

PACIFIC CENTER EAST
SPECIFIC PLAN

VOLUME 2
CITY OF TUSTIN

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PACIFIC CENTER EAST SPECIFIC PLAN

Prepared for:

CITY OF TUSTIN

Prepared by:

PBR

18012 Sky Park Circle
Irvine, California 92714

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City of Tustin

City Council

Richard Edgar, Mayor
Charles Puckett, Mayor pro tem
Leslie Pontious
Jim Potts
Earl Prescott

Planning Commission

Don LeJeune, Chairman
Edmund Shaheen, Vice Chairman
Alden Baker
Casper Kasparian
Marge Kasalek

City Manager

William Huston

Community Development Department

Christine Shingleton, Director
Rita Westfield, Assistant Director
Daniel Fox, Senior Planner-Current Planning
Laura Kuhn, former Senior Planner
Steve Rubin, former Senior Planner

Technical Consultants

PBR - land planning
Austin-Foust Associates - traffic engineering
Robert Bein, William Frost & Associates (RBF) - civil engineering
Western Laboratories - soils engineering

PACIFIC CENTER EAST SPECIFIC PLAN

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1.0 INTRODUCTION

1.1 Background

The Pacific Center East project area includes approximately 126 gross acres in a highly urbanized area located in the southern portion of the City of Tustin. The regional location is shown in Exhibit 1. Regional influences to the site include the John Wayne Airport located two miles to the south, several major regional shopping centers such as South Coast Plaza in Costa Mesa, MainPlace in Santa Ana, the nearby Orange County administrative complex, several major freeways including the adjacent Costa Mesa Freeway, and extensive nearby major convention facilities. The Costa Mesa Freeway is a main transportation corridor linking employment opportunities in central Orange County with other areas. The U.S. Marine Corps (Helicopter) Air Station is located immediately east of the project site.

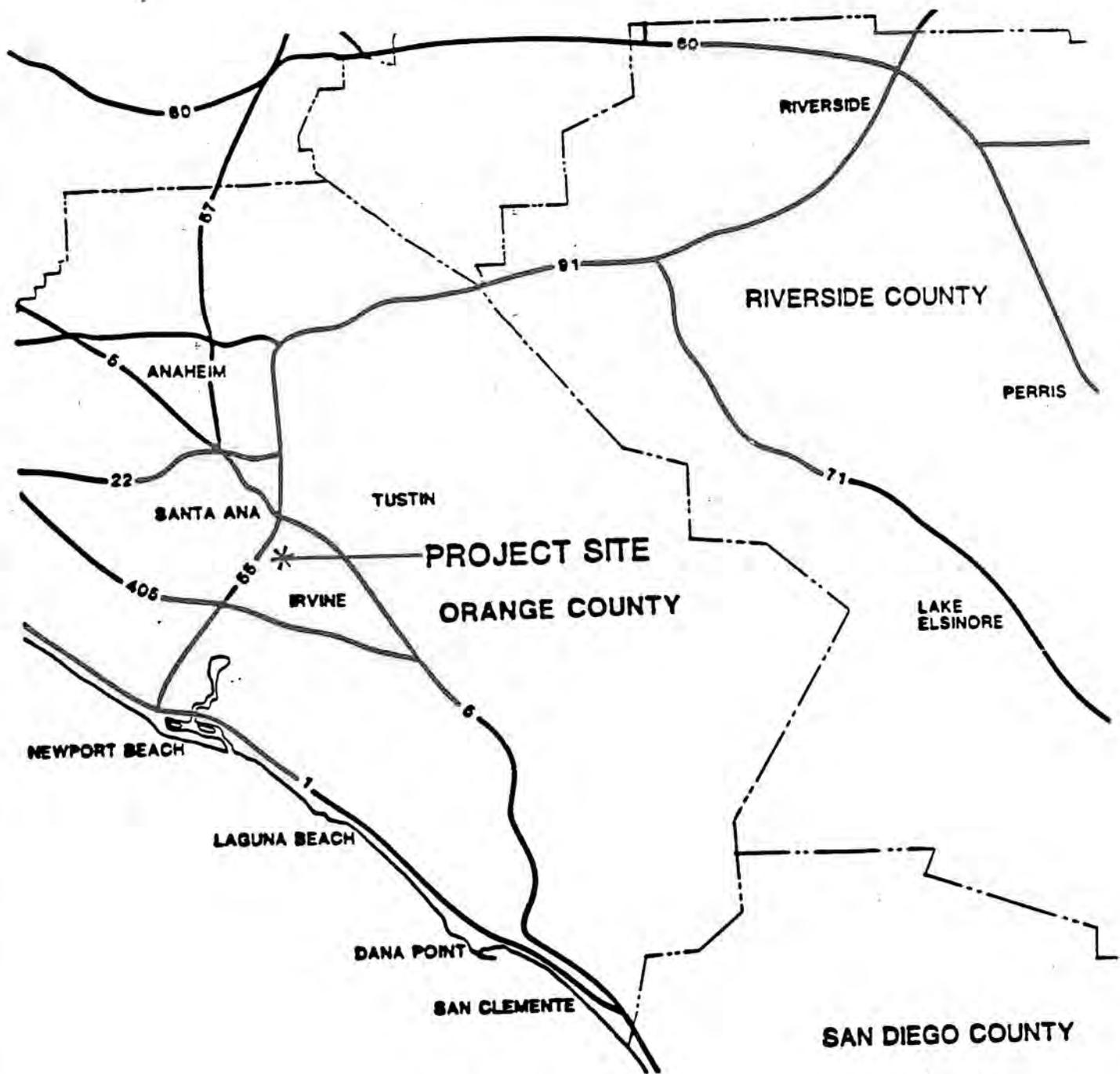
The project area is significant in that it constitutes a major entry to the city from the Costa Mesa Freeway (SR-55) and offers major opportunities for freeway-oriented development. However, inadequate circulation and infrastructure improvements in the area have been a major obstacle to further development.

Congestion on areawide arterials serving the project area, unacceptable levels of service at area intersections and at freeway ramps and limited access characterize the local circulation system. In response to these issues, numerous previous planning studies have been completed for the project area evaluating various proposals including additional access from Newport Avenue, and possible reconfiguration of the Edinger Avenue/SR-55 northbound (eastside) off- and on- ramps.

Since projected circulation improvement costs were prohibitive and an acceptable technical design solution was not identified, these plans were not pursued. Alternative circulation designs for Newport Avenue's extension included an at-grade solution or an overpass of the railroad tracks and flood control channel north of Edinger Avenue with the northbound (eastside) SR-55 ramps located near Valencia Avenue. Recognizing constraints for development within the project area, the city did include the project area within the South/Central Redevelopment Project Area in 1985 in an effort to provide additional development and financial mechanisms for upgrading of the area. More recently, preliminary design plans have been prepared for the extension of Newport Avenue, reconfiguration of the SR-55 northbound (eastside) ramps and an underpass design to extend Newport Avenue beneath the railroad and flood channel. The Department of Transportation has also completed plans and began widening of SR-55 and Interstate 5, which removed uncertainty related to the area and regional circulation system surrounding the project. The widening of SR-55 is now complete adjacent to the project site.

1.2 Purpose

The policies, plans and programs of the city's General Plan and the South/Central Redevelopment Plan establish the basic regulatory framework for the development of the



Regional Location
PACIFIC CENTER EAST
 CITY OF TUSTIN

Pacific Center East Specific Plan

Pacific Center East Specific Plan. Moreover, this specific plan is a major device for implementing the City of Tustin's General Plan for the specific plan area.

This specific plan is designed as a planning tool to develop the necessary detailed planning, support services and facilities, and implementation programs to provide for buildout of the area. The plan further provides as an integrated component, the necessary environmental considerations required under state law. The specific plan, when adopted by city legislative action, serves both a planning function and a regulatory function.

1.3 Authority for the Specific Plan

Government Code Section 65450 establishes the authority for a legislative body to adopt an ordinance or resolution requiring that a specific plan be prepared when it is in the public interest to do so. Specific plans are to implement the General Plan for all or part of an area covered by the general plan. As with General Plans, the Planning Commission must hold a public hearing before the planning agency can recommend the adoption of a specific plan. The City Council may then adopt a specific plan by ordinance or resolution.

After it is adopted, a specific plan has an effect similar to the local general plan. The specific plan may also address other subjects which in the judgment of the city are necessary or desirable to implement the general plan.

1.4 California Environmental Quality Act

A key function of the specific plan is to reduce the need for future detailed planning and environmental review procedures related to future development of the study area. The specific plan and accompanying environmental impact report (EIR) provide the necessary regulations and environmental documentation so that future development proposals in conformance with this specific plan may proceed without a requirement for new or additional environmental documentation.

2.0 PROJECT SETTING

2.1 Location

The Pacific Center East project area is located in the southern portion of the City of Tustin. It is bounded on the west by the Costa Mesa Freeway (SR-55), on the north by the northern limit of the Santa Ana-Santa Fe Channel (F-10), on the east by Red Hill Avenue and MCAS (H) Tustin, and on the south by Valencia Avenue. The local context of the site is shown in Exhibit 2.

2.2 Area Development Trends

The site is partially developed; the northeasterly and easterly portions of the Pacific Center East project area are largely already developed with research and development, office, light industrial, and/or commercial uses. The southwesterly portion of the site is either vacant or used in an interim agricultural use for strawberry production.

Within the city limits south of the project area, there are similar light industrial and research and development uses. The U.S. Marine Corps (Helicopter) Air Station-Tustin is located immediately to the east across Red Hill Avenue.

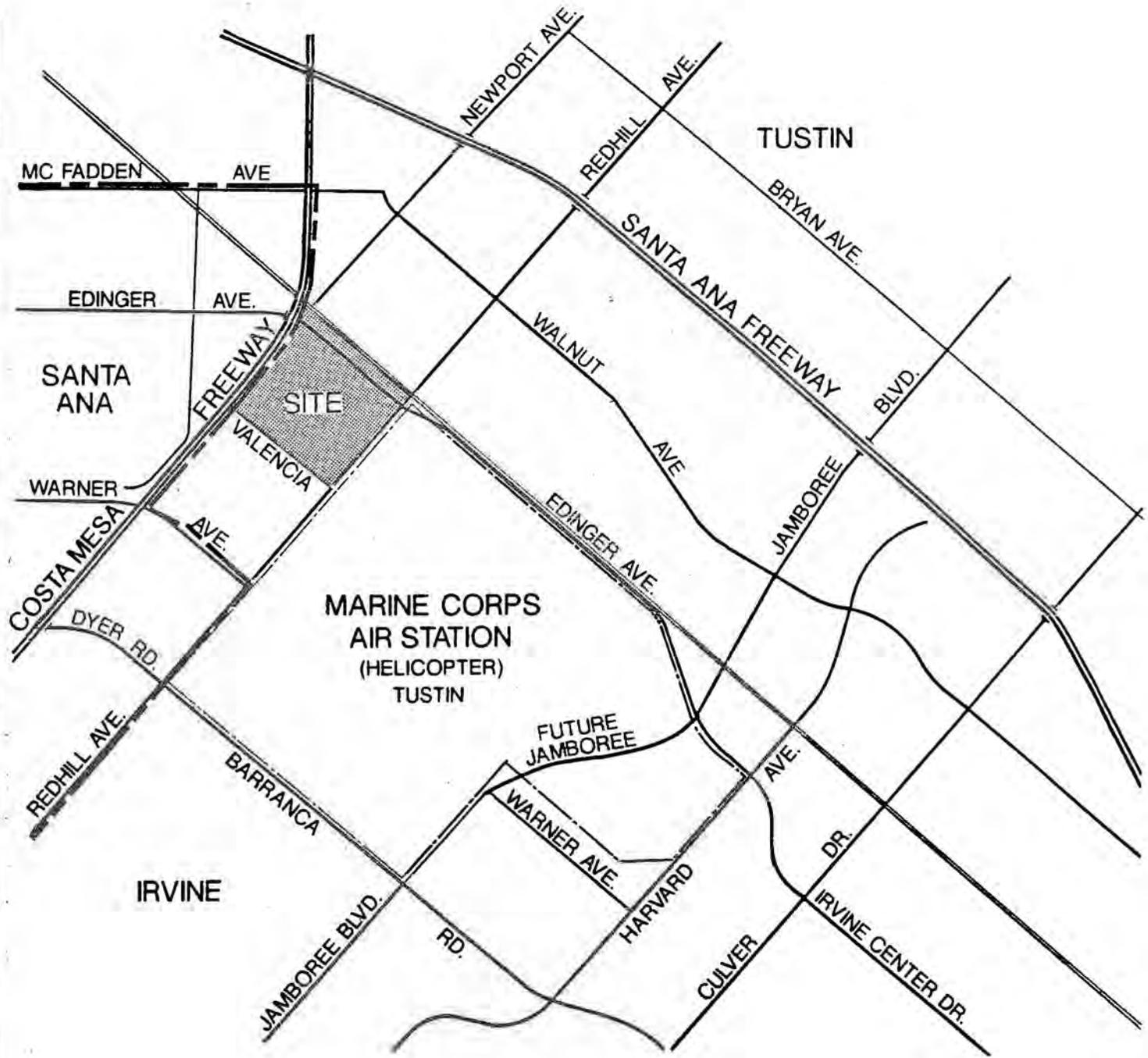
The northern edge of the project area across the AT&SF railroad track and flood control channel are considered as residential development and school district and city corporation yard activities.

West of the Costa Mesa Freeway (SR-55), within the City of Santa Ana, are existing and proposed light industrial and commercial uses including the Santa Ana Auto Mall and the Pacific Center West project which also includes the McDonnell Douglas Industrial Park. Over the next five years, approximately 2.5 million square feet of office space, 1.5 million square feet of industrial space, 400,000 square feet of commercial space could be developed including the possibility of a major 20,000-seat sports arena facility.

Area growth trends continue to stimulate office/industrial development in the City of Tustin as intensification occurs in the Irvine Industrial areas and in the City of Santa Ana north of John Wayne Airport. Development along the freeway corridors throughout central and north Orange County is continuing including demand for additional office/industrial facilities in prime locations. In recognition of these trends, the Southern California Association of Governments has adopted employment projections which identify the specific plan area as one of the rapidly growing employment areas in Orange County.

2.3 Issues, Constraints, Opportunities

The development of the Pacific Center East Specific Plan was accomplished through an orderly process of data collection, issues analysis, definition of alternative planning solutions,



PROJECT AREA
 CITY BOUNDARIES

Site Vicinity
PACIFIC CENTER EAST
CITY OF TUSTIN

NOT TO SCALE

 EXHIBIT 2

Pacific Center East Specific Plan

and refinement of the preferred design concept. Scoping meetings, study sessions and meetings with the Planning Commission, City Council, city staff, property owners, and interested businesses and residents were held in order to solicit input, establish direction and reach a consensus through a process of plan revision and refinement.

In the early stages of plan formulation, a number of issues and constraints and opportunities for future development were identified. The identification and resolution of these concerns has been a central factor in developing a plan and the development guidelines and standards. If the plan is to be successful in meeting the city's objectives, it must address the local issues and constraints and capitalize on the area's advantages.

Environmental Issues

The following environmental issues were identified or evaluated in the planning process:

1. Potential unstable soil conditions.
2. Onsite drainage, storm drain and water and sewer deficiencies in the project area.
3. Potential for cumulative air quality impacts upon adjacent areas.
4. Unacceptable level of service at area intersections and on arterials near the project area.
5. Identification of potential impacts of the project on surrounding land uses, including aesthetics, rail access, and housing demand.
6. Potential impacts of the project on public services.

Project Design Issues

The following design issues were identified or evaluated in the planning process:

1. Lack of an identified image for the project area and the need for design guidelines and development standards to unify the area, provide an identity and define the architectural character of the area.
2. Opportunity for freeway-oriented development adjacent to SR-55 and the existing and planned northbound (eastside) ramps to SR-55.
3. Provision of a variety of business park uses, including commercial, office development, light industrial, and research and development in locations which are compatible with adjacent uses.

4. The need to assemble parcels for public right-of-way to implement a viable circulation system for the area.
5. The opportunity to create consistent and attractive streetscapes throughout the project.

Urban Infrastructure Issues

The following infrastructure issues were identified or evaluated in the planning process:

1. Limited access to the project area and examination of the extension of Newport Avenue as an opportunity to provide additional circulation capacity for the project area and relocation of existing northbound (eastside) ramps to provide efficient access to and from SR-55.
2. Identification of necessary onsite and offsite infrastructure improvements within the project area including provision of necessary public services.
3. The fiscal impacts of the project and the type of financing mechanisms necessary to implement the plan.

