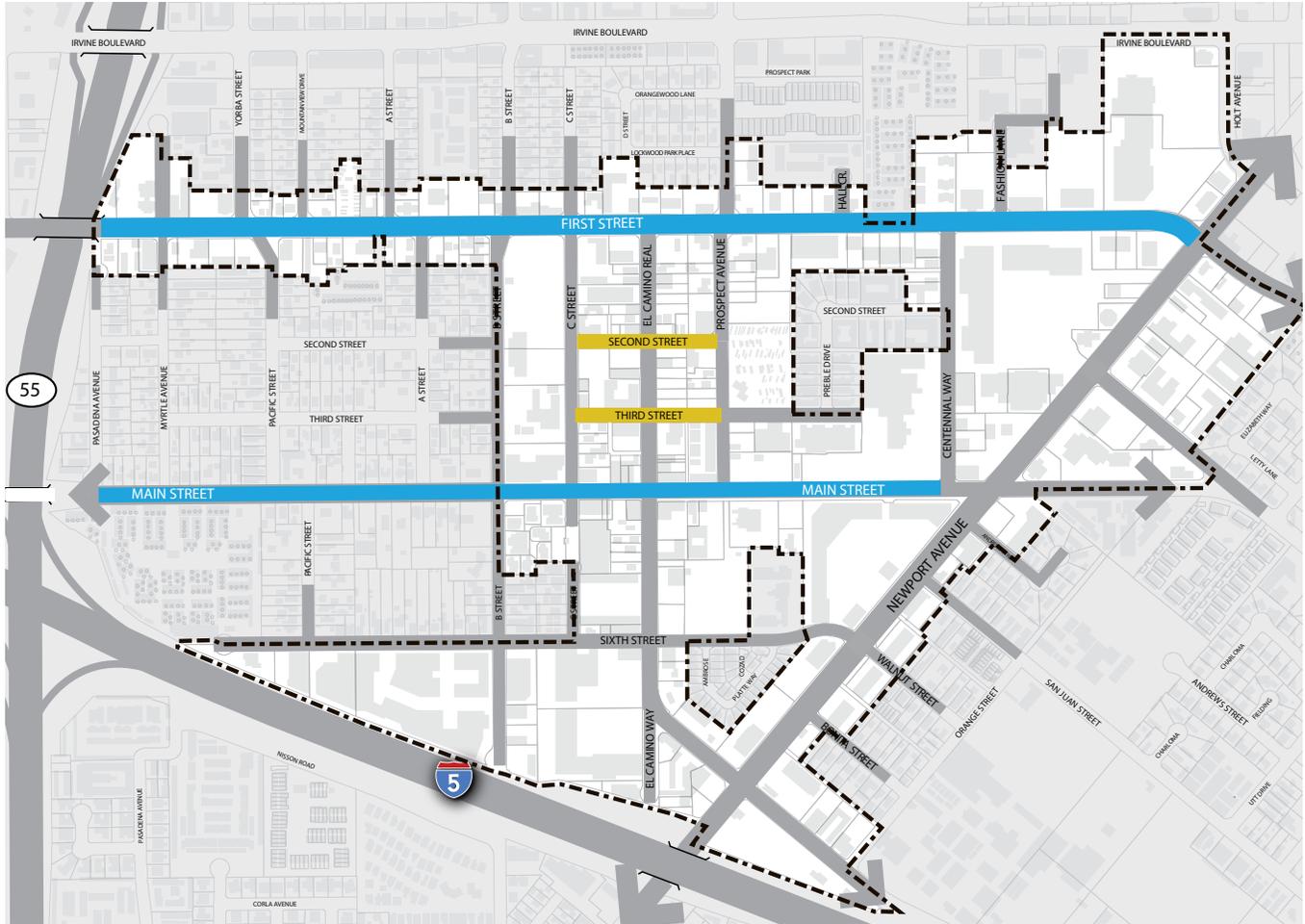
KEY COMPONENT

**Preserve
Old Town**

KEY COMPONENT

**Pedestrian
Orientation**

Figure 2.7 Vehicular Circulation Plan



Legend

- Project Area
- Reduce Vehicular Lanes and/or Add Pedestrian & Bike Improvements
- Convert to One-Way Street with Pedestrian Improvements
- Existing Road



preliminary MPAH reclassification approval and aim to create a pedestrian oriented, walkable and bikeable street. The conceptual plan addresses vehicular, pedestrian, and bicycle travel, on-street parking, pedestrian bulb outs and enhanced crossings, and landscaping to visually support the roadway transformation.

The conceptual Main Street improvements illustrated in Figure 2.9 reduce the street from two lanes to one lane in each direction and modify the 13.5-foot travel lanes to 11 feet westbound and 14 feet eastbound between Prospect Avenue and Centennial Way. This allows space for the addition of a landscaped center median and on-street diagonal parking on the south side of the street. The on-street parking is located within walking distance of Old Town.

The conceptual Main Street improvements provide a 5-foot on-street buffered bicycle lane (Class 2) on the north side of the street, which can be accessed from the off-street bicycle lane (Class 1) along the west side of Newport Avenue. On the south side of the street, the pedestrian sidewalk is expanded to 16 feet including a 5-foot integrated off-street (Class 1) bicycle lane and enhanced with decorative pavement.

A public parklet within the right-of-way on the south side of Main Street provides a location for pedestrians to congregate within sight and walking distance of Old Town. The Library driveway on the north side of Main Street is relocated to align with the Tustin Plaza driveway, eliminating conflicting vehicle turning movements. The aligned driveways allow for bulb outs that narrow the roadway to facilitate pedestrian crossings via a decorative crosswalk.

Future detailed plans for the improvements to the remaining portions of Main Street located outside of the CIP project area will be consistent with the preliminary OCTA reclassification approval.

2.3.1.2 First Street

The conceptual First Street improvements are consistent with the preliminary adoption of the MPAH reclassification amendments and promote a pedestrian friendly corridor by reducing the number of traffic lanes and lane widths, thus expanding opportunities for other modes of travel and transforming the character of the street.

KEY COMPONENT

**Pedestrian
Orientation**

The conceptual improvements, represented conceptually in Figure 2.10, reduce the number of travel lanes from two to one in each direction and narrow the lane widths to 11 feet. This allows for a 5-foot on-street bicycle lane (Class 2), a 17-foot diagonal parking lane on both the north and south sides of the street, and a 16-foot-landscaped median. Pedestrian travel is also facilitated by expanding the existing sidewalk to 8 feet on the north side and 10 feet on the south side.

In that the circulation plans and improvements associated with First Street are conceptual without specific design details, at the time of moving forward with related First Street circulation plans and improvements, it is a City Council policy directive that the conceptual plans will be revisited, additional analysis including environmental analysis will take place, and a public outreach program will be implemented inviting and encouraging public participation in that process. If modifications are determined to be infeasible, the Plan will be modified.

KEY COMPONENT
**Preserve
Old Town**

2.3.1.3 Second and Third Streets

Pedestrian orientation in Old Town is enhanced by transforming Second and Third Streets from two-way to one-way streets flowing in opposite directions to form a couplet. Figure 2.11 and 2.12 illustrate the conceptual revised traffic patterns. The conceptual improvements modify two lanes of opposing traffic into a 14-foot wide one-way street. The pedestrian character is enhanced through 13.5-foot sidewalks with a 4-foot landscaped bulb out on one side. Parking is changed from parallel parking on both sides to a diagonal parking lane (19 foot 10 inch) on one side.

Figure 2.8 Second and Third Street Planned Traffic Movements



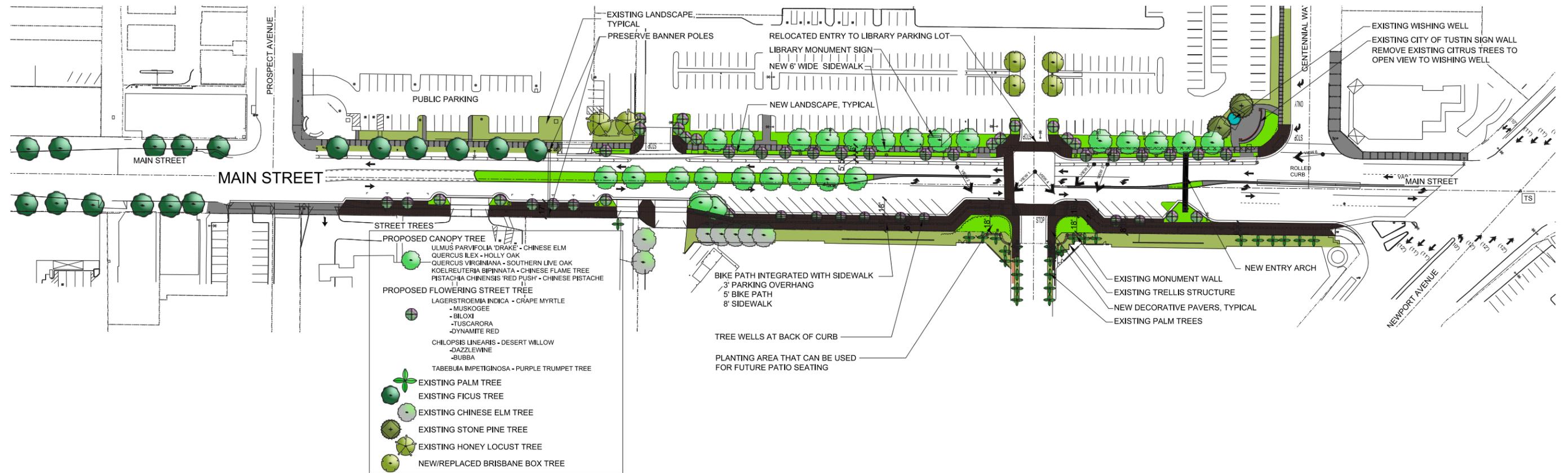


Figure 2.9 Main Street Conceptual Improvements

Figure 2.10 First Street Conceptual Improvements

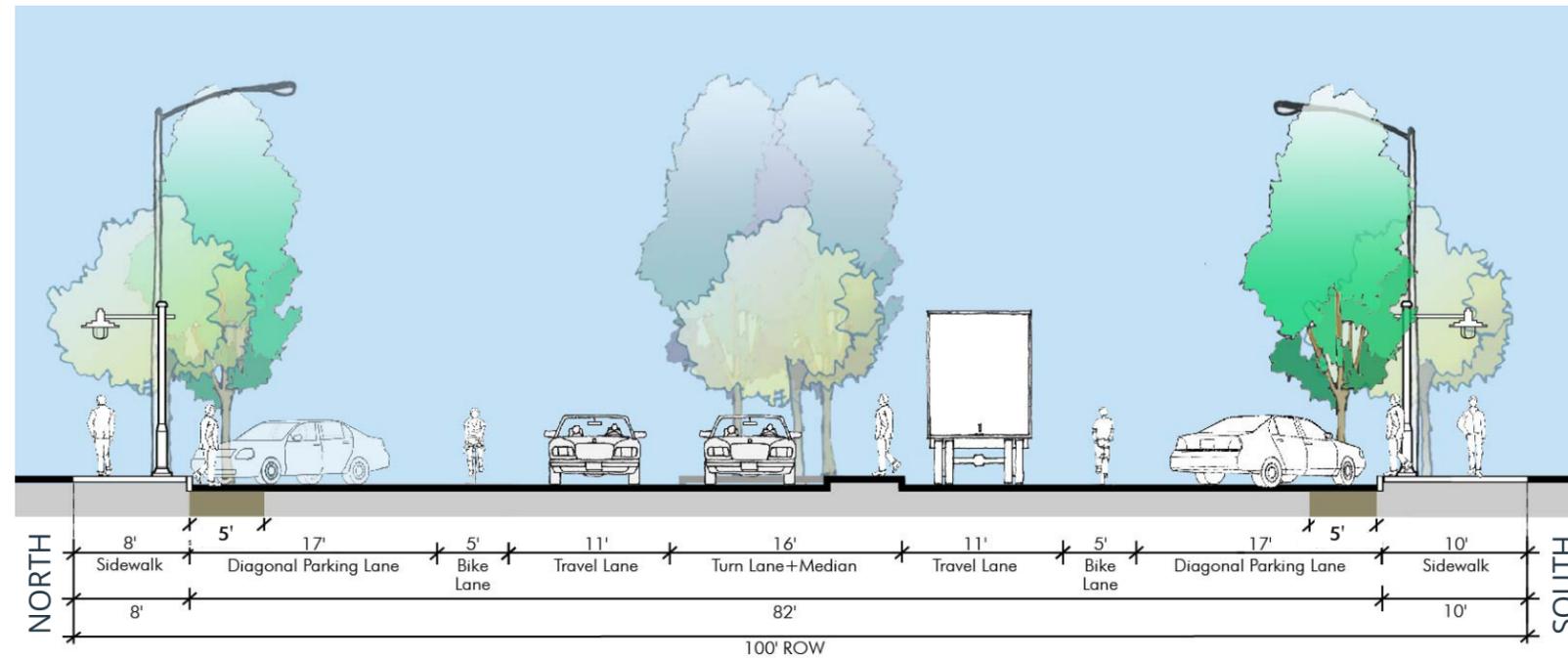


Figure 2.11 Second Street Conceptual Improvements

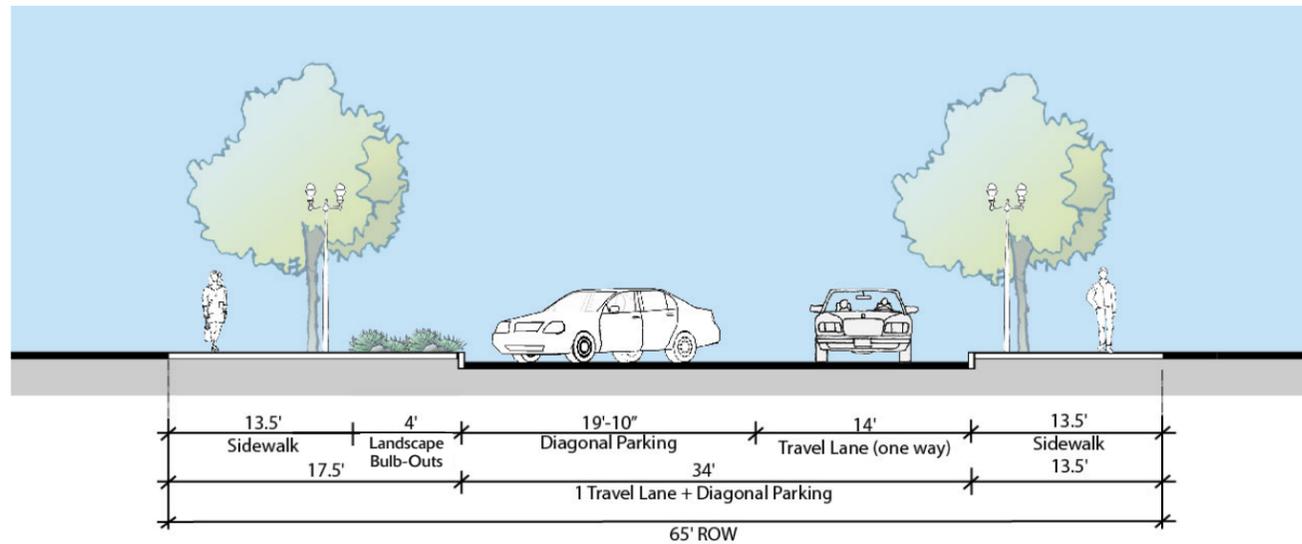
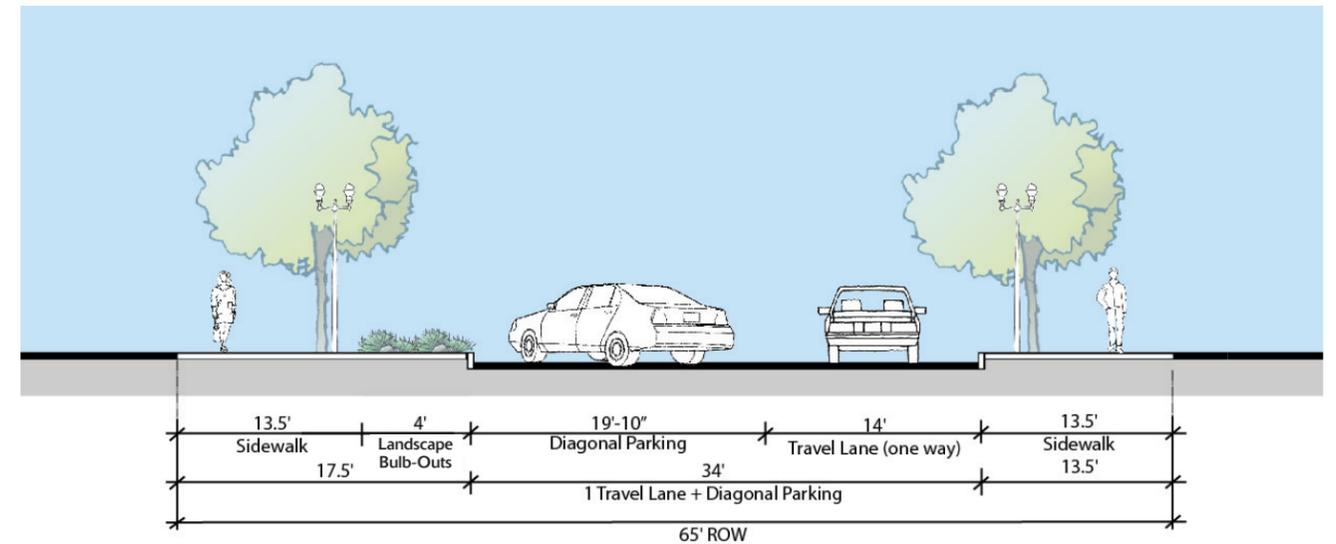


Figure 2.12 Third Street Conceptual Improvements



2.3.2 Vehicular Parking Plan

Adequate and convenient parking to meet the needs of patrons and residents is necessary throughout the DCC. Citizen comments received at the public workshops during the DCCSP public participation process cited Old Town parking shortages. In response to these concerns, the City conducted a detailed parking study in Old Town (Tustin Downtown Commercial Core Parking Study, April 2017). The Study determined that there are currently enough public and private parking spaces to meet the current demand, but available spaces are not always as convenient as desired. Old Town public parking is typically available within walking distance of businesses; additionally, the DCCSP identifies parking alternatives to serve existing patrons and attract new business development to the area. The Study suggested additional off-street public parking areas be considered later, at the point when public on-street parking is more fully utilized and conveniently located spaces in certain areas are consistently unavailable. The Study will be updated over time, and additional public parking will be added as needed.

Many of the historic Old Town buildings were built prior to today's modern use of vehicles and need for on-site parking. On-site parking requirements often limit the feasibility of new development since many lots in Old Town are too small to accommodate both buildings and adequate parking spaces. Therefore, the DCCSP encourages consideration of existing conditions and site constraints, flexibility, and alternative parking arrangements, where appropriate.

Tables 3.2 through 3.4 in Chapter 3 indicate the required number of commercial parking spaces for new development within the various DAs. Tables 5.2 through 5.4 in Chapter 5 indicate the required number of parking spaces for mixed use and multi-family residential.

2.3.2.1 Provisions for Reduced Parking

The DCCSP establishes specific parking provisions, but also provides that parcels located within the DCC may take advantage of alternative parking requirements through various provisions. In Section 6.3.3, Modification of Parking Standards, the DCCSP 1) establishes new provisions unique to the DCC; 2) extracts provisions from the TCC and incorporates them directly into the DCCSP to apply to the DCC; and 3) cites provisions for alternative parking in the TCC that apply to the DCC.

2.3.3 Pedestrian Circulation Plan

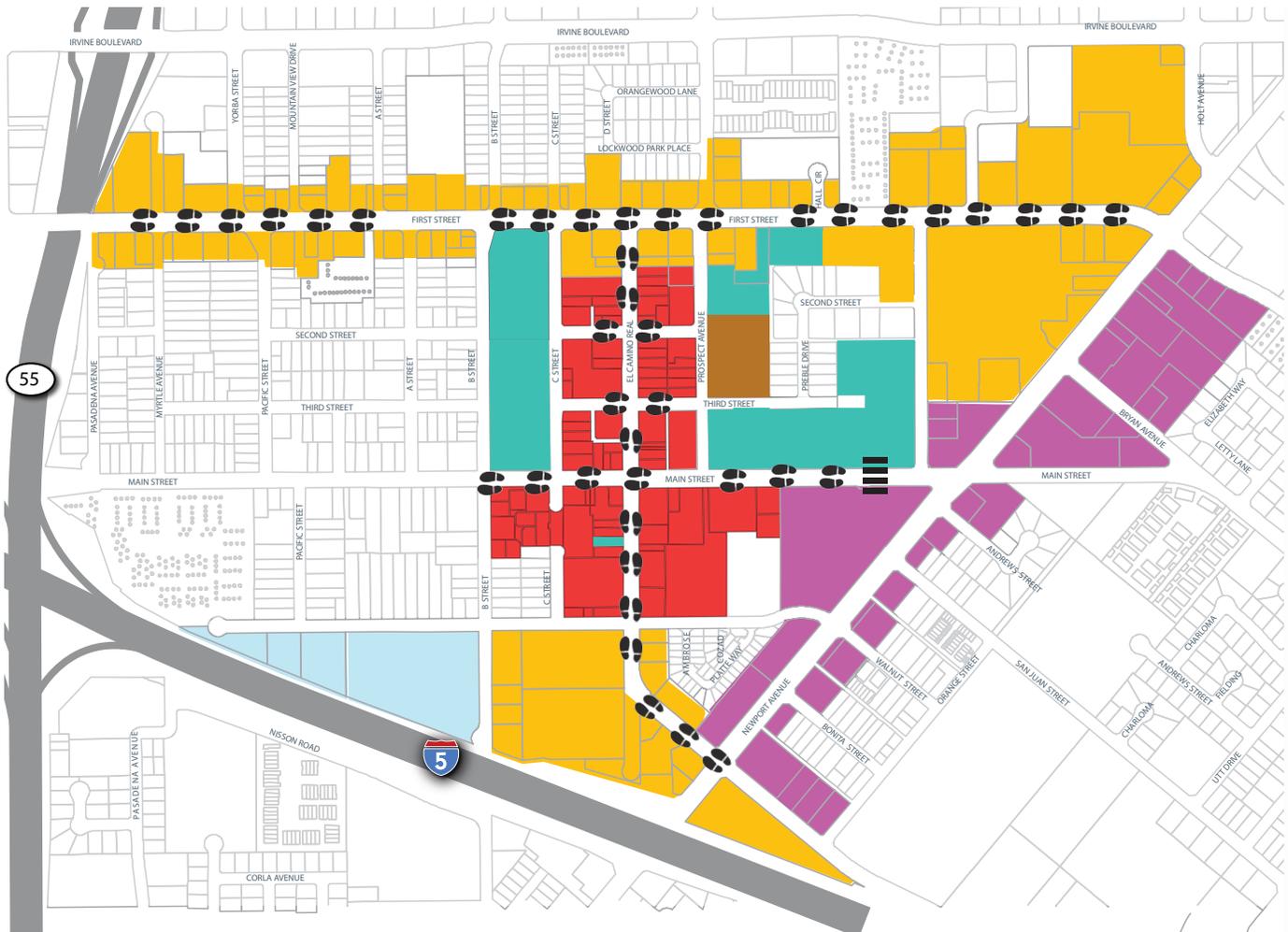
An existing network of sidewalks of varying widths serves the DCC. The DCCSP includes conceptual pedestrian improvements as summarized below and illustrated in Figure 2.13, Pedestrian Circulation Conceptual Improvements. Consideration should be given to the opportunities listed for expanded pedestrian improvements.

KEY COMPONENT
Preserve Old Town

2.3.3.1 Conceptual Pedestrian Improvements

The following pedestrian circulation improvements are planned for the specified streets within the DCC, as illustrated in Figure 2.13.

Figure 2.13 Pedestrian Circulation Conceptual Improvements



Legend

- Pedestrian Improvements
- Enhanced Pedestrian Crosswalk

Widening of Sidewalks – Main Street, First Street, Second Street, Third Street

Decorative Sidewalk Paving – Main Street, El Camino Real

Bulb-Outs – Main Street, First Street, Second Street, Third Street

Crosswalk with Enhanced Paving – Main Street

Pedestrian Gathering Areas – El Camino Real (see description in Section 2.4.6.2, Parklets)

Increasing Sidewalk Widths – Consideration should be given to increasing sidewalk widths in conjunction with new large scale pedestrian-oriented development to allow for greater volumes of pedestrian traffic and activities such as outdoor dining within or adjacent to the right-of-way.

Decorative Crosswalks – Decorative crosswalks should be encouraged to draw attention to crossings to increase pedestrian safety and add visual interest to the street, especially within Old Town and in proximity to future pedestrian-oriented large scale development.

Flashing Light Crosswalks – Especially at unsignalized crossings, consideration should be given to installation of pedestrian-activated flashing light crosswalks.

Accessible Pedestrian Signals – To better serve all users, accessible pedestrian signals, such as those with audible-tactile walk indicators, tactile arrow pushbuttons, or pushbutton locator tones, may be considered at certain signalized crossings.

2.3.4 Bicycle Circulation Plan

New bicycle routes conceptually planned in the DCC will improve access and connectivity. The conceptual bicycle improvements are shown in Figure 2.14, Bicycle Circulation Conceptual Improvements.

The Main Street improvements provide a buffered on-street bicycle lane (Class 2) on the north side from Newport Avenue to Prospect Avenue, which can be accessed from the off-street bicycle lane (Class 1) along the west side of Newport Avenue. On the south side of Main Street, the expanded pedestrian sidewalk includes an integrated off-road bicycle lane (Class 1).

Plans for First Street include a striped on-street bike lane with a striped



buffer (Class 2) on both sides of the street between the 55 Freeway and Newport Avenue. The First Street bicycle lanes also connect with the off-street Class 1 bicycle lane on Newport Avenue.

“Sharrows” (Class 3), which are identified by symbols painted on the roadway pavement to indicate that motor vehicles and bicycles are to share the same travel lane, are proposed for the following street segments:

- Main Street from B Street to Prospect Avenue
- El Camino Real from First Street to Newport Avenue
- B Street between First Street and Sixth Street
- Prospect Avenue between First Street and Main Street
- Centennial Way between First Street and Main Street
- Sixth Street between B Street and Newport Avenue

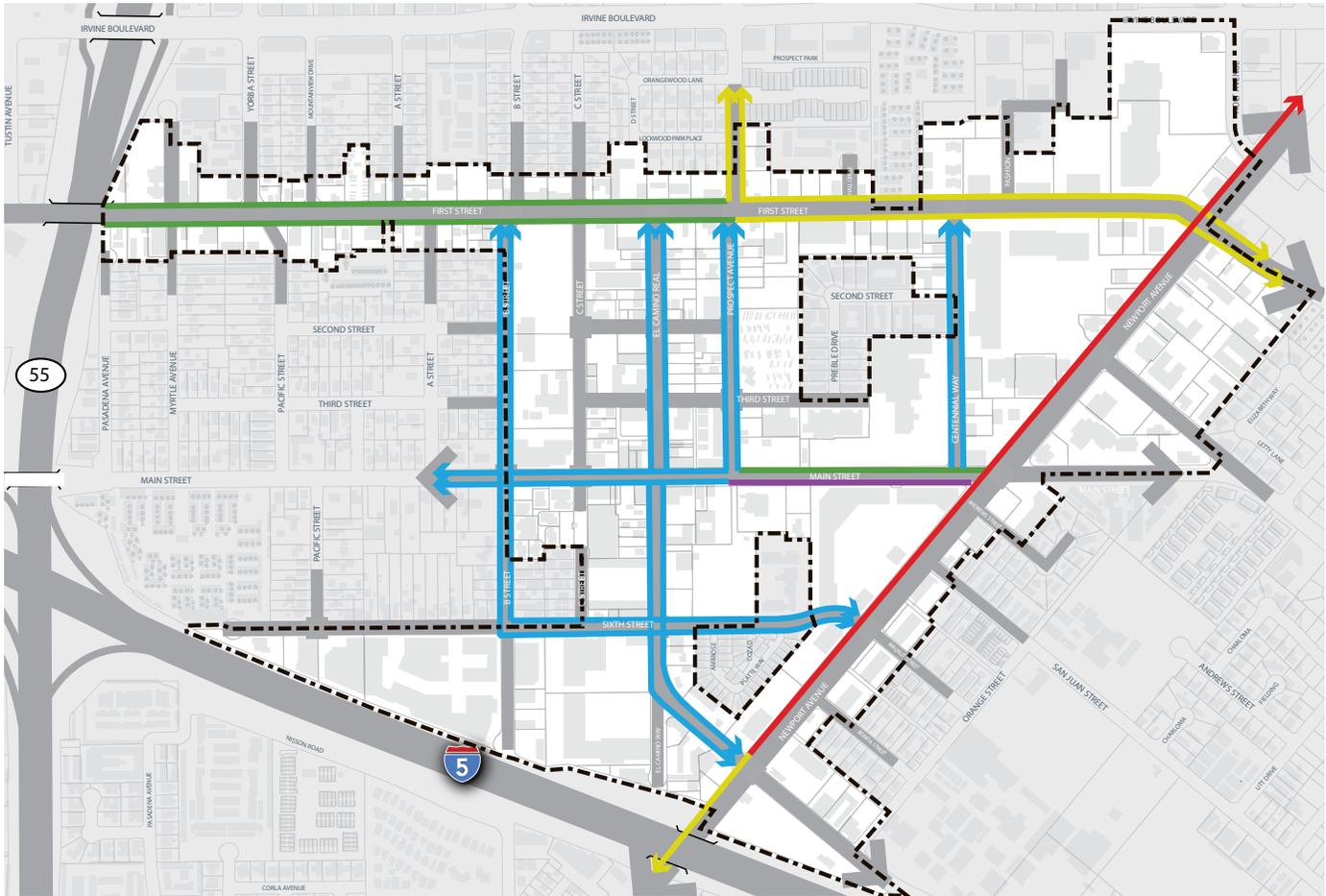
In addition to the conceptual bicycle lane, improvements, increasing the number of bike racks and bike shelters within the public right-of-way (see Section 2.4.5, Street Furniture and Amenities) and within private development (see Section 4.5.2, Parking Lots and Parking Structures) is an important part of encouraging bicycle ridership in the DCC.

2.3.5 Transit Access Plan

Transit service within the DCC is provided along Newport Avenue, First Street and Centennial Way, with bus shelters located at select bus stops. Additional bus shelters are recommended along existing routes to provide transit users adequate shelter while waiting. Shelters that are designed to add to the DCC character are strongly encouraged. Shelter design should be coordinated using roof forms, design details, materials, and colors that reflect the desired building character for the area as expressed in the Design Criteria. Shelter design should maximize visibility, safety, and convenience for transit users as well as help visually brand the area.

New development along transit routes shall be sited for easy access to transit stops and designed with connecting pedestrian walkways to meet the needs of transit ridership and promote transit use. Bus routes could potentially be revised to more directly serve Old Town after economic development occurs and ridership to the area increases.

Figure 2.14 Bicycle Circulation Conceptual Improvements



Legend

- Project Area
- Existing Road

- Existing Class I Bike Facility (Paved Off Road)
- Conceptual DCCSP Class 1 Bike Lane (Paved Off Road)
- Tustin Master Plan of Bikeways Class 2 Bike Lane (Striped On Road)
- Conceptual DCCSP Class 2 Bike Lane (Striped On Road)
- Conceptual DCCSP Class 3 Bike Sharrow (Marked, Shared On Road)



Class 1: Off Road, Paved



Class 2: On Road, Striped



Class 3: Sharrow - On Road, Marked (Not Striped)



Bus shelter design that reflects the DCC character is encouraged.

KEY COMPONENT
**Pedestrian
Orientation**

KEY COMPONENT
**Preserve
Old Town**

2.4 Streetscape and Public Open Space Plan

Within the DCC, streetscape elements including street trees, monumentation, art within the public right-of-way, and street furniture aim to create a consistent image that is visually attractive, identifiable, and memorable. Parks, parklets, “living alleys,” and other open spaces are incorporated into the DCCSP to provide opportunities for recreation, socializing, and community events.

2.4.1 Ficus Tree Replacement Program

Large tree canopies create shade and an inviting streetscape and pedestrian atmosphere. Much of the street tree canopy in the DCC is provided by the Indian Laurel Fig (*Ficus microcarpa*), which is a non-native tree commonly planted for many years throughout Southern California. However, Ficus trees are characterized by invasive roots, which due to the large size of the trees in the DCC, sometimes negatively affect public and private infrastructure and require increased maintenance, including the following:

- Ficus tree roots can penetrate water and sewer pipes, resulting in expensive plumbing repairs for property owners.
- The roots of the Ficus trees sometimes uplift sidewalk pavement,



creating potentially unsafe conditions for pedestrians.

- Maintenance costs increase due to constant grinding or replacement of sidewalks to remove potential trip hazards.
- Ficus trees require an aggressive pruning schedule to manage their size, especially when located close to buildings, and to minimize pavement impacts.
- Litter from fallen fruits and leaves can create a nuisance and requires ongoing clean-up.

The DCCSP includes a program to gradually replace the existing Ficus trees in the DCC with one species. The Street Tree Palette in Section 2.4.2 identifies suggested species to assist in the City's selection process. The replacement tree will continue to provide an attractive streetscape in the DCC, require less maintenance, and conserve water by being more drought-tolerant.

Along many street segments of the DCC, Ficus are the only street trees. Therefore, it is important that their replacement be done in the manner that is least disruptive to the character of the street. The DCCSP provides the following guidelines to direct the method of Ficus replacement.

- Replace every-other Ficus tree with a single tree species to create an alternating pattern of Ficus and new street tree.
- Follow a systematic, phased tree replacement schedule to replace all alternating Ficus trees within the DCC and then cycle back to replace the remaining Ficus trees.
- Replace Ficus with 48-inch box sized tree or larger, wherever possible.
- In addition to this phasing program:
 - Prioritize replacement of Ficus trees that are considered a specific safety hazard.
 - When pavement repair/replacement or other infrastructure work occurs, replace impacted Ficus trees.
 - Replace Ficus trees in conjunction with adjacent site development.

2.4.2 Street Tree Palette

The following list provides a sampling of suggested street tree species appropriate for Ficus replacement within the DCC. One species of tree will be selected by the Public Work's Director, either from the list or a species of the Director's choice, to create consistency throughout the DCC.

Non-Flowering Tree Species

- *Ulmus parvifolia* 'Drake' (Chinese Elm (Drake))
- *Quercus ilex* (Holly Oak)
- *Quercus virginiana* (Southern Live Oak)

Flowering Tree Species

- *Koelreuteria bipinnata* (Chinese Flame Tree)
- *Pistacia chinensis* 'Keith Davey' (Chinese Pistache (Keith Davey))
- *Tabebuia impetiginosa* (Purple Trumpet Tree)
- *Handroanthus impetiginosus* (Pink Trumpet Tree)
- *Koelreuteria paniculate* (Golden Rain Tree)
- *Chilopsis linearis* 'Bubba' (Desert Willow (Bubba))
- *Chitalpa tashkentensis* 'Pink Dawn' (Chitalpa Pink Dawn)
- *Pyrus calleryana* 'Chanticleer' (Callery Pear (Chanticleer))

2.4.3 Monumentation Plan

Monumentation is useful for wayfinding and can become part of an area's culture. The historic mission bells can still be seen along El Camino Real in Old Town Tustin, originally installed on high poles by the California Federation of Women's Clubs to identify the route for early automobile travelers. These bells helped to mark the rough 600-mile route between the California missions along the coast of California.

The DCCSP employs monumentation to identify important gateways into the downtown, help brand the area and draw interest. A phased monumentation program specifies gateway signage within the DCC.



Primary in terms of both objective and timing is to draw attention to Old Town. The Main Street improvements planned under the CIP include installation of a street arch, identifying the eastern entry into Old Town from Newport Avenue and adding aesthetic appeal to the street. Figure 2.13 illustrates the conceptual entry design. The arch is designed to span Main Street near the intersection of Centennial Way.

Similarly, additional Old Town gateway signage is envisioned at the northern entry (El Camino Real at First Street) and the southern entry (El Camino Real at Newport Avenue and/or Sixth Street). Once First Street improvements have been implemented and private development begins to reinforce the transition to a more pedestrian oriented environment, installation of monument signage identifying the eastern and western DCC gateways on First Street is recommended. In addition, a coordinated program of directional and wayfinding signs is needed to assist in branding the area.

Three factors should be considered when creating new signage programs in public rights-of-way:

1. Clustering of signs and avoidance of clutter. This method of clustering signs eliminates “sign clutter” and decreases maintenance and installation costs because of fewer poles.
2. Signage in streets, parking areas, and pedestrian spaces should be treated as both a source of information and public artwork.

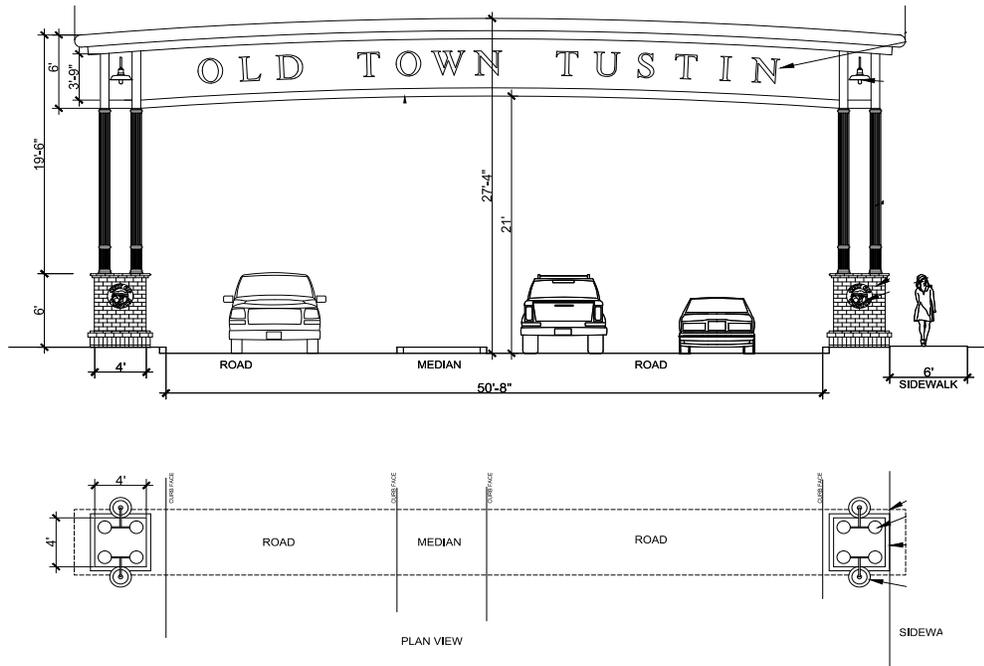


When multiple signs are required, they should be clustered onto one pole when feasible.



One of the original bells in Tustin that guided automobile travelers along El Camino Real can be seen in the background.

Figure 2.15 Conceptual Old Town Entry Monumentation





3. The location of these signs should be coordinated with landscape and street furniture locations to ensure sign visibility. Existing signs that conflict with existing and proposed landscaping and furniture should be moved to more appropriate locations.

2.4.4 Art within Public Rights-of-Way and Public Places

Art within the public rights-of-way and other public spaces creates visual interest and vitality. It also provides opportunities to engage the community, local artists and arts programs. It can reflect local history and culture.

Public art is a pedestrian amenity and should be located in an area suited for pedestrian viewing. The piece should be placed as a focal point in a park or plaza, or situated along a pedestrian path. Public art can be incorporated into standard street elements such as bicycle racks, benches, trash receptacles, and utility boxes. Public art can provide information, such as maps and signs, or educate viewers about Tustin’s history and culture. Art can simply be beautiful or playful. Public art should be accessible to persons with disabilities and placement should not block the sidewalk or pedestrian zone.

The Tustin Pioneers Recognition Program is an effort to honor Tustin’s pioneers by installing bronze busts and plaques in the public right-of-way throughout Tustin. This Program is in its inception, with the first bust of the city’s founder Columbus Tustin, recently installed at the northeast corner of Main Street and El Camino Real in Old Town. To-date, 25 pioneers have been identified for future acknowledgment in the Tustin Pioneers Recognition Program. The Program may be supported through fundraising or privately funded as part of development projects.

Art is also encouraged in conjunction with private development, especially at locations with high visibility to help distinguish the downtown and attract patrons. For large scale developments, the project approval body is encouraged to consider the installation of public art as a condition of approval. Refer to Chapter 4, Commercial Design Criteria, for art requirements on private property.



Public art adds vitality.



Statue of City founder Columbus Tustin.

2.4.5 Street Furniture and Amenities

Street furniture includes benches or other types of seating, trash/recycling receptacles, landscape containers, light fixtures, bike racks, bollards, pole banners, informational kiosks, street banners, and similar amenities (for bus shelters refer to Section 2.3.5, Transit Access Plan). Street furniture encourages social activity by making the public realm more comfortable and convenient and invites people to linger. Street furniture can improve the appeal and character of an area and provide opportunities for incorporating logos, branding, marketing, and announcements of civic events. Street furniture is encouraged along public rights-of-way, especially within Old Town and other pedestrian-oriented streets. The design and placement of street furniture should be:

KEY COMPONENT

**Pedestrian
Orientation**

KEY COMPONENT

**Preserve
Old Town**

- Made of high quality, low maintenance materials
- Placed in well-used areas
- Clustered such as placing trash/recycling receptacles near benches, accented by landscape planters, and protected from the street by bollards
- Designed to reflect a coordinated, consistent theme and color palette for the DCC
- Enhance the desired look for the DCC area as reflected in the DCCSP
- Create an identifiable look and brand for the DCC
- Communicate information to the community and visitors
- Designed to deter anti-social behavior and treated with anti-graffiti coatings
- Not impede pedestrian travel or obscure driver sight lines



2.4.6 Public Open Space

Parks, open space, and public plazas present prime opportunities to foster community physical and mental fitness by promoting exercise, leisure activities, and social interaction. Underutilized parking lots, alleys, and vacant parcels also present opportunities to expand community activities. Figure 2.16, Open Space, shows existing, planned, and potential parks and public open space in the DCC.

KEY COMPONENT

Pedestrian Orientation

2.4.6.1 Peppertree Park

Peppertree Park is located along the northwestern perimeter of DA-4. Its amenities include a large children's play area and a picnic shelter that can be rented for parties. The north end of the park contains a softball diamond. Peppertree Park is ideally located to attract residents from north of First Street and west of the park and patrons or employees from First Street and Old Town. Future mixed use will bring more nearby residents seeking a place to interact and recreate.

2.4.6.2 Parklets

To facilitate community interaction, generate activity, and create a memorable amenity that enhances the Old Town street scene, the City has identified locations within the right-of-way along El Camino Real for public parklets, as illustrated on Figure 2.16, Open Space. These parklets consist of bulb-outs with enhanced paving, landscaping and low walls to create public gathering areas adjacent to the sidewalk and to provide outdoor sidewalk seating areas for restaurants and other businesses. Parklets may be maintained by nearby businesses through a City license agreement processed through the Public Works Department.

The conceptual Main Street improvements also include a parklet within the bulb-outs flanking the northern entrance to Tustin Plaza to foster increased pedestrian activity.

Although the current focus is to implement parklets on El Camino Real and Main Street within the heart of Old Town, expansion of the network of parklets to other areas within the DCC is envisioned and encouraged.



Underutilized urban space can be used for pocket parks through public-private partnerships.

2.4.6.3 Pocket Parks

Opportunities exist within the DCC for partnerships between private land owners and the City to utilize undeveloped green space for public use. These areas should be considered for use as informal gathering areas, ideally with pedestrian amenities such as seating, shade, trash/recycle receptacles, and lighting.

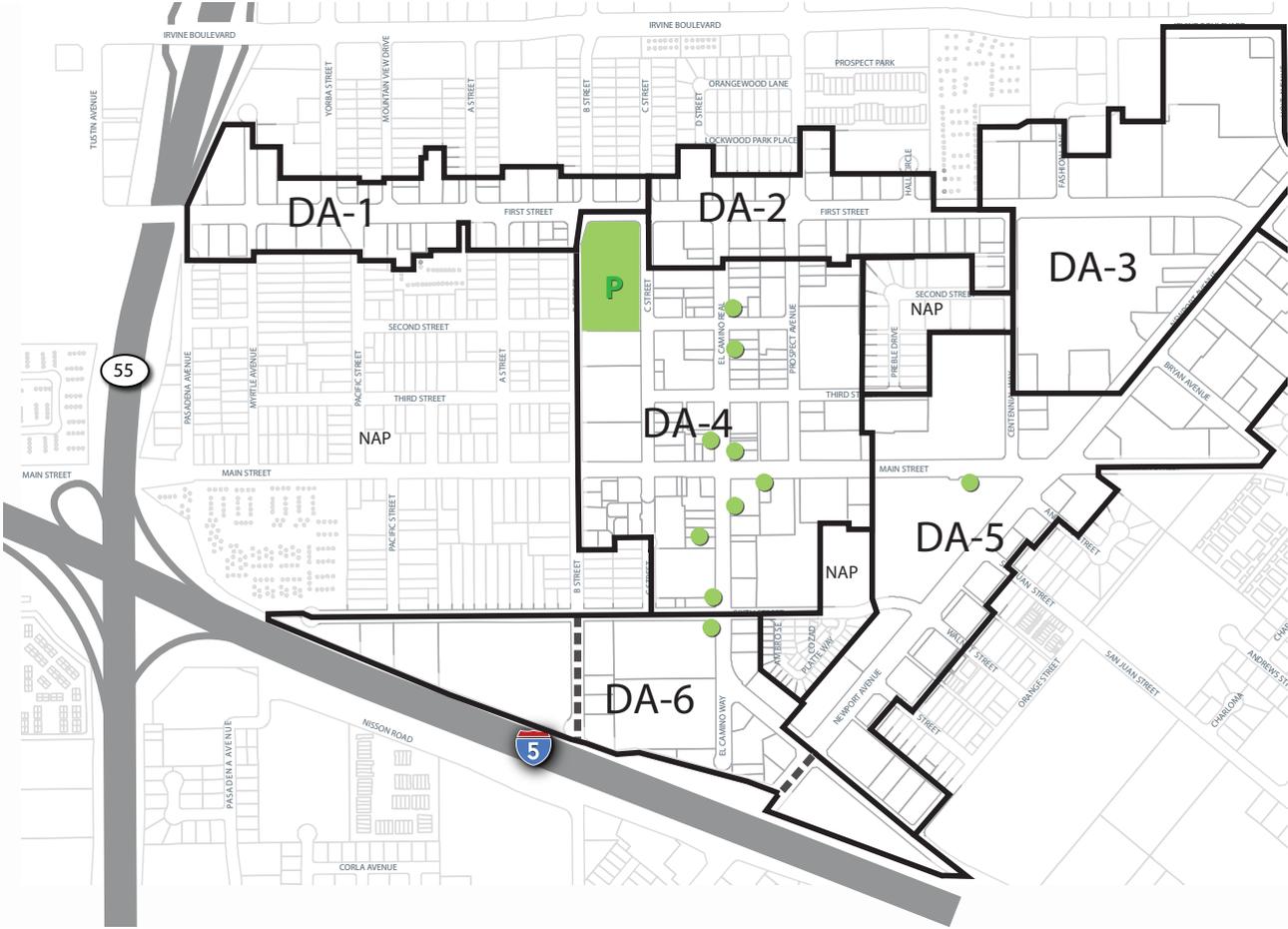
2.4.6.4 Living Alleys

Underutilized public alleys may be revitalized into “living alleys” to serve as community gathering places and extensions of nearby businesses. Enhancement of public alleys with decorative pavement, landscape, and pedestrian amenities such as decorative lighting, seating, and/or shade is encouraged, provided that it does not impact required parking spaces or interfere with travel ways. Living alleys are especially encouraged in DA-1, DA-2 and DA-4.

2.4.6.5 Public Spaces within Private Development

The provision of public gathering spaces and amenities is addressed through the Commercial Design Criteria in Chapter 4 and the Residential Mixed Use Design Criteria in Section 5.4.2. Though on private property, these spaces that are open to the public can contribute significantly to the level of activity and community interaction within the DCC.

Figure 2.16 Open Space



Legend

- P Peppertree Park
- Parklet (Conceptual Locations)

2.4.6.6 Social Activities within Outdoor Spaces

The DCC presents many opportunities to utilize outdoor spaces such as streets, alleys, parking lots, and undeveloped lots to further community activity, interaction, and vitality. These activities could also draw patrons to nearby shops and restaurants. Activities that serve recurring needs are especially encouraged in order to repeatedly and consistently draw in patrons, such as those revolving around fitness or eating. The DCC enjoys several community events including the weekly Farmer’s Market, monthly Jamestown Village Flea Market, and annual events such as the Chili Cook-Off, Garden Walk, and Sip and Stroll. Additional suggestions for consideration include:

- Parking lot fitness events
- Pet expo and costume contest
- Kids talent show
- Food truck nights
- Neighborhood book lending kiosk
- Community 4th of July bike parade
- 5k and 10k runs
- Street/alley markets





2.5 Infrastructure, Utilities and Servicing Plan

Properties in the DCC are served by domestic water, sanitary sewer, storm drain collection and conveyance; electric power, natural gas, and telecommunications infrastructure; and refuse and street sweeping services. The following section identifies the baseline (2016) infrastructure and assesses the potential to service the DCCSP area. For purposes of estimating demand for utilities, this Specific Plan is assumed to add 887 residential units and 300,000 square feet of non-residential uses to the Planning Area. This equates to 2,696 residents at build out and full occupancy; and 840 new employees. Please also refer to the Existing Conditions Report (Appendix B of this DCCSP) and the City of Tustin Water Supply Memo (Appendix F of the EIR).

2.5.1 Domestic Water

The City Water Division maintains the network of domestic water mains that serves the DCC. Potable water is supplied by the Metropolitan Water District and groundwater is pumped from the Santa Ana River Basin via 12 City-operated wells. The system provides sufficient service for both domestic water consumption and fire flow.

The additional residential development planned under the DCCSP will increase the demand for potable water. The City Water Department has analyzed the future increased demand and determined the available water supplies are sufficient to meet the demand. Based on the City's water use per day of 122 gallons per capita in 2015, the estimated 2,696 new residents and 840 new employees would generate an additional water demand of 431,392 gallons per day or 483.2 acre-feet per year (AFY). This additional demand is within the anticipated increase in demand and supply of water in the city, and build out of the Specific Plan would be served from existing water entitlements. Nevertheless, to minimize consumption and demand over time, all new developments will be required to install low-flow plumbing fixtures, plant drought-tolerant landscapes with low volume-low pressure irrigation, and pursue other water conservation measures. As new development occurs, smaller incremental extensions or duplications of distribution pipelines may be required on a project-by-project basis.

2.5.2 Sanitary Sewer

The plan area is served by a network of underground sewage collection lines, with trunk sewers owned and maintained by the Orange County Sanitation District (OCSD) and local sewers by East Orange County Water District (EOCWD). Sizes range from 6- to 27-inch diameter at the downstream confluence (Newport Avenue/I-5). Based on infrastructure assessments conducted during the existing conditions analysis for the DCCSP, it was determined the system is in good condition. Large diameter trunk lines cross the study area (Prospect - El Camino Real) offering a reliable platform for subsequent improvements as needed for new development.

From 2004 to 2006, OCSD conducted a series of sewer improvement projects within the Specific Plan boundary. Specifically, sewer lines were upsized to 27-inch lines along the south end of Newport Avenue, the south end of El Camino Way, and the west end of Sixth Street. These improvements, as well as all other lines within the Specific Plan boundary, were implemented to accommodate future growth projections and have sufficient capacity to handle the increased flows of the projected population growth related to the Specific

Plan. Nonetheless, additional collector sewers of 8-inch diameter may be needed as development progresses. Sewer capacity studies to ensure downstream capacity or to identify incremental “relief sewers” may be required at a project level.

2.5.3 Storm Water Management

The project area is served by a network of underground drainage pipes ranging in size from 18 to 66-inch diameter at the downstream confluence point (Newport Avenue/I-5). Catch basins and other structures are owned and maintained by the City and drain into Orange County Flood Control District (OCFCD) facilities downstream. Existing drainage infrastructure is able to provide reliable platforms for subsequent improvements as needed for new development.

The addition of mixed use development, including multi-family housing, office, and commercial retail uses, may require site-specific hydrology studies for each project to identify any associated storm drain improvements. With the implementation of local Low Impact Development (LID) ordinances and compliance with local storm water permits, storm flows are anticipated to remain relatively unchanged. Since the 220- acre area is already developed with an expanse of impervious surfaces, based on previous less stringent storm water standards that permitted more runoff, it is anticipated that the current storm drain system will accommodate future development.

New development and redevelopment throughout the Specific Plan area will require compliance with local Regional Water Quality Control Board (RWQCB) water quality control requirements (best management practices [BMPs] and LID facilities). The City has been progressive in implementing LID BMPs and has exceeded expectations within the county-wide Drainage Area Management Plan/Water Quality Management Plan standards. Several neighborhood-level opportunities exist in the DCC for larger and more effective biofiltration swales, dry wells, and rain gardens.

2.5.4 Electric Power, Gas, Telecommunications and Servicing

The Specific Plan area is served by Southern California Edison (SCE) for electric power, Southern California Gas (SCG) for natural gas, and AT&T and Cox Cable for telecommunications. Most systems are underground. Each service provider can upgrade distribution systems and site delivery metering on a project-by-project basis. Within the DCC single-family residential refuse and recycling collection service is provided by CR&R Incorporated using a three cart system for recyclables, green waste and refuse. Commercial businesses and multi-family residential dwellings, which are subject to phased requirements for recycling dependent on the amount and type of waste generated, may contract with CR&R for recycling services. Street sweeping within the DCC is performed by the City of Tustin four times per month on an assigned day of the week.



3

COMMERCIAL PERMITTED USES AND DEVELOPMENT STANDARDS

Ch. 3 Commercial Permitted Uses and Development Standards

INSIDE THIS CHAPTER

3.1 Commercial Permitted Uses

3.2 Special Use Restrictions

3.3 Commercial Development Standards by Development Area

This Chapter specifies the permitted non-residential uses within the designated land use categories and provides the commercial development standards for the DAs (except for DA-6A, which is addressed in Chapter 5, Residential Provisions, due to its Multi-Family Residential (MF) land use designation).

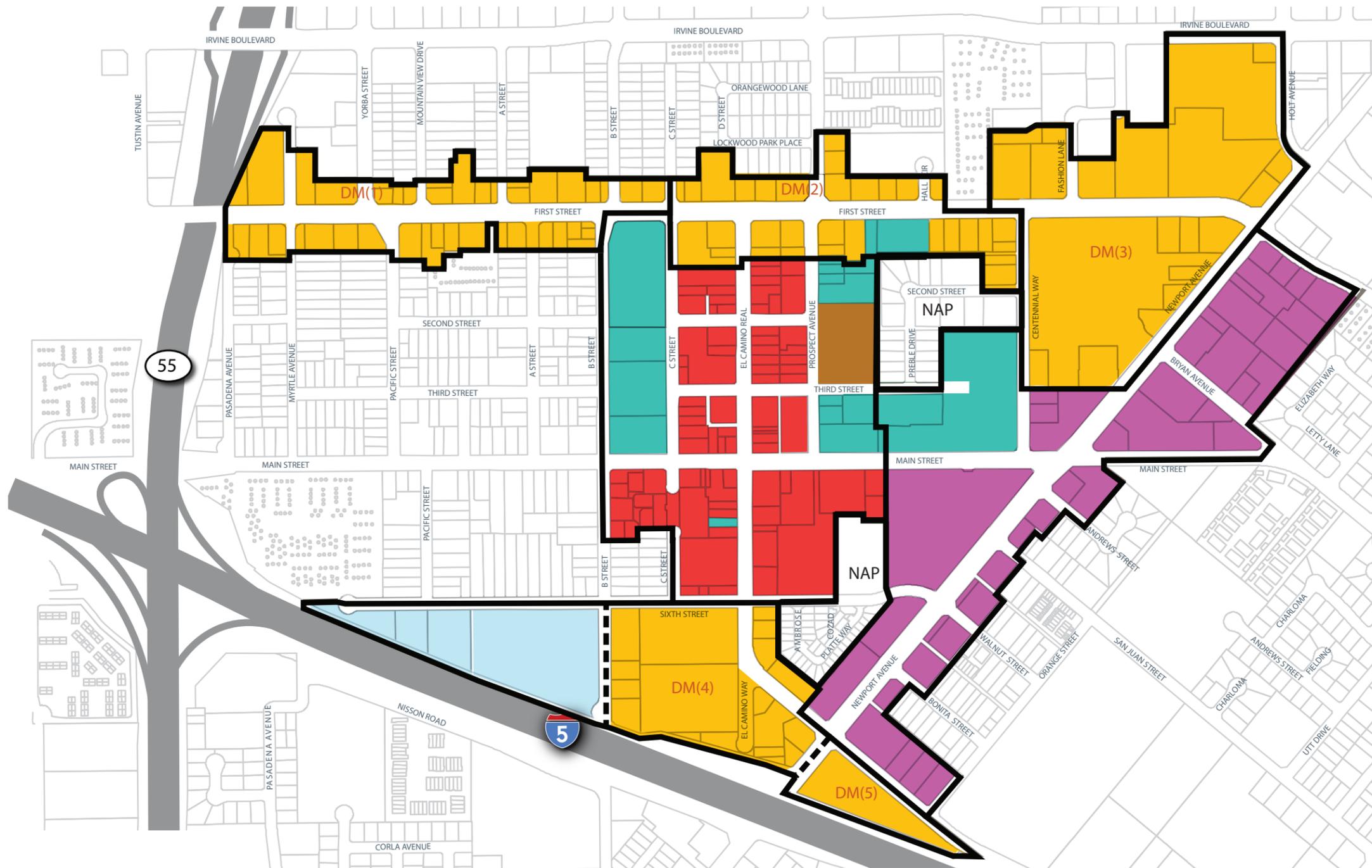
3.1 Commercial Permitted Uses

The DCCSP specifies six primary land use designations: Downtown Mixed Use (DM), Old Town (OT), Downtown Commercial (DC), Civic/Institutional (CI), Multi-Family (MF), and Mobile Home (MH). Downtown Mixed Use is further divided into five land use sub-designations based on characteristics including location, proximity to sensitive uses such as residential, and parcel size, which guide allowable uses. The land use designations are mapped in Figure 3.1, Land Use Designations.