3.0 PROJECT DESCRIPTION, PROGRAMS AND GUIDELINES

3.1 Introduction

The overall concept for the Pacific Center East Specific Plan is intended to provide a planned community development which encourages a variety of office, commercial, regional and technology uses. These intense land uses are to be concentrated at the southwesterly portion of the project area with the potential development intensity diminishing toward the north and northwesterly edges of the site in proximity to existing residential uses. The major components of the plan include the following:

- ▶ Land Use Plan
- ▶ Urban Design Concept
- ▶ Circulation Plan
- ▶ Landscape Concept Plan
- ▶ Utilities Concept Plans
- ▶ Signage Concept Plan
- ▶ Phasing Concept Plan

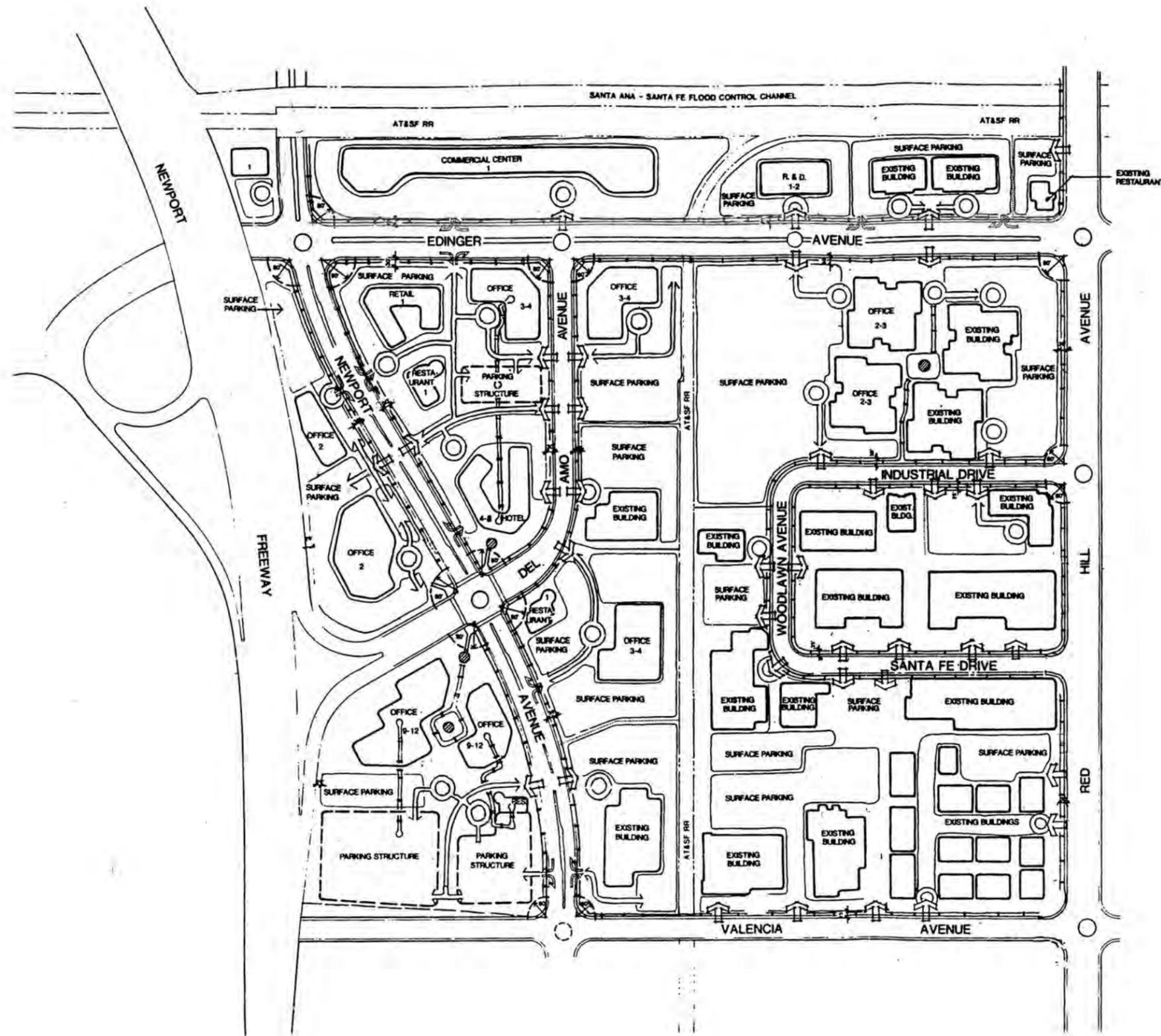
The procedures and components of each of the above plans are outlined in this chapter of the specific plan. Some material set forth in this chapter is intended to be as directives and shall be implemented according to language contained herein. Other material is conceptual and intended to guide and not restrict creative solutions.

3.2 Urban Design Concept

The design concept for the Pacific Center East Specific Plan provides for an integrated environment of commercial, office, regional and technology uses. The project is to be linked physically through the plan's arterial system and provide opportunities for public transportation and internal pedestrian access. Aesthetic integration will be achieved through the development of architectural standards, streetscape design, landscaping and entry features (Exhibit 3).

Design objectives for the specific plan are listed below:

1. To provide a range of uses and intensities which meet the city's future needs for mixed land uses and wide variety of facilities and services in the project area. Technology uses would be retained as a key component of the project area with its conversion to office use discouraged. Commercial, daycare and hotel uses would take advantage of the existing freeway interchange locations and the market opportunities of the area.
2. To promote building forms that relate to the scale and character of surrounding development while also relating new development to pedestrian functions.



LEGEND

	RESEARCH AND REDEVELOPMENT
	HEIGHT OF BUILDING (IN NUMBER OF FLOORS)
	RIGHT-IN / RIGHT-OUT ACCESS ONLY
	FULL TURN ACCESS*
	AUTO CIRCULATION / DROP OFF
	PEDESTRIAN CIRCULATION
	SIGNALIZED INTERSECTION
	INTERSECTION CORNER CUT OFF
	SPECIAL LANDSCAPE FEATURE
	POTENTIAL SIGNALIZED INTERSECTION

NOTE: See Exhibit 8, 9h, and 9i for median detail.

* Full turn access may be restricted due to added traffic volumes.

**Urban Design Concept
PACIFIC CENTER EAST
CITY OF TUSTIN**

3. To design height and bulk limitations which conform to the land use intensity in a given "planning area."
4. To create a project identity for the Pacific Center East Specific Plan Area encouraging architectural design of the highest quality.
5. To integrate existing uses, new development and potential future redevelopment uses.
6. To consider, subject to the constraints of proper community design, the existing patterns of ownership and land use in the alignment of major streets, the definition of "planning areas," the allocation of building intensities, and the distribution of uses by land use types.

In order to accomplish the above objectives, design principles or guidelines to be applied within the project area are described on the following pages. These guidelines are depicted graphically on Exhibit 3.

Intersections

1. A thematic gateway entry should be developed at the Newport Avenue and Del Amo Avenue intersection. This major intersection should include special landscape architectural accent features to suggest a "sense of place" or arrival statement to the project. The design elements should contain monument signage, accent landscape and some type of special focal point feature.
2. Setback areas at major arterial intersections should contain project identity through use of signage monumentation, accent landscaping and berming. Buildings should be set back from the ultimate right-of-way of major arterials utilizing an 80-foot right angle or radius distance from the intersection of ultimate extended right-of-way as shown in Urban Design Concept.
3. Setback areas/secondary intersections should also contain accent landscape and berming. This setback is obtained by placing a 60-foot right angle or radius distance from the intersection of extended right-of-way lines as shown in the Urban Design Concept. Secondary intersections occur at Newport Avenue and Valencia Avenue, Industrial Drive and Red Hill Avenue, and Santa Fe Drive and Red Hill Avenue.

Project Circulation

1. The circulation system should be designed to accommodate both area and project traffic, including public transit, rail access and pedestrian networks. Convenient and direct linkages should be provided between uses and within nearby, complementary uses.

Pacific Center East Specific Plan

2. A special treatment of promenade crossings should be encouraged at major intersections to indicate a major pedestrian crossing is being approached. Where applicable, median planting, seating areas and special features may complement this treatment. The promenade crossing may consist of special paving to separate vehicular and pedestrian space. The promenade should extend through the core of the office center and continue its link with the regional center, and would encourage pedestrian access to surrounding development.
3. Access points to individual sites within the project area should be located as far as possible from street intersections with the actual location of all access points to be subject to City Engineer approval.
4. The parking areas should be designed to link buildings to onsite pedestrian walkways and the street sidewalk system and to minimize potential pedestrian/vehicle conflicts.
5. Wherever possible, facilitate public access across sites where important pedestrian movement occurs.

Streetscape/Edge Treatment

1. A visually integrated project area is desired and should be created by providing continuity in the edge treatments, streetscape design and landscape treatments for the project. These treatments should emphasize a human scale to the traveler and soften building architectural treatments. More detail on the landscape concept for the project is provided in Section 3.5 of the specific plan.

Buildings, Form, Access, Scale, Siting

1. Architectural variety and diversity should be promoted within the project area, while maintaining the identity for the entire project.
2. Multiple buildings on a given site should be situated to create pedestrian spaces, courtyards and create a strong identity for building entries.
3. A variation of building height and massing in groups of buildings and within individual buildings should be encouraged.
4. Building entrances should create a series of steps or transition, as follows, in the process of arrival:

First - transition from the street scene to the site, then transition to a parking area, then to the parking space.

Second - transition from the walk from parking to the pedestrian collector walk, which is connected to the building courtyard.

Third - transition from the courtyard (drop-off area) to the building entrance and into the building itself. This process is one of slowing down, an ever-increasing enrichment of materials, textures, ornament, shade, fragrance, etc., which culminates in the entrance and the courtyard and arrival at the building.

Entrances must be placed in such a way that people who approach the building see the entrance. The entrance must also be ornamented.

5. Remove unsightly and cluttering elements from street right-of-way. Clutter is produced by unplanned elements in street right-of-way. The placement of overhead wires underground, where feasible, should continue at the most rapid pace possible. Every other element in street areas, including public signs, should be examined with a view toward improvement of design and elimination of these unnecessary elements to improve visual clarity.
6. Building ornaments should be used to identify special parts of the building, to break down elements into comprehensible parts of human scale, to give texture to the building, to add interest, and to express a common vocabulary. Ornamentation can be as simple as a color change, a change of plane, and application of pattern or as complex as carved stone in intricate patterns.
7. Ranging of elements from front to rear:

The concept places the enriched elements closest to the building users. It is closely linked to the front and high elements to the rear. The architectural form is stepped up and away from the use, giving depth to the overall architectural form. This is also consistent with the design principle of courtyards; the highest level of detail should be placed within the courtyard.

Street facades if removed from pedestrian access will have a lower level of detail. The ranging of elements from the front to rear is also consistent with the courtyard design principle, the greatest amount of stepping within the architectural form should be placed on the courtyard side of the buildings. The street sides of the buildings if removed from pedestrian access will be vertical in nature. The use of geometric form, composition and roof variation will be employed in these areas to vary the architectural form.

8. With orientation and visibility opportunities at the SR-55 freeway, special emphasis should be given to architecture, including building geometry and the relationship of building forms oriented toward the freeway, Newport Avenue, Del Amo and Edinger.

Pacific Center East Specific Plan

9. Development within the project should be encouraged to use sufficient vertical and horizontal articulation to avoid "box" architecture and long, uninterrupted blank walls.
10. Structures should be designed to create transition in form and scale between large buildings and adjacent smaller buildings.
11. Site buildings to create pedestrian spaces, courtyards and plazas. The experience should be visually diverse, stimulating and create a sense of vitality and excitement.
 - ▶ Internal walls shall be utilized to provide continuous pedestrian access from parking (surface and structured) to plazas and entry points to buildings.
 - ▶ To encourage pedestrian movement along a designated route, building materials shall be coordinated with hardscape to provide a continuous and clearly identifiable walkway.
 - ▶ Where a pedestrian walkway meets a vehicle drive, pathway material or other accent/special paving material shall be utilized to highlight the pedestrian crosswalk.
12. Color, material, textures and finishes which shall be used to achieve the maximum quality and which are encouraged, as well as those elements and materials which are to be avoided, are listed in Table 1.

3.3 Land Use Plan

The land use plan for Pacific Center East shown in Exhibit 4 designates a variety of land uses linked by an appropriate proposed circulation system. Four major land use designations are designated within the specific plan boundary. (A graphic display of these areas is shown on Exhibit 5.) The approximate net average within each center and each land use designation's purpose can be described as follows:

Commercial Center: The commercial center designation (8.9 net acres) provides for freestanding or commercial center uses and is well suited to the development opportunities offered by SR-55, local streets, and nearby employment centers. Uses permitted within the commercial center designation include financial institutions, sporting goods, clothing, business/office supply, and other destination and service uses. The commercial center will establish its own identity and is not exclusively reliant on surrounding uses.

Regional Center: The regional center designation (33.2 net acres) is formed by the revised circulation system and provides opportunities for a mix of land uses, including hotels, commercial and office uses. Both onsite employees and area commuters are potential users

Table 1
BUILDING MATERIAL/COLOR CRITERIA MATRIX
MATERIALS

Encouraged

Discouraged

Commercial Center

- ▶ Warm, reflective, light-tinted glass (includes high-performance glass)
- ▶ Exterior wood must be painted or solid base paint

- ▶ Untinted, mirrored glass and highly reflective gold glass
- ▶ No build-up on membrane roofing

Regional Center

▶ **Hotel/Commercial**

- ▶ Smooth precast concrete
- ▶ Concrete with light-colored aggregate or integral color
- ▶ Stucco or similar materials are permitted only in areas of secondary visual importance such as soffit
- ▶ Tinted or lightly reflective glass

- ▶ Wood beams and siding
- ▶ Tilt-up concrete
- ▶ Concrete block massing

▶ **Office**

- ▶ Same criteria as Office Center

- ▶ Same criteria as Office Center

Office Center

Windows

Windows

- ▶ Transparent glass
- ▶ Tinted glass
- ▶ Lightly reflective color glass

- ▶ Mirror glass
- ▶ Black-tinted glass

Pacific Center East Specific Plan

Encouraged

Discouraged

Walls

- ▶ Concrete with light-colored aggregate or integral color
- ▶ Painted concrete
- ▶ Light-colored metal systems
- ▶ Stucco or similar materials are permitted only in areas of secondary visual importance such as soffits
- ▶ Marble, stone veneer

Walls

- ▶ Wood
- ▶ Tile with concrete
- ▶ Concrete block masonry

Roofs

- ▶ Wood shingles
- ▶ Asphalt shingles
- ▶ Spanish tile

Land Use - Technology Center

- ▶ Smooth, precast or tilt-up concrete
- ▶ Smooth metal panel systems
- ▶ Tinted or lightly reflective glass

- ▶ Wood beams and siding
- ▶ Brick, Spanish tile
- ▶ Corrugated metal
- ▶ Ribbed concrete
- ▶ Highly reflective glass

All Materials are Subject to Approval of the City Pursuant to Design Review

COLOR

Encouraged

Discouraged

Commercial Center

- ▶ Light natural concrete or stone finish
- ▶ Generally one wall color

- ▶ Dark natural finish
- ▶ Dark wall finishes
- ▶ Multiple colors, stripes

Encouraged

Discouraged

Regional Center

▶ **Hotel/Commercial**

- ▶ Accent colors and subtle changes in wall color may be used in a limited manner to add interest and to identify entries.

- ▶ Dark natural finishes
- ▶ Accent applied to wall surface
- ▶ Roof materials shall be light color

▶ **Office**

- ▶ Same criteria as Office Center

- ▶ Same criteria as Office Center

Office Center

- ▶ Wall colors should generally be one shade unless otherwise approved by owner or its designee
- ▶ Accent colors and subtle changes in wall color may be used in a limited manner to add interest and to identify entries
- ▶ Accent colors may be used on architectural elements (ie., reveals, mullions, or special features).
- ▶ Roof materials shall be light colors.

- ▶ Accent colors may not be painted directly on wall surfaces or used as wide bands.

Land Use - Technology Center

- ▶ Light, natural concrete or stone finish
- ▶ Generally one wall color
- ▶ Generally one accent color
- ▶ Accent only on special features, reveals and mullions
- ▶ Service door colors the same as adjacent walls

- ▶ Dark wall finishes
- ▶ Multiple colors, stripes
- ▶ Accent applied to wall surface



Land Use Plan
 Pacific Center East
 City of Tustin

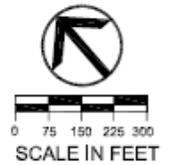
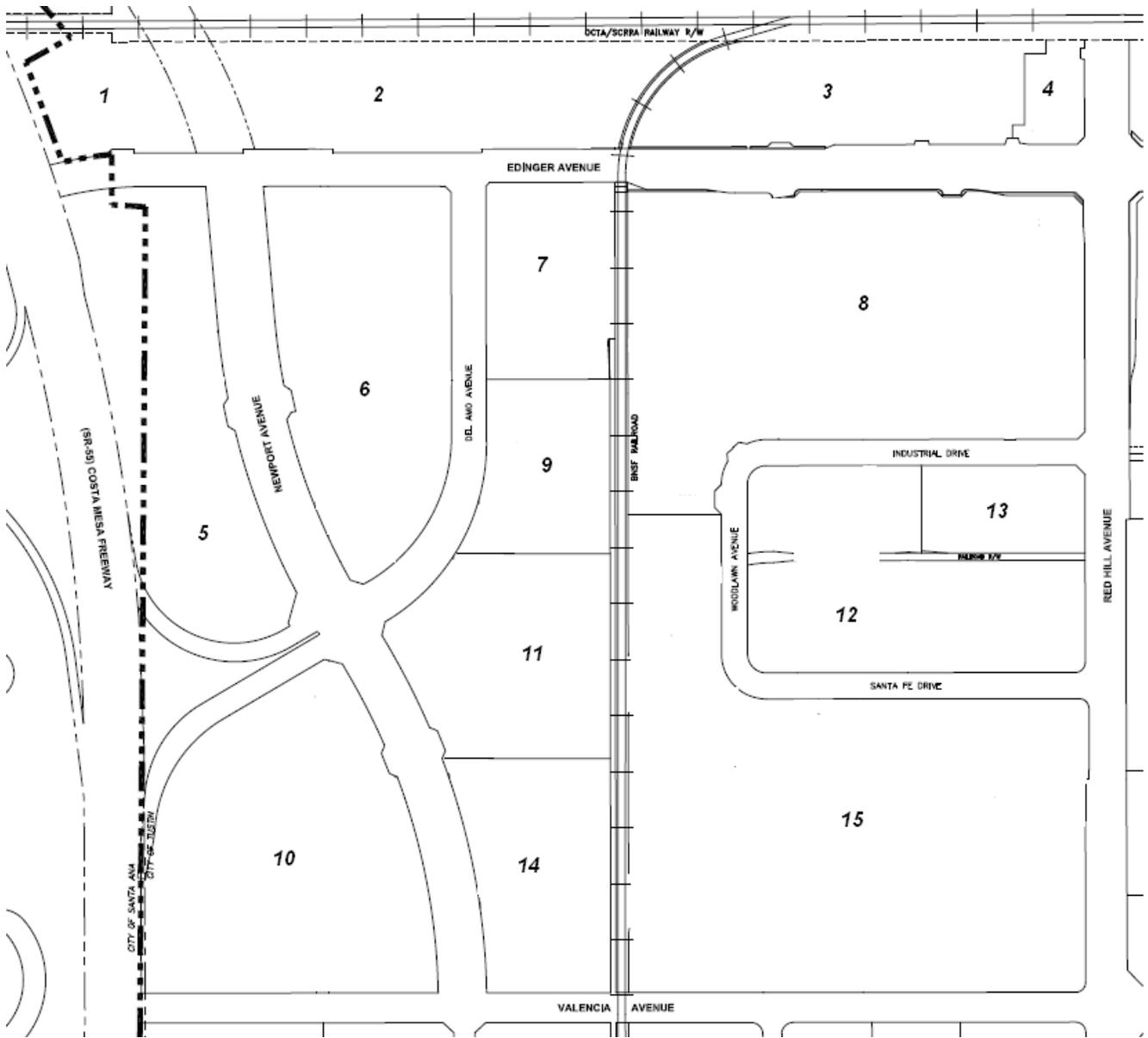


Exhibit 4

- Commercial Center
- Regional Center
- Office Center
- Technology Center



Planning Areas
Pacific Center East
 City of Tustin



Exhibit 5

of the center, as well as area business travelers. Four separate planning areas in close proximity will be identified by their architectural and landscape elements.