Table 3.1, Commercial Permitted Use Table, organizes land uses under eight categories: Retail, Services, Automotive, Food Service, Hospitality, Medical, Secondary Retail and Service Uses, and Civic and Public Uses. (For residential mixed use and multi-family residential refer to Chapter 5, Residential Provisions, and Table 5.1, Residential Permitted Use Table.) Table 3.1 specifies individual uses within each category and indicates whether the uses are permitted by right, require a Conditional Use Permit, or are prohibited under the DCCSP land use designations and sub-designations.

- "P" indicates a permitted use.
- "C" indicates the use requires discretionary approval of a Conditional Use Permit (CUP) (refer to Chapter 6, Administration and Implementation Plan).
- "- -" indicates a prohibited use that is not allowed because it is deemed inconsistent with the intent of the DCCSP.

For Nonconforming Uses and Structures, refer to TCC Section 9273, Legal Nonconforming Structures and Uses. For Temporary Uses, see TCC Section 9270b, Regulations, Approval of Temporary Uses.



Legend

- Downtown Mixed Use (DM) including subareas DM(1) to DM(5)
- Downtown Commercial (DC)
- Mobile Home (MH)
- Multi-Family (MF)
- Old Town (OT)
- Civic/Institutional (CI)
- NAP = Not a part

Figure 3.1 Land Use Designations

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Table 3.1 – Commercial Permitted Use Table

LAND USE	DCCSP DESIGNATIONS										See foot-note
	DM					OT	DC	CI	MF	MH	
Corresponding DA Number	(1)	(2)	(3)	(4)	(5)						
	DA-1	DA-2	DA-3	DA-6B	DA-6C	Portion DA-4	Portion DA-5	Portion DA-2, 4, 5	DA-6A	Portion DA-4	
RETAIL											
Antique shops	P	P	P	P	P	P	P	--	--	--	
Apparel, shoes, and accessories stores	P	P	P	P	P	P	P	--	--	--	
Appliance stores	P	P	P	P	P	--	P	--	--	--	
Art galleries	P	P	P	P	P	P	P	--	--	--	
Bicycle shops including repair within enclosed buildings	P	P	P	P	P	P	P	--	--	--	
Bookstores	P	P	P	P	P	P	P	--	--	--	
Ceramic/pottery shops (no on-site manufacturing)	P	P	P	P	P	P	P	--	--	--	
Consignment shops	P	P	P	P	P	P	P	--	--	--	
Convenience markets	C	C	C	C	C	C	C	--	--	--	
Department stores	--	--	P	--	--	--	P	--	--	--	
Drug stores and pharmacies	P	P	P	P	P	P	P	--	--	--	
Electronics stores	P	P	P	P	P	P	P	--	--	--	
Florists	P	P	P	P	P	P	P	--	--	--	
Food specialty markets	P	P	P	P	P	P	P	--	--	--	
Furniture stores	P	P	P	P	P	P	P	--	--	--	
General retail stores	P	P	P	P	P	P	P	--	--	--	
Gift and collectible shops	P	P	P	P	P	P	P	--	--	--	
Hardware stores	P	P	P	P	P	P	P	--	--	--	
Hobby shops	P	P	P	P	P	P	P	--	--	--	
Household goods	P	P	P	P	P	P	P	--	--	--	
Jewelry stores	P	P	P	P	P	P	P	--	--	--	
Liquor stores	C	C	C	C	C	C	C	--	--	--	1
Micro-breweries or micro-winerries (retail sales with tasting only)	C	C	C	C	C	C	C	--	--	--	1
Music stores	P	P	P	P	P	P	P	--	--	--	
Nurseries and garden supply	P	P	P	P	P	P	P	--	--	--	
Office supplies	P	P	P	P	P	P	P	--	--	--	
Outdoor kiosks, outdoor sales/markets	C	C	C	C	C	C	C	C	--	--	1
Paint and wallpaper stores	P	P	P	P	P	--	P	--	--	--	
Pawn shops	--	C	C	C	C	--	C	--	--	--	
Pet stores including supplies	P	P	P	P	P	P	P	--	--	--	

LAND USE	DCCSP DESIGNATIONS										See foot-note
	DM					OT	DC	CI	MF	MH	
Corresponding DA Number	(1)	(2)	(3)	(4)	(5)	OT	DC	CI	MF	MH	
	DA-1	DA-2	DA-3	DA-6B	DA-6C	Portion DA-4	Portion DA-5	Portion DA-2, 4, 5	DA-6A	Portion DA-4	
Smoke shops	P	P	P	P	P	P	P	--	--	--	
Specialty stores	C	C	C	C	C	C	C	--	--	--	2
Sporting goods stores	P	P	P	P	P	P	P	--	--	--	
Supermarkets, grocery stores	P	P	P	P	P	P	P	--	--	--	
SERVICES											
Amusement, arcades, and private recreational facilities (which include video and vending machines or other such contrivances in excess of five which are identical to the principal business)	C	C	C	C	C	C	C	--	--	--	
Animal hospitals and clinics including boarding (indoor only)	P	P	P	P	P	--	P	--	--	--	1
Assisted living, rest homes, extended care facilities, convalescent hospitals	C	C	C	C	C	C	C	--	--	--	
Banks, financial institutions, credit unions	P	P	P	P	P	P	P	--	--	--	
Banquet and meeting facilities	C	C	C	C	C	C	C	C	--	--	3
Barber, beauty, and nail salons	P	P	P	P	P	P	P	--	--	--	
Bars and cocktail lounges	C	C	C	C	C	C	C	--	--	--	1
Body art facilities	P	P	P	P	P	P	P	--	--	--	4
Commercial recreation	C	C	C	C	C	C	C	--	--	--	
Day care centers (child or elder care)	C	C	C	C	C	C	C	C	--	--	1
Donation centers ancillary to retail businesses	--	--	C	--	C	--	C	--	--	--	
Dry cleaners, laundry services (drop-off/pick-up only)	P	P	P	P	P	P	P	--	--	--	
Entertainment venues including live music, karaoke music studios, comedy clubs, night clubs, dance clubs	C	C	C	C	C	C	C	--	--	--	
Fortune-telling businesses	C	C	C	C	C	C	C	--	--	--	5
Gyms, health clubs (indoor only)	C	C	C	C	C	--	C	--	--	--	
Instructional studios including painting, music, tutoring	P	P	P	P	P	P	P	--	--	--	
Laundromats (self-service only)	P	P	P	P	P	--	P	--	--	--	
Massage establishments	P	P	P	P	P	P	P	--	--	--	6
Mortuaries	--	--	--	--	--	C	C	--	--	--	
Offices including general and professional	P	P	P	P	P	P	P		--	--	2
Pet grooming shops	P	P	P	P	P	P	P	--	--	--	
Pet services including, veterinarian, doggie daycare, or overnight boarding (indoor only)	C	C	C	C	C	C	C	--	--	--	



LAND USE	DCCSP DESIGNATIONS										See foot-note
	DM					OT	DC	CI	MF	MH	
Corresponding DA Number	(1) DA-1	(2) DA-2	(3) DA-3	(4) DA-6B	(5) DA-6C	Portion DA-4	Portion DA-5	Portion DA-2, 4, 5	DA-6A	Portion DA-4	
Photography studios	P	P	P	P	P	P	P	--	--	--	
Postal and package delivery services	P	P	P	P	P	P	P	--	--	--	
Print and reprographic shops	P	P	P	P	P	P	P	--	--	--	
Reverse vending machines	P	P	P	P	P	--	P	--	--	--	1
Studios including dance, martial arts, pilates, yoga, fitness training	P	P	P	P	P	P	P	--	--	--	
Tailor and alteration shops	P	P	P	P	P	P	P	--	--	--	
Theaters including movie or indoor performance	--	C	C	C	C	C	C	--	--	--	
AUTOMOTIVE											
Automotive parts and accessories stores	P	P	P	P	P	P	P	--	--	--	
Automotive repair within an enclosed building, excluding body work	--	C	C	--	--	--	C	--	--	--	
Car washes including full service, drive-thru, and do-it-yourself	--	C	C	C	C	--	C	--	--	--	
Gas stations including convenience market	--	--	C	--	--	--	C	--	--	--	
Tire installation, smog check and oil-change facilities within an enclosed building, excluding body work	--	C	C	--	--	--	C	--	--	--	
FOOD SERVICE											
Bakeries, candy stores, donut shops, ice cream shops, yogurt shops	P	P	P	P	P	P	P	--	--	--	
Cafes, delicatessens	P	P	P	P	P	P	P	--	--	--	
Coffee and tea houses	P	P	P	P	P	P	P	--	--	--	
Restaurants	P	P	P	P	P	P	P	--	--	--	
HOSPITALITY											
Bed and breakfast establishments	C	C	--	--	--	C	--	--	--	--	
Boutique hotels	C	C	C	C	C	C	--	--	--	--	
Hotels, motels, extended stays	C	--	C	--	--	--	C	--	--	--	
MEDICAL											
Clinics for out-patients only, including medical clinics, healthcare centers, urgent cares	C	C	C	C	C	--	C	--	--	--	
Medical and dental offices, including labs	P	P	P	P	P	P	P	--	--	--	
Ophthalmologists and optometry services including eyeglass stores	P	P	P	P	P	P	P	--	--	--	
Physical therapy facilities	P	P	P	P	P	P	P	--	--	--	

LAND USE	DCCSP DESIGNATIONS										See foot-note
	DM					OT	DC	CI	MF	MH	
Corresponding DA Number	(1)	(2)	(3)	(4)	(5)	OT	DC	CI	MF	MH	
	DA-1	DA-2	DA-3	DA-6B	DA-6C	Portion DA-4	Portion DA-5	Portion DA-2, 4, 5	DA-6A	Portion DA-4	
Psychiatrists, psychologists, social workers, counselors and therapists offices	P	P	P	P	P	P	P	--	--	--	
SECONDARY RETAIL AND SERVICE USES											
Alcoholic beverage sales (on-site and off-site)	C	C	C	C	C	C	C	--	--	--	1
Drive-thru, drive-in, drive-up facilities	C	C	C	--	C	--	C	--	--	--	
Outdoor seating	P	P	P	P	P	P	P	--	--	--	7
CIVIC AND PUBLIC USES											
Assembly uses	C	C	C	C	C	C	C	C	--	--	3 & 8
Civic centers, community centers, senior centers	--	--	--	--	--	--	--	P	--	--	3
Fraternal organizations, lodges, clubs	C	C	C	C	C	C	C	C	--	--	3
Governmental and public administration offices	--	--	--	--	--	--	--	P	--	--	
Libraries	--	--	--	--	--	--	--	P	--	--	3
Museums and cultural facilities	C	C	C	--	--	C	--	P	--	--	3
Outdoor amphitheaters or performance facilities	--	--	--	--	--	--	--	P	--	--	3
Post offices	--	--	--	--	--	--	--	P	--	--	
Parking lots and structures (commercial)	C	C	C	C	C	C	C	C	--	--	
Parking lots and structures (public)	P	P	P	P	P	P	P	P	--	--	
Public parks, playgrounds, recreation centers, open space	--	--	--	--	--	--	--	P	--	--	
Schools (private) including pre-schools, nursery schools, vocational schools	C	C	C	C	C	C	C	--	--	--	
<p>* Key:</p> <p>P Permitted Use requires Design Review in compliance with Chapter 6 for new construction</p> <p>C Requires discretionary approval of a Conditional Use Permit in compliance with Chapter 6</p> <p>-- Prohibited Use</p> <p>Footnotes:</p> <ol style="list-style-type: none"> Reference TCC Section 9271 for specific provisions; however, for alcoholic beverage sales establishments, distance requirements from residential uses shall not apply within the DCCSP. Reference TCC Section 9297 for definition. If development is public/government, it is permitted (P); if private, it is conditional (C). Reference TCC Section 3141 for definition and Article 4, Chapter 1, Part 4 for standards. Reference TCC Section 3141 for definition. Reference TCC Section 3141 for definition and Article 3, Part 6, Massage Establishments, for standards. Reference TCC Section 9277 for outdoor seating provisions under general regulations. Reference TCC Section 9245b1. 											

3.2 Special Use Restrictions

3.2.1 Historic Residential Structures within DA-1 and DA-4

Existing single family residential structures listed within the City's official historic survey are encouraged to be preserved and used as a residence, or preserved and used as a commercial use. Historic residential structures are subject to the following provisions:

1. Residential use of a historic single-family residential structure is consistent with the City's Cultural Resources District Residential Design Guidelines and the Secretary of the Interior's Standards for Rehabilitation, and is a permitted use as specified under the land use designation in which the structure is located, as shown in Table 5.1, Residential Permitted Use Table.
2. Commercial use of historic single-family residential structures is a permitted use as specified under the land use designation in which the structure is located, as shown in Table 3.1, Commercial Permitted Use Table. Commercial use of an existing historic single family residential structure shall not be changed to another commercial use (e.g., office to retail) without the prior approval of the Community Development Department.
3. Historic single-family residential structures lawfully converted to commercial use are considered more conforming and cannot be converted back to residential use and must thereafter be maintained as a commercial use.

3.2.2 Uses within Planned Community Designations

All existing Planned Communities (PCs) within the DCC will receive the applicable DCCSP land use designation. To facilitate implementation of the DCCSP, when new development on former PC designated parcels occurs, the proposed development shall conform to the requirements of the DCCSP. However, the Prospect Village live/work project located at the northwest corner of Prospect Avenue and Main Street was approved by Ordinance 1361, which shall be retained to identify the commercial uses permitted within this development. Through Ordinance 1361 this property will retain the PC standards approved as part of the project and if damaged by a natural occurrence, the development may be rebuilt consistent with the original approved PC provisions, or may redevelop consistent with the DCCSP provisions.

3.3 Commercial Development Standards by Development Area

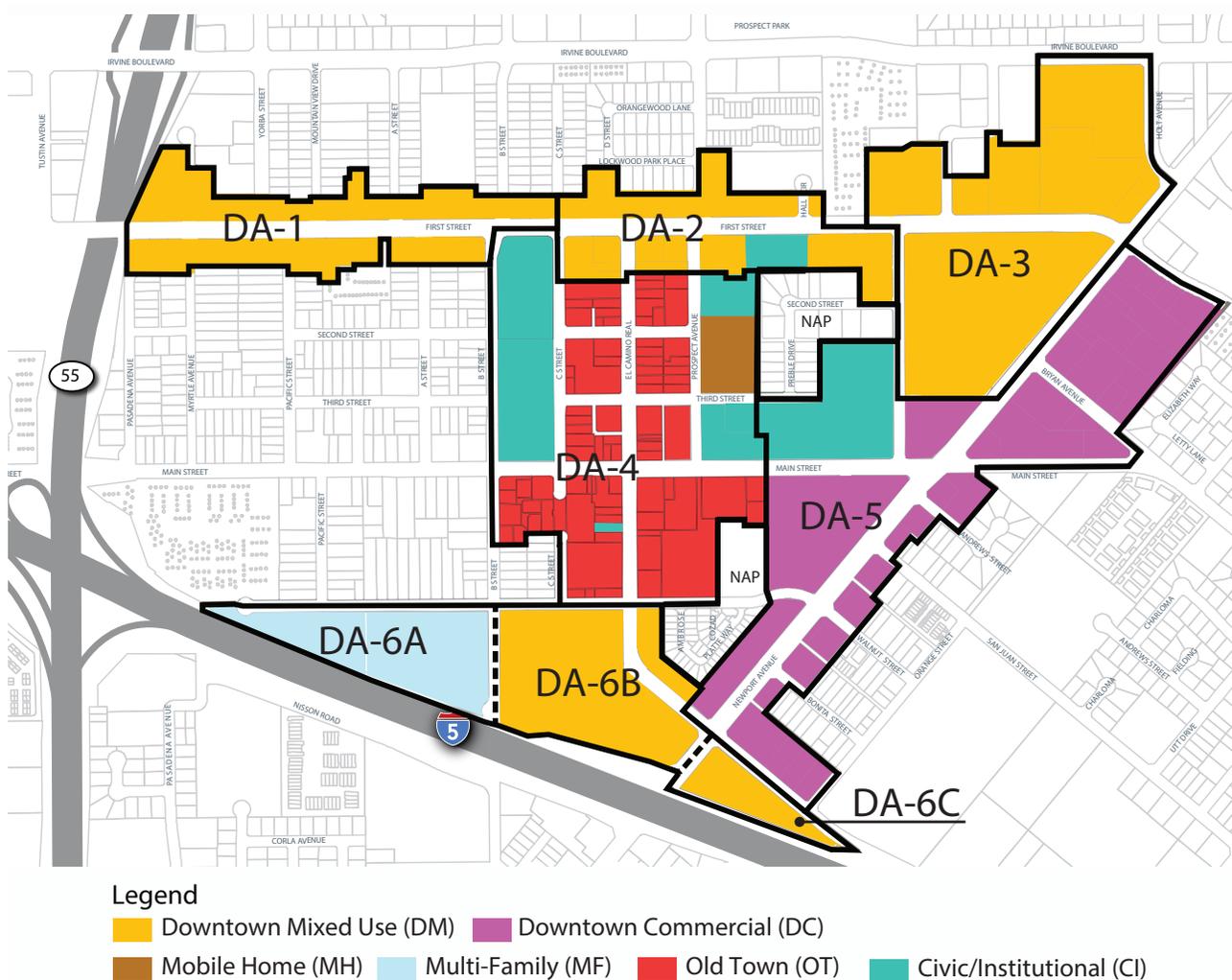
The DCCSP commercial development standards reflect the urban design vision for the six DAs, as discussed in Section 2.2, Urban Design Plan. Development standards, including building setback, height, tenant space size, parking, signage, landscaping and open space are provided in Tables 3.2 through 3.4.

Building setbacks specified in Table 3.2 through Table 3.4 shall be measured as the perpendicular distance from the ultimate street right-of-way line, or property line, to the exterior wall of the closest structure.

For standards not specifically addressed in the DCCSP, the provisions of the TCC apply.

The Commercial Design Criteria in Chapter 4 present additional design-related provisions through text and photographic examples.

Figure 3.2 Development Areas (DAs) and Land Use Designations



**Table 3.2 – Development Standards for Commercial Buildings within
DA-1, DA-2, DA-3, DA-6B & DA-6C**

COMMERCIAL BUILDINGS		
Development Standards for DA-1, DA-2, DA-3, DA-6B & DA-6C		
<p>These Development Standards shall apply to all commercial buildings (for retail, service, secondary retail/service, automotive, food service, hospitality, medical, or civic/public uses) within DA-1, DA-2, DA-3, DA-6B and DA-6C. See also Figure 3.3, Preferred and Discouraged Building Siting and the Commercial Design Criteria in Chapter 4.</p>		
Commercial Development Standards	DA-1 & DA-2	DA-3, DA-6B & DA-6C
Building Setbacks¹		
Front setback ²	0 feet minimum	
Interior side setback From adjacent residential zone From adjacent non-residential zone	5 feet minimum 0 feet minimum	
Corner side setback	0 feet minimum	
Rear setback From adjacent residential zone From adjacent non-residential zone or alley	10 feet minimum 0 feet minimum	
Building Height (Parking structures below buildings, rooftop uses (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	3 stories maximum First floor plate 16 foot minimum	4 stories maximum (5 stories maximum adjacent to freeway) First floor plate 16 foot minimum
Tenant Space Maximum Size	10,000 square feet (applies to DA-1 only)	none
Parking Spaces	Per Tustin City Code Also see Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards	
Signage	Per Chapter 4, Commercial Design Criteria and TCC	
Landscaping	Per Chapter 4, Commercial Design Criteria and TCC	
Public Open Space	Per Chapter 4, Commercial Design Criteria	
<p>Note:</p> <ol style="list-style-type: none"> Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure. Parking shall not be permitted in between the street and the front of the building. 		

Table 3.3 – Development Standards for Commercial Buildings within DA-4

COMMERCIAL BUILDINGS Development Standards for DA-4	
<p>These Development Standards shall apply to all commercial buildings (for retail, service, secondary retail/service, food service, hospitality, medical, or civic/public uses) within DA-4. See also Figure 3.3, Preferred and Discouraged Building Siting and the Commercial Design Criteria in Chapter 4.</p>	
Commercial DA-4 Development Standards	
Building Setbacks¹	
Front setback ²	0 feet minimum
Interior side setback From adjacent residential zone From adjacent non-residential zone	10 feet minimum 0 feet minimum
Corner side setback	0 feet minimum
Rear setback From adjacent residential zone From adjacent non-residential zone or alley	10 feet minimum 0 feet minimum
Building Height (Parking structures below buildings, rooftop uses (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	3 stories maximum First floor plate 16 foot minimum
Tenant Space Maximum Size	10,000 square feet
Parking Spaces	Per Tustin City Code Also see Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards
Signage	Per Chapter 4, Commercial Design Criteria and TCC
Landscaping	Per Chapter 4, Commercial Design Criteria and TCC
Public Open Space	Per Chapter 4, Commercial Design Criteria
<p>Note:</p> <ol style="list-style-type: none"> Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure. Parking shall not be permitted in between the street and the front of the building. 	



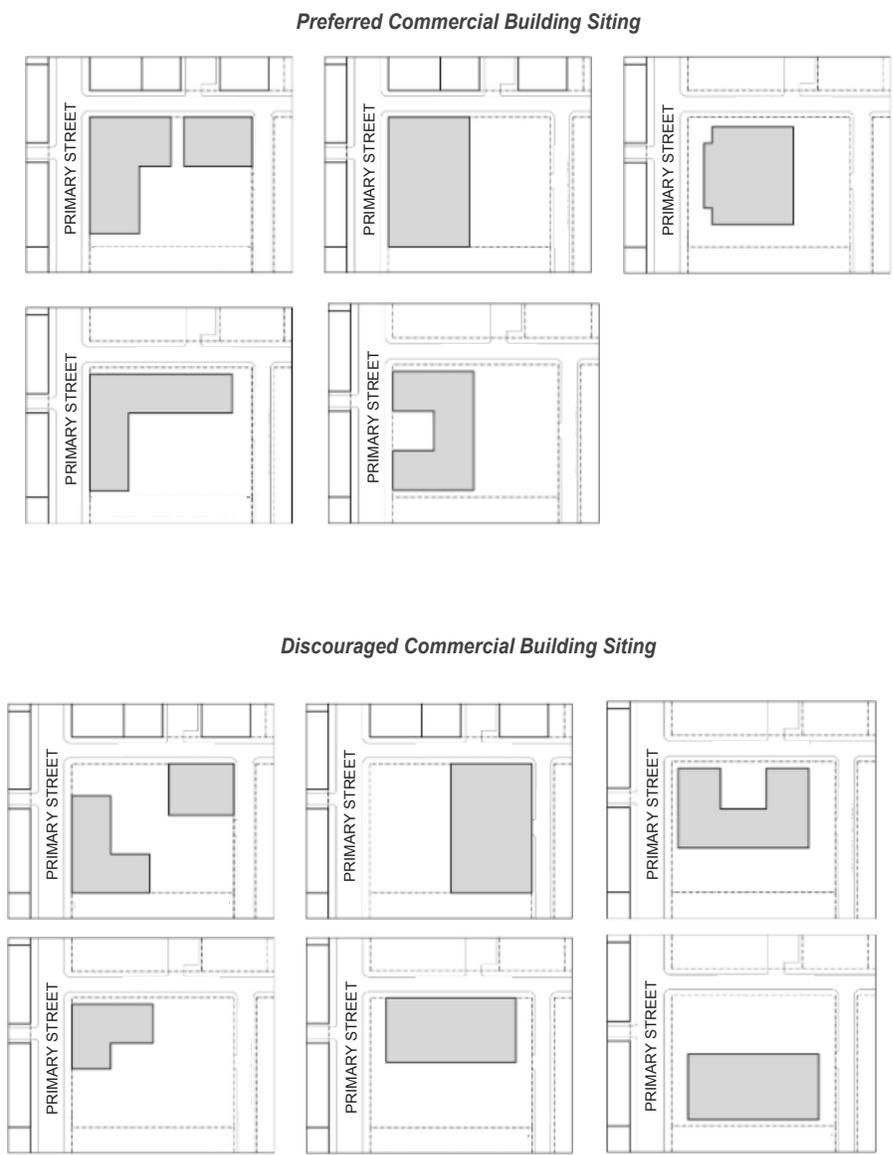
Table 3.4 – Development Standards for Commercial Buildings within DA-5

COMMERCIAL BUILDINGS Development Standards for DA-5	
<p>These Development Standards shall apply to all commercial buildings (for retail, service, secondary retail/service, automotive, food service, hospitality, medical, or civic/public uses) within DA-5. See also Figure 3.3, Preferred and Discouraged Building Siting and the Commercial Design Criteria in Chapter 4.</p>	
Commercial DA-5 Development Standards	
Building Setbacks¹	
Front setback	0 feet minimum
Interior side setback	0 feet minimum
Corner side setback	0 feet minimum
Rear setback From adjacent residential zone From adjacent non-residential zone or alley	15 feet minimum 5 feet minimum
Building Height (Parking structures below buildings, rooftop uses (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	4 stories maximum First floor plate 16 foot minimum
Parking Spaces	Per Tustin City Code Also see Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards
Signage	Per Chapter 4, Commercial Design Criteria and TCC
Landscaping	Per Chapter 4, Commercial Design Criteria and TCC
Public Open Space	Per Chapter 4, Commercial Design Criteria
<p>Note: 1. Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure.</p>	

3.4 Commercial Siting Diagrams

The diagrams in Figure 3.3, Preferred and Discouraged Building Siting, expand on the Commercial Development Standards presented in Tables 3.2 through 3.4, which generally avoid stringent setback requirements to allow flexibility instead and an emphasis on form. These diagrams illustrate preferred commercial building siting and discouraged building patterns relative to street location. The diagrams apply to commercial development in all DAs, except DA-6A, as commercial use is not a permitted use in this DA. These diagrams may be more loosely applied in DA-5 to take into consideration its established auto-oriented pattern.

Figure 3.3 Preferred and Discouraged Building Siting





4

COMMERCIAL DESIGN CRITERIA

Ch. 4 Commercial Design Criteria

4.1 Introduction

The Commercial Design Criteria implement the goals and vision for the Downtown Commercial Core (DCC) by providing design direction for new commercial development to shape and enhance the evolving character of private property within the DCC. The Commercial Design Criteria communicate to developers, builders, architects, engineers, and landscape architects how to design commercial sites and buildings to be consistent with the DCCSP.

The Commercial Design Criteria ensure quality developments that build upon existing positive attributes and capitalize on opportunities to improve the built environment within the DCC. These Commercial Design Criteria address architectural style, building design, provisions specific to each Development Area (DA), and general design criteria for topics including signs, parking, landscape, and lighting.

While the Commercial Development Standards contained in Chapter 3 provide regulations for the DCC that are numerically based (such as setback and height), the Commercial Design Criteria in this Chapter communicate and define additional regulations through words and photographic examples. The Commercial Design Criteria impart clear direction, yet allow sufficient flexibility to permit creativity and innovation. Compliance with these Commercial Design Criteria as well as the Urban Design Plan (Chapter 2) and the Commercial Development Standards (Chapter 3), are prerequisites to submittal of commercial development applications for property within the DCC.

Within the following Commercial Design Criteria:

- “Shall” indicates a mandatory provision.
- “Should,” “recommended,” “encouraged” or like terms indicate design that is promoted but not required.
- “May” indicates design that is permitted but not required.
- “Prohibited” indicates design that is not allowed.
- Photographic illustrations are offered as a general visual aid and are not meant to dictate the precise design.

INSIDE THIS CHAPTER

4.1 Introduction

4.2 Architectural Styles

4.3 Building Design

4.4 Development Area Design Criteria

4.5 General Design Criteria



For projects located in Old Town (DA-4) or involving identified historic resources in any DA, including preservation and rehabilitation, additions, or adaptive reuse, please also refer to the adopted Cultural Resources District -

Commercial Design Guidelines (CRD-CDG) and Cultural Resources District – Residential Design Guidelines (CRD-RDG). Also refer to the CRD-CDG for detailed provisions regarding outdoor restaurant seating along public sidewalks, which by this reference shall serve as the design guidelines for outdoor dining within the public right-of-way for all property in the DCC. The CRD-CDG concepts pertaining to Infill Development have been incorporated into the provisions of these Commercial Design Criteria and, therefore, shall apply to all commercial development within the DCCSP.

4.2 Architectural Styles

A building's architectural style is fundamental to its overall appearance and is the primary design element for creating an identifiable DCC.

KEY COMPONENT

**Preserve
Old Town**

4.2.1 Existing Historic Architectural Styles

The following subsections describe the historic buildings within Old Town Tustin including the origin of these architectural styles and their defining architectural characteristics. Photographs illustrate both historic and contemporary examples of these styles.

A. Neo-Classical (1900-1925)

This period emphasized the simplicity and purity of Greek and Roman classical architecture. Flat rooflines feature generally formal façades, incorporating full height columns with decorative capitals, keystones, symmetrical windows, and horizontal ornamentation. Often classical porticos are used to add balance to the very formal symmetry of the style. The exterior materials include concrete, plaster, stone, light cream colored brick, and sometimes decorative terra cotta.

B. Victorian (1880-1930)

One of the most prevalent subtypes during this time was Italianate Victorian. Buildings of this style are generally one to two stories, typically with a flat roofline and projecting cornices as well as decorative



NEOCLASSICAL



VICTORIAN



SPANISH COLONIAL REVIVAL



modillions or brackets. This style also features symmetrical windows, horizontal ornamentation, exterior wood siding, and a central entrance flanked by large display windows.

C. Spanish Colonial Revival (1915-1930)

This style is based on the architecture associated with the Spanish colonization of the Americas. It is characterized by plaster or stucco exterior finishes, clay tile, shed or flat roofs, and terracotta or cast concrete ornaments. Buildings typically exhibit horizontal massing, asymmetrical shape, and a rectangular, courtyard, or L-shaped building layout. Other features may include small porches or balconies, arcades, double-hung windows, awnings, or iron trim.

4.2.2 Architectural Styles for New Development

The architectural heritage of Old Town is an asset that the community wishes to retain and promote. Use of an identifiable palette of architectural styles helps to create visual harmony and promotes an identifiable image within the DCC.

New development within the DCC shall be based on the historic architectural styles found in Old Town, as identified in Section 4.2.1, Existing Historic Architectural Styles, subject to the following provisions.

1. DA-1, DA-2, DA-3, DA-5, and DA-6

New buildings within DA-1, DA-2, DA-3, DA-5, and DA-6 may utilize the styles identified in Section 4.2.1, Existing Historical Architectural Styles, or a contemporary interpretation. New buildings within these DAs need not exhibit a majority of the identified character defining features.

2. DA-4

New buildings within DA-4 shall employ one of the architectural styles identified in Section 4.2.1, Existing Historic Architectural Styles, to maintain continuity with the Old Town historic buildings. New buildings within DA-4 shall exhibit a majority of the character defining features identified in Section 4.2.1 for the corresponding style.

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4.3 Building Design

The Commercial Design Criteria contained in the following subsections apply to all commercial development within the DCC, addressing building form, mass, scale, roof forms, architectural details, windows and doors, colors, materials, and service areas to ensure development is high quality, visually appealing, and functional. These concepts apply to all types of architectural styles, whether close interpretations of historic styles or more contemporary adaptations for properties outside of Old Town (DA-4). The context of certain DAs inspires particular types of architectural details, in which case these location-sensitive provisions have been addressed within the Development Area Design Criteria in Section 4.4.

4.3.1 Building Form, Mass, and Scale

Building form, which refers to the shape or configuration of a building, is a key architectural component in creating visually-appealing development. Mass and scale create a relationship with the surrounding area and greatly influence viewer perception of a building.

- 1. Comparable to nearby buildings.** A proposed building's volume, proportion, and scale should be comparable to nearby existing buildings.
- 2. Complement architecture.** Building form shall complement the architectural style employed.
- 3. Articulated.** Buildings shall be vertically and horizontally articulated by varying mass and form to minimize the appearance of scale and avoid monotonous, box-like facades. Upper floors are encouraged to be stepped back to reduce the appearance of building scale.
- 4. Breaks in roofline.** Interesting building forms shall be created by designing breaks in the roofline.



This circular roof feature creates a visual focal point.

4.3.2 Roof Forms

The roofline is a significant component of building composition and creates an interface with the building façade.

- 1. Consistent with architectural style.** Roofing forms, style, pitch, and details shall be consistent with the selected architectural style.
- 2. Roof materials.** Roof materials shall create continuity in texture, color, and character with the architectural style.
- 3. Building mass.** Varying rooflines and offsets to roof planes shall be employed to create interesting building mass.
- 4. Overhangs.** Overhanging roofs should extend well past the building façade to create shadows and depth



This tower roof element is consistent with the form and detail employed in the rest of the building.

4.3.3 Architectural Details

Architectural details add visual interest to a building and avoid a monotonous, unarticulated façade. These features also highlight significant elements of a building such as the entry.

- 1. Details on all elevations.** Buildings shall employ architectural details on all elevations to ensure attractive, high quality design. Alternatively, a living landscape wall may be used on one or more elevations.



A living landscape wall adds embellishment to a façade.

- 2. Compatible with architectural style.** Architectural details shall be compatible with the architectural style of the building. Architectural details may vary depending on their function and location on the building, but should be in harmony with each other.
- 3. Choice of three elements.** Visually interesting buildings shall be ensured through use of at least three of the following design elements on each façade (each façade need not employ the same design elements):
 - Material change
 - Texture change
 - Color change
 - Expression lines (such as cornices)
 - Recessed windows and doors
 - Expansion joints
 - Decorative woodwork or ironwork
 - Piers, columns, or pilasters
 - Projecting element such as colonnade, arcade, bay, or tower



Changes in materials, textures, and colors create visual interest.



Cornice lines add detail.



Regularly spaced expansion joints form patterns on the building facade.



Ironwork elements are an appropriate way to add architectural detail especially in the Spanish Colonial Revival style.



An example of an arcade.



An awning above a doorway highlights the building entry.



A portico creates a covered entry and design focus.

4. Building entries. In order to provide visual relief and human scale, building entries shall feature at least one of the following:

- Single-story entry element (such as a portico or covered porch)
- Architectural projection at entry
- Distinct roof element above entry
- Entry awning

4.3.4 Windows and Doors

Window and door details create a strong visual impact through their placement and design. The appropriate proportion of windows and doors to wall massing varies according to the architectural style.

- 1. Consistent with building architecture.** The configuration, placement and style of windows and doors, including patterns, materials, finishes, and colors, shall be consistent with the building architecture.
- 2. Patterns.** The pattern of openings shall correspond with the overall rhythm and proportion of the building.
- 3. Views into stores.** In pedestrian oriented areas, windows shall be placed and sized to create views into stores from nearby walkways or gathering areas. Windows shall feature attractive displays or well-designed views into the stores. Vertical blinds and closed curtains shall be prohibited for first floor businesses located in pedestrian-oriented areas.
- 4. Window materials.** Window frame materials including factory-painted, extruded aluminum, hollow steel frame, and wood are recommended.



The window patterns are key to this building's design.



Windows with attractive, eye-catching displays make pedestrian-friendly streets.

Multi-paned windows should be composed of true divided lights or dimensional surface muntins applied on the exterior side of the windows. Simulated between-the-glass muntins are strongly discouraged.

5. Defined entrances. Front doors and entrances to buildings should be clearly defined and articulated by awnings, overhangs, projections, or canopies so they are easily recognizable from pedestrian and vehicular vantage points. All patron or resident entries shall be visually attractive.



An overhang emphasizes the building entry.

6. Recesses and trim. Windows and doors shall not be flush with the building wall without trim. Instead, windows and doors should be slightly recessed and feature trim to emphasize the mass and integrity of walls and enhance the visual play of light and shadow. Additionally, projecting bay or display windows may be used.

7. Awnings encouraged. The use of awnings is encouraged, especially on pedestrian-oriented buildings.

8. Awning design. If awnings are employed, they shall feature design, materials, and colors that complement the building. The awning should be located within the individual structural bay and should not hide architectural detailing. If used on buildings with multiple storefronts, the



Consistently designed awnings make a strong statement.

same style, material, and color should be provided for each. Awnings shall be constructed of quality fabric that resists fading, mildew, and water degradation. The use of vinyl and plastic awnings shall be prohibited. If lighting is used, it should consist of decorative fixtures placed above the awning.

- 9. Frames.** Modular storefront systems and window frames shall complement the architectural style, building materials, and colors. Shiny silver or gold door or window frames and reflective glass shall be prohibited.



The window and door frames on this restaurant are coordinated with the overall color scheme to make a strong statement.

4.3.5. Materials and Colors

Visual harmony is created when the palette of materials and colors used on nearby buildings is complementary. However, sufficient variation in materials and colors is necessary to create visual interest.

- 1. Harmony with architecture.** A building's materials and colors shall be in harmony with its architecture.
- 2. Provide ground floor interest.** On the ground floor within pedestrian-oriented areas, materials should be composed and detailed to provide visual interest.
- 3. Combining materials.** The combination of two or more materials on one façade is encouraged to create texture; but heavier, more substantial materials should be located below lighter materials.
- 4. Material changes.** Changes in material shall only occur at changes in plane.
- 5. Preferred materials.** Traditional materials including brick, stone, and wood are encouraged. Highly reflective and mirrored surfaces shall be prohibited in DA-4 (except glass used for windows in a traditional manner). Traditional materials mixed with contemporary materials may be used in DA-3, 5 and 6C.
- 6. Materials to avoid.** Faux, fake, synthetic, or foam materials are discouraged. Materials should reflect their own identity rather than imitate other materials. Materials should have a long life and age well.
- 7. Graffiti coatings.** Graffiti-resistant paints and coatings shall be used when appropriate to the finish material.
- 8. Color variety.** A variety of complementary colors or shades shall be utilized to visually break up facades.
- 9. Three-color scheme.** Building color schemes should focus on three colors: base color, major trim color, and minor trim color.
- 10. Color families.** Recommended color families for building walls include: 1) subtle, warm earth tones and natural shades, 2) neutral color palettes, 3) white or off-whites, and 4) red, brown, tan or terra cotta colored brick.
- 11. Wall colors.** Overly bright colors, patterns, or stripes shall be avoided on building walls.

Overly bright building wall colors and garish accent hues are not appropriate in the DCC.



- 12. Accents.** Accent elements may be bolder in color, but should avoid garish hues.
- 13. Paint to conceal.** Service doors and mechanical screens shall be painted the same color as the adjacent wall.
- 14. Site furniture materials and colors.** The materials and colors of site furnishings including light fixtures, seating, umbrellas, trash/recycling receptacles, and bollards shall be coordinated with the surrounding architecture.

4.3.6 Service Areas

Service areas include loading areas, loading doors, ground or wall-mounted equipment, rooftop equipment, and refuse collection areas. The placement and design of these elements shall minimize their prominence when viewed from surrounding development, nearby roof tops, streets, pedestrian walkways, gathering areas, and parking lots.

- 1. Delivery/loading areas.** Delivery vehicles shall have clear and convenient access to delivery/loading areas to minimize disruptions to business patrons and traffic circulation. Delivery/loading areas shall be separated from general parking areas and pedestrian walkways. Loading doors, service areas, and equipment areas shall be oriented or screened so they are not easily visible from residential areas and public viewsheds. Screening shall be accomplished with landscaping, walls, fences, or other architectural treatments. To be effective, a screening height of six feet is recommended. Delivery/loading areas that are visible from customer parking lots shall be maintained and kept clean.



2. Ground equipment. Ground-mounted equipment shall be screened to limit visibility from residential areas and public viewsheds. Screening may be accomplished with walls that are designed as an integral part of the architecture or landscape elements that are consistent with the landscape concept.

3. Electrical rooms. Electrical equipment rooms shall be located within the building envelope. Pop-outs or shed-like additions shall be prohibited unless fully integrated into the building architecture.

4. Wall-mounted equipment. Wall-mounted items such as roof ladders or electrical panels shall be located away from the street façade and painted, screened, or incorporated into the architectural elements of the building so they are not visually apparent from residential areas and public viewsheds.

5. Downspouts and gutters. Downspouts and rain gutters should be incorporated within the building design or placed out of public view when feasible. If they are located in a visible area, their design should complement the style of the building.



Visible downspouts should reflect the building's architectural style.

6. Rooftop equipment. Rooftop equipment, including mechanical equipment, electrical equipment, skylights, vents, exhaust fans, and mechanical ducts, shall be designed so they are not visible from residential areas and viewsheds. Rooftop equipment screens shall be provided and shall be integrated into and complement the architecture of the building.

7. Trash enclosures. Refuse collection areas (trash enclosures) shall be located away from residences whenever possible to minimize nuisance. In addition, refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance. Refuse containers shall be screened within a permanent, durable roofed enclosure. The design of the enclosure shall complement the architectural style of adjacent buildings and use similar, high-quality materials. Landscaping or trellis work shall screen enclosures visible from residential areas, walkways, or streets and shall be permanently maintained.



Trash enclosures should use traditional materials where feasible to complement the building.

4.4 Development Area Design Criteria

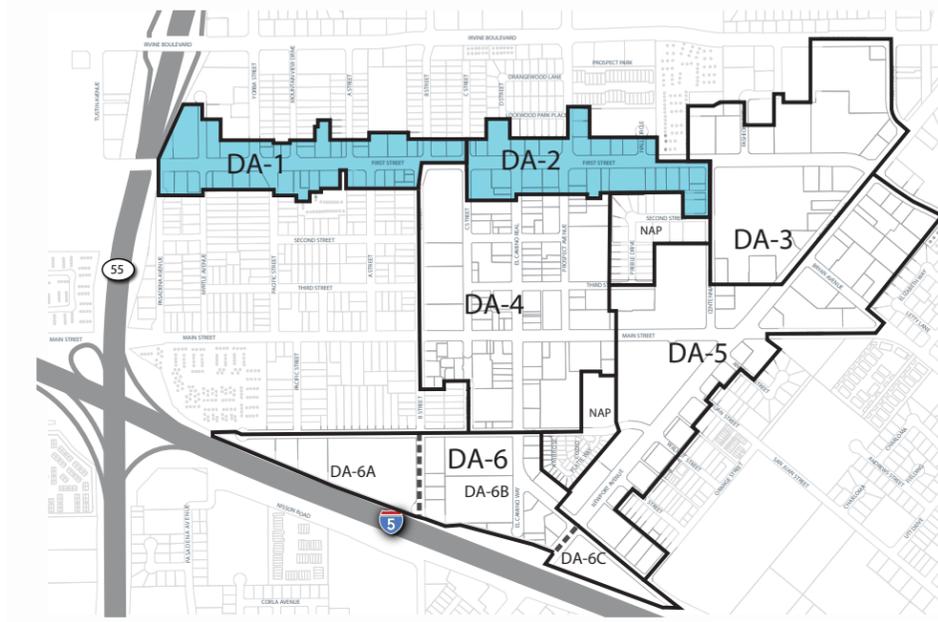
The following subsections are customized to particular DAs within the DCC to ensure that new development is consistent with the goals outlined in Chapter 1, Introduction, and the concepts presented in Chapter 2, Development Plan. For each DA or group of DAs, first a collage of photo images illustrates the vision for new development. Second, detailed design criteria are provided, which address the context of the built environment for the DAs and capitalize on opportunities such as existing and planned infrastructure and resources in the design of new development. (DA-6A is addressed in Chapter 5, Residential Provisions, since its land use designation is multi-family residential).

KEY COMPONENT

Pedestrian Orientation

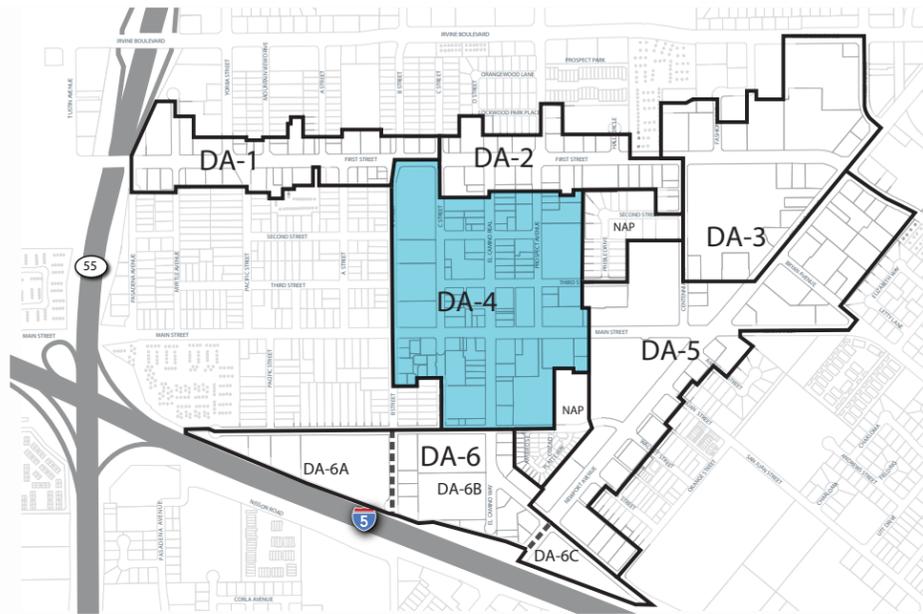
4.4.1. Design Criteria Specific to DA-1, DA-2, and DA-4

The photos on this page provide examples of the vision for new development within DA-1 and DA-2, consistent with the Design Criteria for this subsection.





The photos on this page provide illustrative examples of the vision for new development within DA-4, consistent with the Design Criteria for this subsection.



This section addresses criteria for DA-1, DA-2 and D-4 to both maintain and further promote pedestrian orientation. Within DA-1 and DA-2, pedestrian friendly enclaves are fostered along First Street, further facilitated by planned street improvements to reduce vehicular travel lanes, enhance pedestrian and bicycle use and provide diagonal parking (for a detailed description refer to Section 2.3.1, Vehicular Circulation Plan). Within DA-4 the following criteria are intended to preserve and enhance the pedestrian-oriented street scene in Old Town by continuing the pattern established at the intersection of Main Street and El Camino Real.

1. Building siting. Buildings shall face the street and be sited adjacent to the sidewalk or immediately behind a public or semi-public pedestrian space, such as an outdoor seating area, patio, or courtyard, creating a continuous pattern to promote pedestrian orientation and an active street edge. Buildings shall be oriented parallel to lot lines.



Buildings may be set back from the street to provide a gathering or seating area.

2. Strip malls prohibited. Linear strip malls with parking in front shall be prohibited.

3. Avoid building gaps. Gaps or voids between buildings resulting from building side setbacks should be avoided whenever possible. Where breaks cannot be avoided, such as for access driveways to rear parking, such gaps shall be enhanced with decorative paving, landscaping, and pedestrian-oriented lighting.



Buildings shall be placed near the corner to create a strong street edge.

4. Corner placement. On corner parcels, buildings shall be placed at the corner adjacent to the sidewalk with building entry from either the primary street, secondary street, or the corner.

5. Entry along street. Active commercial uses and building entries should be located along the primary street at the ground-floor level. Buildings may be designed to provide access for patrons from both the street and rear of the building.

6. Design of dual entries. If a building includes a rear entry as well as a street entrance, the rear entry shall be designed to be aesthetically pleasing through the use of architectural accents, materials, colors, landscaping, and/or pedestrian oriented lighting.

7. Access. Buildings should be designed to facilitate access to transit and bike facilities.

8. Roof design. Interesting building forms shall be created by designing breaks in the roofline or using multi-planed pitched roofs.



This building uses multi-planed pitched roofs to articulate the building mass.

9. Pedestrian focused design. Buildings shall feature pedestrian scaled architectural details and entries.



Pedestrian scaled facades and entries create a pleasant place to walk.

10. Prominent windows. To create attractive and active pedestrian street scenes, commercial buildings shall feature prominent windows adjacent to sidewalks. Windows shall either provide views into the storefront or be display-only type windows enclosed by backdrops. Windows shall comprise a minimum of 60 percent of the first floor front façade. Use of commercial clerestory and transom windows is encouraged to provide a continuous horizontal band of windows across the upper portion of storefronts. Irregular, modernistic window shapes and patterns shall be prohibited.



Buildings shall feature display windows to enhance pedestrian orientation.



Upper floor windows shall have a vertical orientation where they are taller than they are wide.

11. Window orientation. Upper floor windows shall have a vertical or portrait orientation in which the window is taller than it is wide.

12. Awnings. Awnings are encouraged; refer to the provisions in Section 4.3.4, Windows and Doors.

13. Modular storefront systems.



Modern modular storefront systems are not appropriate for Old Town.

Modular storefront systems characterized by large window panels, glass doors, and metal trim shall be prohibited in DA-4 and discouraged in DA-1 and DA-2. Windows and doors shall feature traditional designs and materials in DA-4. In DA-1 and DA-2 traditional design or a contemporary adaptation may be used.

14. Prohibited materials. The following materials shall be prohibited: highly reflective surfaces, split face block, exposed concrete block, reflective glass, metal siding, plastic siding, and metal roofing.

15. Respect adjacent historic structures. Buildings adjacent to historic structures shall be designed in a manner that safeguards the prominence and integrity of the historic structure. Refer to requirements in the Cultural Resources District Commercial Design Guidelines (Appendix F), which include the Secretary of the Interior’s standards.

16. Vehicular access. Vehicular access to parcels should be provided from rear alleys when available. For corner parcels without alleys, parcels should take vehicular access from the secondary street wherever possible. Vehicular entries to parcels situated at or near corners shall be placed as far from the intersection as possible. Vehicular entries shall conform to line of sight requirements.

17. Location of parking. Vehicular parking shall be located to the rear of parcels and be screened from the street by buildings to the greatest extent feasible. Parking adjacent to parcels with residential zoning shall be separated by a 6-foot high block wall. If the residential use is a multi-story structure with views of the parking area, canopy trees should be used for screening.

18. Pedestrian pathways. Pedestrian access from rear parking lots to street frontages shall be provided along driveways or via separate pedestrian pathways and shall be visually enhanced through at least two of the following:

- Delineate pedestrian pathways through decorative paving.



This pathway leading from the street to rear parking is made attractive through decorative paving and landscaping.

- Provide landscaping along pedestrian pathways to soften the appearance.
- Use attractive pedestrian scaled light fixtures to illuminate pathways.
- Employ public art along pedestrian pathways.

19. Living alleys. Pedestrian amenities such as decorative lighting, seating, and/or shade are encouraged in alleys within DA-1, DA-2 and DA-4 to create a “living alley” public gathering area.

20. Pedestrian oriented signage.

Buildings shall feature pedestrian-oriented signage (such as projecting signs) designed and placed to be easily readable from the sidewalk. Pedestrian-oriented signage may also be accompanied by street oriented signage (such as fascia signs, awning valance signs, and wall signs). Signs are further addressed in Section 4.5.1, Signage, and shall be consistent with the sign area provisions of the Tustin City Code.

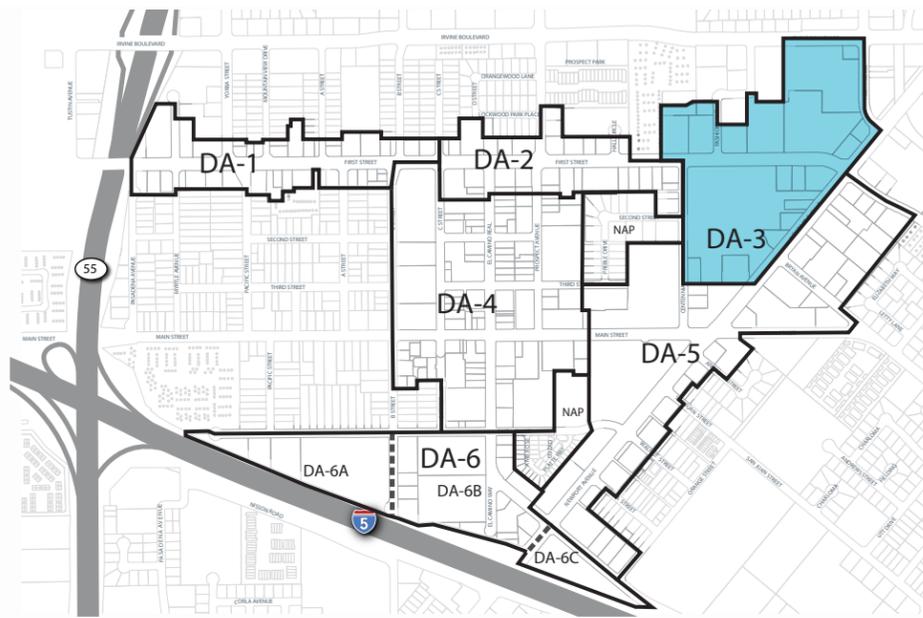


Projecting signs are designed to be easily readable by pedestrians.

KEY COMPONENT
**Pedestrian
Orientation**

4.4.2. Design Criteria Specific to DA-3

The photos on this page provide examples of the vision for new development within DA-3, consistent with the Design Criteria for this subsection.





The larger parcels within DA-3 allow for village-like enclaves that create a “destination” by providing opportunities for shopping, entertainment, and leisure in close proximity. The design criteria for this DA fosters development with a strong pedestrian orientation and activity throughout the day.

- 1. **Siting of commercial use.** Commercial use shall be sited along Newport Avenue, First Street, and Irvine Boulevard.
- 2. **Prominent windows.** To create attractive and active pedestrian scenes, building fronts shall feature prominent windows adjacent to walkways. Windows shall either provide views into the storefront or be display-only type windows with enclosed backdrops. Windows shall comprise a minimum of 60 percent of the first floor front façade.
- 3. **Uses on non-principal streets.** Development sited adjacent to Centennial Way and Holt Avenue may consist of commercial buildings, mixed use buildings with ground floor commercial or residential dwellings as part of an integrated horizontal mixed use development (see the Residential Design Criteria in Chapter 5).
- 4. **Building articulation and detail.** Building elevations along street frontages shall provide significant building articulation and architectural detail to avoid large blank walls and reduce building bulk.
- 5. **Pedestrian plazas.** Within centers greater than three acres adjacent to or near Newport Avenue, a major pedestrian plaza framed by commercial buildings shall be sited with views from the street and be easily accessible from the sidewalk and bike path located at the street. The plaza shall also provide views of and access to the adjacent storefronts. Refer to Section 4.5.5, Major Pedestrian Plaza, below for specific design criteria.



A large pedestrian plaza creates a community gathering place and helps attract patrons to businesses.



A pedestrian node provides amenities such as seating, shade, landscaping, public art, and lighting in a small-scale format.

- 6. **Pedestrian nodes.** Parcels within DA-3 that are not part of a center greater than three acres shall provide a minimum of one pedestrian node that includes seating, shade, and pedestrian-oriented landscaping and lighting sited in a prominent location.
- 7. **Access to transit and bicycle facilities.** Buildings shall be sited to provide convenient access to the transit stops on First Street, Centennial Way, and Newport Avenue and the bike path along Newport Avenue. New development adjacent to future transit stops shall also be sited to provide convenient access.
- 8. **Pedestrian connections.** Pedestrian walkways shall provide convenient and easily discernible connections between commercial areas and public gathering areas. Pedestrian walkways shall feature landscaping, enhanced paving, pedestrian scaled lighting fixtures, and wayfinding signage.
- 9. **Outdoor dining.** Restaurants or tenant spaces designed for take-out food uses such as ice cream or sandwich shops shall be located and designed to accommodate outdoor dining with views onto the pedestrian plaza and/or Newport Avenue.



Take-out food uses shall provide outdoor seating within public gathering areas.

- 10. **Entry driveways.** Primary entry driveways for large centers shall be designed to allow vehicles to quickly funnel off the street to an interior parking lot or structure in order to prevent traffic backup on the street. Entry driveway throats should be designed to minimize interference with street traffic.
- 11. **Centralized access.** If individual parcels located along Newport Avenue develop separately from adjacent centers, vehicular access shall nevertheless be provided exclusively from within the center via an access agreement to avoid excessive curb cuts along the street.
- 12. **Secondary access.** Secondary vehicular access to large centers within DA-3 may be provided from First Street, Centennial Way, and/or Holt Avenue.
- 13. **Design of parking lots and structures.** Parking lots shall be obscured from the street through building placement, and parking structures shall employ architectural details, high quality materials, and screening as addressed in Section 4.5.2, Parking Lots and Parking Structures.
- 14. **Link parking and buildings.** Landscaped walkways shall link parking lots and parking structures to the primary buildings within the site.