The focus of the regional center is commercial use, which provides specialized services to consumers, including specialty restaurants, lodging, commercial recreation, daycare, and selected personal services. The specific plan encourages a mix of commercial uses which provide both daytime and evening activity.

Office Center: The office center designation (36.7 net acres) provides for professional and corporate office areas within the specific plan area. Both mid-rise and low-rise offices are encouraged, with Planning Area 10 proposed as a corporate headquarters office complex. Future office development would also be expected adjacent to the Pacific Bell facilities including professional offices south of Industrial Drive.

The specific plan allows a mixture of office products onsite, including corporate and general offices in small and large facilities. The specific plan encourages the integration of office, support commercial and selected employee services in Planning Area 10, encouraging a distinct identity as a major office center.

Technology Center: The technology center designation (44.2 net acres) provides for research and development uses, with supporting office and commercial use. The designation reflects existing research and development, light industrial and business commercial uses east of Del Amo Avenue.

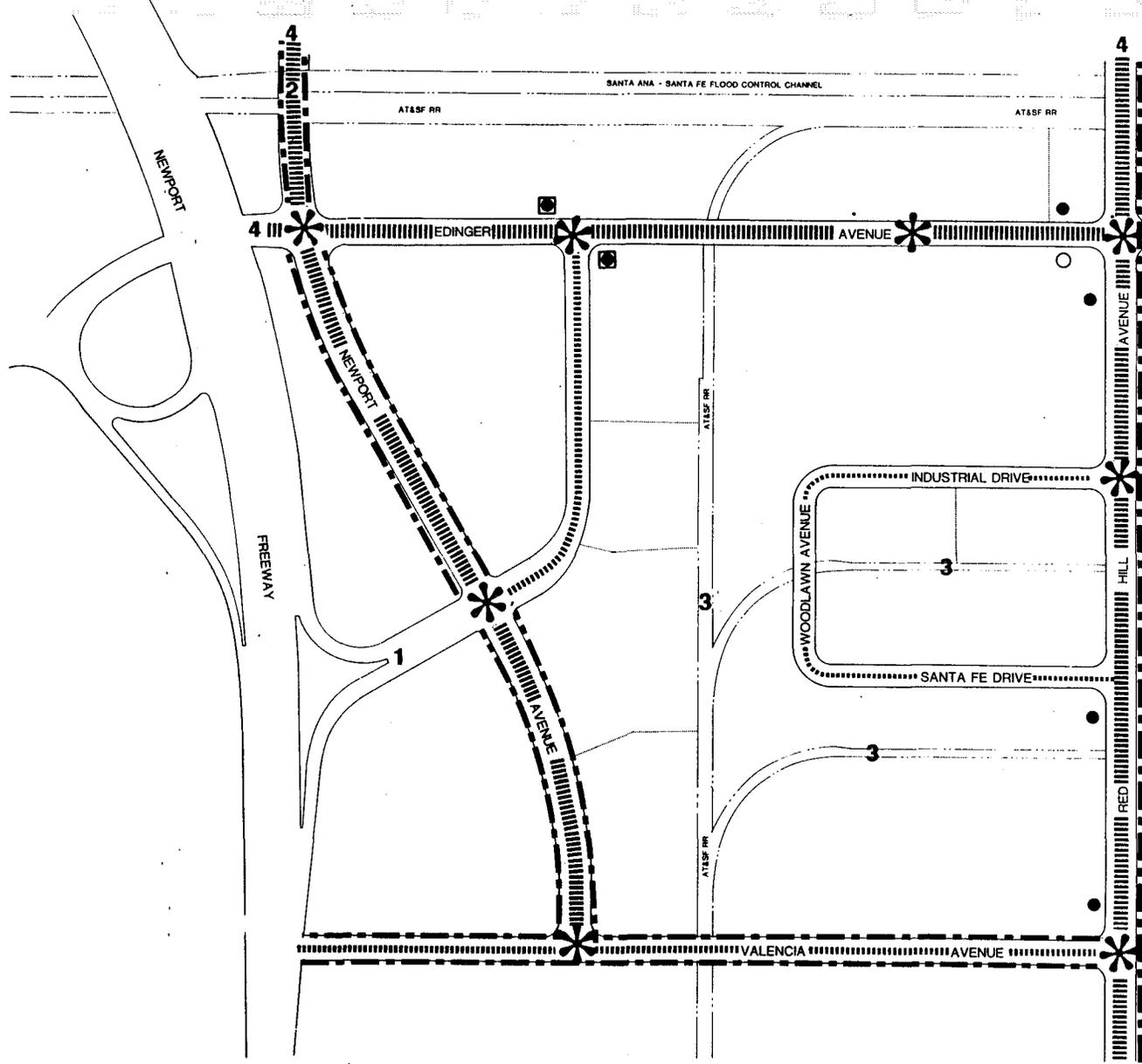
The technology designation encourages continued development of research and development uses and discourages intrusion of exclusive office use in these areas.

3.4 Circulation Plan

The circulation plan (Exhibit 6) provides the internal circulation system and basic standards for safe, efficient vehicular movement within and around the specific plan area. This plan consists of mandatory alignments for arterial roadways and their right-of-way, as well as the reconfiguration of the northbound (eastside) SR-55 ramps into the specific plan area. The circulation plan also proposes the extension of Newport Avenue through the project area as recommended by the Circulation Element of the General Plan. Further, this plan specifies typical roadway sections to be implemented as development occurs.

The circulation plan reflects the following elements:

- ▶ Future traffic projections for onsite land uses.
- ▶ Future traffic projections for cumulative projects and areawide traffic growth in the surrounding vicinity.



LEGEND

-  BIKEWAY
-  MAJOR
-  SECONDARY
-  LOCAL
-  EXISTING BUS STOPS
-  PROPOSED BUS STOP
-  PROPOSED BUS TURNOUTS
-  PROPOSED AND EXISTING SIGNALS
-  INTERCHANGE RECONFIGURATION
-  UNDERPASS
-  RAILROAD RIGHT OF WAY
-  SCENIC HIGHWAY

NOTE: Actual design and specific location for bikeway will be determined during specific planning area concept review and will be subject to review by Community Development and the City Engineer.

**Circulation Plan
PACIFIC CENTER EAST
CITY OF TUSTIN**

Pacific Center East Specific Plan

- ▶ Circulation needs for emergency vehicles.
- ▶ Plans for reconfiguration of the northbound (eastside) SR-55 ramps.
- ▶ The city's proposed goals, objectives and policies included in the circulation element.

The circulation element relies primarily on the existing major arterials in the project area, included Red Hill Avenue and Edinger Avenue. Newport Avenue would be extended through the site as a major arterial. An underpass for Newport Avenue will allow traffic to pass under the railroad tracks and flood control channel.

Street Cross-Sections

Street cross-sections have been designed for the specific plan area and are shown on Exhibits 7a, 7b. These may be redesigned with minor changes per the approval of the Director of Public Works without requiring an amendment to the specific plan.

Standard arterial highway cross-sections represent the obvious and accepted answer to most situations, and the logical point of departure of any consideration of alternative cross-sections. However, the standard cross-sections do not always provide the best solution to any given need or set of objectives. Reasonable flexibility should be available in how arterial highway capacity and functional requirements can best be satisfied.

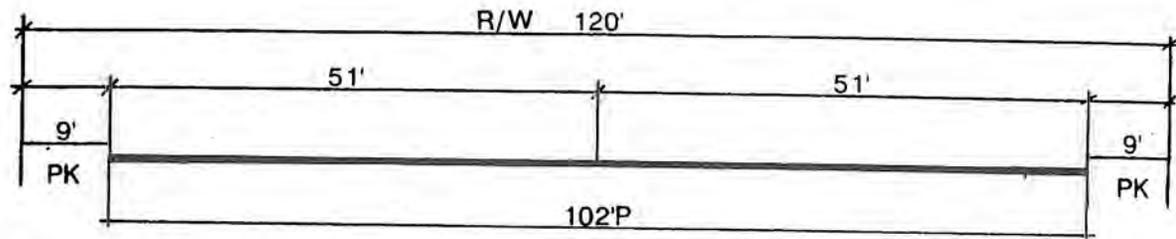
In certain instances it may be desirable to encourage variety in the cross-section of various routes or different portion of the same route to satisfy intersection capacity requirements, create visual identity and landscaping opportunities, etc. An off-center or non-symmetrical design may be appropriate where conditions are different on opposite sides of the highway, particularly with respect to such factors as the adjacent land uses, slope conditions, intersections, sidewalks, parking lanes, utilities and bikeways.

The following is a brief description of each of the proposed roadway classifications identified in the specific plan and illustrated in Exhibits 3-5.

Major Arterial Highway - is designed as a six-lane divided roadway with a typical right-of-way width of 120 feet and a roadway width from curb to curb of 102 feet. Where a full 120 feet of right-of-way is not feasible due to existing structures or topography, a 100-foot right-of-way can be developed to accommodate a six-lane divided facility. This requires no on-street parking. Access is restricted to local intersections and commercial driveways as approved by the City Engineer.

Secondary Arterial Highway - is designed as a four-lane roadway, with a typical right-of-way width of 80 feet and a roadway width from curb to curb of 64 feet. Access should be restricted to local intersections and commercial driveways as approved by the City Engineer.

**MAJOR
ARTERIAL HIGHWAY**
(Newport Avenue, Edinger Avenue,
Red Hill Avenue)



LEGEND

R/W=WIDTH IN FEET
P=WIDTH OF PAVEMENT IN FEET
PK=WIDTH OF PARKWAY IN FEET

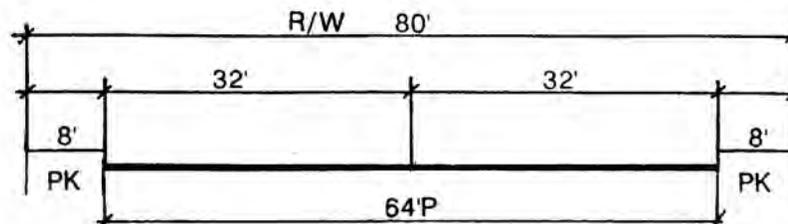
NOTE: See plan views for median, bikeway, sidewalk detail and transition of Newport Avenue from 120' ROW south of Kenyon Drive to 100' ROW to Edinger Avenue

Street Sections
PACIFIC CENTER EAST
CITY OF TUSTIN

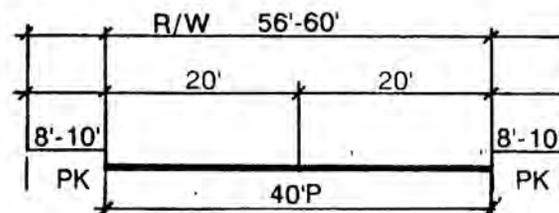
SOURCE: AUSTIN-FOUST ASSOCIATES



SECONDARY ARTERIAL
(Valencia Ave./Del Amo Ave.)



LOCAL STREET
(Industrial Drive)
(Woodlawn Ave.)
(Santa Fe Drive)



LEGEND

R/W=WIDTH IN FEET

P=WIDTH OF PAVEMENT IN FEET

PK=WIDTH OF PARKWAY IN FEET

SOURCE:AUSTIN-FOUST ASSOCIATION INC.

NOTE: See plan views for median and sidewalk detail

Street Sections
PACIFIC CENTER EAST
CITY OF TUSTIN



EXHIBIT 7b

Local Collector Street (Two-Lane Undivided) - The standard local street provides two traffic lanes with an eight-foot parking lane and a ten-foot parkway on each side of the street. The overall right-of-way width varies from 56 feet to 60 feet dependent upon the width of the parkway and whether or not parking is prohibited. Roadway width should be less than 40 feet curb to curb. Direct access is possible subject to the approval of the City Engineer.

Major and secondary arterial intersections should be designed to maximize capacity. Design features may include the need for additional right-of-way to accommodate dual left-hand turn lanes and designated right-turn lanes.

In addition to actual street classifications, the Circulation Plan also identifies a number of other circulation components as follows:

- ▶ Newport Avenue extension
- ▶ Proposed SR-55/Edinger Avenue northbound (eastside) ramps upgrade
- ▶ Railroad
- ▶ Bus stops
- ▶ Scenic highways
- ▶ Bikeways

A discussion of each of these components follows.

1. **Newport Avenue Extension** - could be implemented either as an overpass or an underpass. The extension would occur within a 120-foot right-of-way and be striped for six lanes with no on-street parking. The proposed Newport Avenue extension will transition from a 100-foot right-of-way in proximity to Kenyon Drive to a 120-foot right-of-way within 375 feet and continue within a 120-foot right-of-way to Valencia Avenue (Exhibits 9h, 9i). If an overpass is implemented, the extension will be highly visible from adjacent properties but the roadway would not be subject to potential flooding. As conceptually proposed as an underpass, the extension would require construction of temporary railroad and flood channel structures while the underpass is under construction. The railroad plans require approval by AT&SF and the California Public Utilities Commission. The flood channel designs require approval by the Orange County Flood Control District. When fully constructed, a pump system would assure ponding does not occur beneath the underpass.

Either an overpass or underpass design likely requires acquisition of public right-of-way on both sides of the extension. The east side properties below Kenyon Drive would likely be impacted and it is estimated that approximately 25 units on the east side of the roadway must be acquired for right-of-way and to maintain access via a new frontage road. The flood channel conduit necessitated by an underpass is discussed in Section 3.6. Conceptual

Pacific Center East Specific Plan

plans for the Newport Avenue extension and underpass are included in Appendix E of the Draft EIR.

2. **SR-55/Edinger Avenue Northbound (Eastside) Ramps Upgrade** - an upgrade of the ramps has been conceptually designed by Dokken Engineering and is currently being reviewed by Caltrans (more detail is provided in Appendix E of the Draft EIR). The ramp redesign would allow appropriate stacking distance for vehicular traffic and facilitate weaving movements with the SR-55 traffic flows. The Del Amo Avenue redesign also distributes traffic flows to two intersections with Edinger Avenue. Both of the Newport Avenue intersections with Del Amo Avenue and with Edinger Avenue would be signalized.

3. **Railroad Spurs/Crossings** - occur throughout the project site. Newport Avenue extension will be separated from rail operations. An existing rail spur located west of the existing Del Amo Avenue alignment will be removed prior to grading. The eastern spur will remain and crosses both Valencia Avenue and Edinger Avenue. Since the spurs are used at slow speeds and only periodically, only the crossings are signalized.

4. **Bus Stops** - are currently located at two locations on Edinger Avenue and at three locations southbound along Red Hill Avenue, as shown in Exhibit 3-4. Existing bus stops are signposts, without benches, shelters or bus pads. If the Orange County Transit District or City of Tustin requests expanded facilities, additional right-of-way or facilities will be required by dedication from adjacent developments. Design of turnouts shall be to Orange County Transit District standards. Two additional bus turnouts and one additional bus stop are proposed onsite (Exhibit 6).

5. **Scenic Highways** - The scenic highways and roadways plan provides an official city guide for the identification of scenic amenities within the city. Objectives which will be implemented within the specific plan area are: 1) creation of landscaped major arterials along the Newport Avenue extension south of the flood control channel, 2) buffering of freeway frontages by slopes and landscaping, and 3) incorporation of bicycle lanes and landscape areas along Newport Avenue and Edinger Avenue for transportation, recreation, and aesthetic benefits.

The specific plan proposes either painted or raised medians along Edinger Avenue subject to further planning by the city and county for super streets, and landscaped medians along Newport Avenue.

6. **Bikeways** - are designated for two streets in the site vicinity. The east side of Red Hill Avenue which is outside the project site, is designated as a Class II (on-street) bikeway in the General Plan and Newport Avenue south of Sycamore is a local bikeway. An on-street bikeway is proposed along Valencia Avenue and along Newport Avenue north of Edinger Avenue. The bikeway along Newport Avenue between Edinger Avenue and Valencia Avenue could be either an off-road bikeway or on-street bikeway. The actual design and location for bikeways will be determined during specific planning area concept review and during roadway design subject to final approval of the Community Development Department.