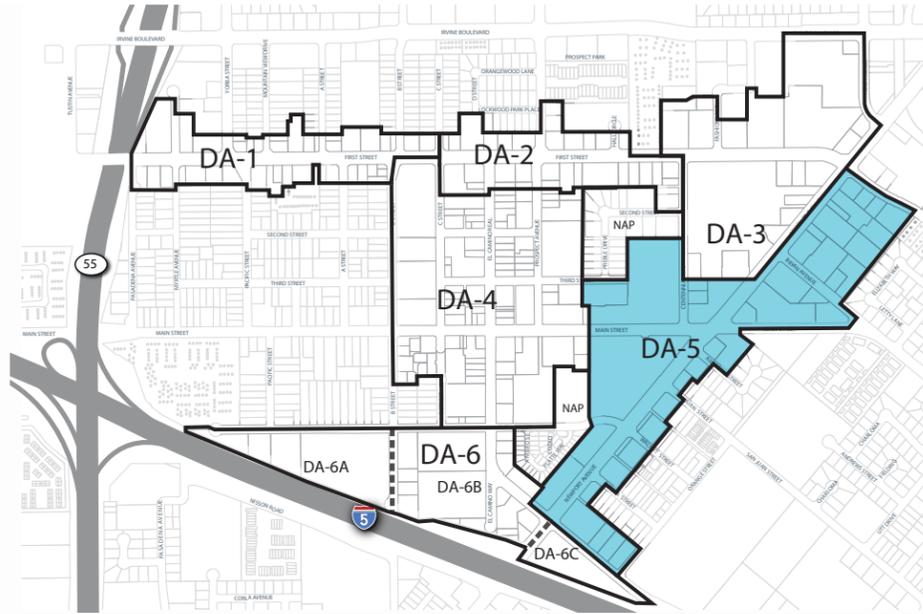


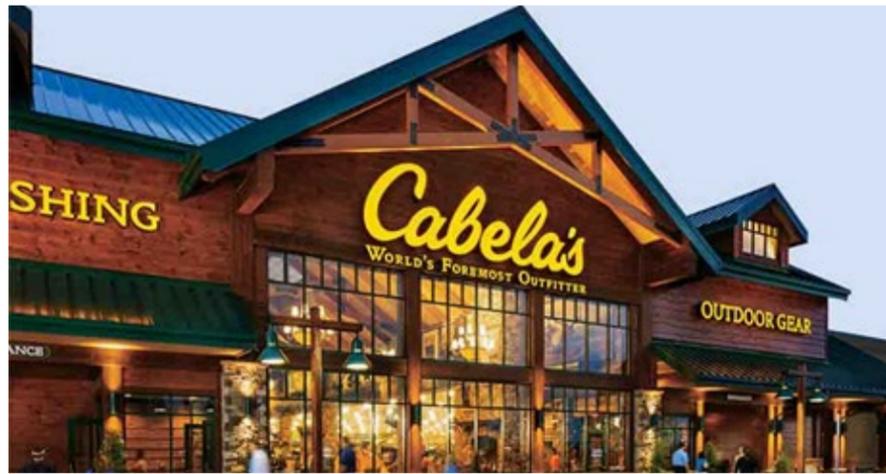
Walkways shall provide attractive and functional connections to parking areas.

KEY COMPONENT
Newport Avenue Commercial Emphasis

4.4.3. Design Criteria Specific to DA-5

The following photos provide examples of the vision for new development within DA-5, consistent with the Design Criteria for this subsection.





Building facades should be highly detailed.

This subsection addresses commercial development located on Newport Avenue within DA-5, which is primarily auto-centric because of the adjacent high volume street. DA-5 does not allow residential mixed use.

1. Building siting. Buildings are encouraged to be sited close to Newport Avenue to create a strong building edge along the street and to provide screening for parking lots, which should be located within the interior of the parcels. Additional buildings may be sited within the parcel interior.

2. Corner placement. On corner parcels, buildings are encouraged to be placed at the corner and along the street frontages to continue the building edge and screen interior parking lots. Corner lots should feature buildings that are designed to reflect the prominence of their location.

3. Building articulation and detail. Building elevations visible from the street or in the central portion of the site shall provide significant building articulation and architectural detail to avoid large blank walls and reduce building bulk.



Contemporary-looking materials such as metal can be combined with traditional materials like brick.

4. Contemporary styles and materials. Building architecture may be a contemporary interpretation of the

architectural styles addressed in Section 4.2, Architectural Styles, and may use modern building materials; however, use of traditional design and materials or a combination of traditional and contemporary is encouraged.

5. Strip malls prohibited. Linear strip malls with parking in front shall be prohibited.

6. Entry driveways. Vehicular access to parcels larger than three acres shall be consolidated via a primary entry driveway featuring landscaping. The entry driveway may be designed with a landscaped center median or roundabout.



This large shopping center features a roundabout at the main entry.

7. Minimize curb cuts. Curb cuts along Newport Avenue shall be minimized. Parcel access via one or more of the following is encouraged:

- Consolidated entry driveway;
- Shared entry driveway with reciprocal access to adjacent parcels; and/or
- Entry from a secondary street, or alley if present, especially for small corner parcels.

8. Circulation hierarchy. On-site vehicular circulation patterns shall be designed with a clear hierarchy to aid vehicular access, parking, wayfinding,

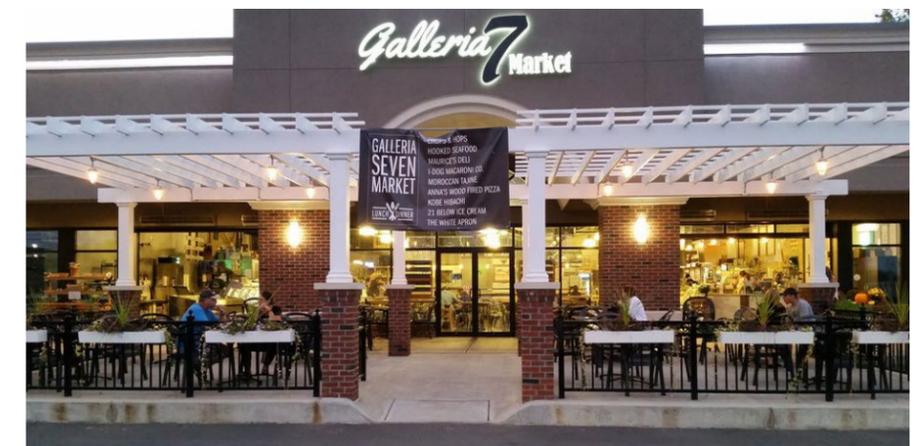
and safety. Individual parking spaces should not be directly accessible from entry driveways or major circulation aisles. Direct access to parking spaces should only be via parking aisles.

9. Pedestrian access. Patron access to buildings shall be provided from the parking lot or structure. Buildings may be designed to provide access from both the parking lot/structure and street-adjacent sidewalk.

10. Connection to transit stops and bike path. Shopping centers or parcels greater than three acres located adjacent to the bike path or transit stops on the west side of Newport Avenue shall provide pathway connections from the street to the anchor building(s).

11. Pedestrian connections. Shopping centers over three acres in size shall provide strong pedestrian connections between parking lots/structures and buildings via at least one of the following methods:

- Site buildings so that active pedestrian areas, such as outdoor dining, can be seen from parking lots.
- Employ pedestrian walkways with landscaping and enhanced paving extending from buildings into parking lots/structures to draw pedestrians to building entries.
- Provide a plaza area with seating, landscaping, and shade to create an inviting entry point from the parking lot/structure to the building(s).



Outdoor dining helps bridge the gap between the parking lot and building interior and provides visible activity.



Pedestrian nodes provide space for gathering and activity.

12. Required pedestrian amenities. Pedestrian-oriented amenities, which may be sited near anchor tenants, prominent areas, or with visibility from the street, shall be included for each project along Newport Avenue.

For shopping centers with greater than 50,000 square feet (sf) of building area, a minimum of one plaza shall be provided with the following features:

- Multiple seating options, including tables
- Shade for all table seating and a minimum of half of all other seating via architectural elements, landscape materials, or canopies/umbrellas
- Pedestrian oriented landscaping and lighting
- Focal point such as public art or sculpture
- Multiple trash/recycling receptacles

For buildings or groups of buildings with 10,000 to 50,000 sf of building area on a parcel, a minimum of one seating node in proximity to a major tenant, or other prominent location, shall be provided with the following features:



Outdoor seating invites people to linger.

- Seating, including tables
- Shade for table seating via canopies/umbrellas
- Pedestrian oriented landscaping and lighting
- Multiple trash/recycling receptacles

For buildings or groups of buildings with less than 10,000 sf of building area on a parcel, a minimum of one outdoor area for each take-out food service use shall be provided with the following features (adjacent food service uses may have a combined seating area):

- Table seating
- Shade for all table seating via canopies, umbrellas, or building overhangs
- One or more trash/recycling receptacle(s)

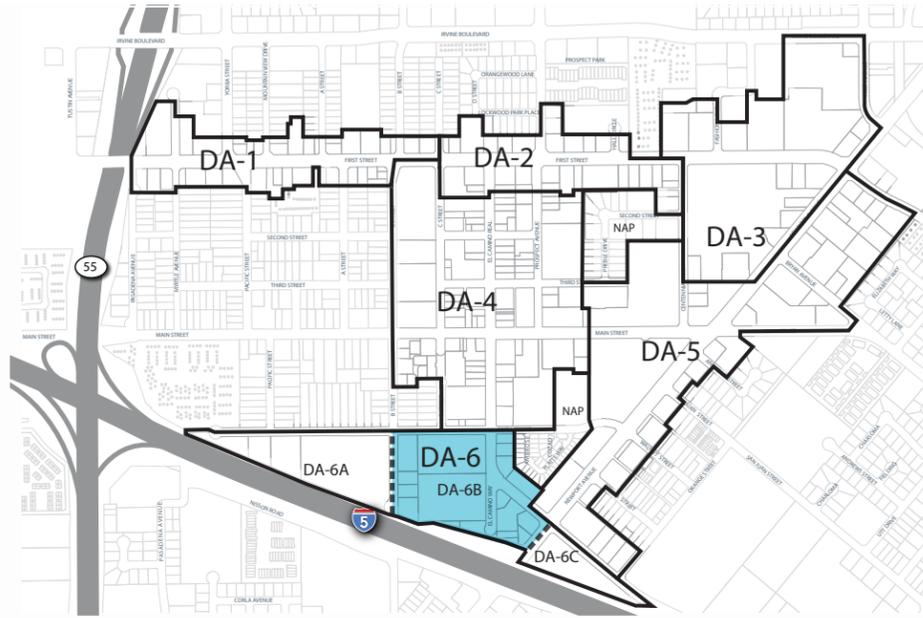


KEY COMPONENT

Pedestrian Orientation

4.4.4. Design Criteria Specific to DA-6B

The following sample photos provide a visual representation of the development desired for DA-6B, consistent with the Design Criteria for this subsection.





Buildings along El Camino Real shall form a strong and active street edge.

The vision for DA-6B is to create a “destination” by providing opportunities for shopping, entertainment, and leisure in an inviting setting focused on El Camino Real close to Old Town. The design criteria for this DA fosters development with a strong pedestrian orientation and activity throughout the day.

1. Building siting. Commercial buildings shall be sited along both sides of El Camino Real south of Sixth Street. Additional buildings may be sited within the interior of parcels on the west side of the street. Buildings located on the east side of El Camino Real may be small due to shallow parcel depth.

2. Prominent windows. To create attractive and active pedestrian street scenes, buildings fronting on El Camino Real south of Sixth Street shall feature prominent windows adjacent to the sidewalk. Windows shall either provide views into the storefront or be display-only type windows enclosed by backdrops. Windows shall comprise a minimum of 60 percent of the first floor front façade.



Display windows are an important feature for pedestrian oriented streets.



Architectural style and building siting within the northern portion of DA-6B shall reflect Old Town character.

3. Design along El Camino Real. Pedestrian-friendly building siting and design is encouraged, with architectural design on the east side of El Camino Real complementing the design on the west side, in a smaller scale.

4. Northern portion to reflect Old Town. The design of development in the northern portion of DA-6B shall reflect the historic architectural styles and building siting found in Old Town (DA-4), creating both a visual link and a pedestrian circulation pattern that draws pedestrians northward. The remainder of DA-6B, especially adjacent to Newport Avenue, may exhibit a contemporary interpretation of the Old Town historic architectural styles.

5. Building articulation and detail. Building elevations along El Camino Real and Sixth Street shall provide significant building articulation and architectural detail to avoid large blank walls and reduce building bulk.

6. Corner of El Camino Real and Newport Avenue. Buildings on or near the corner of Newport Avenue and El Camino Real shall feature statement architectural elements, enhanced architectural details, and may be accompanied by iconic public art appropriate to Tustin or Old Town to attract drivers along the street and freeway. Buildings visible from Newport Avenue shall not be plain or generic in design.

7. Pedestrian plaza. A major pedestrian plaza framed by commercial buildings shall be provided on the west side of El Camino Real near to and easily accessible from El Camino Real, and preferably visible from Newport Avenue. The plaza shall provide easy access to and views of surrounding storefronts. Refer to Section 4.5.5, Major Pedestrian Plazas, for specific design criteria.



Public gathering areas create destinations to draw patrons.

8. Outdoor dining. Restaurants or tenant spaces designed for take out food uses such as ice cream or sandwich shops shall be located and designed to accommodate outdoor dining with views onto the pedestrian plaza and/or street frontage.

9. Pedestrian connections within site. Pedestrian walkways shall provide convenient and easily discernible connections between commercial areas and public gathering areas. Special design emphasis shall be placed on walkways that provide connection to the southern corners of El Camino Real and Sixth Street to encourage and facilitate pedestrian access from DA-6B to Old Town. Pedestrian walkways shall feature landscaping, enhanced paving, pedestrian scaled lighting and fixtures, and wayfinding signage.

10. Connections across streets. Strong pedestrian connections both across El Camino Real and north to Old Town shall be provided.

11. Entry driveway. Parking shall be designed for vehicles to quickly funnel off the street to an interior parking lot or structure in order to prevent traffic backup on the street. Entry driveway throats should be designed to minimize interference with street traffic and maintain visual clearance.

12. Design of parking lots and structures. Parking lots shall be obscured from the street through building placement, and parking structures shall employ architectural details, high quality materials, and screening, as addressed in Section 4.5.2, Parking Lots and Parking Structures.

13. Link parking and buildings. Walkways with pedestrian friendly landscaping and lighting shall link parking lots and parking structures to the primary buildings within the site.

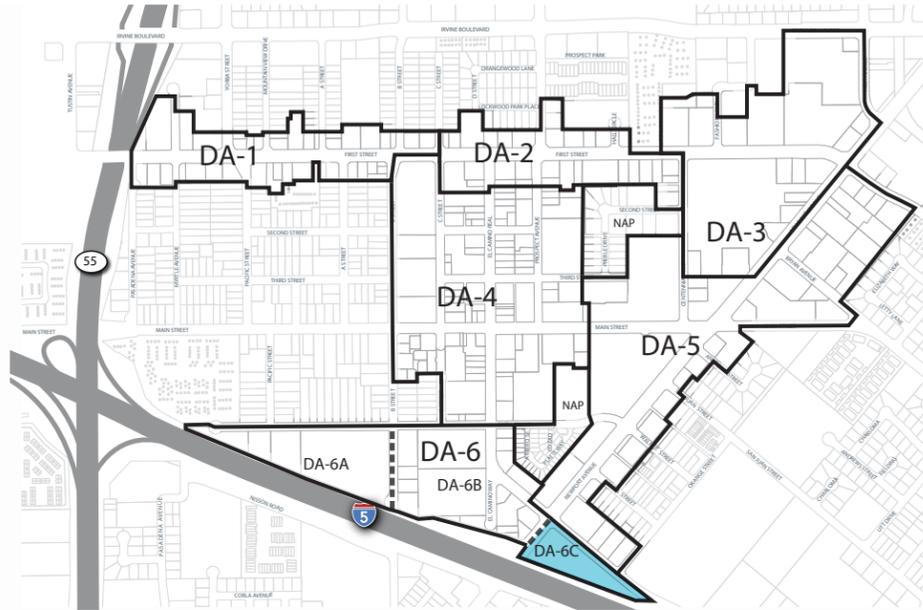


KEY COMPONENT

Pedestrian Orientation

4.4.5. Design Criteria Specific to DA-6C

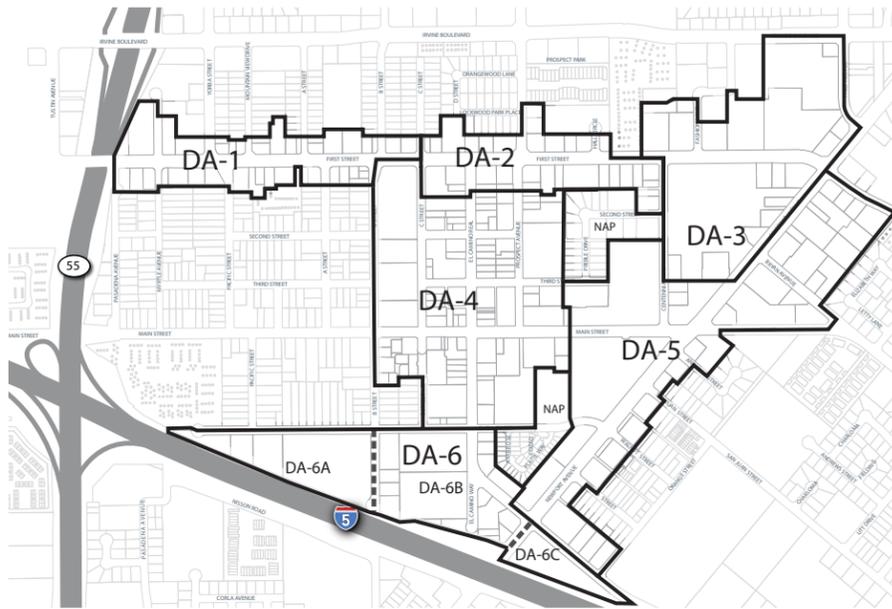
The following sample photos provide a visual representation of the development desired for DA-6C, consistent with the Design Criteria for this subsection.



A portion of DA-6C is visible from Newport Avenue; however, the majority extends along the southern portion of El Camino Real, where there is less draw for retail use, and backs on I-5.

- 1. Building siting.** Buildings should form a distinct edge along the street and should screen parking lots/structures.
- 2. Building articulation and detail.** Building elevations shall provide significant building articulation and architectural detail to avoid large blank walls and reduce building bulk.
- 3. Vehicular access.** Vehicular access from Newport Avenue should be avoided to prevent traffic back-up at the nearby I-5 exit. Access should be from El Camino Real. An internal circulation route is encouraged to serve uses in the northern portion of the DA.
- 4. Pedestrian connections.** Pedestrian walkways should provide convenient and easily discernible connections between various uses and public gathering areas. Pedestrian walkways should feature landscaping, enhanced paving, and pedestrian scaled lighting fixtures.
- 5. Gathering area.** A public pedestrian gathering area shall feature seating, shade, and landscaping. Public art is also highly encouraged.





KEY COMPONENT
Pedestrian Orientation

4.5 General Design Criteria

4.5.1 Signage

Signs are important for business identification and contribute greatly to the appearance of streets and shopping areas. Refer to the TCC to determine the allowable sign area for a building or parcel and the required review process. Also refer to the TCC for temporary use signs.

4.5.1.1 General Sign Design Criteria

- 1. Message.** Signs should express easy to read, simple, and direct messages.
- 2. Complement architecture.** Sign types, shapes, materials, font styles, and color combinations shall be compatible with and complement the building architecture.
- 3. Prohibited signs.** Pole-mounted signs, pylon signs, and canned signs (internally illuminated plastic panels in a sheet metal box enclosure) shall be prohibited.
- 4. Position of light source.** The light source for externally illuminated signs shall be positioned so that light does not shine directly on adjoining properties, cause glare, or shine in the eyes of motorists or pedestrians.

5. Raceways. If electrical raceways or conduits are required, they shall be concealed from view and mounted internally. Channel letters shall not be mounted on exposed raceways.

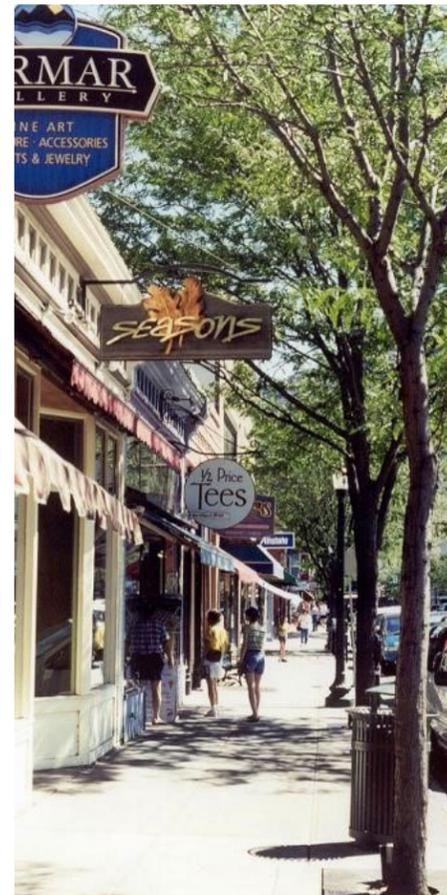


Exposed raceways shall be prohibited.

6. Removed signage. If signage is removed, any discolored areas or holes shall be patched and painted to seamlessly blend in with the building.

7. General provisions. Refer to the TCC for other general sign provisions, including address requirements.

8. Requirement for Master Sign Plan. Shopping centers or multi-tenant buildings shall establish a Master Sign Plan (refer to the TCC) to ensure consistency of theme and design, including type of sign, placement, font, style of illumination, and colors.



The placement and orientation of projecting signs allow for easy viewing from the pedestrian walkway.

4.5.1.2 Signage in DA-1, DA-2, and DA-4

Appropriately designed signage in DA-1, DA-2, and DA-4 is an effective way to communicate that reflects historic building patterns.

The following criteria address signage for new pedestrian-friendly development along First Street in DA-1 and DA-2 and within DA-4 in Old Town. (For new signs on existing buildings within DA-1 and DA-2 that are not pedestrian-oriented, such as monument signs, refer to the TCC for appropriate non-pedestrian signage provisions.)

1. Sign types. Allowable sign types for pedestrian-oriented buildings in DA-1, DA-2, and DA-4 include the following, which shall be oriented and scaled to be read from the sidewalk.

- Projecting/blade signs – Projecting signs are cantilevered signs that are structurally affixed and oriented perpendicularly to the building facade. Blade signs are a type of projecting sign that are suspended under a bracket, armature, or other mounting device.



An example of a blade sign.



An example of awning signage placed on the canopy.

- Awning signs – Signage may be located on the canopy or incorporated into the valance, which hangs along the front of the awning.



An awning with signage on the valance.



A sign located on the building fascia.

- Fascia signs – Fascia signs are a type of building-mounted signage affixed to the fascia, which is the front wall of building.

- Wall signs – Wall signs are attached to the wall of a building with the exposed face of the sign parallel to the wall plane. Wall signs in pedestrian-oriented areas in DA-1, DA-2 and DA-4 are encouraged to employ unique shapes, motifs, and materials. Shiny, plastic channel letter signs are discouraged within pedestrian oriented areas of DA-1, DA-2, and DA-4. External illumination via individual wall-mounted vintage-looking exterior light fixtures and vintage looking custom signs are encouraged.



An example of a wall sign.

- Painted wall signs – Painted wall signs are a type of building sign featuring lettering painted on the wall



Painted wall signs create a vintage look.

surface, which is often brick, and typically made to resemble faded historic signs. Painted wall signs may be illuminated with individual wall-mounted vintage-looking exterior light fixtures.

4.5.1.3 Signage in DA-3, DA-5, DA-6B and DA-6C

DA-3, DA-5, DA-6B and DA-6C feature large parcels appropriate for larger uses including shopping centers, multi-tenant developments, and mixed use. These types of development typically require a large number of signs, which may be oriented to vehicles and/or pedestrians. Therefore, these DAs require special provisions to ensure that signage is coordinated, creates a quality appearance, and complements building architecture. DA-3, DA-5, DA-6B and DA-6C shall require approval of a master sign plan in conformance with the TCC.



Pedestrian sidewalk signs are allowed within DA-4 only.

4.5.1.4 Special Signage Allowed Only in DA-4

Old Town (DA-4) features appropriately scaled pedestrian oriented streets and sidewalks. One of the goals of the DCCSP is to draw in new development, especially to the vacant parcels, to form a larger continuous walking area and increase Old Town patronage. To address this desire, the DCCSP allows specific pedestrian-oriented signage on the public sidewalks or private walkways within Old Town to create a pedestrian-friendly ambience and make visitors aware of the shopping, dining, and entertainment opportunities.

1. Pedestrian sidewalk signs.

Pedestrian sidewalk signs shall be allowed only within DA-4, provided they comply with the following criteria.

- Sign face area shall be limited to four square feet (sf) per side. Signs may be double sided.
- Maximum height of sign including supporting structure shall be 42 inches.
- Signs shall be placed as follows:
 - o Shall not block pedestrian traffic flow and shall ensure a minimum of four feet of unobstructed sidewalk or walkway width;
 - o Shall be adjacent to the building within the span of the storefront; and
 - o Shall not be located adjacent to the curb, in the parkway, nor within the street.
- Shall be made of high quality materials .
- Plastic sign faces shall be prohibited.
- Shall obtain Design Review approval from the Community Development Department and enter into an irrevocable license agreement, or other necessary approval as required by the Public Works Department.



Pedestrian sidewalk signs shall be high quality.



Plastic sign faces are prohibited on sidewalk signs.

4.5.2 Parking Lots and Parking Structures

The location and design of parking has a large influence on the appearance of the built environment. The provision of parking seeks to reflect a shift in emphasis from an auto-centric to a more balanced, pedestrian friendly urban design.

4.5.2.1 General Parking Criteria

- 1. Motorcycle spaces.** Dedicated motorcycle spaces are encouraged, especially in small or otherwise unusable areas, so that full size spaces can be maintained for parking vehicles.
- 2. Tandem spaces.** Tandem parking spaces are prohibited per the TCC, except the DCCSP allows tandem spaces for residential parking within a parking structure.
- 3. Alley parking.** Alley parking may be enhanced with decorative pavement and landscape. Pedestrian amenities such as decorative lighting, seating, and/ or shade may also be provided to create a “living alley” public gathering area, provided it does not reduce required parking nor impinge on the vehicular travel lane without City authorization. Living alleys are especially encouraged in DA-1, DA-2 and DA-4.



Parking lots adjacent to the street shall provide a landscape screen.

4.5.2.2 Parking Lot Criteria

- 1. Location of parking.** On-site parking is encouraged to be located behind the building(s). If it is not reasonably feasible to locate parking behind a building, as determined by the approval body, it may be provided along the side of the building if significantly screened from the street by a landscaped area that includes trees and shrubs.
- 2. Driveways.** Entry driveways are encouraged to use enhanced paving to reinforce and call attention to the pedestrian path of travel.
- 3. Reciprocal access.** Where feasible, reciprocal access shall be required and designed to facilitate circulation between adjacent properties to reduce the number of needed curb cuts. Property owners shall enter into reciprocal access agreements as determined by the Community Development Director.
- 4. Parking hierarchy.** Parking lots shall be designed with a clear hierarchy to aid vehicular access/parking, minimize pedestrian and vehicular conflicts, and enhance safety.
- 5. Landscaping.** Parking lots shall utilize landscaping to break up expanses of parking and to provide shade per the TCC.



Landscaping breaks up expanses of pavement.



Creative parking lot enhancements are encouraged.

- 6. Biofiltration and bioretention.** Within parking lots, landscape areas are encouraged to be designed as rain gardens and provide stormwater management in an aesthetically pleasing manner instead of relying on concrete stormwater facilities.

- 7. Design creativity.** Parking areas may employ creative techniques such as decorative paving and public art to make them more attractive and less utilitarian.



Public art can enliven a parking lot.

- 8. Safety.** Adequate areas for safe vehicular maneuvering, unloading, and emergency access shall be accommodated on site.

- 9. Loading areas.** Parking areas shall be arranged to minimize conflicts with commercial loading activities.

- 10. Solar panels.** Solar panels may be utilized in parking areas for the provision of shade if the supporting structures are architecturally compatible with the nearby buildings.



Solar panels must be architecturally compatible with nearby buildings.



11. Wayfinding. Directional or wayfinding signage shall be used to identify or direct vehicles to off-site parking.

12. Bicycle racks. New development is encouraged to provide on-site decorative racks for bicycle parking. Bicycle racks shall also be provided as specified in the TCC. Bicycle racks should:

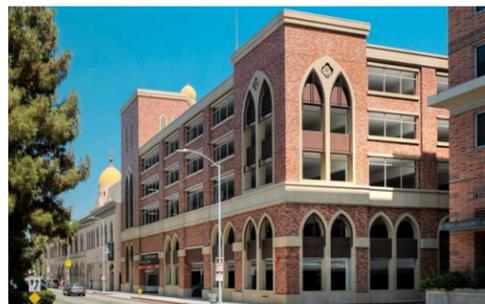
- Support the bicycle in at least two places, preventing it from falling over, and allow locking of the frame and one or both wheels with a U-lock. Undulating “wave” racks, schoolyard “wheel bender” racks, and spiral racks shall be prohibited due to the high risk of theft or damage to bicycles.
- Resist cutting, rusting, bending or deformation.
- Be placed in a convenient and attractive location under the cover of a building roofline, when feasible, in areas that cyclists are most likely to travel.
- Be securely anchored to the ground through use of proper anchors to prevent vandalism and theft.