General Circulation Standards

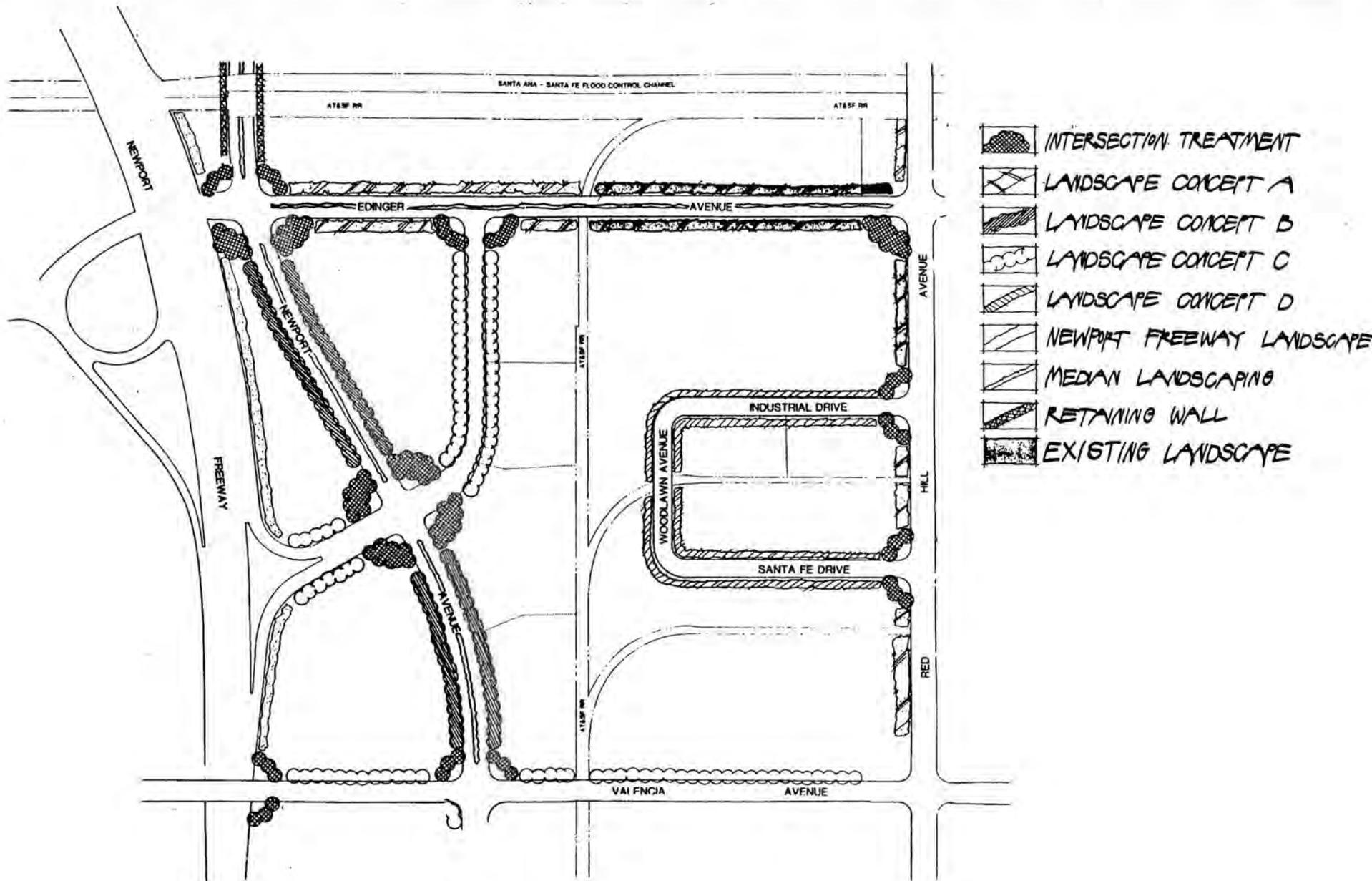
- a. No development shall be permitted on parcels without improved street access. For purposes of this standard, a street is a maintained public street subject to the approval of the City Engineer and Public Works Director.
- b. Portions of any lot within any future right-of-way shown in this specific plan shall not be occupied by structures, other than those encroachments normally permitted in rights-of-way. All other required setbacks, yards and open spaces shall be calculated on the basis of the establishment of the future right-of-ways; future right-of-way lines shall be considered to be lot lines for purposes of determining such setbacks, yards and open spaces.
- c. Dedication of the right-of-way shown or in this specific plan shall be required as a condition of approval for any development.
- d. Installation of curbs, gutters, sidewalks, street and alley paving, street lighting and street trees shall be subject to the provisions of the Tustin city code.
- e. No structures shall be permitted to be developed over existing pipeline power line or railroad alignments or easements, except where these can be relocated.
- f. Full-turn access or right-turn in and out access points shall be conceptual only subject to traffic verification by City Traffic Engineer and acceptance by City Engineer during site plan review.

3.5 Landscape Concept Plan

The landscape concept plan (Exhibit 8) is an integral element of achieving a distinctive development character for the specific plan area and a means of integrating the diverse uses proposed within the specific plan area. This character shall be reinforced through the coordinated design of entry monumentation, integration of each land use designation and emphasis on access, signage, building height and streetscapes.

Required guidelines are specified for the following categories:

- ▶ Major entries
- ▶ Streetscapes
- ▶ Plant palette
- ▶ Hardscape



Landscape Concept Plan
PACIFIC CENTER EAST
 CITY OF TUSTIN

Pacific Center East Specific Plan

The landscape concept plan establishes a guideline for evaluating consistency of design between the ultimate development pattern and its incremental buildout. As development and redevelopment occur, landscape plans shall be approved which are consistent with and implement these concepts. The categories listed above are described in the following exhibits and tables.

The primary objectives of the landscape concept are as follows:

- a. To design a streetscape for Newport Avenue which becomes a focal point for the specific plan area.
- b. To unify diverse uses and differing architectural styles and building heights.
- c. To integrate existing projects and existing landscaping within the landscape concept and utilize ornamental plantings which fit with the vegetative types currently established in the specific plan area.
- d. To enhance and maintain view corridors and building prominence within the project area, through use of vertical trees, accent shrubs, broad plant spacing and sensitive siting of development.
- e. To provide an attractive project edge adjacent to SR-55 which announces arrival to the project and provides a scenic corridor.

Landscape elements which provide landscape continuity include major entries, streetscapes, and hardscape elements. Guidelines for each of these elements are discussed below and illustrated in the following exhibits. Signage is discussed in Section 3.7.

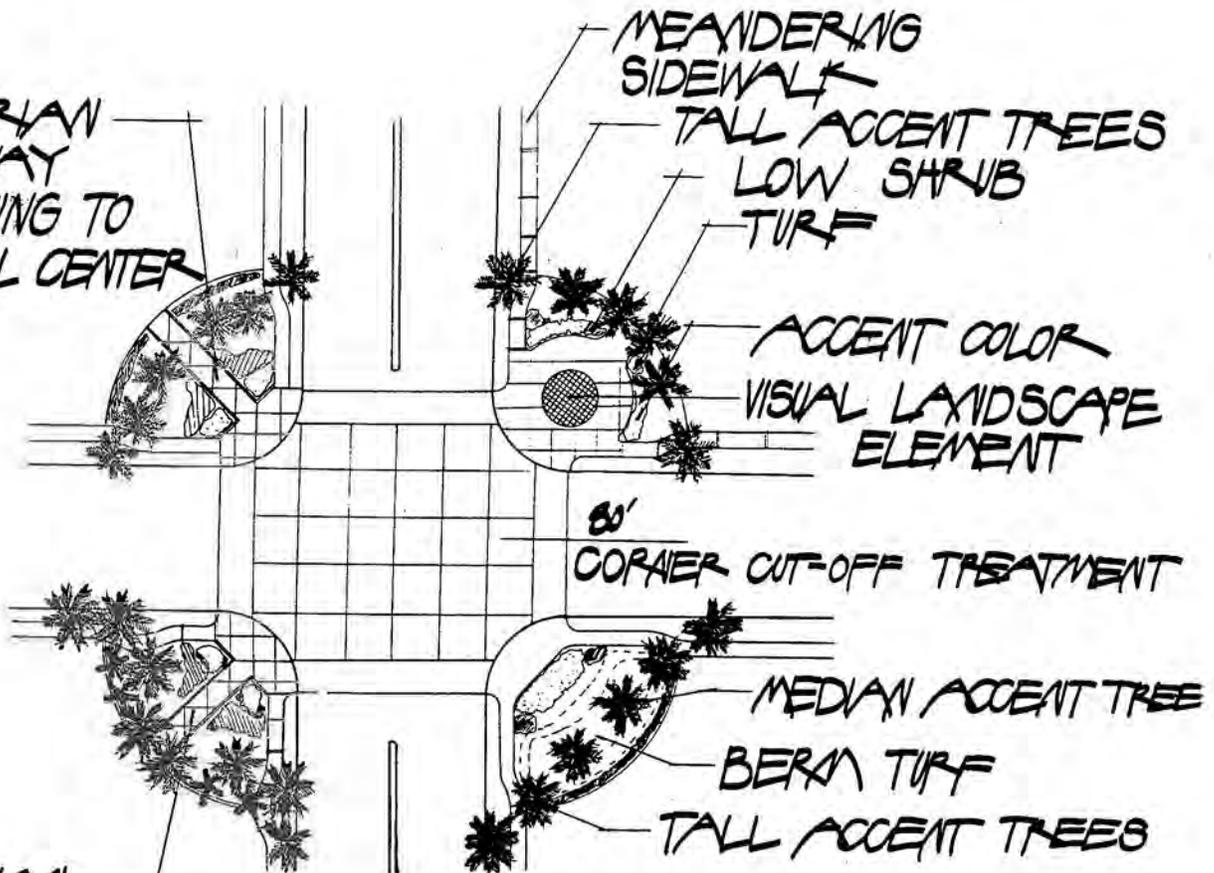
Major Entries

- a. Entries to Pacific Center East shall be designed as special accent points which "announce" site entry and establish the unique theme and character of the development (Exhibits 9a-9d).
- b. The character and concept of entry ground signs and intersection treatment will provide initial impressions of the development. The scales, form and materials of signing shall be consistent with materials used elsewhere in the landscape concept.

Streetscapes

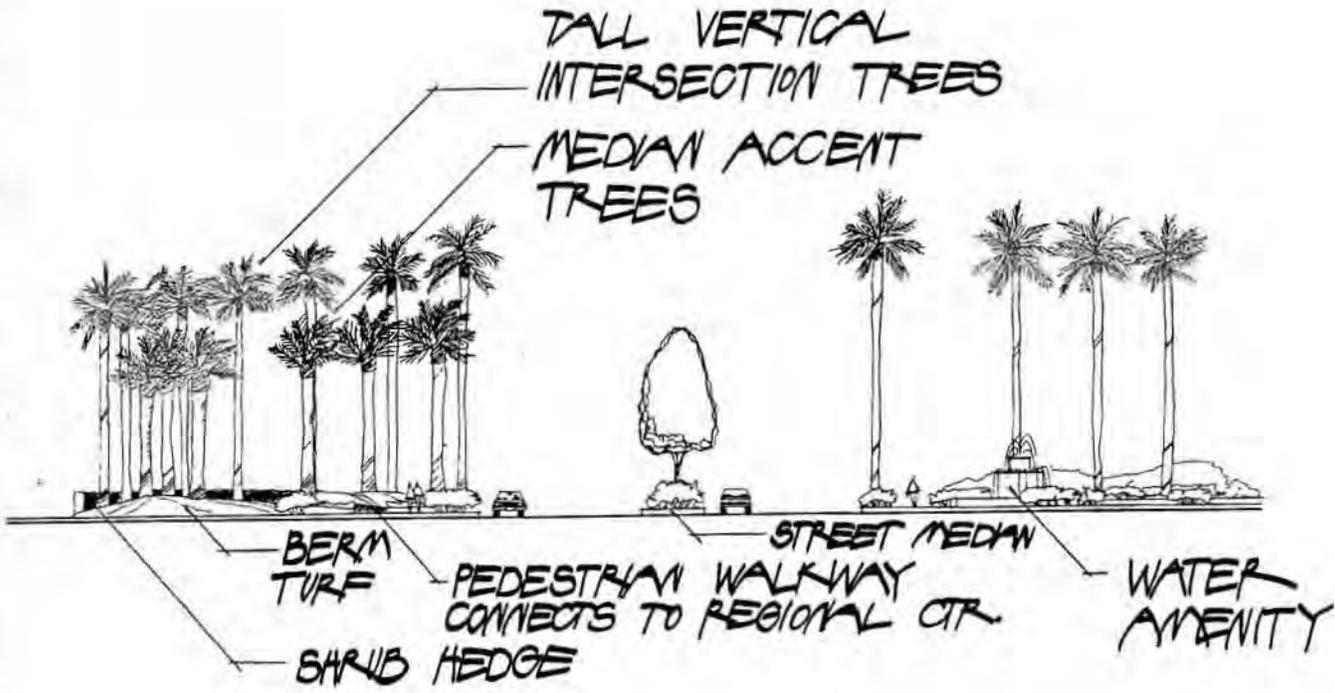
- a. The design of streetscapes will provide a sense of continuity and relationship between various uses and locations within the project. Landscape details along streets include parcel entry monumentation, streetscapes and vegetation to reinforce a "cosmopolitan" atmosphere.

PEDESTRIAN
WALKWAY
CONNECTING TO
REGIONAL CENTER

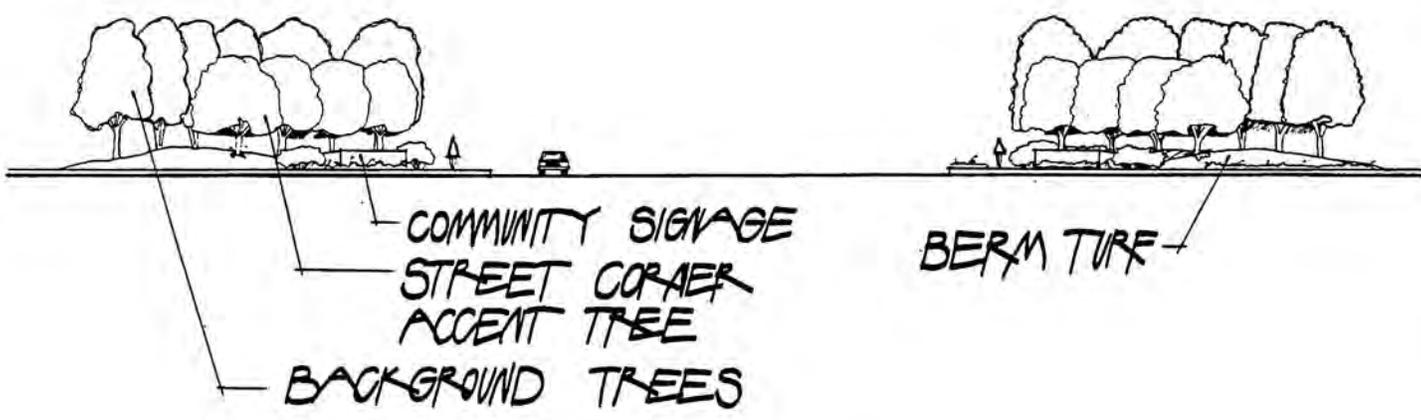
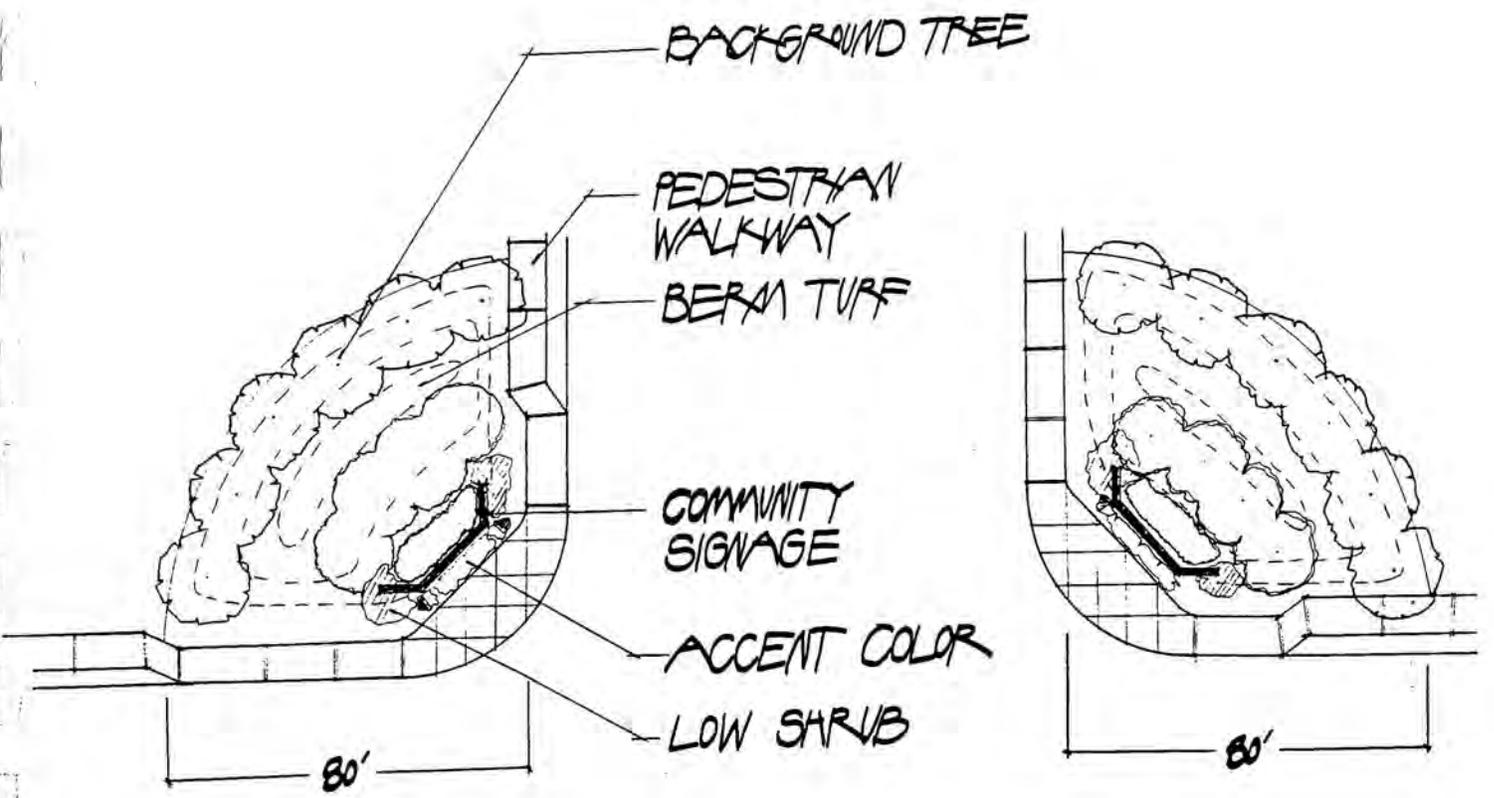