Avoid use of these types of bike racks.



An example of an appropriate bike rack.



Parking structures shall be articulated, detailed, and constructed of quality materials.

4.5.2.3 Parking Structure Criteria

1. Exterior design. Parking structures shall be designed to be visually compatible with nearby buildings, and the desired character for the respective DA, by employing

architectural details, openings or “window” patterns, and quality decorative materials on all elevations.

2. Integrated retail. Ground-level retail should be located along the parking structure façade to increase visual integration of the structure with nearby buildings and enhance pedestrian orientation by increasing activity.



Incorporating retail into the ground floor of parking structures is encouraged.

3. Ventilation and screening. Parking structures should provide natural ventilation when possible and screening of vehicles and headlights.

4. Interior color. Interior walls and ceilings should be a light color to improve illumination. Interior mechanical equipment and piping should be painted to match the interior of the structure.

5. Safety and surveillance. Parking structures, including the location of elevators and stairs, shall provide natural surveillance by ensuring visibility from public areas. Elevator doors shall be located so they are highly visible, and the elevator shaft and cab should have clear glass facing the public view. Subterranean levels shall use offset sloping ramps to allow for open and unobstructed visibility for floor surveillance.

6. Landscaping. Landscaping is encouraged on all sides of parking structures to screen and reduce the visual mass of the structure. Landscaping may consist of the following:

- Trees and other landscape materials planted at the ground level near the base of the parking structure;
- Hanging baskets or receptacles attached to the parking structure containing plant materials that cascade down or entwine the structure;
- Trellises constructed of durable materials that permit vines to attach for support, creating a “living wall.”



A living wall of climbing plants helps to screen a parking structure.

7. Pedestrian area. A plaza or courtyard area should be provided and include amenities that encourage pedestrian use such as seating, decorative lighting, landscaping, and public art.

8. Pedestrian circulation. Pedestrian paths shall be clearly delineated where adjacent to vehicular entries/exits and where paths cross vehicular travel lanes. Use of decorative hardscape to emphasize pedestrian paths and areas is encouraged. Pedestrian circulation should be separated from vehicular circulation whenever possible.

9. Signage. Signage and directional arrows indicating entrances, exits, elevators, and emergency buzzers/telephones shall be clearly displayed.



Interior parking structure signage does not need to be boring.

4.5.3 Landscape and Open Space

Innovative and creative landscape design, selection and placement of plant materials can set apart the DCC and contribute to its “sense of place.” Refer to the water efficiency provisions of the TCC and the City’s landscape guidelines regarding landscape materials and installation methods.

- 1. Purpose of landscaping.** Landscape and decorative hardscape shall enhance the character of buildings, address the Tustin climate, and facilitate sustainable water use and drainage strategies.
- 2. Preserve trees.** Mature trees should be preserved whenever feasible, except when diseased or causing significant damage to structures or infrastructure. If trees cannot be preserved, a certified arborist shall evaluate the potential for tree relocation.

- 3. Coordinated design.** Building landscape should be coordinated and provide continuity with landscapes on adjacent properties.



A small plaza or courtyard helps to integrate parking structures into pedestrian oriented development.

- 4. Landscaping of pedestrian areas.** Creatively designed landscape shall be utilized throughout pedestrian plazas, gathering areas, and nodes to create shade and contribute to a pleasing atmosphere.



Landscaping shall be designed to provide biofiltration and bioretention functions.

- 5. Service areas.** Landscaping should be used to screen service areas including trash and utility enclosures.
- 6. Biofiltration and bioretention.** Biofiltration and bioretention features such as bioswales, rain

gardens, drywells, and biofiltration strips adjoining pavements shall be incorporated into landscape design whenever feasible.

- 7. Landscape design.** Landscape design should provide for year-round beauty, employ vertical and horizontal layers, and create a repeating pattern of plants to visually connect the landscaped space.



This creative landscape element is attractive and provides shade.

- 8. Living walls.** Landscaping may be used on building facades to provide visual interest and/or screening. “Living walls” that feature plants or vines growing on lattice, wire grids, or armatures may be used to alleviate blank walls. Living walls shall be well maintained.



Planters and pots add visual interest.

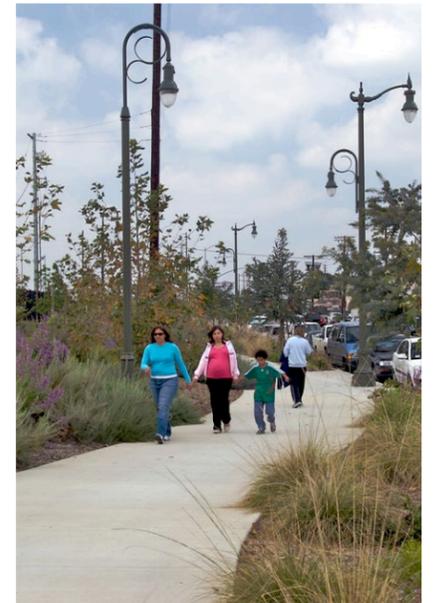
- 9. Containers.** Planters and pots provide visual interest and color accents to sidewalks, courtyards, and plazas. Planters and pots should be made of materials that complement the building architecture and include a self-watering irrigation system. Plants shall be well maintained.

- 10. Landscape lighting.** Up-lighting of trees and string lights within trees are effective, attractive techniques that are encouraged

4.5.4 Lighting

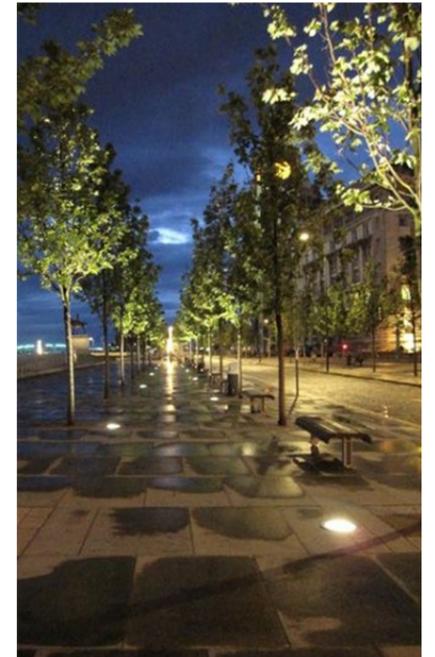
This section addresses exterior lighting in the DCC. The intent is to ensure that the design of fixtures contributes to the character of development and that light does not negatively impact adjacent properties.

- 1. Minimize light intrusion.** Lighting shall be located and designed to provide safety and security without unnecessary spillover or glare upon adjacent buildings and properties, especially residential uses.
- 2. Location and amount of lighting.** The location of lighting shall respond to the anticipated use and not exceed the amount of illumination required by users.



Well-illuminated pathways leading to parking areas are important.

- 3. Adequate levels of lighting.** Parking lots/structures, pedestrian pathways, outdoor gathering spaces, building entries, and other pedestrian areas shall provide adequate levels of lighting. Within parking lots/structures, illumination shall be concentrated not only within vehicle parking areas but also along the pedestrian paths leading from commercial and residential areas.



Up-lighting enhances trees and landscaping.

- 4. Lighting control.** Lighting sources should be kept as low to the ground as possible while ensuring safe and functional levels of illumination. Area lighting shall be directed downward or employ control features to avoid offsite lighting or lighting of the night sky.



5. Aesthetic design. Exterior lighting shall be designed as an integral part of the building and landscape design. Fixtures shall be architecturally compatible with the building and the character of the area.



Up-lighting of buildings enhances the pedestrian ambiance.

6. Ambiance. In addition to lighting that serves a functional purpose, lighting that provides ambiance, especially in pedestrian oriented areas, is encouraged if approved through Planning Department Design Review. Suggestions include:



Decorative lighting in pedestrian oriented areas is encouraged.

- Up-lighting of buildings and structures;
- Landscape up-lighting;
- String lights within tree branches;
- String lights used to create a canopy effect over courtyards, seating areas or walkways; and
- Temporary installations of string lights to highlight building forms.

7. Height of poles. The height of ground-mounted light poles shall be in scale with the building and site and should not exceed the building height. Pedestrian light poles along pathways, walkways, and sidewalks should be 10 to 16 feet high.

8. Bigger is not better. The use of a number of smaller lights is preferable to larger, more intense lights.

9. Illuminated signs. The light source for externally illuminated signs shall be positioned so that light does not shine directly on adjoining properties, cause glare, or shine in the eyes of motorists or pedestrians.

4.5.5 Major Pedestrian Plazas

A major pedestrian plaza that provides the following amenities is encouraged to create a “destination” to help draw patrons.

1. Seating. Public seating conducive to groups as well as individuals should be provided. Seating should consist of at least two of the following types:

- Benches
- Tables and chairs
- Modular seating (such as stationary or moveable cubes)
- Low walls or planter ledges
- Unique seating that contributes to the character of the plaza

2. Shade. Shade should be provided for the majority of the public seating, in at least one of the following ways:

- Architectural elements such as pergolas or arcades
- Awnings, canopy structures, sail canopies or umbrellas
- Shade trees (minimum 36 inch box)



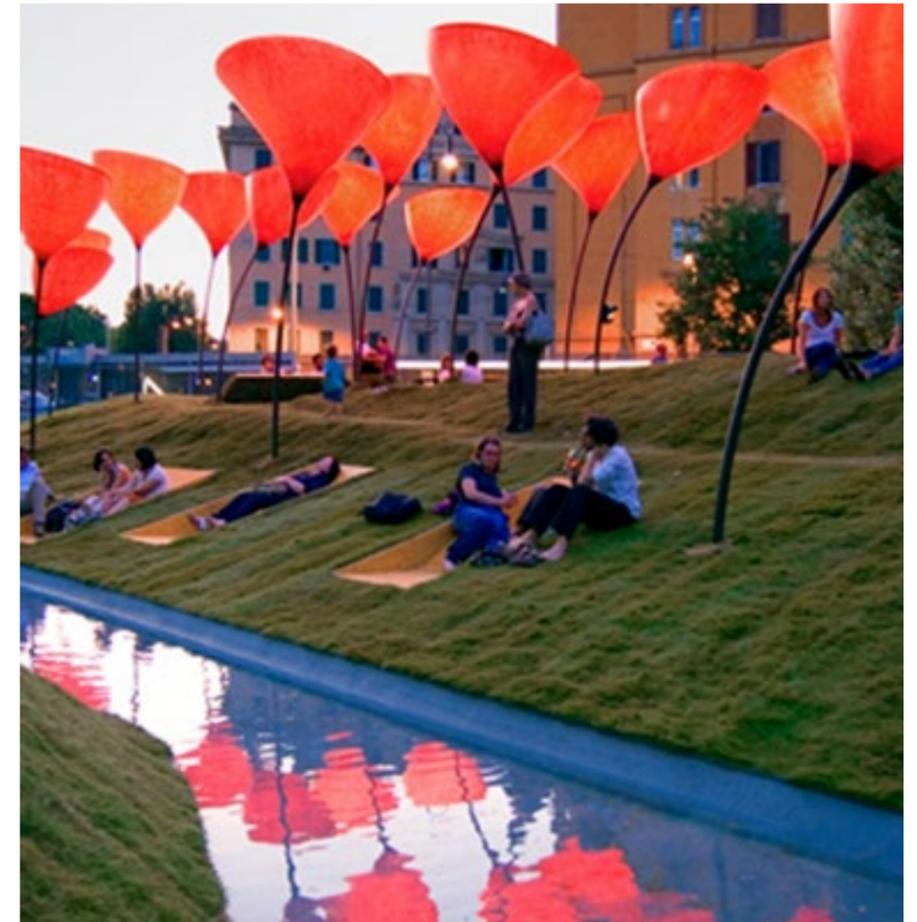
A large architectural element provides shade and is recognizable and memorable.



Sails are an artistic way to create shade.

3. Focal point. A major design focal point should be included, consisting of at least one of the following:

- Public art (including Pioneer Bust Program)
- Water feature, if designed to conserve and recycle water



Public art attracts people and can be part of an area's image branding.

- Kinetic sculpture (three-dimensional figure that moves, generally powered by wind or a motor)
- Interactive sculpture or art

4. Decorative paving. Use of decorative paving material should be included in the plaza design.

5. Abundant landscaping. Landscape should be provided within the interior and around the perimeter of the plaza in a quantity sufficient to create a lush landscaped environment that is well maintained.

6. Lighting. The plaza should include pedestrian scaled decorative lighting that complements the appearance of the site furniture.

7. Site furniture. Site furnishings such as trash/recycling receptacles, bike racks, pole banners, and bollards are encouraged.



5

RESIDENTIAL PROVISIONS

Ch. 5 Residential Provisions

The following Chapter addresses residential mixed use and multi-family residential development in the DCC, which require a Residential Allocation Reservation (RAR) discretionary entitlement as described in Chapter 6, Administration and Implementation Plan. The Residential Permitted Use Table, Residential Development Standards, and Residential Design Criteria work together to designate the appropriate locations and required high-quality character for residential use in the DCC.

INSIDE THIS CHAPTER

5.1 Residential Use within the DCC

5.2 Residential Permitted Uses

5.3 Residential Development Standards

5.4 Residential Design Criteria

5.1 Residential Use within the DCC

Prior to adoption of the DCCSP, the project area was zoned for commercial use. The DCCSP retains commercial focus; however, it introduces residential use to serve as an economic engine to invigorate the businesses within the DCC area by increasing the area's population base and demand for goods and services. This follows the trend of American housing preferences, which are experiencing a long-term shift away from auto-oriented, single family suburbs, and favoring walkable, mixed use neighborhoods.

The DCCSP does not entitle residential use by right. Rather, the DCCSP establishes a process to consider residential use based on the merit of a proposal's design. Residential proposals deemed to substantially comply with the provisions contained within this document by the approval authority (specified in Chapter 6, Administration and Implementation Plan) may be allocated a portion of the available units remaining within the finite residential housing allocation bank established by the DCCSP.

Residential mixed use will only be considered on properties with a land use designation of DM or OT in DAs-1, 2, 3, 4, 6B or 6C if part of an integrated commercial-residential project. Mixed use requires commercial use on the first floor to support economic development and residential units above or oriented away from principal streets. Multi-family residential development will only be considered on properties with a designation of MF within DA-6A. (Residential use of any type is not allowed within DA-5 to maintain its commercial emphasis.)

Residential use within the DCC requires approval of a discretionary entitlement. Residential projects, whether mixed use or multi-family, require an initial step to reserve housing units from the residential allocation bank. The residential bank establishes a maximum of 887 new residential units in the DCC, which are divided among the DAs. (The approved Vintage project located on Sixth Street has been allocated 140 of the units assigned to DA-6.) (Chapter 6, Administration and Implementation Plan, establishes the discretionary residential unit allocation entitlement process, the distribution of units by individual DA, provisions for transfer of units, and required findings for projects, including those with a residential allocation.)



Residential use must be high quality consistent with the development standards and design criteria presented in the DCCSP. Residential projects require Design Review as well as any other necessary entitlements such as subdivision maps or conditional use permits. The project features of each discretionary residential application including, but not limited to, location, parcel size and shape, adjacent uses, circulation, site planning, architectural design, open space and amenities, landscape, and parking, shall be reviewed to determine the allowable number of residential units if the discretionary entitlement is approved.

KEY COMPONENT
Mixed Use

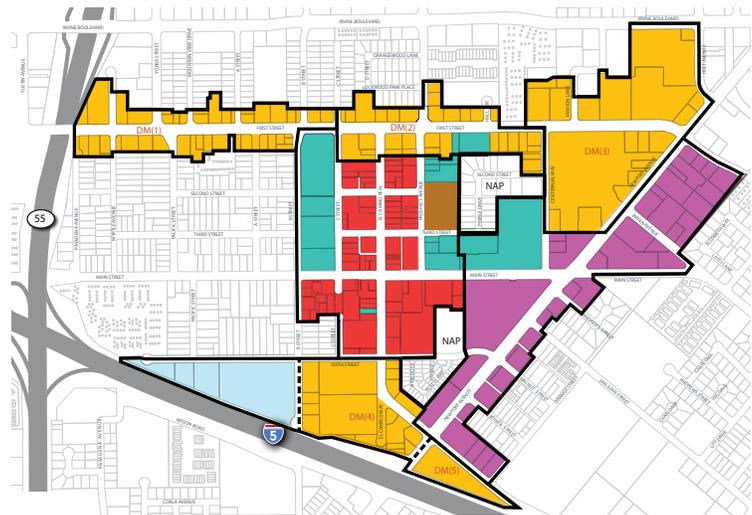
KEY COMPONENT
Multi-Family Residential

Projects involving residential use must also comply with the City's affordable housing ordinance. For provisions related to historic residences within the DCC, refer to the Cultural Resources District Residential Design Guidelines.

5.2 Residential Permitted Uses

As previously described in Chapter 2, Development Plan, the DCCSP specifies six primary land use designations: Downtown Mixed Use (DM), Old Town (OT), Downtown Commercial (DC), Civic/Institutional (CI), Multi-Family (MF), and Mobile Home (MH).

Table 5.1, Residential Permitted Use Table, displays uses for the residential category. The Table specifies individual uses within the residential category and indicates whether the uses are permitted by right, require discretionary approval of a Residential Allocation Reservation (RAR) (refer to Chapter 6, Administration and Implementation Plan), or are prohibited under the DCCSP land use designations and sub-designations. (For commercial uses refer to Chapter 3, Commercial Permitted Uses and Development Standards and Table 3.1, Commercial Permitted Use Table.)



Legend
Downtown Mixed Use (DM) including subareas DM(1) to DM(5) Downtown Commercial (DC)
Mobile Home (MH) Multi-Family (MF) Old Town (OT) Civic/Institutional (CI)
NAP = Not a part

In Table 5.1:

- "P" indicates a permitted use.
- "R" indicates the use requires discretionary approval of a Residential Allocation Reservation (RAR) (refer to Chapter 6, Administration and Implementation Plan).
- "--" indicates a prohibited use that is not allowed because it is deemed inconsistent with the intent of the DCCSP.

Table 5.1 – Residential Permitted Use Table

LAND USE	DCCSP DESIGNATIONS										See foot-note
	(1)	(2)	DM			OT	DC	CI	MF	MH	
Corresponding DA Number	DA-1	DA-2	DA-3	DA-6B	DA-6C	Portion DA-4	Portion DA-5	Portion DA-2,4,5	DA-6A	Portion DA-4	
RESIDENTIAL											
Residential in historic single-family residences	P	--	--	--	--	P	--	--	--	--	1
Multifamily residential	--	--	--	--	--	--	--	--	R	--	2, 3
Mixed-use residential	R	R	R	R	R	R	--	--	--	--	
Live/work units	--	--	R	R	R	R	--	--	--	--	4, 5
Mobile home parks	--	--	--	--	--	--	--	--	--	P	
Key: P Permitted Use requires Design Review in compliance with Chapter 6, Administration and Implementation Plan. R Requires discretionary approval of a Residential Allocation Reservation in compliance with Chapter 6, Administration and Implementation Plan. -- Prohibited Use											
Footnotes: 1. See DCCSP Section 3.2.1 of Special Use Restrictions. 2. Parcels existing at the time of adoption of the DCCSP within the MF land use designation are permitted one residential unit by right, subject to TCC Section 7262, Design Review, provided the unit is deducted from the residential bank (refer to Chapter 6). 3. See DCCSP Section 3.2.2 of Special Use Restrictions and Ordinance 1472 for approved Vintage multi-family residential project in Multi-Family (MF). 4. Prohibited on principal streets; only allowed on non-principal streets (refer to Figure 2.3, Principal or Non-Principal Streets) and subject to the Development Standards, Design Criteria, Entitlement Processing and Required Findings applicable to vertical mixed Use. 5. See DCCSP Section 3.2.2 of Special Use Restrictions and Ordinance 1361 for allowable uses within Prospect Village live/work project.											

5.3 Residential Development Standards

The following Tables 5.2 through 5.4 present the Residential Development Standards for residential use, which requires approval of a Residential Allocation Reservation (RAR) and any other applicable discretionary entitlement as provided in Chapter 6, Administration and Implementation Plan. Table 5.2 addresses both the residential and commercial components of mixed use development in DA-1, DA-2, DA-3, DA-6B and DA-6C. Table 5.3 addresses the residential and commercial components of mixed use development in DA-4. Residential use is prohibited in DA-5. (Regarding the Prospect Village live/work development, refer to Section 3.2.2, Uses within Planned Community Designations.)

Table 5.4 provides the Development Standards for multi-family residential development within DA-6A. (Within DA-6A, Ordinance 1472 approved the Vintage multi-family residential project on the south side of Sixth Street west of B Street through a Planned Community designation, which shall remain valid as it establishes the allowable uses and documents the approved development.)



Table 5.2 – Development Standards for Mixed Use Buildings within DA-1, DA-2, DA-3, DA-6B & DA-6C

MIXED USE BUILDINGS		
Development Standards for DA-1, DA-2, DA-3, DA-6B & DA-6C		
<p>These Development Standards shall apply to both the residential and commercial components of mixed use development within DA-1, DA-2, DA-3, DA-6B and DA-6C. As indicated in Table 5.1, Residential Permitted Use Table, residential mixed use requires approval of a discretionary Residential Allocation Reservation (RAR), in addition to any other entitlements required by the project, as provided in Section 6.1.3, Entitlement Processes. See also Section 5.4, Residential Design Criteria, and Chapter 4, Commercial Design Criteria, for commercial components of mixed use.</p>		
Mixed Use Development Standards	DA-1 & DA-2	DA-3, DA-6B & DA-6C
Building Setbacks¹		
Front setback ²		0 feet minimum
Interior side setback From adjacent residential zone From adjacent non-residential zone		5 feet minimum 0 feet minimum
Corner side setback		0 feet minimum
Rear setback From adjacent residential zone From adjacent non-residential zone or alley		10 feet minimum 0 feet minimum
Building Height (Parking structures below buildings, rooftop uses (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	3 stories maximum First floor plate 16 foot minimum	4 stories maximum (5 stories maximum adjacent to freeway) First floor plate 16 foot minimum
Non-Residential Tenant Space Maximum	10,000 square feet (applies to DA-1 only)	none
Open Space (Per dwelling unit) Private requires minimum depth of 4 ft. and includes private patios, private balconies, private roof decks Common includes courtyards, common balconies, common roof decks, pools/spas, recreation facilities, pedestrian paths	Both private and common open space are required as follows: Minimum 100 sf private open space (consisting of a minimum of 50 sf enclosed storage) Minimum 200 sf common open space	
Parking Spaces See Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards		
Residential units	2 spaces per dwelling unit on-site within enclosed garage or parking structure	
Guest parking	0.25 space per unit located within 300 feet of units served	
Non-residential parking	Per Tustin City Code	
Signage	Per Chapter 4, Commercial Design Criteria and Tustin City Code	
Landscaping	Per Chapter 4, Commercial Design Criteria, Section 5.4, Residential Design Criteria and Tustin City Code	
Public Open Space	Per Chapter 4, Commercial Design Criteria and Section 5.4, Residential Design Criteria	
<p>Note:</p> <ol style="list-style-type: none"> Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure. On-site parking shall not be located between the street and the front of the building. 		

Table 5.3 – Development Standards for Mixed Use Buildings within DA-4

MIXED USE BUILDINGS	
Development Standards for DA-4	
<p>These Development Standards shall apply to both the residential and commercial components of mixed use development within DA-4. As indicated in Table 5.1, Residential Permitted Use Table, residential mixed use requires approval of a discretionary Residential Allocation Reservation (RAR) entitlement, in addition to any other entitlements required by the project, as provided in Section 6.1.3, Entitlement Processes. See also Section 5.4, Residential Design Criteria, and Chapter 4, Commercial Design Criteria, for commercial components of mixed use.</p>	
Mixed Use Development Standards DA-4	
Building Setbacks¹	
Front setback ²	0 feet minimum
Interior side setback From adjacent residential zone From adjacent non-residential zone or alleyzone	10 feet minimum 0 feet minimum
Corner side setback	0 feet minimum
Rear setback From adjacent residential zone From adjacent non-residential zone or alley	10 feet minimum 0 feet minimum
Building Height (Parking structures below buildings, rooftop uses, (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	3 stories maximum First floor plate 16 foot minimum
Non-Residential Tenant Space Maximum Size	10,000 square feet
Open Space (Per dwelling unit) Private requires minimum depth of 4 ft. and includes private patios, private balconies, private roof decks Common includes courtyards, common balconies, common roof decks, pools/spas, recreation facilities, pedestrian paths	Both private and common open space are required as follows: Minimum 100 sf private open space (consisting of a minimum of 50 sf enclosed storage) Minimum 200 sf common open space
Parking Spaces See Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards	
Residential units	2 spaces per dwelling unit on-site within enclosed unit garage or parking structure
Guest parking	0.25 space per unit located within 300 feet of units served
Non-residential parking	Per Tustin City Code
Signage	Per Chapter 4, Commercial Design Criteria and Tustin City Code
Landscaping	Per Chapter 4, Commercial Design Criteria, Section 5.4, Residential Design Criteria and Tustin City Code
Public Open Space	Per Chapter 4, Commercial Design Criteria and Section 5.4, Residential Design Criteria
<p>Note:</p> <ol style="list-style-type: none"> Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure. On-site parking shall not be located between the street and the front of the building. 	



Table 5.4 – Development Standards for Multi-Family Residential Buildings within DA-6A

MULTI-FAMILY RESIDENTIAL BUILDINGS	
Development Standards for DA-6A	
<p>These Development Standards shall apply to multi-family residential development within DA-6A. As indicated in Table 5.1, Residential Permitted Use Table, multi-family residential requires approval of a discretionary Residential Allocation Reservation (RAR) entitlement, in addition to any other entitlements required by the project, as provided in Section 6.1.3, Entitlement Processes. See also Section 5.4, Residential Design Criteria</p>	
Multi-Family Residential Development Standards DA-6A	
Building Setbacks¹	
Front setback ²	5 feet minimum
Interior side setback	3 feet minimum
Corner side setback	5 feet minimum
Rear setback	5 feet minimum
Building Height (Parking structures below buildings, rooftop uses (including gardens, lounges, and pools/spas), and architectural features (including chimneys and towers) shall not be included in the height measurement.)	2 stories maximum adjacent to a public street 4 stories maximum in interior 5 stories maximum adjacent to freeway
Open Space (Per dwelling unit) Private requires minimum depth of 4 ft. and includes private patios, private balconies, private roof decks Common includes courtyards, common balconies, common roof decks, pools/spas, recreation facilities, pedestrian paths	Both private and common open space are required as follows: Minimum 100 sf private open space (consisting of a minimum of 50 sf enclosed storage) Minimum 200 sf common open space
Parking Spaces See Sections 2.3.2.1, Provisions for Reduced Parking and 6.3.3, Modification of Parking Standards	
Residential units	2 spaces per dwelling unit on-site within enclosed garage or parking structure
Guest parking	0.25 space per unit located within 300 feet of units served
Landscaping	Per Section 5.4, Residential Design Criteria and Tustin City Code
Note: 1. Setback shall be measured as the perpendicular distance from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure. 2. On-site parking shall not be located between the street and the front of the building.	

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5.4 Residential Design Criteria

A major component of the DCCSP is the introduction of residential use through a discretionary entitlement, as outlined in Chapter 6, Administration and Implementation Plan. With discretionary approval, residential mixed use is an option in all DAs except DA-5 and DA-6A. Multi-family residential is allowed with discretionary approval in DA-6A (however within DA-6A, each parcel existing at the time of approval of the DCCSP is permitted one single-family detached dwelling unit by right, subject to Design Review, provided the unit is deducted from the residential bank discussed in Chapter 6).

There are two basic types of mixed use development: vertical and horizontal. Vertical mixed use occurs when the residential use is located above the first floor commercial use. Horizontal mixed use occurs when a residential use is located on the first floor in a separate building from the commercial use on the same parcel.

In addition to the following Residential Design Criteria, see Figure 2.3, Principal or Non-Principal Streets, to identify which streets within the DCC are appropriate for integrated vertical mixed use development (principal or non-principal streets), and horizontal mixed use (non-principal streets only).

These Residential Design Criteria establish high quality standards for mixed use and multi-family residential development in the DCC. (The design of the commercial component of mixed use is addressed in Chapter 4, Commercial Design Criteria.)

Within the Residential Design Criteria:

- “Shall” indicates a mandatory provision.
- “Should,” “recommended,” “encouraged” or like terms indicate design that is promoted but not required.
- “May” indicates design that is permitted but not required.
- “Prohibited” indicates design that is not allowed.
- Photographic illustrations are offered as a general visual aid and are not meant to dictate the precise design.

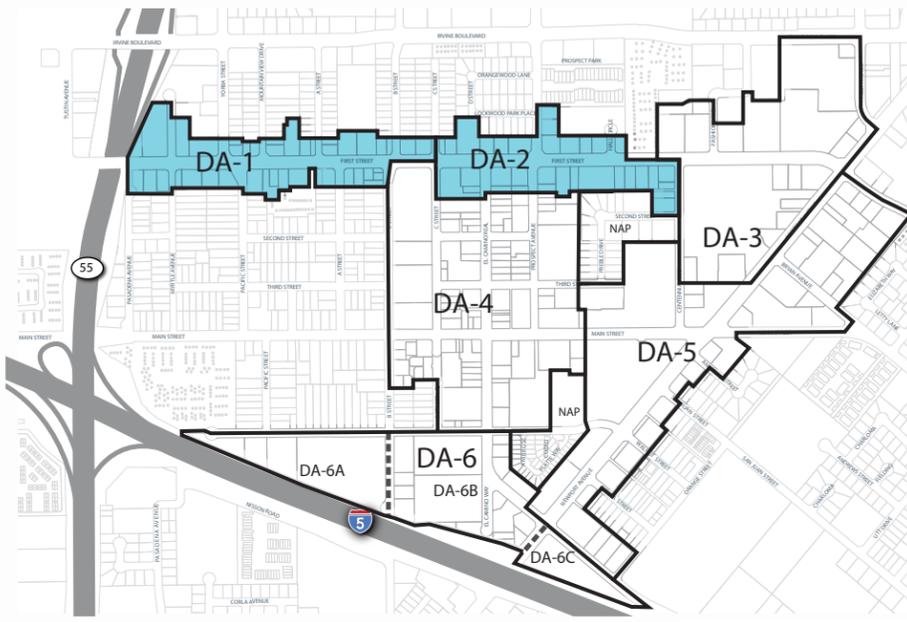
5.4.1 Residential Mixed Use Vision

The introduction of residential mixed use creates the opportunity to enhance pedestrian orientation and encourage lively development within the DCC.

KEY COMPONENT
Mixed Use

5.4.1.1 Vision for DA-1 and DA-2

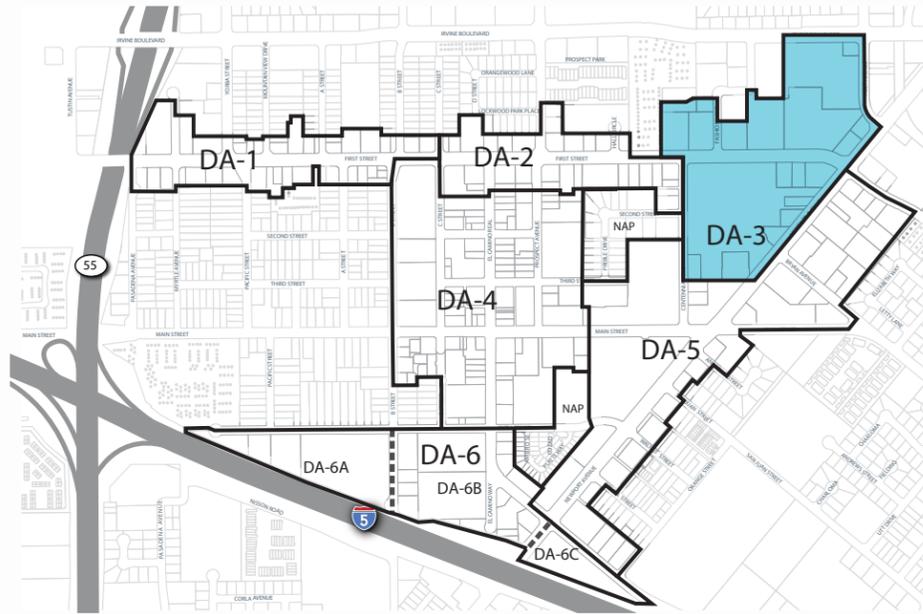
The following photos provide examples of the vision for residential mixed use development in DA-1 and DA-2, consistent with the Design Criteria for this subsection. Due to the small parcel sizes, mixed use within DA-1 and DA-2 is anticipated to be a vertical format.





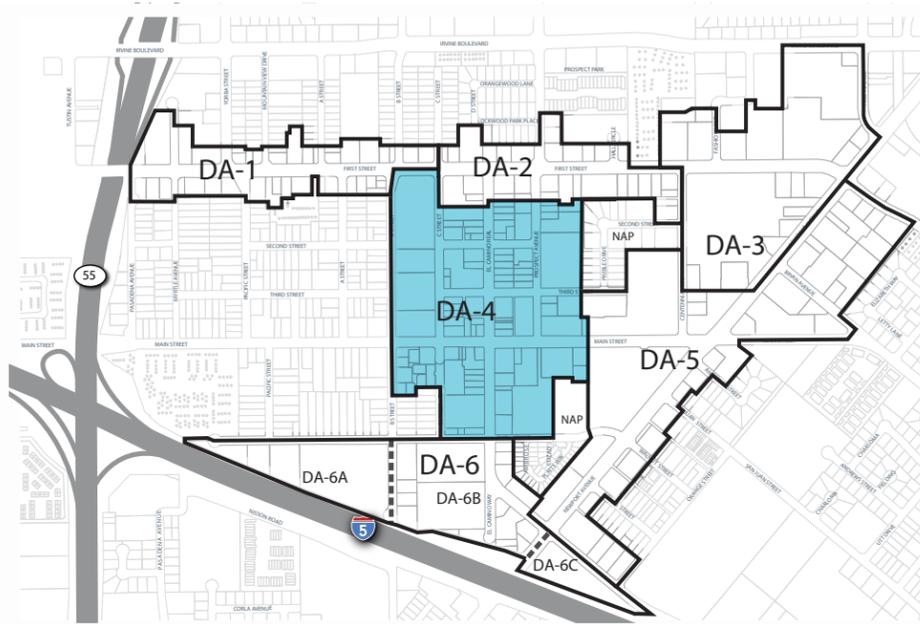
5.4.1.2 Vision for DA-3

The following photos provide examples to illustrate the vision for residential mixed use development within DA-3, consistent with the Design Criteria for this subsection. Mixed use within DA-3 is anticipated to include both vertical and horizontal formats.



5.4.1.3 Vision for DA-4

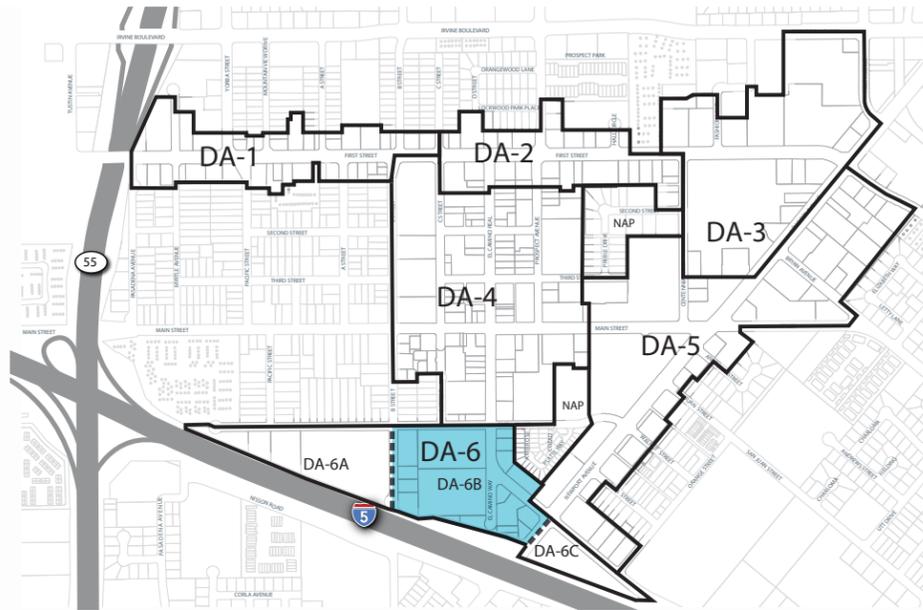
The following photos present examples of the vision for mixed use development in DA-4, consistent with the Design Criteria for this subsection. While horizontal and vertical format is allowed, due to the generally small parcel sizes, mixed use within DA-4 is anticipated to typically be vertical format.





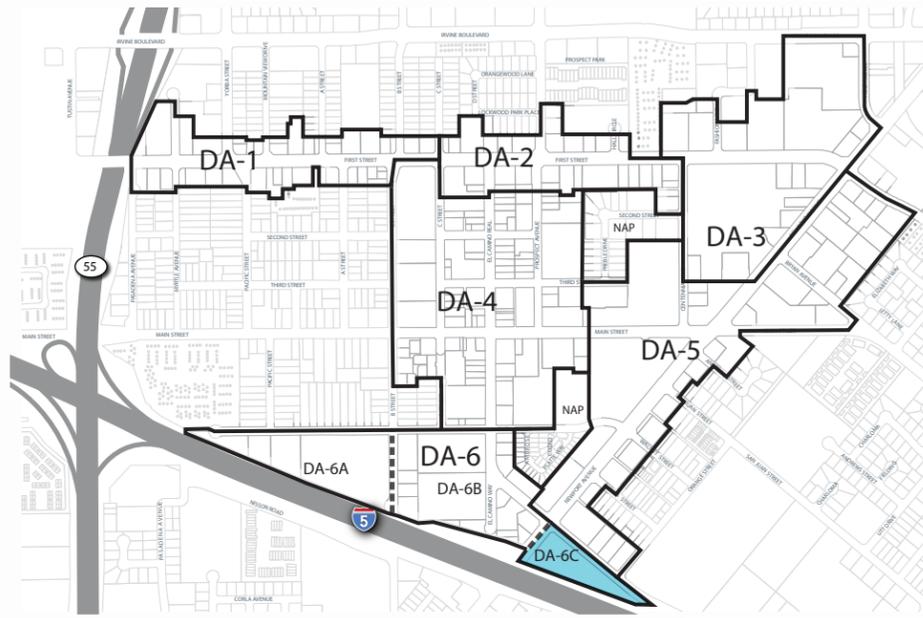
5.4.1.4 Vision for DA-6B

The following photos provide examples to illustrate the mixed use vision for DA-6B, consistent with the Design Criteria for this subsection. Mixed use within DA-6B is anticipated to include both vertical and horizontal formats.



5.4.1.5 Vision for DA-6C

The following photo examples provide a visual representation of the residential mixed use vision for DA-6C, consistent with the Design Criteria for this subsection. Mixed use within DA-6C is anticipated to be horizontal format.



Corner buildings may place the primary commercial entry at the corner.



5.4.2 Residential Mixed Use Design Criteria

- 1. Building siting.** Vertical Mixed use buildings located adjacent to a street shall be sited and oriented so that the primary commercial entry is from the sidewalk. Buildings on corner lots may place the primary commercial entry facing the intersection or the primary street. Secondary commercial and residential entrances may be from the street, interior courtyards and parking lots.

- 2. Consider adjoining properties.** The siting and design of buildings shall take into consideration adjoining properties (especially historic properties) to minimize nuisances related to noise, light intrusion, shadows, viewsheds, and traffic impacts on either property and ensure design compatibility. Upper floors are encouraged to be stepped back to promote compatibility with adjoining properties.

- 3. Design of mixed use street frontage.** With vertical mixed use, the street frontage (excluding driveways and pedestrian walkways) shall consist of commercial use with display windows. A public or semi-public pedestrian space may be incorporated into a portion of the commercial frontage or the building may be set back to accommodate a pedestrian area in front of the building. The minimum interior depth of commercial spaces associated with vertical mixed use shall be 45 feet.

- 4. Separate entrances.** For vertical mixed use buildings, upper story residential entrances shall be clearly distinguished in design and location from commercial entrances.



The large opening on the left side provides an entry to the residential units separate from the store entrances.



Stoops provide a transition from private to public space.

- 5. Access to ground floor units.** Horizontal mixed use buildings should be designed to provide direct access to the exterior from individual ground floor dwellings to create activity along the building facade. Ground floor units may be raised above adjacent sidewalks and include stoops or covered porches. To contribute to the vibrancy, units should be designed with social spaces, such as living and dining rooms, placed near the street.
- 6. Vehicular access.** Vehicular access should be provided from side streets and alleys whenever possible. The number of curb cuts for vehicular entry into the site shall be minimized.
- 7. Parking for commercial and residential uses.** Parking areas should be located and designed to facilitate access to both commercial and residential entries. Clear and attractive pedestrian pathways shall be provided between the parking areas and buildings. Parking should be integrated within the project and visually de-emphasized to the greatest extent possible.
- 8. Separate resident parking.** Parking areas for commercial patrons and residential guests shall be clearly marked and separated from resident parking. Restricted access to parking for residents is encouraged via a gate code or other security mechanism. The DCCSP allows tandem spaces for residential parking within a parking structure.

- 9. Bicycle Parking.** Mixed use development is encouraged to provide bicycle racks as discussed in Section 4.5.2, Parking Lots and Parking Structures.
- 10. Parking location.** Parking is encouraged to be located behind the building(s) near the rear of the parcel. Where possible, rear parking lots shall be sited ccontiguously and designed with reciprocal access so vehicles can travel from one parking lot to another, reducing the number of needed curb cuts.
- 11. Screening parking adjacent to street.** If it is not reasonably feasible to locate parking behind a building, as determined by the approval body, it may be provided along the side of the building if significantly screened from the street by a minimum six foot wide landscape strip that includes trees and shrubs. On-site parking shall not be located in front of mixed use buildings.
- 12. Other landscape screening.** Parking lots shall utilize landscaping to diminish the view of vehicles from on-site and adjacent uses. Tall perimeter landscaping should be used to screen ground floor views and canopy trees should be dispersed throughout the lot to screen upper floor views.
- 13. Shared parking.** Opportunities for shared parking should be considered when the peak parking demand times differ for the various commercial land uses. Refer to DCCSP Section 2.3.2.1, Provisions for Reduced Parking, Section 6.3.3, Modification of Parking Standards, and TCC Section 9264a, Reduction of Off-Street Parking Requirements – Joint Use Parking Areas.
- 14. Service areas.** Loading areas, service entrances, and trash enclosures shall be located to avoid impact to pedestrian and vehicular circulation and screened to prevent intrusion upon public or neighboring views.
- 15. Open space for residents.** Private and common open space for residents shall be incorporated into mixed use developments. Common open space may be provided through rooftop amenities such as gardens, lounges and/or pools /spas. (Refer to Tables 5.2 and 5.3 for required open space square footage and types of open space amenities.)



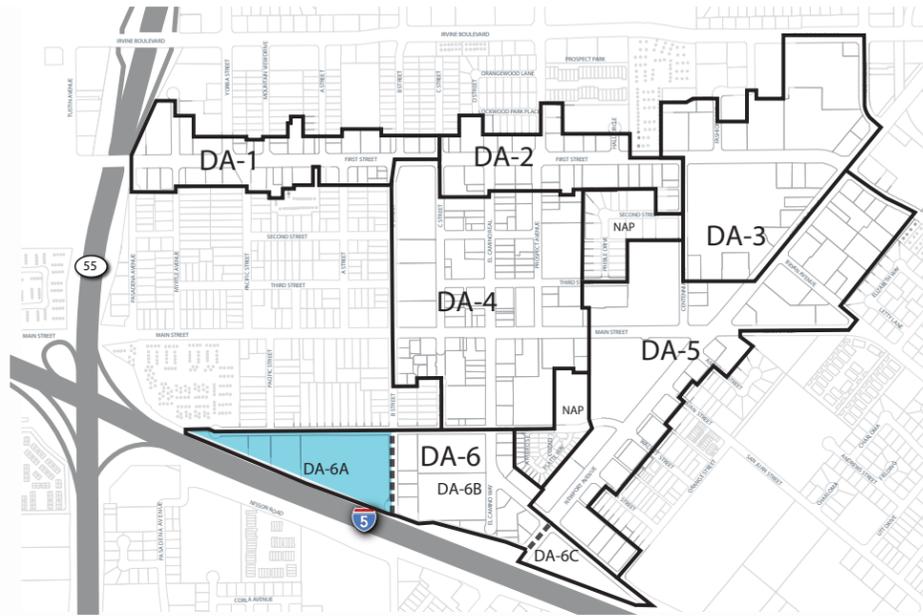
Common open space can be provided on the building roof.

- 16. Public open space.** Public open space shall also be provided, typically near the street or adjacent to commercial uses.
- 17. Connecting land uses through open space.** In horizontal mixed use, connecting plazas, paseos, open space, and pathways shall be used to integrate adjacent land uses on the site. For example, a public plaza that opens to the street can serve as a physical link to adjacent commercial and residential uses.
- 18. Common open space amenities.** Common open space shall feature seating conducive to both groups and individuals, shade, and pedestrian-oriented landscaping and lighting.

KEY COMPONENT
Multi-Family Residential

5.4.3 Multi-Family Residential Vision & Design Criteria

The following photo examples provide a visual representation of the development desired for DA-6A, consistent with the Design Criteria for this subsection.





These Multi-Family Residential Design Criteria reflect the neighborhood context within DA-6A. On the north side of Sixth Street are single-family homes within the Cultural Resources District. The following criteria emphasize compatibility with the adjacent neighborhood.

- 1. Reflect adjacent architectural styles.** The building massing, articulation, architectural features, and materials shall be strongly influenced by historic architectural styles within the adjacent Cultural Resources District neighborhood.
- 2. Building massing.** The building massing for multi-family buildings shall be reminiscent of single-family dwellings and shall avoid long, monolithic or rectangular block-shaped buildings.
- 3. Requirements for attached units.** Multi-family buildings may consist of attached units, if a high degree of change in horizontal and vertical wall planes is provided, or may be detached or semi-detached units.
- 4. One-story elements along front façade.** Multi-family buildings shall employ one-story elements such as covered porches and porticos along the front façade of each unit facing Sixth Street.
- 5. Building articulation and detail.** Building elevations visible from a street or freeway shall provide significant building articulation and architectural detail to avoid large blank walls and reduce building bulk.
- 6. Parking location.** Resident parking for DA-6A shall be provided within enclosed garages and guest parking shall be screened from the street through building placement.
- 7. Open space.** Open space shall be carefully designed to encourage gathering and social interaction. The detailed design of open space areas shall make them comfortable and inviting to ensure that they are well used. Common open space may be provided through roof-top amenities such as gardens, lounges and/or pools/spas. Common open space shall feature seating conducive to both groups and individuals, shade, and pedestrian-oriented landscaping and lighting.
- 8. Pedestrian nodes.** Each multi-family development shall provide at least one pedestrian node adjacent to Sixth Street to facilitate community interaction and gathering. Examples of nodes may include, but are not limited to:
 - Raised landscape planters that provide ledge seating shaded by canopy trees
 - Flower or succulent gardens with seating

- Wide entry stoops
- Individual unit landscaped courtyards or patios



6

ADMINISTRATION AND IMPLEMENTATION PLAN

Ch. 6 Administration and Implementation Plan

The following provisions establish the DCCSP administration and implementation plan.

6.1 Administration

This Section outlines the processing of entitlement applications and delineates administration roles under the DCCSP.

6.1.1 Responsibility

The Community Development Department shall be responsible for the administration of the DCCSP, including processing applications, assistance to applicants, administration of the Residential Allocation Bank, and reviewing projects for compliance with the DCCSP.

The Public Works Department shall be responsible for overseeing improvement projects involving streets, public parking, and other public properties within the DCCSP boundaries.

6.1.2 Applicability

All development and land uses within the DCCSP boundaries shall substantially comply with the provisions, standards, and criteria set forth in this document. Where conflicts exist between the standards of the DCCSP and those in the TCC, the standards contained in the DCCSP shall apply. Any area of site development, administration, review procedures for implementing projects, landscaping requirements, parking regulations, or other provisions not expressly addressed in this document shall be subject to the provisions of the TCC. Where the DCCSP is silent, the provisions of the TCC shall prevail. A portion of the DCC is within the Cultural Resources District (CRD); provisions within TCC Section 9252, Cultural Resource District, including required Certificates of Appropriateness and the CRD Design Guidelines, remain in force and apply to all properties within the CRD and to structures more than 50 years old within the DCC.

6.1.3 Entitlement Processes

6.1.3.1 Design Review, Variances, Conditional Use Permits, and Other Entitlements

Applications for Design Review are generally required for all projects and shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Section 9272, Design Review, of the TCC.

INSIDE THIS CHAPTER

6.1 Administration

6.2 Administrative Specific Plan Adjustments

6.3 Minor Specific Plan Modifications

6.4 Specific Plan Amendments

6.5 Severability

6.6 Required Findings

6.7 Implementation

6.8 Funding



Applications for Zoning Permits shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Section 9290, Zoning Permits, of the TCC.

Applications for Temporary Use Permits shall be processed in accordance with Section 9270b, Approval of Temporary Uses, of the TCC.

Applications for Variances shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Section 9292, Variances, of the TCC.

Applications for Conditional Use Permits shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Section 9291, Conditional Use Permits, of the TCC.

Applications for Subdivisions shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Article 9, Chapter 3, Subdivision Code, of the TCC. Approval of a Development Agreement shall be required for approval of a Subdivision Map.

Applications for Development Agreements shall be processed in accordance with Section 6.1.4, Approval Authority, of this Specific Plan and Article 9, Chapter 6, Development Agreements, of the TCC. Approval of a Development Agreement shall be required for approval of a Subdivision Map.

Applications for Exceptions shall be processed in accordance with Section 6.3.2, Exceptions, of this Specific Plan.

6.1.3.2 Residential Allocation Reservation from Residential Allocation Bank

Residential mixed use in any DA (except where it is prohibited in DA-5 and DA-6A) and multi-family residential in DA-6A require approval of a discretionary Residential Allocation Reservation (RAR), as specified in Table 5.1, Residential Permitted Use Table, to allocate residential units to the project from the DCC Residential Allocation Bank (see Table 6.1).

Table 6.1, Residential Allocation Bank, specifies the potential dwelling unit allocation available for each DA and for transfer from other DAs. The Residential Allocation Bank establishes a maximum number of 887 new dwelling units that may be developed within the DCC area (including 140 units previously allocated to the approved Vintage project on Sixth Street), except as otherwise provided in Article 9, Chapter 1, Incentives for the Development of Affordable Housing. From the effective date of adoption of the DCCSP, the following units shall be allocated as assigned to the respective DAs.

Table 6.1 Residential Allocation Bank

Development Area (DA)	Initial Allotment of Dwelling Units	Maximum Number of Units That May Be Transferred to this DA ²
DA-1	45	11
DA-2	92	23
DA-3	200	50
DA-4	150	38
DA-5	0	0
DA-6	400 ¹	100
Total	887	222

Notes:

1. The 400 units allocated to DA-6 include 140 units for the Vintage condominium project on Sixth Street.
2. The maximum number of units that may be transferred as part of the RAR entitlement process (without requiring Planning Commission approval) is 25 percent of the original unit allocation.

A limited number of existing historic single family residential units are located within the DCCSP boundaries that are not counted toward and do not subtract from the allotment of dwelling units identified in the Residential Allocation Bank.

6.1.3.3 Residential Allocation Reservation (RAR) Review Process

The following lists the two phases of the RAR process under the DCCSP in sequential order. Phase one and two of RAR applications may be processed concurrently subject to CCD approval. Dwelling units may not be approved as part of an entitlement action without completing phase one and two of the RAR process. Dwelling units may be transferred from one DA to another through approval of a RAR, as described in Section 6.3.1, Transfer of Residential Units. A flowchart of the RAR process is provided in Figure 6.1.

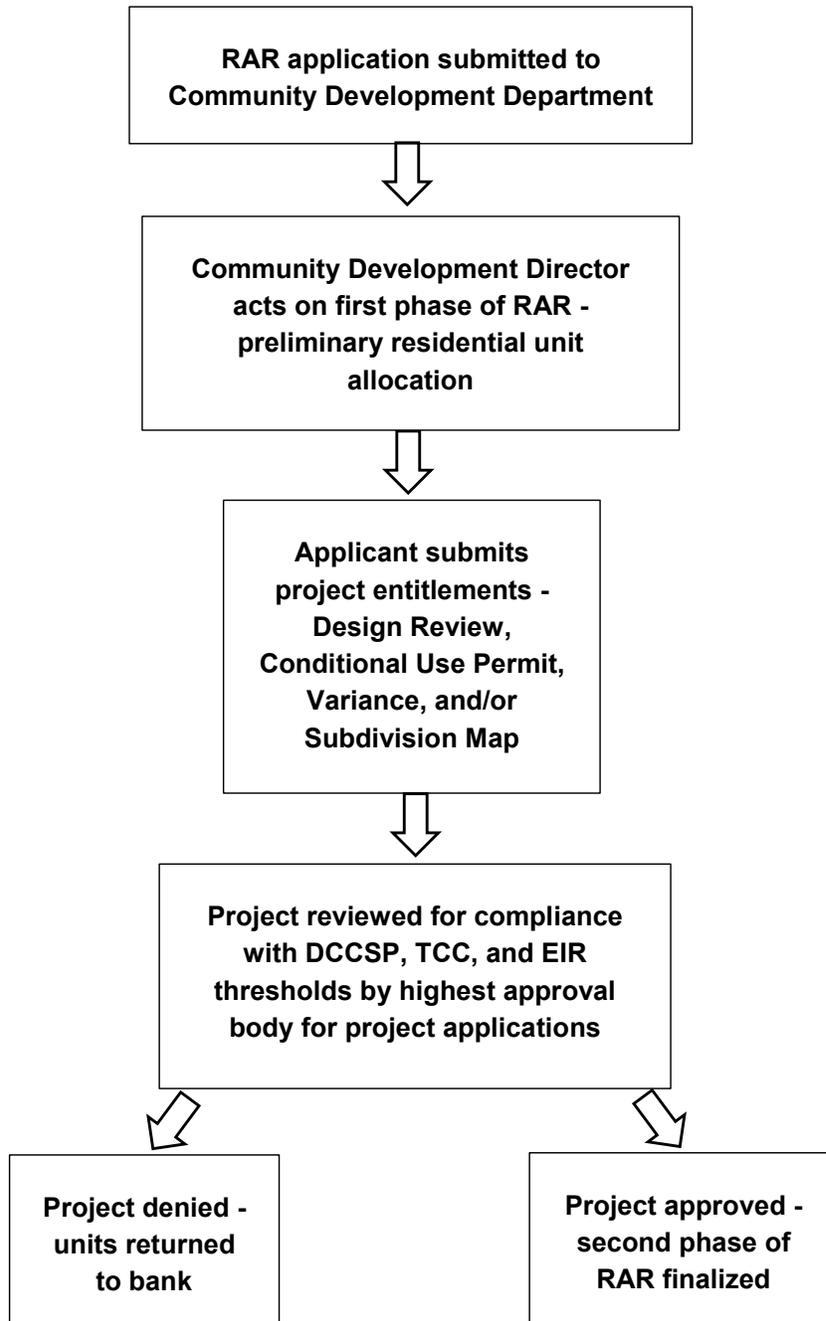
The following list describes the RAR process in sequential order.

Phase One

1. The applicant shall submit the RAR application on the appropriate form to the Community Development Department (CDD). The RAR application shall include the required items to be considered complete for processing.
2. The Community Development Director shall review and consider whether a proposed project substantially complies with the DCCSP Development Plan (Chapter 2), Residential Provisions (Chapter 5), Commercial Standards and Commercial Design Criteria (Chapters 3 and 4) for mixed use projects, and Required Findings (Section 6.6) and whether residential units are available in the Residential Allocation Bank.



Figure 6.1 RAR Entitlement Processing Flowchart



3. If the Community Development Director determines that the project substantially complies and residential units are available, the Director shall make the Required Findings (refer to Section 6.6) and approve the first phase of the RAR process, which shall provide a preliminary allocation of residential units for the project. The first phase approval shall be accompanied by conditions of approval establishing the time limitation for submittal of the second phase of the RAR process, which shall include applications and submittals required for the project such as Design Review, Conditional Use Permit, Variance, and/or Subdivision Map(s). Following phase one approval of the RAR, final approval of the RAR (phase two) shall not exceed a period of 180 days. A time extension may be considered at the discretion of the CDD. The RAR preliminary allocation of units in the first phase shall not be considered final until approval of the requested entitlement(s) in the second phase.

Phase Two

4. The approval authority indicated in Section 6.1.4 shall review and act on the requested entitlement(s) in the second phase (including any requests for unit transfers in compliance with Section 6.3.1), and in conjunction approve, approve with conditions, or deny the RAR. This determination is subject to appeal pursuant to the appeal procedures in the TCC.

Other

5. Following final approval of the RAR and requested entitlement(s), all construction related permits, including demolition, grading, and building permits, must be obtained within 180 days. Following building permit issuance, construction must commence within 180 days. Time extensions may be considered at the discretion of the CDD.
6. The allocation of units shall expire and the units redeposited into the Residential Allocation Bank for use by other projects if any of the following occur: 1) the time limits established in the phase one conditions of approval expire without submittal of phase two entitlement applications, 2) the entitlement(s) associated with phase two of the RAR application are denied, 3) construction related permits are not obtained within the required time limits or approved for extension, or 4) permits are not utilized within 180 days.
7. The CDD shall monitor the Residential Allocation Bank and deduct residential units reserved under phase one, approved under phase two, transferred, expired or constructed through the RAR process.