PEDESTRIAN
WALKWAY CONNECTING
TO OFFICE CENTER

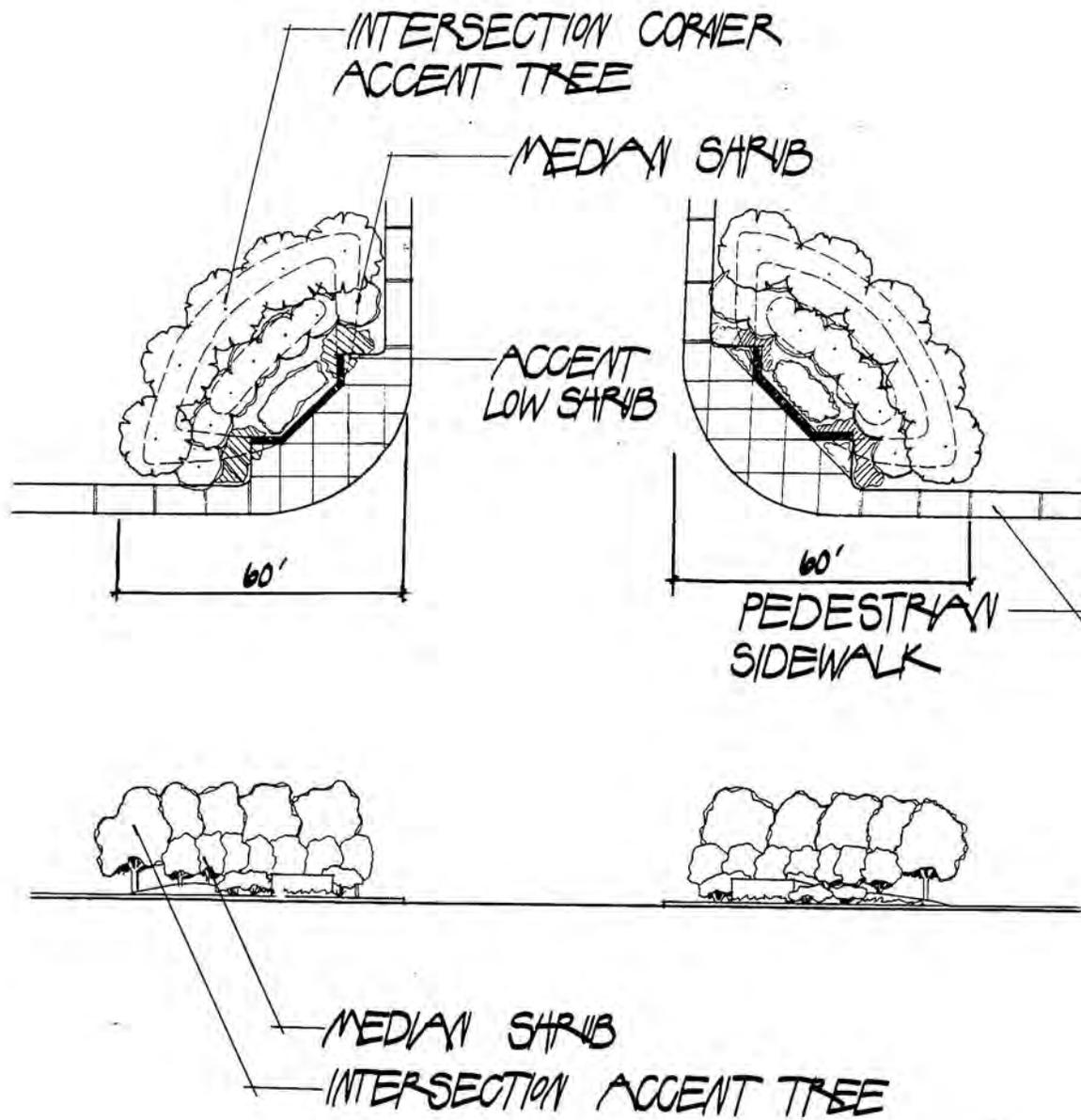
Thematic Intersection/Plan View
PACIFIC CENTER EAST
CITY OF TUSTIN



Thematic Intersection/Elevation
PACIFIC CENTER EAST
 CITY OF TUSTIN



Primary Intersection Treatment
PACIFIC CENTER EAST
 CITY OF TUSTIN



Secondary Intersection Treatment
PACIFIC CENTER EAST
 CITY OF TUSTIN

- b. Extensive visibility of the buildings will be incorporated into the overall concept.
- c. Existing street landscaping will be extended to provide continuity between existing projects and infill development or redevelopment.
- d. A special intersection treatment for Newport Avenue/SR-55 ramps shall be designed to add accent to the regional center and provide identity and direction toward major elements.
- e. Remove unsightly and cluttering elements from street right-of-way. Clutter is produced by unplanned elements in street right-of-way. Utility lines serving the site shall be placed underground. Every other element in street areas, including public signs, should be examined with a view toward improvement of design and elimination of these unnecessary elements to improve visual clarity.

A narrative description of landscape treatments for certain streets follows. Exhibits 9e-9j illustrate these concepts.

Red Hill Avenue - The existing landscaping at Red Hill Avenue and Edinger Avenue adjacent to the Pacific Bell project will remain and the current landscape palette should be extended in both directions. The meandering sidewalk should also be matched and extended along the west side of Red Hill Avenue and the south side of Edinger Avenue within the proposed 30-foot landscape setback.

Edinger Avenue - The north side of Edinger Avenue should indicate a proposed 20-foot landscape setback. Existing landscaping at the corner of Red Hill Avenue and Edinger Avenue should remain and blend westerly into the new landscape.

Valencia Avenue - The existing landscape on the south side of Valencia Avenue outside of the project area would remain, but a northside 25-foot landscape setback is proposed.

Newport Avenue - The proposed landscape concepts along both sides of Newport Avenue, between Edinger Avenue and Valencia Avenue, should incorporate a 30-foot landscape setback. North of Edinger Avenue, the setback may be reduced with the proposed retaining wall. The setback for Newport Avenue between Edinger and Sycamore will be established during review of street improvement plans.

Del Amo Avenue - The Del Amo Avenue streetscape proposes a landscape treatment on both sides of the street within a 30-foot setback. A meandering sidewalk should be included along both sides of the street. The landscape palette should propose a primary streetscape treatment because of the image when entering the project from the freeway.

5' MEANDERING
SIDEWALK

PARKWAY TREE

ACCENT SHRUB

BERM TURF

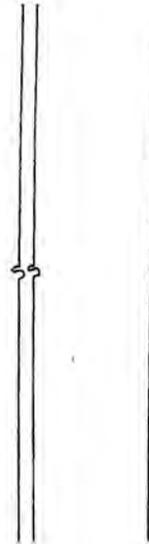
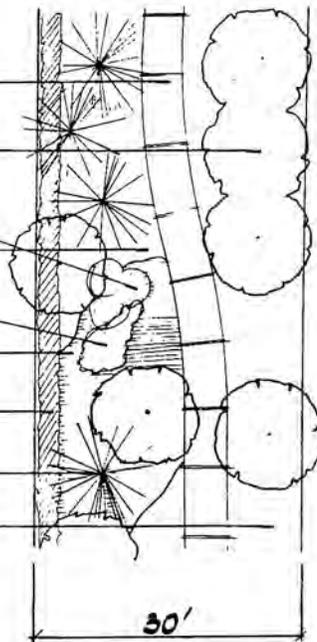
ACCENT COLOR

50% GROUND COVER

SHRUB HEDGE

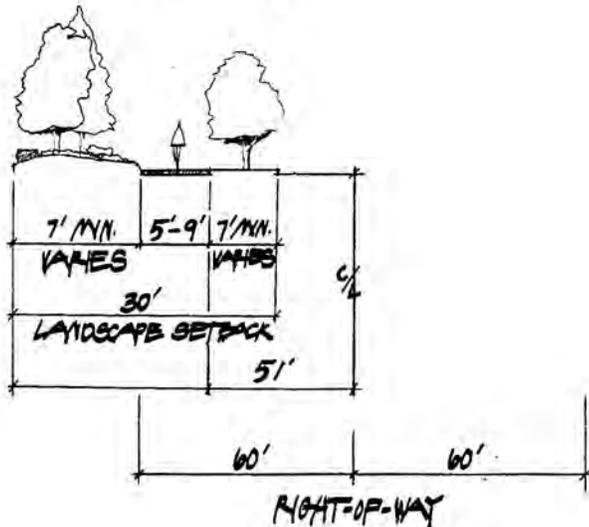
BACKGROUND TREE

TURF



NOTES:

- 1) A sidewalk easement and landscape maintenance agreement must be recorded by the developer when sidewalks occur outside of the street right-of-way.
- 2) A nine foot sidewalk width is required when sidewalks extend to the curb along major arterials.

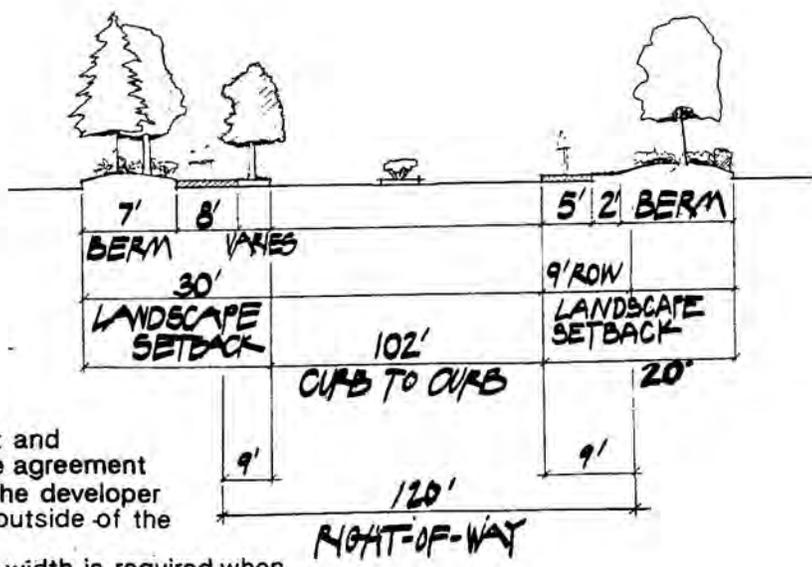
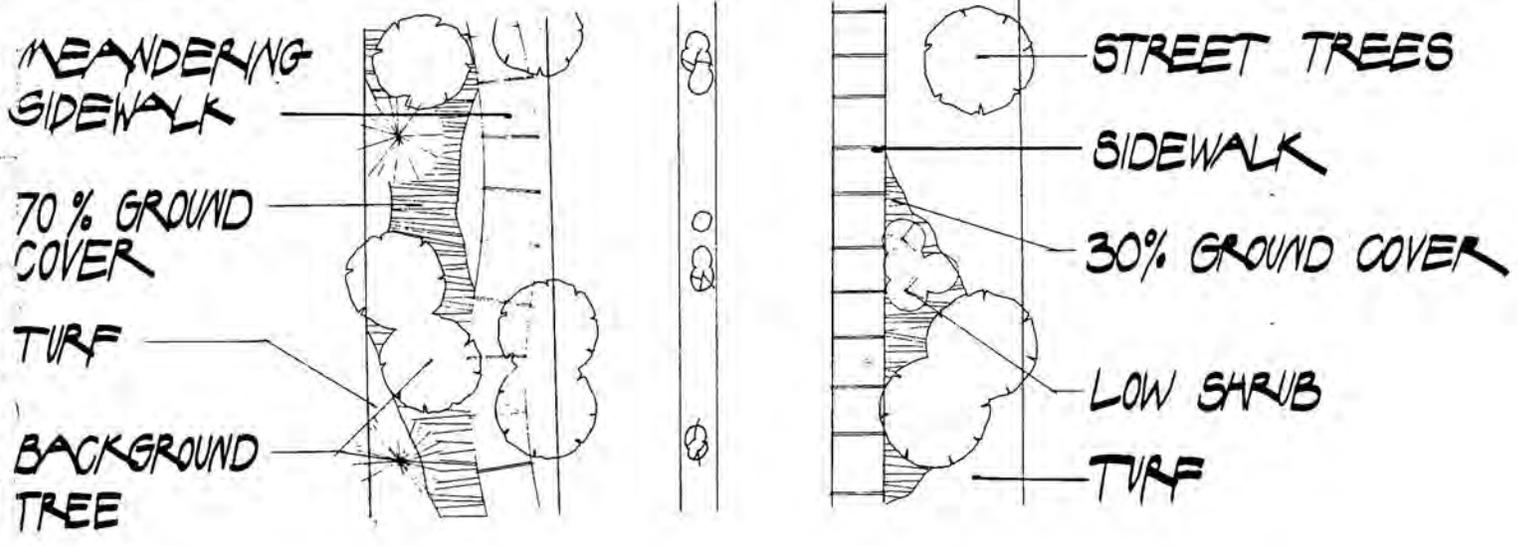


Red Hill Avenue
PACIFIC CENTER EAST
CITY OF TUSTIN



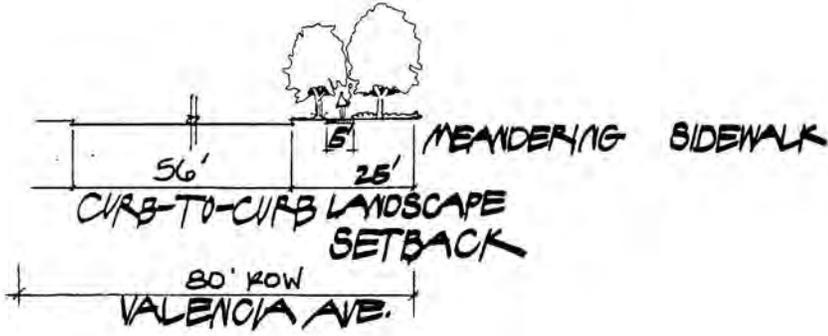
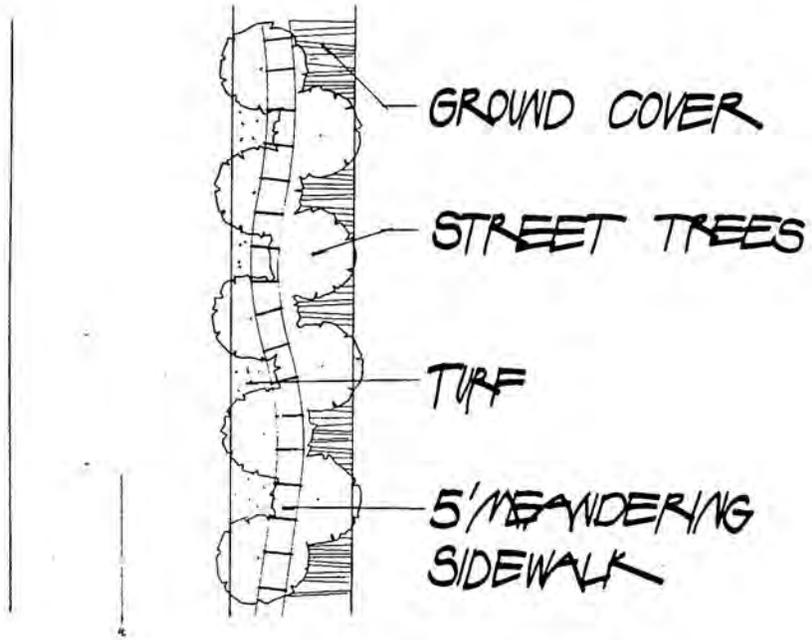
pbr

EXHIBIT 9e



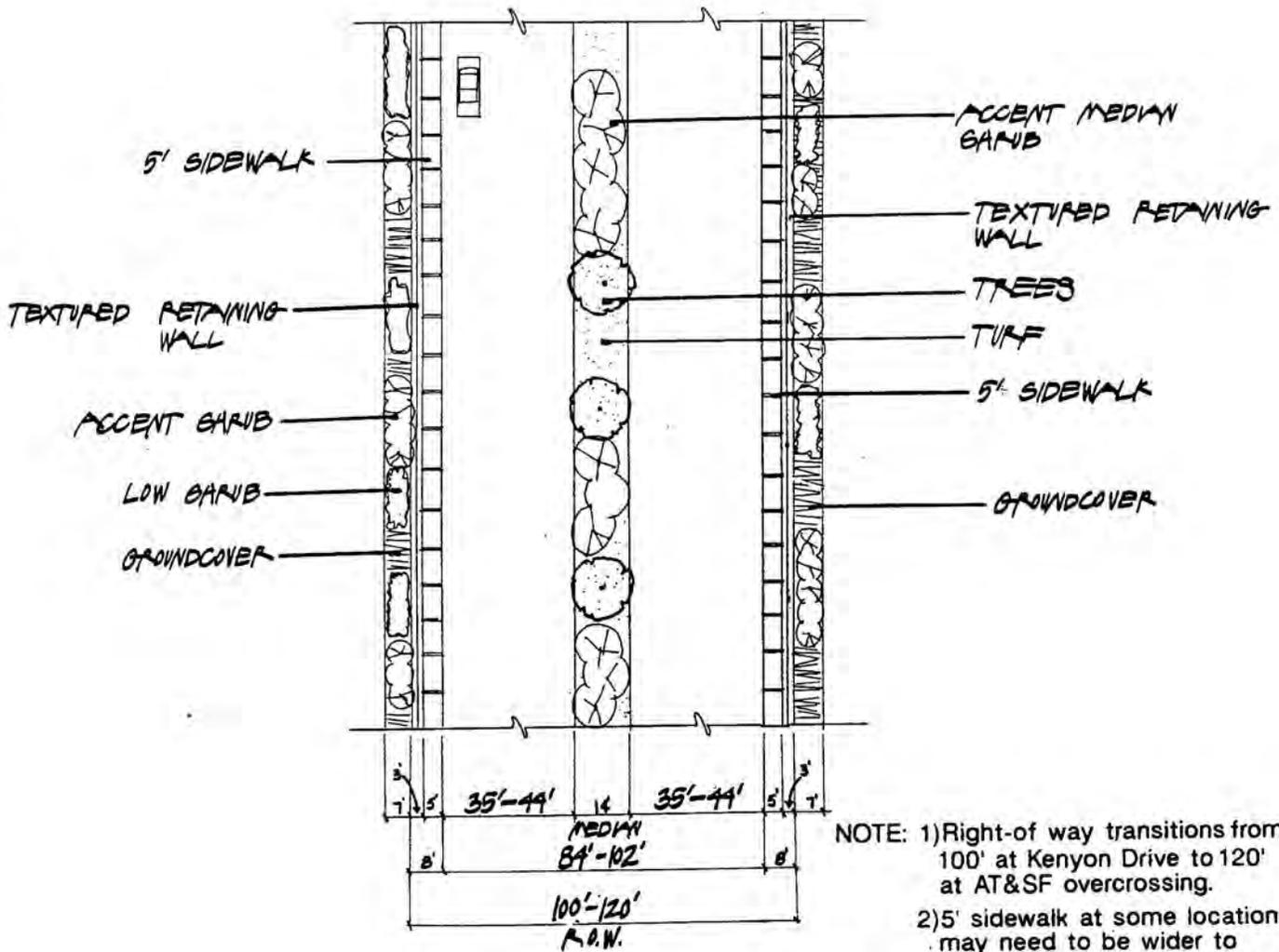
- NOTES:
- 1) A sidewalk easement and landscape maintenance agreement must be recorded by the developer when sidewalks occur outside of the street right-of-way.
 - 2) A nine feet sidewalk width is required when sidewalks extend to the curb along major arterial
 - 3) Either a raised or painted median may be developed

Edinger Avenue
PACIFIC CENTER EAST
 CITY OF TUSTIN

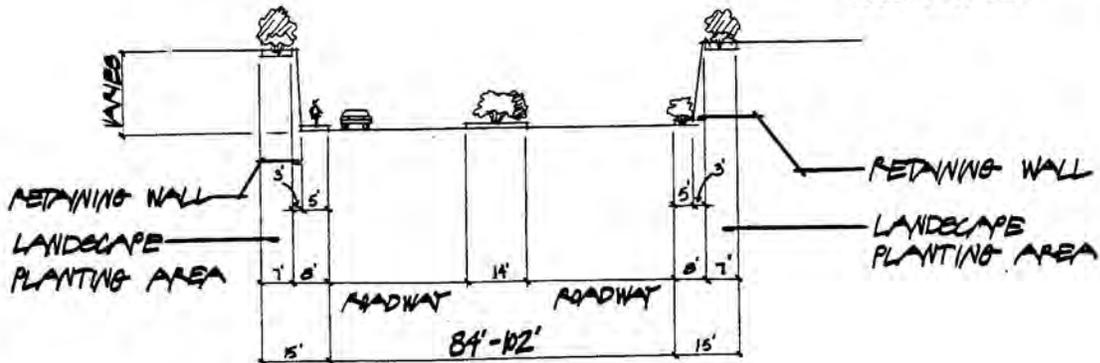


- NOTES:
- 1) A sidewalk easement and landscape maintenance agreement must be recorded by the developer when sidewalks occur outside of the street right-of-way.
 - 2) A nine feet sidewalk width is required when sidewalks extend to the curb along major arterial.

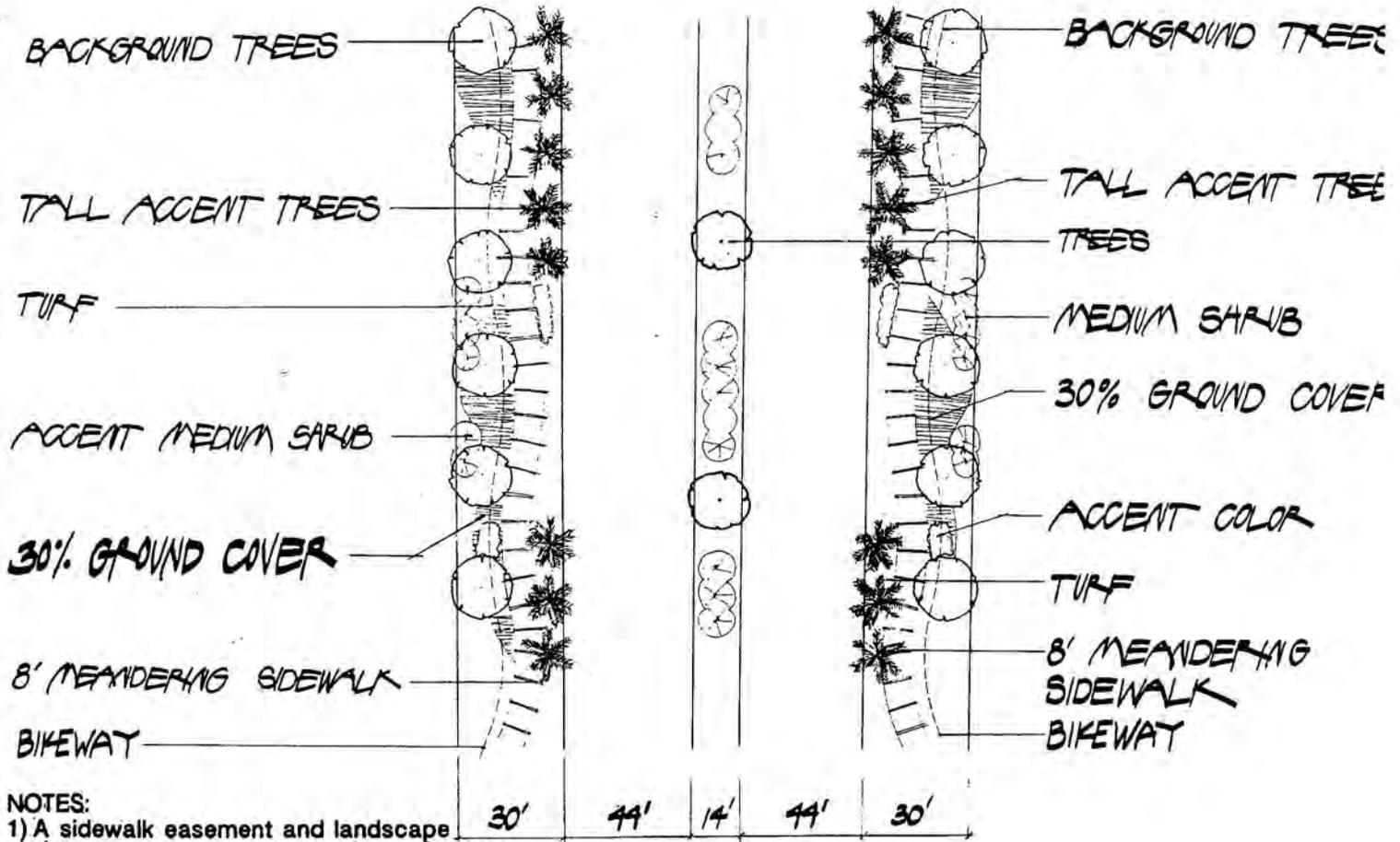
Valencia Avenue
PACIFIC CENTER EAST
 CITY OF TUSTIN



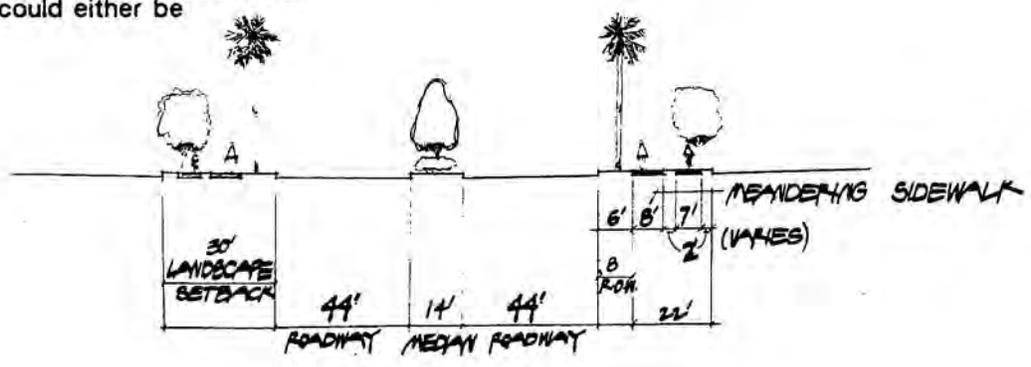
- NOTE: 1) Right-of way transitions from 100' at Kenyon Drive to 120' at AT&SF overcrossing.
- 2) 5' sidewalk at some locations may need to be wider to accommodate street light sign locations, etc.



Newport Avenue North of AT&SF
PACIFIC CENTER EAST
 CITY OF TUSTIN



- NOTES:
- 1) A sidewalk easement and landscape maintenance agreement must be recorded by the developer when sidewalks occur outside of the street right-of-way.
 - 2) A nine feet sidewalk width is required when sidewalks extend to the curb along a major arterial
 - 3) Actual bikeway locations could either be off-road or on-street



Newport Avenue South of AT&SF Railroad
PACIFIC CENTER EAST
CITY OF TUSTIN

ACCENT MEDIAN STREET TREE

BACKGROUND TREE

SHRUB HEDGE

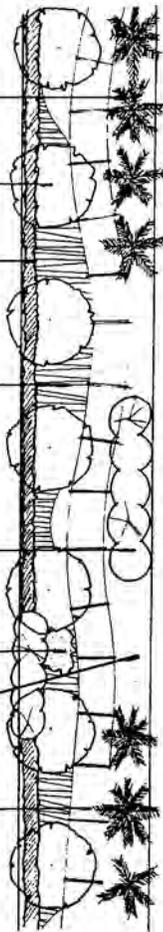
90% GROUND COVER

ACCENT STREET SHRUB

ACCENT COLOR

TURF

8' MEANDERING SIDEWALK

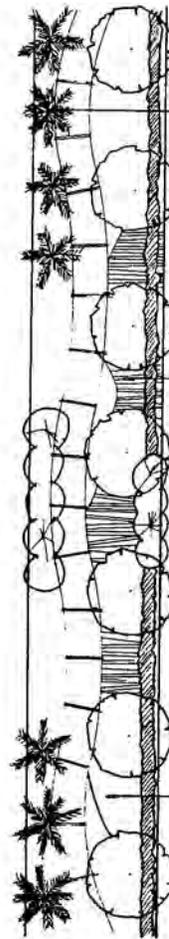


ACCENT MEDIAN STREET TREE

(LESS DENSE THAN NEWPORT AVENUE TREES)

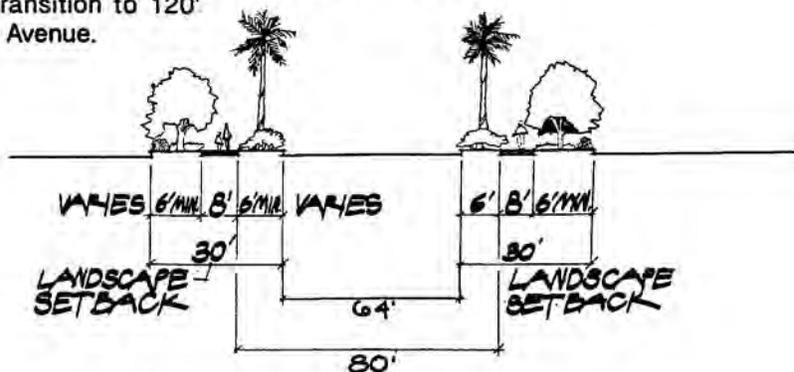
50% GROUND COVER

TURF



NOTES:

- 1) A sidewalk easement and landscape maintenance agreement must be recorded by the developer when sidewalks occur outside of the street right-of-way.
- 2) Right-of-way transition to 120' ROW occurs toward Newport Avenue.



Del Amo Avenue
PACIFIC CENTER EAST
 CITY OF TUSTIN

Pacific Center East Specific Plan

Woodlawn Avenue, Industrial Drive, Santa Fe Drive - All streets should incorporate the proposed landscape treatment within a 25-foot setback. A standard sidewalk parkway treatment is proposed for these streets (Exhibit 9k).

SR-55 Freeway - The SR-55 freeway portion of the development is the most visible aspect of the project. From an image identity standpoint, the edge condition (Exhibit 9l) should indicate quality characteristics through landscape treatment. While a 20-foot landscape buffer is proposed along a majority of the freeway frontage, this width may be reduced to range of between six and fifteen feet in Planning Areas 1 and 5 due to site and slope constraints subject to design review processing.

The plant palette listed in Table 2 is an example of appropriate plant materials which could be used in all street sections within Pacific Center East including parking structure edges, and is designed to enhance the urban design concept for the project. Equivalent species may be substituted for those listed with approval by Public Works and Community Development.

Submittal of concept landscaping and final landscaping plans will be evaluated during design review and plan check processes and will be subject to City of Tustin landscaping standards and submittal requirements.

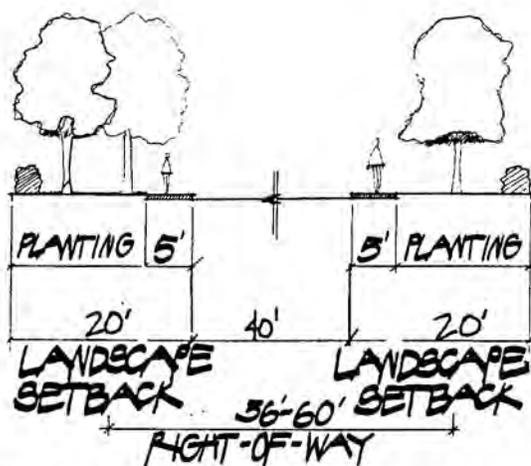
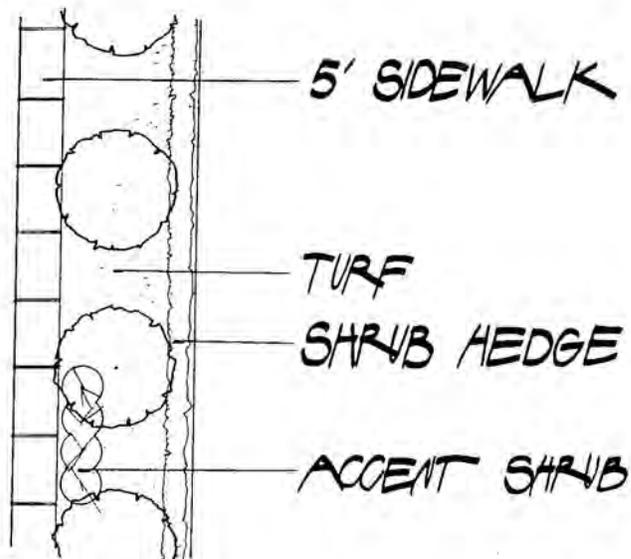
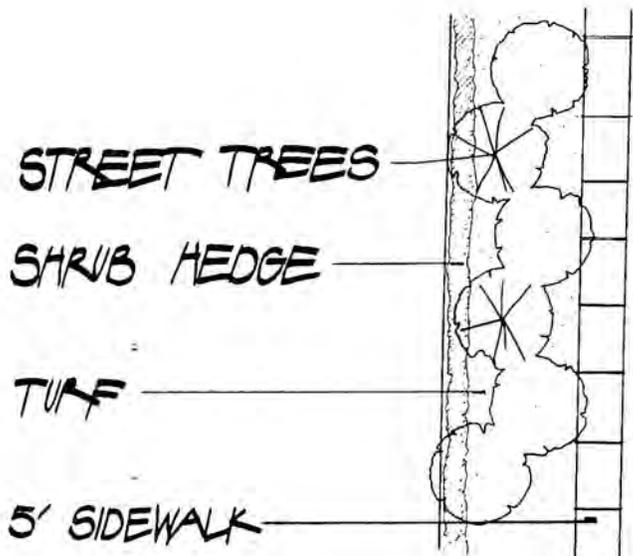
Hardscape Elements

Hardscape design elements to be incorporated into the landscape treatments in the Pacific Center East project area shall include, but not be limited to, light fixtures, bollards, special pavement and walkway treatments, trash receptacles, planters, walls and fences. Hardscape elements shall function to allow a coordinated and consistent visual and physical connector between buildings and landscape treatments within the project.

3.6 Grading Concept Plan

The grading proposed onsite provides development areas and roadways with proper surface drainage. Currently, drainage on the site is southerly and southeasterly towards the existing storm drain facility in Red Hill Avenue. Grading will generally occur within these trend line areas (see Exhibit 10).