6.1.4 Approval Authority

1. For entitlements other than RARs, the approval authority shall be as specified in the TCC.
2. The Community Development Director shall be responsible for acting upon applications for RARs as specified in Section 6.1.3.3, including RAR applications requesting transfer of up to 25 percent of the original allocation of units in the subject DA, as discussed in Section 6.3.1, Transfer of Residential Units. A RAR approval shall not be transferred without the expressed written approval of the Community Development Director.
3. The Community Development Director reserves the right to forward a RAR to the Planning Commission for consideration and action.



4. The Planning Commission shall be responsible for acting upon RAR applications requesting transfers greater than 25 percent of the allocated housing units.

6.1.5 Noticing Requirements

1. Notice of Public Hearings
 - a. Public hearings required by the TCC shall be noticed as specified in the TCC.

6.1.6 Appeals

Decisions of the Community Development Director may be appealed to the Planning Commission and decisions of the Planning Commission may be appealed to the City Council in accordance with the procedures set forth in TCC Section 9294, Appeals.

6.1.7 Interpretations

If ambiguity arises concerning the meaning or appropriate application of provisions of the DCCSP, the Community Development Director shall make the appropriate determination. The Community Development Director may deem it appropriate for the Planning Commission to consider and make certain determinations. In so doing, the Community Development Director or Planning Commission, as applicable, shall consider the following factors and document findings accordingly:

- The case is similar to previous interpretation of similar provisions;
- The interpretation responds satisfactorily to the vision, intent, and purpose of the Specific Plan;
- The interpretation does not alter the policy intent of the Specific Plan;
- The decision constitutes a sound precedent for other similar situations; and
- The resulting project is consistent with the General Plan.

The Community Development Director shall also be responsible for interpreting whether a proposed use in the DCC is similar in character to the uses allowed in Table 3.1, Commercial Permitted Use Table or Table 5.1, Residential Permitted Use Table, based on the criteria for use determination contained in TCC Section 9298b. The Community Development Director may refer the matter to the Planning Commission for a use determination.

6.1.8 Legal Nonconforming Structures and Uses

Legal nonconforming structures and uses shall be subject to the provisions of Section 9273, Legal Nonconforming Structures and Uses, of the TCC.

6.1.9 Definitions

Whenever the following terms are used in the DCCSP, they shall have the meaning specified below unless the context clearly indicates the contrary. Terms not listed shall be defined per the TCC, or if not listed in the TCC, in accordance with accepted usage.

Building Setback shall mean the perpendicular distance measured from the ultimate street right-of-way line, or the property line, to the exterior wall of the closest structure.

Commercial Use as applied within the DCCSP shall mean a non-residential use such as retail, service, automotive, medical, food service, and hospitality. Office use is generally considered a commercial use within the DCCSP, except where provisions aim to create pedestrian activity, office use shall be considered distinct from commercial use.

Living Alley shall mean a transformed traditional alley or a new street with a low volume of traffic that is designed as a shared place for cars and people, where areas of exclusive pedestrian use are given design priority, yet vehicles are allowed to share space with pedestrians and bicyclists in a controlled and safe manner.

Living Wall shall mean a building wall visually enhanced through live plantings grown in a supported vertical system generally attached to an external wall. Living walls incorporate vegetation, growing medium, irrigation and drainage into a single system incorporating multiple container plantings to create a vegetation cover.

Mixed Use shall mean a building or development with both residential use and commercial use. There are two basic types of mixed use development: vertical and horizontal. Vertical mixed use occurs when the residential use is located above the ground floor commercial use. Horizontal mixed use occurs when a residential use is located on the first floor in a separate building from commercial use, but on the same parcel.

Pedestrian Node shall mean a gathering space typically formed at the crossroads of pedestrian circulation routes and around activity areas, typically including seating, public art, sculpture, shade, landscaping and/or lighting to accommodate and promote pedestrian activity, contributing to a pleasant aesthetic environment.

6.2 Administrative Specific Plan Adjustments

The DCCSP allows for administrative clarifications provided they do not alter the overall intent or requirements contained in the DCCSP. The Community Development Director shall have the authority to review and make decisions regarding administrative adjustments. Administrative adjustments may consist of:

1. Refinements in Specific Plan language that increase clarity.
2. Corrections of conflicting or confusing language or inadvertent errors in the originally approved plan.

6.3 Minor Specific Plan Modifications

DCCSP modifications that are considered minor shall fall within the approval authority indicated in the subsections below.



6.3.1 Transfer of Residential Units

The DCCSP allows for transfer of allocated residential units from one DA to another through approval of a RAR. The Community Development Director may approve a transfer of units up to 25 percent of the original DA unit allocation, as indicated in Table 6.1, Residential Allocation Bank, and shall determine the donating DA(s). Transfers greater than 25 percent shall be reviewed and acted upon by the Planning Commission, which will take into consideration the donating DA(s) recommended by the Community Development Director. Transferred units shall be deducted from the donating DA(s) through an accounting record maintained by the Community Development Department so that the maximum number of new units within the DCCSP shall not exceed 887 units.

6.3.2 Exceptions

1. The Community Development Director shall have the authority to issue Exceptions to the Commercial and/or Residential Development Standards in compliance with the limitations established under Zoning Administrator duties in TCC Section 9299b, Zoning Administrator Duties; (1), Minor adjustments, (2), Variances when required by the City Code, and, (3), Conditional Use Permits when required by the City Code, provided Exceptions are consistent with the Required Findings in Section 6.6.
2. The Community Development Director shall have the authority to issue Exceptions that provide for flexibility in fulfilling the intent of the Development Plan (Chapter 2), the Commercial Design Criteria (Chapter 4) or the Residential Design Criteria (Chapter 5), provided Exceptions are consistent with the Required Findings in Section 6.6.

6.3.3 Modification of Parking Standards

Parcels located within the DCC may take advantage of alternative parking requirements through various provisions:

- The DCCSP establishes new provisions unique to the DCC within the following subsections.
- The DCCSP incorporates alternative parking provisions from the TCC that were originally applicable only within the Parking Overlay District and/or the Cultural Resources District to now apply to all parcels within the DCC as provided below.
- The DCCSP references existing provisions for alternative parking requirements in the TCC below.

For modifications that are discretionary, the review authority shall be the approval body for the related project entitlement.

1. Mixed Use Parking Alternatives
 - A. Mixed Use Commercial Parking Reduction

A reduction in the required on-site commercial parking spaces for mixed use development may be approved as provided in this subsection since patrons to commercial establishments

located within mixed use development have the ability to park once for a multiple purpose trip. (Required parking for residential units shall not be reduced.) Under the project Design Review application, the project approval body may conditionally approve up to a 20 percent reduction in parking space requirements for non-residential uses within a mixed use development based on an applicant's submittal of a parking analysis prepared by a California licensed traffic engineer, provided the findings specified in Section 6.6, Required Findings, are met.

B. Mixed Use Residential Parking on Adjacent Site(s)

Within mixed use development, the required residential parking spaces (including tenant and guest spaces) shall not be reduced in number, but may be provided, in part or in whole, on an immediately abutting parcel as provided in this subsection. Under the project Design Review application, the project approval body may conditionally approve the location of required tenant and/or guest parking spaces on an immediately abutting parcel provided the applicant has submitted the following (or they are made conditions of approval): 1) a signed reciprocal access agreement between the owners of the subject parcels, if deemed necessary by the Community Development Director given the parking layout and circulation pattern, 2) a signed binding lease agreement between the owners of the subject parcels to the satisfaction of the City Attorney, 3) a covenant or other mechanism to the satisfaction of the City Attorney recorded against the subject parcels, and 4) documentation supporting the findings specified in Section 6.6, Required Findings.

2. Non-Residential Parking Alternatives

Through this subsection, the following provisions taken from the TCC are incorporated into the DCCSP and made applicable to all non-residential uses on parcels located in the DCC.

A. Commercial Parking Alternatives in the DCC

- 1) Commercial parking requirements within the DCC may be modified under any one or a combination of the following provisions:
 - a. On-site parking requirements may be waived upon presentation to the City of a long-term lease for private off-site parking accommodations within 500 feet of the business or activity to be served. The lease shall remain a condition of the business license.
 - b. All or a portion of the required number of commercial parking spaces may be satisfied by depositing with the City a Parking Exception Fee to be used for public parking accommodations within the area. The amount of the Parking Exception Fee shall be determined by Fee Resolution of the City Council. The Planning Commission shall make a determination that the proposed project requesting payment of the Parking Exception Fee meets the following findings.



- i. The proposed project is an infill project located within the DCC.
- ii. The proposed project is considered to be relatively small.
- iii. The proposed project has incorporated building or site design enhancements that make it an outstanding addition to the DCC.
- iv. The proposed project is aesthetically superior to one that provides all required parking on site.
- v. The project applicant shall agree to pay an annual fee for each commercial parking space not provided on site.

3. Other Parking Alternatives

The TCC provides the following parking alternatives that may be used within the DCC in compliance with the provisions of the cited sections.

- A. Joint Use of Parking Areas (Section 9264a)
- B. Historic Resource Residential Parking (Section 9264b)
- C. Outdoor Restaurant Seating (Section 9277)
- D. Off-Site Parking (Section 9266b2), which shall be limited to non-residential uses
- E. Reduction in Parking Due to American Disabilities Act Upgrade (Section 9264c)

4. Review of Approved Modification to Parking Standards

A property owner/applicant who has received approval of a modification of parking standards for a property located within the DCC shall agree to regular inspections and review of the off-street parking conditions. The Community Development Director shall provide notice to the property owner/applicant that the approved modification of parking standards is under review and subject to violation of the TCC and/or permit revocation if one of the following occur: 1. the City receives a complaint related to sufficient parking supply associated with the use that was permitted with the modification of parking standards; or, 2. the Community Development Director or designee observes a parking condition that differs from that analyzed and predicted in the analysis used in support of issuing the modification of parking standards and that condition is an undersupply of parking and/or is detrimental to the public's health, safety or welfare.

During the City's review of the permit, the Community Development Director shall have the authority to request the property owner or applicant requisition a licensed California traffic engineer to prepare a new parking study that assesses to the satisfaction of the Community Development Director the current parking conditions and recommends measures to rectify the undersupply of parking, including, but not limited to parking attendants, permit parking and/or time-restricted parking. The Community Development Director shall have the authority to approve additional conditions of approval to specifically address the situation that led to the complaint or undersupply of parking spaces. The additional conditions of approval may alter previous operational conditions. Alternatively, the Community Development Director shall have the authority to refer the modification of parking

conditions to the original approval body or the Planning Commission for review and modification of the approved permit or revocation of the permit. Failure to comply with the City's requests or findings and conditions of approval of the permit to the satisfaction of the Community Development Director may be cause for modification or revocation of the permit.

6.4 Specific Plan Amendments

Specific Plan Amendments are required when proposed changes to text or exhibits significantly alter the intent of the DCCSP and/or proposed projects exceed the thresholds established above in Section 6.3, Minor Specific Plan Modifications. Specific Plan Amendments shall be processed in the same manner as the original DCCSP approval, which includes review by the Planning Commission and adoption by the City Council.

6.5 Enforcement and Severability

The regulatory portions of this Specific Plan have been adopted by Ordinance and therefore are subject to penalty and citation provisions of the Tustin City Code, in addition to the City's authority to seek civil litigation in a court of law.

If any section, subsection, subdivision, sentence, clause, phrase, figure, exhibit, table, or portion of this Specific Plan is found to be invalid or unconstitutional by a court having jurisdiction, such a decision shall not invalidate the remaining portions, in whole or in part, of the Specific Plan.

6.6 Required Findings

The following findings shall serve as the basis for determining approval, approval with conditions, or denial of applications for projects within the DCC. These findings are organized into four categories: A applies to all new development, B applies to residential mixed use or multi-family residential projects, C applies to mixed use or multi-family projects that request a transfer of residential units from another DA, and D applies to Exceptions.

- A. The following findings shall be made for all new development approved within the DCCSP:
 1. The development is consistent with the overall vision, goals, and intent of the DCCSP.
 2. The development is consistent with the policies and intent of the Development Plan (Chapter 2), including the Urban Design Plan (Section 2.2).
 3. The project, including its proposed uses and requested entitlements, complies with the Permitted Use Tables (Tables 3.1 and 5.1), and complements and contributes to the project area.
 4. The development substantially complies with the Commercial Development Standards (Chapter 3) and the Residential Development Standards (Section 5.3), as applicable.



5. The development substantially complies with the Commercial Design Criteria (Chapter 4) and the Residential Design Criteria (Section 5.4), as applicable.
 6. Compliance with applicable Development Standards and Design Criteria is demonstrated and ensured through Design Review of the project site plan, building elevations, floor plan, parking plan, landscape plan, lighting plan, access plan, refuse plan, and any other applicable plan(s) or document(s).
 7. Required parking for the project is provided entirely on-site, or by an alternative provided in Section 6.3.3, Modification of Parking Standards, which is approved concurrently or made a condition of approval. Approval of a parking alternative will have a beneficial impact and does not adversely affect development or the surrounding area.
 8. The development demonstrates high quality architectural design and site planning that reflects the nature of the site and the surrounding area through the following:
 - a. Incorporating roof forms and facades that provide building articulation, create visual interest, and reduce the appearance of uniform building massing.
 - b. Creating a design that is both cohesive and varying in respect to architectural style, architectural details, windows, doors, colors, and materials.
 - c. Facilitating pedestrian orientation through building placement, building scale, and architectural design.
 - d. Ensuring associated elements including parking, service areas, landscaping, lighting, and pedestrian access and amenities are functional and serve to enhance the overall appearance and experience of the project.
 - e. Siting and designing structures that relate to and respect adjacent development, particularly historic structures and sensitive land uses.
 9. For properties located within DA-4, or structures identified on the City's historic survey located anywhere within the DCCSP, the proposed development is of the highest quality and is consistent with the Cultural Resources District-Commercial Design Guidelines and/or Residential Design Guidelines, as applicable, and strives to preserve historic resources.
 10. The development is supported by existing infrastructure, or will be supported by infrastructure improvements paid for by the developer.
 11. The developer consents to enter into a mutually agreeable Development Agreement if the City requests.
 12. The project impacts are assessed through the approved DCCSP Program EIR or through subsequent environmental documentation in compliance with the CEQA Guidelines.
- B. In addition to the findings in A. above, the following findings shall be required for residential mixed use or multi-family residential projects:
1. The project complies with the Residential Allocation Reservation (RAR) application process as provided in Section 6.1.3, Entitlement Processes, and units are available within the applicable DA.

2. The development provides a sufficient mix of uses, allocated as follows:
 - a. Commercial space is sufficient and placed along the frontage on principal streets.
 - b. Office use is generally reserved for upper floors, unless ground floor office meets one of the following criteria:
 - i. Located behind commercial development at least 45 feet deep; or
 - ii. Designed with prominent display windows similar to retail uses.
 - c. Residential units are located on upper floors in the case of vertical mixed use or on non-principal street frontages for horizontal mixed use.
 3. The proposed development is consistent with the City's affordable housing requirements.
 4. The proposed development provides at least one of the following public benefits as part of the development, subject to the review and approval by the project approval body:
 - a. Provision of affordable housing significantly greater than the mandatory requirements contained in the City's affordable housing ordinance.
 - b. A public amenity that is easily accessible is included such as a public plaza that provides, at a minimum, seating, landscaping, and lighting.
 - c. Prominent public art, which is located and featured within easy public view.
 - d. Installation of a specific public infrastructure above any required by the project.
 - e. Off-site parking improvements above any required by the project.
 - f. Recreational amenities above any required by the project.
 - g. Other, as proposed and deemed important to and acceptable by the City.
 5. The development facilitates multi-modal transportation through building siting and design that provides convenient access for transit users, pedestrians and bicyclists. Where a development is adjacent to an existing or planned transit stop, building siting and orientation provide convenient access for transit users.
 6. The developer submits building plans to the Building Division within six months after project approval to fully construct the project in a timely manner within the period specified in the RAR conditions of approval.
 7. The applicant has sufficiently demonstrated its level of development experience to the City.
- C. In addition to the findings in A and B above, the following findings shall be required for residential mixed use or multi-family applications for Residential Allocation Reservations (RARs) that include a request for a transfer of residential units from another DA, in compliance with Section 6.3.1, Transfer of Residential Units.
1. The Community Development Director has identified units available for transfer from another DA, which are not part of a foreseeable development project within the transferring DA.



2. A transfer of residential units is desirable because it creates a better high quality project that provides additional housing units, including affordable units or units restricted to special occupancy (seniors, disabled persons, or veterans), yet maintains compliance with the Development Standards (Chapter 3 and/or 5), Design Criteria (Chapter 4 and/or 5), and all other applicable provisions of the DCCSP.
 3. A transfer of residential units will benefit the project area by creating a more desirable project and will not negatively impact the donating or receiving DAs, as determined by the approval authority. The approval authority has no knowledge of pending development applications within the donating DA(s) that desire to utilize the units approved for transfer.
- D. In addition to the findings in A (and B and C when applicable), the following findings shall be required for granting of an Exception per Section 6.3.2.
1. An Exception to the provisions of the Development Plan (Chapter 2), Commercial Design Criteria (Chapter 4), or Residential Design Criteria (Chapter 5) meets at least one of the following required findings and the granting of an Exception will not be detrimental to the health, safety, or general welfare of residents, businesses, workers, property, or improvements in the area or to the City in general.
 - a. The Exception is due to a physical limitation on the site caused by one or more of the following:
 - i. Lot shape, topography, size or orientation
 - ii. Lot location, including the nature of the adjacent street
 - iii. Location or design of adjacent structures, particularly historic resources
 - b. Strict compliance without an Exception would preclude fulfillment of the Development Plan (Chapter 2) for the DA in which the project is located and/or the applicable Design Criteria (Chapter 4 and/or 5). The granting of an Exception will not constitute a grant of special privilege inconsistent with the limitations upon other similar properties.
 - c. The Exception provides a design solution that implements the intent of the DCCSP, specifically the Development Plan (Chapter 2) for the DA in which the project is located and the applicable Development Standards.
 - d. With an approved Exception, the development fulfills the intent of the provision in an alternative, high-quality manner that positively affects the development and does not have negative effects on adjacent properties, as determined by the review authority. Or, alternatively, the review authority deems the provision is not applicable to the proposed development.

6.7 Implementation

The various components presented in the DCCSP are supported by Table 6.2, Implementation Action Plan. The Implementation Action Plan provides a concise list of the improvements and programs desired to implement the vision contained within the DCCSP and indicates where such discussion is located in the DCCSP text.

In addition to the specific implementation actions presented in the previous chapters and sections of the DCCSP, several optional/alternative implementation tools identified in Table 6.2 are broadly presented for consideration here:

6.7.1 Business Improvement District (BID)/Parking District

Consideration should be given to forming a DCCSP Business Improvement District (BID) as a means for DCCSP property owners to support area-wide improvements, such as landscaping improvements and gateway signage, which can enhance patronage of area businesses. In addition, a Parking District would provide a funding source to manage or create public parking opportunities in the DCCSP.

6.7.2 Positive Business Climate Marketing

Consideration may be given to working with the Tustin Chamber of Commerce to aggressively market the DCC and foster a supportive regulatory climate to attract a variety of industries such as specialty retail, restaurants, health care, and innovative or entrepreneurial businesses.

6.7.3 Parking Awareness Program

Consider launching an outreach program and developing outreach materials for distribution to businesses to encourage business owners and their employees to park at locations that maximize available on- and off-street parking for customers. Program materials may address the economic benefits of increasing available patron parking through utilizing off-site employee parking. The program could seek support for outreach efforts from the Chamber of Commerce and local businesses.

6.7.4 Development Incentives

Consideration should be given to establishing development incentives to promote private revitalization efforts.

- Encourage businesses to provide outdoor dining.
- Foster development of public gathering areas such as plazas on private property.
- Encourage developers and investors to purchase and consolidate adjacent small lots to facilitate development.

Incentives to be considered may include:

- Allowing additional signage for the provision of high quality outdoor dining.



- Reducing requirements for common open space in mixed use projects if public space provided in the development is far superior to requirements.
- Reducing park fee requirements in exchange for provision of greatly enhanced public space within a development.

6.7.5 General Plan Amendments, Zone Changes and CEQA Compliance

The following are required for implementation of the DCCSP:

General Plan Amendments

Concurrent with the adoption of the DCCSP, the City of Tustin General Plan is amended by Resolution to provide consistency between the documents. The amendments to the General Plan include:

- General Plan Land Use Element Amendment: to update the Land Use Map to show the boundaries of the DCCSP and re-designate land uses within the Specific Plan area to “DCCSP - Downtown Commercial Core Specific Plan.”
- General Plan Circulation Element Amendment: to be consistent with the circulation changes resulting from conceptual improvements to Main Street, First Street, Second Street and Third Street.

Zone Changes

The DCCSP is adopted by Ordinance and defines the zoning regulations for the properties within its boundaries. The adoption of the Specific Plan is accompanied by a concurrent zoning map amendment to designate the area “Downtown Commercial Core Specific Plan (SP-12).”

To implement the DCCSP, associated actions include rescission of the First Street Specific Plan, which regulated land now located within the DCCSP boundaries, and rescission of the Planned Community (PC) zoning designation that was originally applied with the approval of the following projects that are now covered by the DCCSP: Tustin Village, Tustin Plaza, 13682 Newport Avenue, Blockbuster Music Plaza and Ambrose Lane Area B.

California Environmental Quality Act (CEQA) Compliance

The DCCSP has been prepared in conjunction with a Program-level Environmental Impact Report (EIR), which identifies potential impacts resulting from the proposed development and establishes mitigation measures that reduce them to a less than significant level, where feasible. As the lead agency, the City of Tustin will implement a monitoring program for the approved mitigation measures. To assist in this monitoring effort, a Mitigation Monitoring Program has been developed by the City as part of environmental findings and included in the Final EIR certified by the Tustin City Council.

The DCCSP EIR serves as the primary environmental document for future development undertaken within the Specific Plan area. The EIR is anticipated to be the definitive environmental document for project implementation within the Specific Plan area, including serving as a Program EIR for purposes of backbone

infrastructure improvements. Future development projects that require discretionary review will be examined against the analysis prepared for the EIR to determine if additional environmental documentation must be prepared. Developments that do not require additional discretionary review will not be subject to additional environmental documentation. However, a project applicant will be required to submit documentation substantiating the findings that the project conforms with the Specific Plan, and the potential environmental effects are within the parameters and timeframe analyzed within the Specific Plan EIR.

Additional environmental documentation may be required for future development projects proposed within the Specific Plan area pursuant to State law. However, documents for subsequent site-specific projects may use the “tiering” concept, as provided by Section 15385 of the State CEQA Guidelines. The tiering concept is a process by which the City of Tustin, as lead agency, can adopt the programmatic EIR focusing on the “big picture.” Future site-specific development projects will be evaluated on a project-specific basis, and may be excused from repeating the broad environmental analysis examined in the comprehensive, programmatic EIR for the Specific Plan area. Streamlined CEQA review may be used for site-specific future development provided the project is consistent with the findings of the EIR, the mitigation measures described in the EIR, and the City’s General Plan and Zoning Ordinance.

6.8 Funding

The funding tools and programs indicated in Table 6.3, Public Improvements Funding Matrix, are known financing opportunities available to the City at the time of DCCSP preparation. To support the breadth of the DCCSP conceptual improvements, a variety of funding sources will be needed for implementation. This is reflected in Table 6.3 through numbers assigned to the potential funding sources to indicate the relative proportion of funding that may be available from the various sources for each of the DCCSP improvements. Funding is available at federal, state and local levels, and may be pursued by the City and/or partner agencies to implement planned public improvements and supplement annual operations and maintenance costs. Funding is discretionary in nature, and the City is not required by the DCCSP to undertake any approach or expenditure. Due to the nature of changing economic conditions and trends, it may be necessary to periodically revisit and reprioritize the implementation steps for revitalizing the DCC. The tools and funding sources are crafted with an understanding that market shifts and changing economic conditions require flexibility to accommodate new development and facilitate investment.

The DCCSP includes policies, development standards, and design criteria that are designed to improve the physical environment of the DCC and create conditions that will attract new private investment. Successful implementation of the vision and concepts in this document will rely on a concerted and cooperative public-private partnership. The majority of new investment and construction on private property in the DCC will need to be made by developers, entrepreneurs, investors, and property owners. To expedite this investment, the City may choose to initiate improvements in public rights-of-way and public property.

Many of the public funding sources needed to pay for improvements outlined in the DCCSP depend upon new development. Further, sufficient funds usually do not exist at the beginning of a planning process to carry



Table 6.2 Implementation Action Plan

IMPLEMENTATION ACTION PLAN	
Improvement Projects	
Project	DCCSP Section Reference
Main Street Improvements	2.3.1.1
First Street Improvements	2.3.1.2
Second and Third Street Improvements	2.3.1.3
Vacating Commercial Alleys – Newport Avenue	2.2.4
Ficus Tree Replacement	2.4.1
Parklets and Public Open Space	2.4.6
Bicycle Improvements	2.3.4
Pedestrian Improvements	2.3.3
Wayfinding and Monumentation Signage	2.4.3
Street Furniture and Amenities Installation	2.4.5
Public Art Installation	2.4.4
Bus Shelters	2.3.5
Programs	
Description	DCCSP Section Reference
Comprehensive Parking Strategy	2.3.2
Parking Management Plan	2.3.2
Special Events/Programming	2.4.6.6
Business Improvement District (BID)	6.7.1
Positive Business Climate Marketing	6.7.2
Parking Awareness Program	6.7.3

out all the planned public improvement work up front. While the City is responsible for public improvements, such as street changes, private development needs to contribute proportionate to a project’s impact on the larger system. This approach avoids incurring new general obligation debt for improvements to maintain the City’s overall fiscal strength. The phasing of public improvements should be carefully matched with private development activity. Public improvement costs should be aligned with immediate needs and opportunities and incorporate private financing approaches whenever possible. Ultimately, market conditions determine when and how much development occurs.

A secondary “beneficiary pays” approach is based on the idea that property owners who benefit from the investment should take the lead in funding the improvements. This approach can be implemented through various types of assessment districts. By using benefit assessment districts as the primary vehicle for financing improvements within the DCC, general fund revenues can be reserved for projects that produce broader citywide benefits – benefits that are likely to be felt beyond the DCCSP boundaries. Judicious application of assessment districts in close coordination with property owners can serve to finance the critical infrastructural investments that will specifically benefit the DCC.

Table 6.3, Key to Abbreviations:

Rule 20A = California Public Utilities Commission (CPUC) Rule 20

Power line undergrounding funded via charges on electricity bills.

BID = Business Improvement District

A defined area in which businesses agree to pay an additional tax to fund projects within the district's boundaries.

CFD = Community Facilities District

A district established by a local government to collect special assessments to finance construction or acquisition of capital facilities.

CIP = Capital Improvement Program

The City of Tustin's budget and plan for capital improvements over a five-year period.

Dev. Agrmts = Development Agreements

An agreement for public benefits to be funded by private parties in exchange for the approval of a discretionary entitlement for development.

DIF = Development Impact Fees

Fees collected by the City of Tustin or other public agencies as a requirement prior to construction of a development project.

EIFD = Enhanced Infrastructure Financing District

A government entity separate from the local city or county that can be used to finance projects with community-wide benefits, including infrastructure, child care centers, affordable housing, and parking facilities.

FAST = Fixing America's Surface Transportation Act

A Federal funding source for surface transportation infrastructure planning and investment.

ISRF = Infrastructure State Revolving Fund Program

State financing available to public and certain non-profit corporations for infrastructure and economic development projects, excluding housing.

LLD = Landscape and Lighting District

A district established to provide and maintain landscaping and lighting in public areas.

MAD = Maintenance Assessment District

A district established to provide maintenance services within a certain area that exceeds the City's standard services.

PID = Parking Infrastructure Assessment District

A district established to acquire land, construction, and operate and maintain parking facilities.



Table 6.3 – Public Improvements Funding Matrix

PUBLIC IMPROVEMENTS FUNDING MATRIX												
Key to Funding Priority: 1 = primary financing, 2 = supplemental financing, 3 = opportunity financing												
Public Improvement	Funding Source											
	Fed	State		City			Assessments				Owner	
	FAST	ISRF	Gas Tax	City General Fund/ CIP	DIF	Rule 20A	BID/ CFD	LLD	MAD	PID	EIFD	Dev. Agrmts
Wayfinding and monumentation signage				2	1		1		2			2
Decorative lighting				2	1		1	2	2		2	1
Street furniture				3	1		1		2			2
Art within public ROW or public space												1
Sidewalk improvements				2	1		1		2			1
Crosswalk paving and enhancements			2	1	1		1		2			1
Intersection decorative paving			3	2	1		1		2			1
Parklets, bulb-outs, and medians				1	1		2	2	2			1
Bike lanes	3			2	1				2			1
Street trees, understory and median plantings				3	1		1	2	2			1
Bulb-out landscape improvements			3	3	1		1	2				1
Understory planting areas for bioswales				2	1		1	2				1
Utility upgrades		2		3	2	3	3		2		3	1
Parking improvements				2	1		3			1		3

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