The grading concept is based upon certain design assumptions which may change as development occurs. Design options are permissible, based upon approval of the City



Industrial Drive
PACIFIC CENTER EAST
 CITY OF TUSTIN

Table 2
PLANT PALETTE

	A	B	C	D	E	F	G	H	J	K	L	M
Accent Trees												
1. Canary Island date palm (Phoenix canariensis)	X											
2. Date Palm (Phoenix dactylifera)					X						X	
3. Mexican fan palm (Mexican washingtonia)				X								
Street Trees												
4. London plane (Plantanus acerifolia 'Bloodgood')						X	X					
5. Red-flowering iron bark (Eucalyptus leucoxyton 'Rosea')		X										
6. Bluegum eucalyptus (Eucalyptus globulus)							X					
7. Natal coral (Eurythrina humeana)		X	X									
8. Ponderosa pine (Pinus ponderosa)						X						
9. Lodgepole pine (Pinus contorfa)							X					
10. Canary Island pine (Pinus canariensis)												X

LEGEND:

- A = Thematic intersection
- B = Primary entry
- C = Secondary entry
- D = Newport Avenue
- E = Del Amo Avenue
- F = Edinger Street
- G = Red Hill Avenue
- H = Local
- J = Valencia Avenue
- K = Newport Avenue retaining wall north of Edinger Avenue
- L = SR-55 edge
- M = Parking structure edge (Valencia Avenue only)

Table 2 (Cont'd)
PLANT PALETTE

	A	B	C	D	E	F	G	H	J	K	L	M
11. Japanese live oak (<i>Quercus myrsinifolia</i>)						X			X			
12. Laurel sumac (<i>Rhus laurina</i>)								X	X			
13. Carrotwood (<i>Cupaniopsis anacardioides</i>)				X		X		X				
14. American sweet gum (<i>Liquidambar styraciflua</i> 'Palo Alto')				X		X	X		X			
15. Indian laurel (<i>Ficus microcarpa nitida</i>)								X	X			
Shrubs												
16. Compact xylosma (<i>Xyloxma congestum</i> 'Compacta')		X				X	X				X	
17. Boxwood (<i>Buxus sempervirens</i>)				X			X	X	X			
18. Compact escallonia (<i>Escallonia</i> 'Pink Princess')			X	X							X	
19. Wheeler dwarf pittosporum (<i>Pittosporum tobira</i> 'Wheeler's Dwarf')					X							

LEGEND:

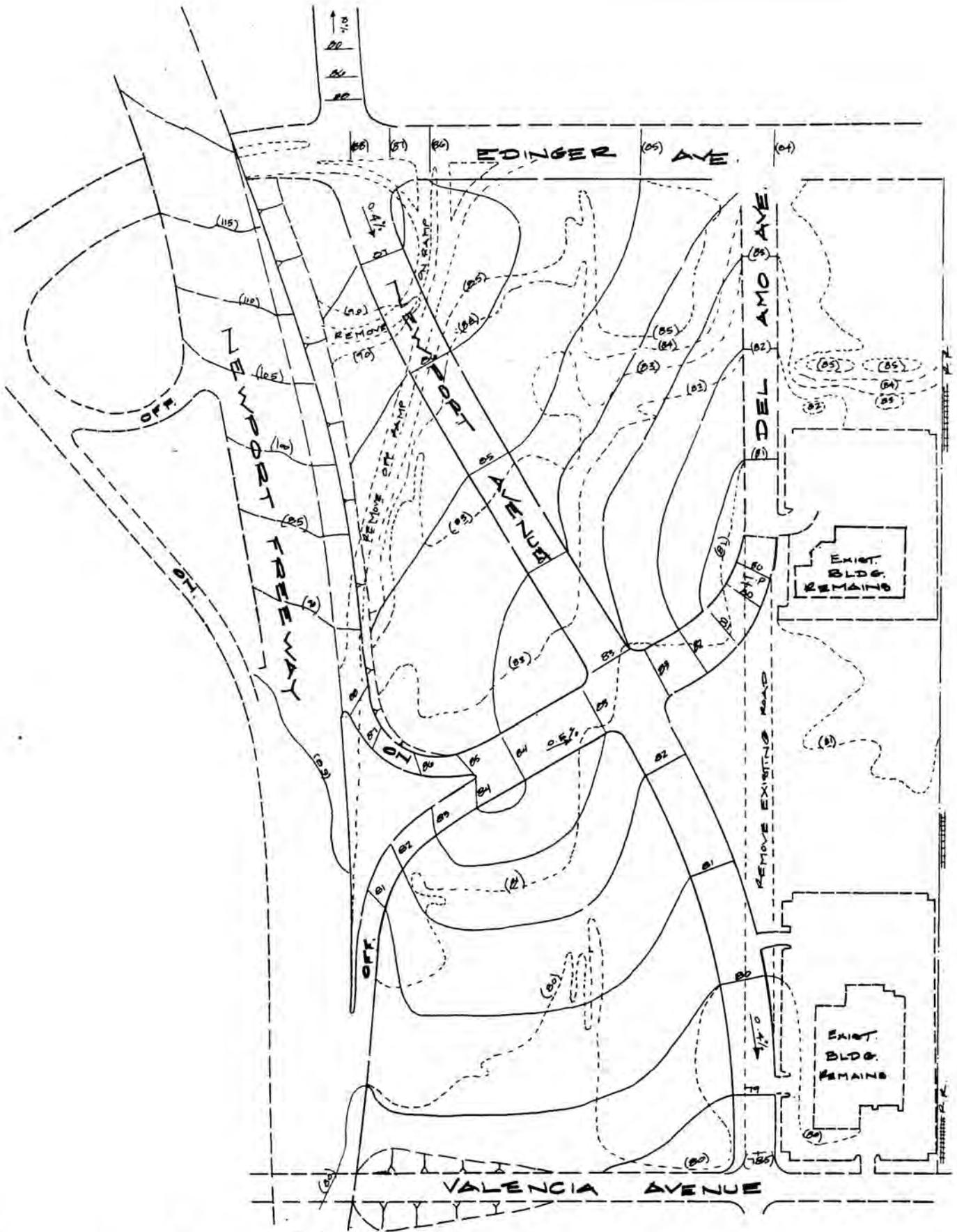
- A = Thematic intersection
- B = Primary entry
- C = Secondary entry
- D = Newport Avenue
- E = Del Amo Avenue
- F = Edinger Street
- G = Red Hill Avenue
- H = Local
- J = Valencia Avenue
- K = Newport Avenue retaining wall north of Edinger Avenue
- L = SR-55 edge
- M = Parking structure edge (Valencia Avenue only)

Table 2 (Cont'd)
PLANT PALETTE

	A	B	C	D	E	F	G	H	J	K	L	M
20. <i>Raphiolepis indica</i> (<i>Raphiolepis indica</i>) (Ballerina, Clara Coates, Crimson, Pinkcloud, Rosea dwarf)								X				
Goundcover/Vine												
21. Star jasmine (<i>Trachelospermum jasminoides</i>)				X			X	X				
22. Trailing Gazania (<i>Gazania rigens leycolaena</i>)				X	X	X						
23. Boston ivy (<i>parthenocissus tricuspidata</i>)								X			X	
Fescue/Perennial												
24. "Gem Turf" (Turf 'Gem Turf')	X	X	X	X	X	X	X	X			X	X
Accent/Color												
25. Day lilies (<i>Hemerocallies hybrids</i>)	X	X		X		X						
26. Bird of paradise (<i>Strelitzia reginae</i>)	X	X			X							
27. Detil yucca (<i>Yucca baccata</i>)	X	X										
28. <i>Bergenia ligulata</i> (<i>Bergenia ciliata</i>)												
29. Spanish bluebell (<i>Endymion hispanicus</i>)	X		X	X	X	X		X				
30. <i>Impatiens wallerana</i> (<i>Impatiens sultanii</i>)	X		X				X	X				

LEGEND:

- A = Thematic intersection
- B = Primary entry
- C = Secondary entry
- D = Newport Avenue
- E = Del Amo Avenue
- F = Edinger Street
- G = Red Hill Avenue
- H = Local
- J = Valencia Avenue
- K = Newport Avenue retaining wall north of Edinger Avenue
- L = SR-55 edge
- M = Parking structure edge (Valencia Avenue only)



Grading Concept
PACIFIC CENTER EAST
 CITY OF TUSTIN

Engineer, which are consistent with the provisions of this specific plan as well as the provisions of the City of Tustin. All earthwork shall be conducted in accordance with the city's grading ordinance and manual.

3.7 Utilities Concept Plan

The utilities concept plan primarily describes the manner in which water, sewer, and storm drain facilities will be provided for the specific plan area. Proposed upgrading of drainage, sewer and water facilities has been designed to provide an adequate level of service for the maximum level of planned development.

WATER CONCEPT PLAN

The water and sewer concept plans were developed in consultation with the City Engineering Department staff of the City Water Department, and a hydrological analysis prepared by Robert Bein, William Frost and Associates (RBF) in January 1990. The City of Tustin provides water service to the project site. The city's water distribution network which serves the project site includes 12-inch water mains in Valencia Avenue, Del Amo Avenue and Red Hill Avenue; 10-inch mains in Santa Fe Drive, Industrial Drive and Woodlawn Avenue; and an 8-inch main in Edinger Avenue. These lines primarily will serve existing and proposed development to the west of the project site and improve fire flows throughout the lines' service area. City fire flow criteria for commercial and light industrial uses requires a flow of 3,000 to 4,000 gallons per minute (gpm) delivered at a minimum pressure of 20 psi for a duration of four hours.

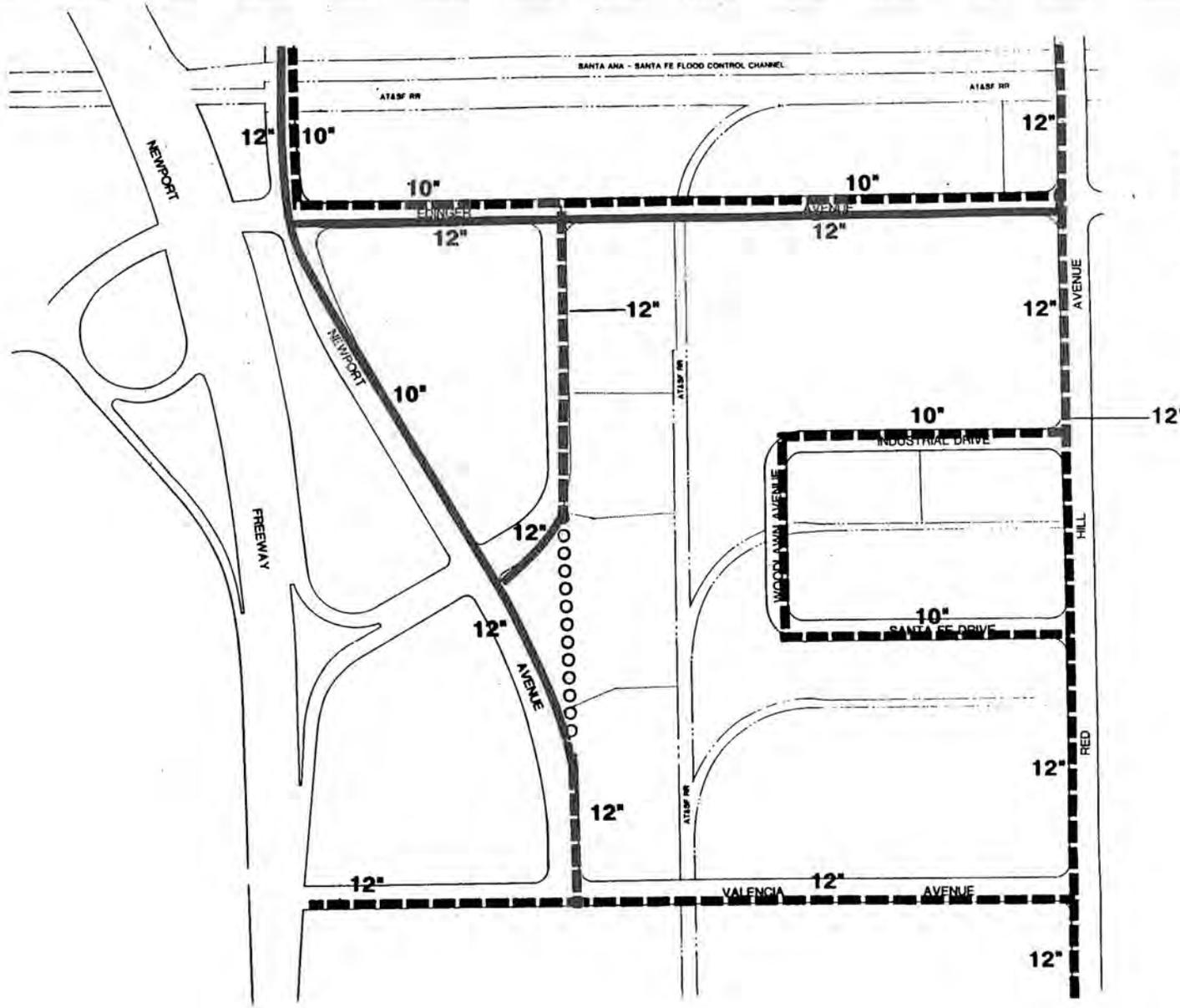
Recommended improvements include increasing the main in Edinger Avenue from 8 to 12 inches and installing a 10- and 12-inch line along Newport Avenue (Exhibit 11). Feeder lines serving individual developments will be extended from the main lines. The proposed water distribution system must be reviewed by the Tustin Water District.

A final decision on water line and sewer line locations and sizing shall be determined and approved by the City Engineer and the Tustin Water District as part of subsequent development approval. All water distribution infrastructure will be located within existing and future planned streets.

SEWER CONCEPT PLAN

The Tustin Water District is also responsible for the collection of wastewater within the project area, while effluent is treated at the County Sanitation District of Orange County (CSDOC) Fountain Valley facilities.

Existing CSDOC trunk sewer lines which serve the project area are located in Red Hill Avenue, Valencia Avenue and Edinger Avenue. Local, City of Tustin collector sewers (10-inch diameter) which feed into the trunk lines currently existing in Industrial Drive, Santa Fe Drive, and Del Amo Avenue. While there are no current capacity problems in the area,



LEGEND

-  EXISTING
-  PROPOSED
-  TO BE ABANDONED

Water Concept Plan
PACIFIC CENTER EAST
 CITY OF TUSTIN

SOURCE: Robert Bein, William Frost & Associates

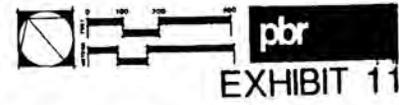


EXHIBIT 11

CSDOC plans to construct a 27-inch parallel sewer line in Red Hill Avenue by 1996 or 1997. New 10-inch sewer lines will be constructed within the proposed Newport Avenue extension and portions of the existing line within Del Amo Avenue will be abandoned. An additional 10-inch line will be provided parallel to the existing line in Valencia Avenue (Exhibit 12).

STORM DRAIN CONCEPT PLAN

As stated previously, drainage on the site is in a southerly and southeasterly direction toward the existing storm drain facility (FO9P15) in Red Hill Avenue. Drainage north of the site is conveyed southerly to the Santa Ana-Santa Fe Channel (F10) which is parallel to the northern project boundary and the AT&SF railroad. Drainage west of the site is conveyed via a drainage ditch to the F08 drainage facility.

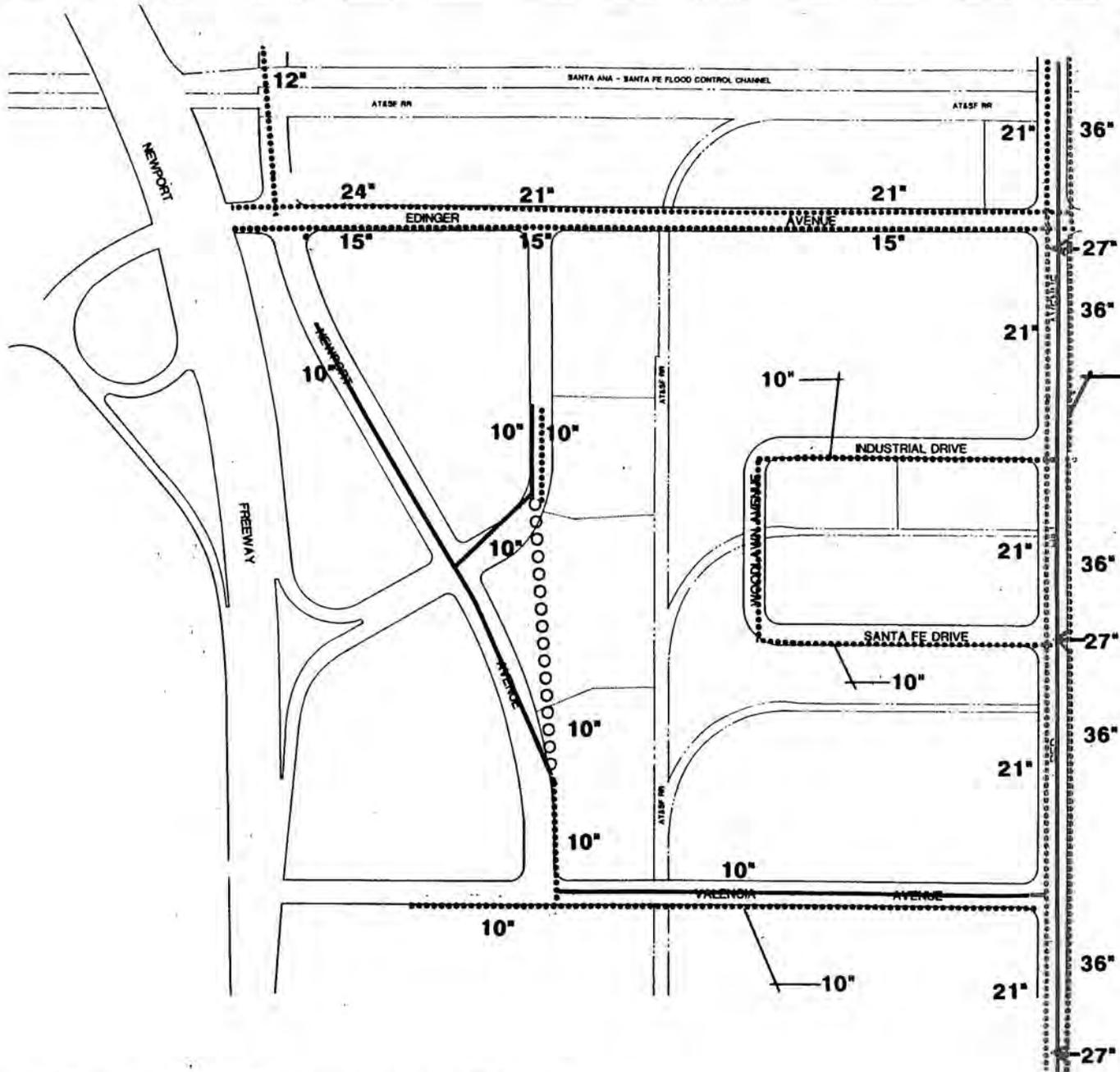
The recommended onsite storm drain facilities proposed for the drainage concept plan are shown in Exhibit 13. The conceptual drainage plan is based on the proposed land plan and building intensity anticipated in the specific plan area. Final precise size and locations of proposed storm drain lines shall be subject to approval by the City Engineer subject to the provisions of the city's standard improvement plans. The storm drain concept plan was developed in consultation with staff of the Orange County Flood Control District, Dokken Engineering, RBF and ASL Engineering. As recommended in the technical analysis prepared by RBF, both onsite storm drains and reconstruction of the flood channel overpass are required to mitigate potential 100-year storm flows.

The conceptual plan for the Newport Avenue underpass, flood channel and railroad extension was previously illustrated in Exhibit 8. The flood conduit is sized to accommodate 100-year storm flows. In addition, a pump drain system in the Newport Avenue underpass will prevent flooding of the street, assuring safe passage during rainstorm or flood events. Although right-of-way impacts would increase compared to an underpass, if an overpass alternative is implemented, then channel improvements will be made as specified in the Santa Fe Channel Hydrology Report (OCEMA 1986). As noted in the RBF engineering report, channel improvements are required to prevent flooding of the project area in the event of a 100-year flood. Either flood channel design must be approved by the Orange County Flood Control District and by the City Engineer.

OTHER UTILITIES

Electricity

Electricity service to the project area is provided by Southern California Edison Company. Power is presently supplied to the project area via overhead and underground lines including the 66 kv overhead lines along Del Amo Avenue. The 66 kv lines are proposed to be relocated along the future extension of Newport Avenue (Exhibit 14).



LEGEND

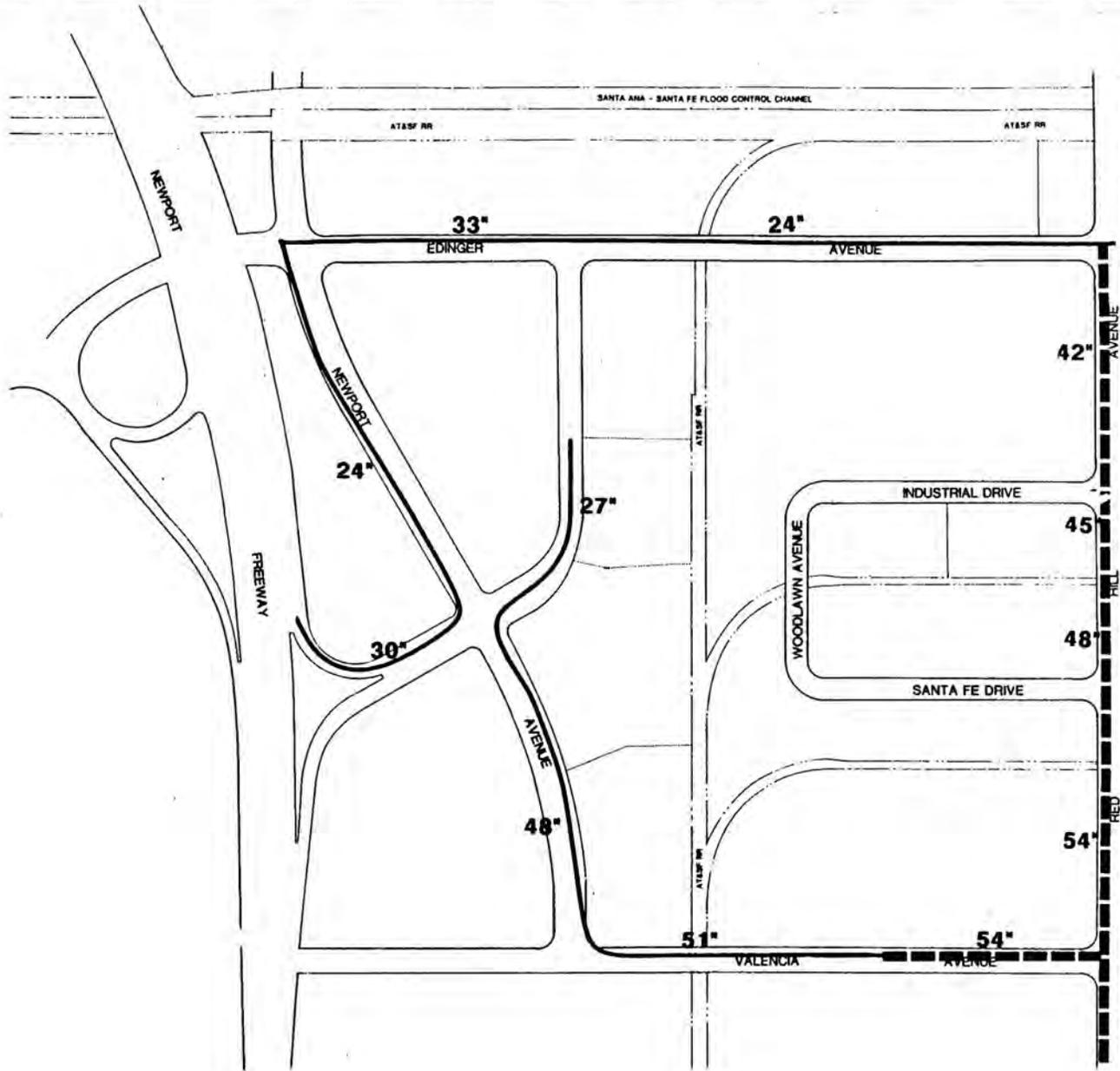
-  EXISTING
-  PROPOSED
-  TO BE ABANDONED

FUTURE CSDOC PARALLEL IN REDHILL FROM SKYLINE (NORTH OF PROJECT) TO MCGAW (SOUTH OF PROJECT) SCHEDULED CONSTRUCTION 1996/97.

Sewer Concept Plan
PACIFIC CENTER EAST
 CITY OF TUSTIN

SOURCE: Robert Beln, William Frost & Associates





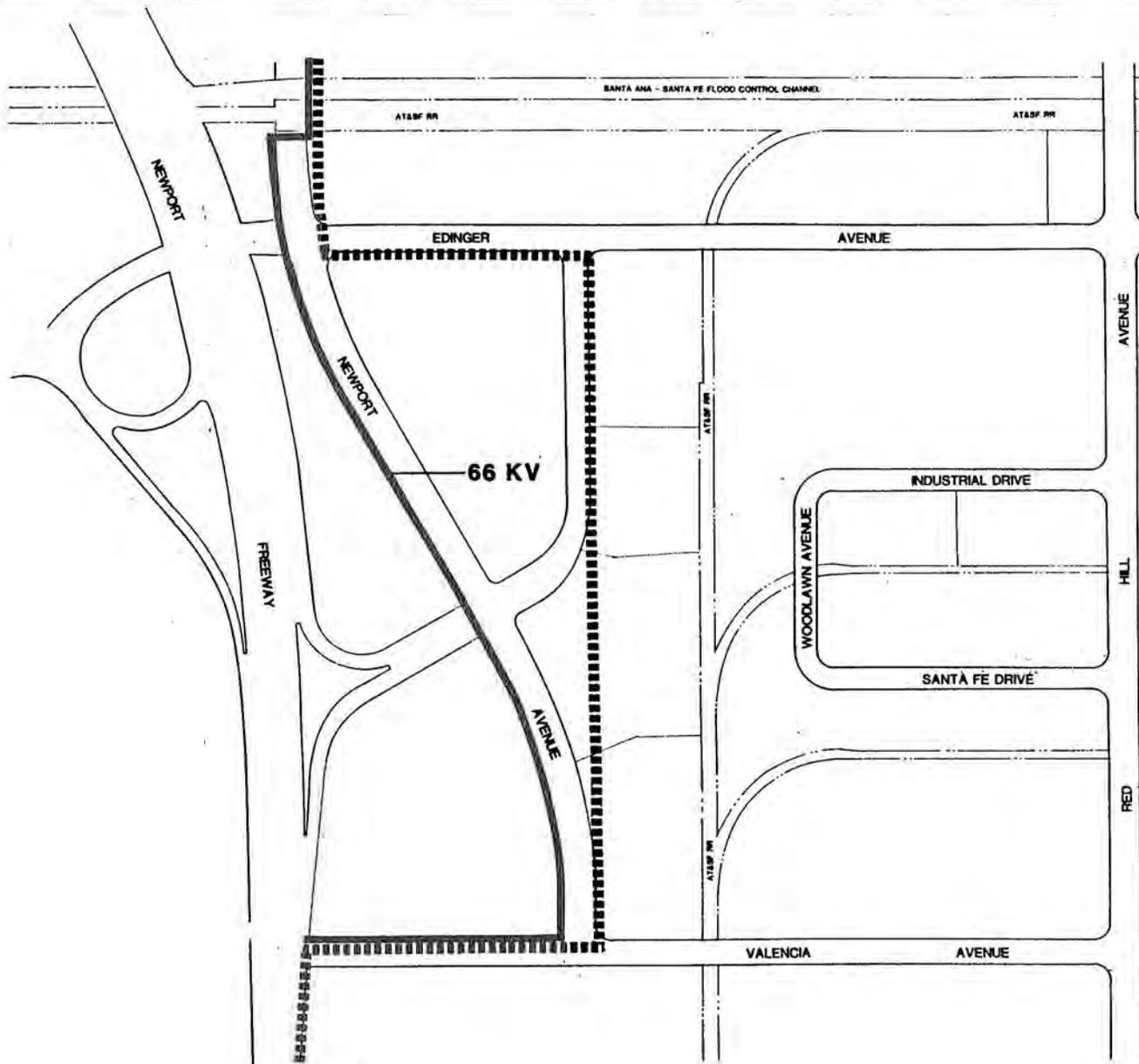
LEGEND

-  EXISTING
-  PROPOSED

Storm Drain Concept Plan
PACIFIC CENTER EAST
 CITY OF TUSTIN

SOURCE: Robert Bein, William Frost & Associates





LEGEND



EXISTING



PROPOSED UNDERGROUND TRANSMISSION LINE

NOTE: Undergrounding is preferable but may be cost prohibitive; the final decision on whether to underground the 66KV line shall be determined by the Community Development Department and Public Works Department

**Electrical Concept Plan
PACIFIC CENTER EAST
CITY OF TUSTIN**



Telephone

Pacific Bell provides telephone service to the project site from existing underground facilities. Appropriate easements will be provided for any new telephone facilities as required by Pacific Bell. The existing underground cable lines and conduits will require modification to provide service to the entire project area.

Natural Gas

The Southern California Gas Company provides natural gas service to the project area from 4-inch gas mains located in Red Hill Avenue and a 30-inch main in Edinger Avenue. The gas company has indicated that adequate gas service can be extended throughout the site. The proposed distribution system is shown in Exhibit 15.

Cable Television

Continental Cable Television and Community Cable Vision provide service to the project area. They have indicated that they will be able to provide all necessary facilities for the proposed project.

Railroad Operations

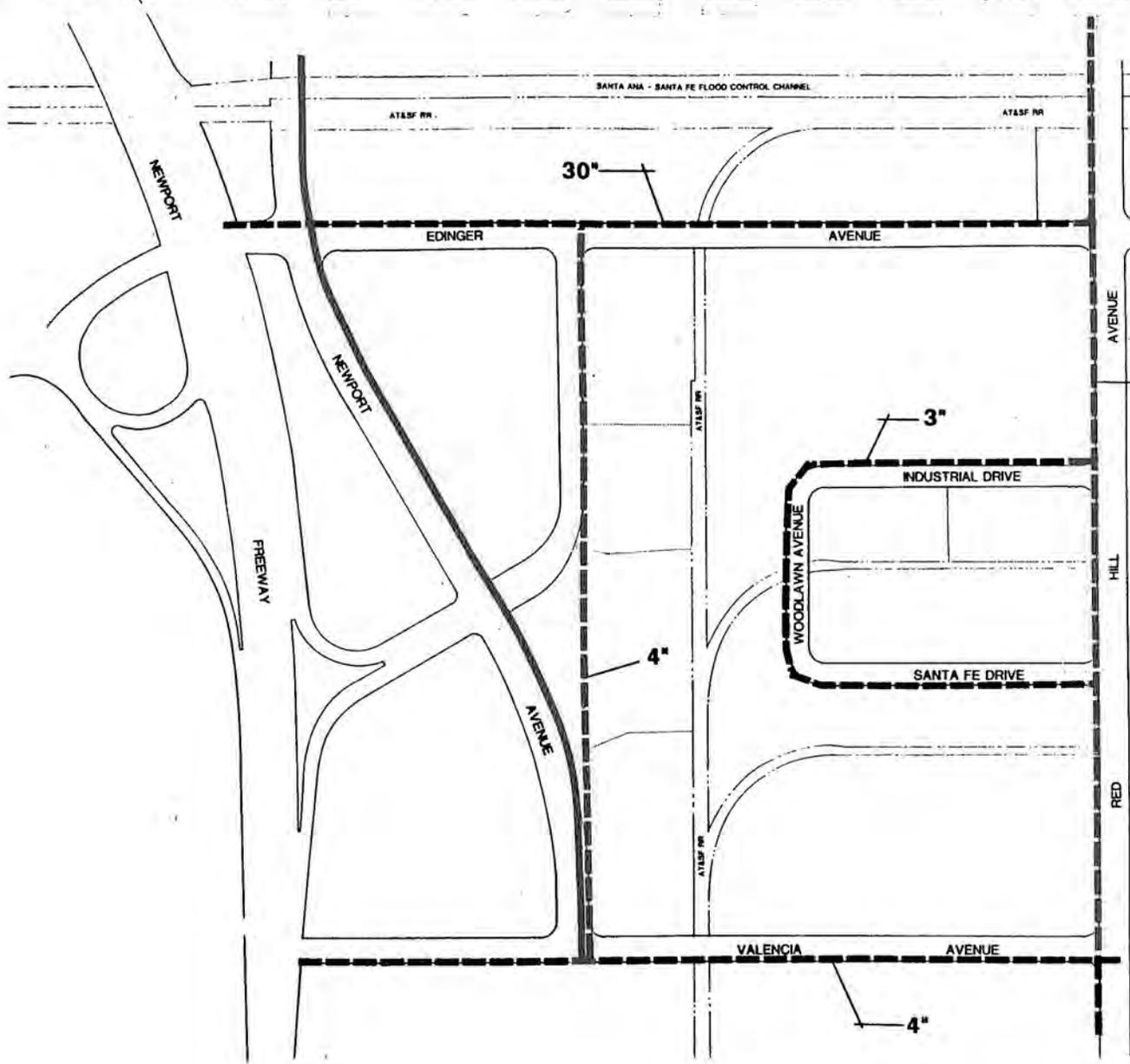
The main track of the AT&SF railroad runs parallel to Edinger Avenue north of the site. The west lead track (Newport Lead) extends from the main track onto the project site west of Del Amo Avenue. The east lead track (Irvine Lead) is a longer track east of the Newport Lead which extends to the 405 freeway. One to twenty road switchers operate on the Irvine Lead Monday through Friday. Current daily operations on the main track include 16 Amtrak trains, two freight trains, and one road switcher. There are no current operations on the west lead track.

Project implementation will result in removal of the Newport Lead prior to initial grading. However, AT&SF railway company has indicated that their removal of the track will not impact operations since there are no active customers on this lead. The Irvine Lead will remain to serve customers to the south.

The existing at-grade railroad track north of Edinger Avenue may be reconstructed as an underpass or an overpass. Either plan will require approval of the city, the Atchison Topeka and Santa Fe Railroad and the Public Utilities Commission. Plans for construction will be phased so that current rail service is not interrupted.

General Utility Standards

1. No structures shall be permitted to be developed over existing pipeline, power line or other similar easements, except where these can be relocated, and with the approval of the City Engineer.

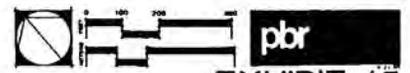


LEGEND

-  EXISTING
-  PROPOSED

Natural Gas Concept Plan
PACIFIC CENTER EAST
 CITY OF TUSTIN

SOURCE: Robert Bein, William Frost & Associates



pbr
 EXHIBIT 15

2. All onsite water supply, wastewater collection, storm drainage, and sewage lines and facilities shall be provided by a developer in accordance with the provisions of the adopted master plans for water, sewers, and drainage, unless costs are otherwise determined as a result of a negotiated development agreement or borne by the Redevelopment Agency.
3. All utility lines exclusively serving a development shall be placed underground by the developer in accordance with the city's policies for undergrounding utilities.
4. Adequate water for estimated domestic consumption and for fire flow requirements, as determined by the Orange County Fire Department, shall be provided by a developer, unless costs are otherwise determined as a result of a negotiated development agreement.
5. Costs for improvements to water drainage, and sewer systems included in the adopted master plans for water, drainage and sewers shall be assigned to each development lot or parcel in accordance with a cost-benefit formula enabled by the City Engineer, based on current engineering construction costs, as amended from time to time, or as otherwise determined as a result of the negotiated development agreement.
6. Development of the project area shall comply with the requirements of the Orange County Fire Department in providing adequate fire flow, number and location of hydrants, building clearances, and street turning radii for access.

3.8 Signage Plan

The purpose of this section is to provide guidelines and standards for project identification signage throughout the specific plan area to achieve a visually coordinated, balanced and appealing signage system particularly one which promotes compatibility with the architectural design concept, the circulation plan and the landscape concept plan.

General Provisions

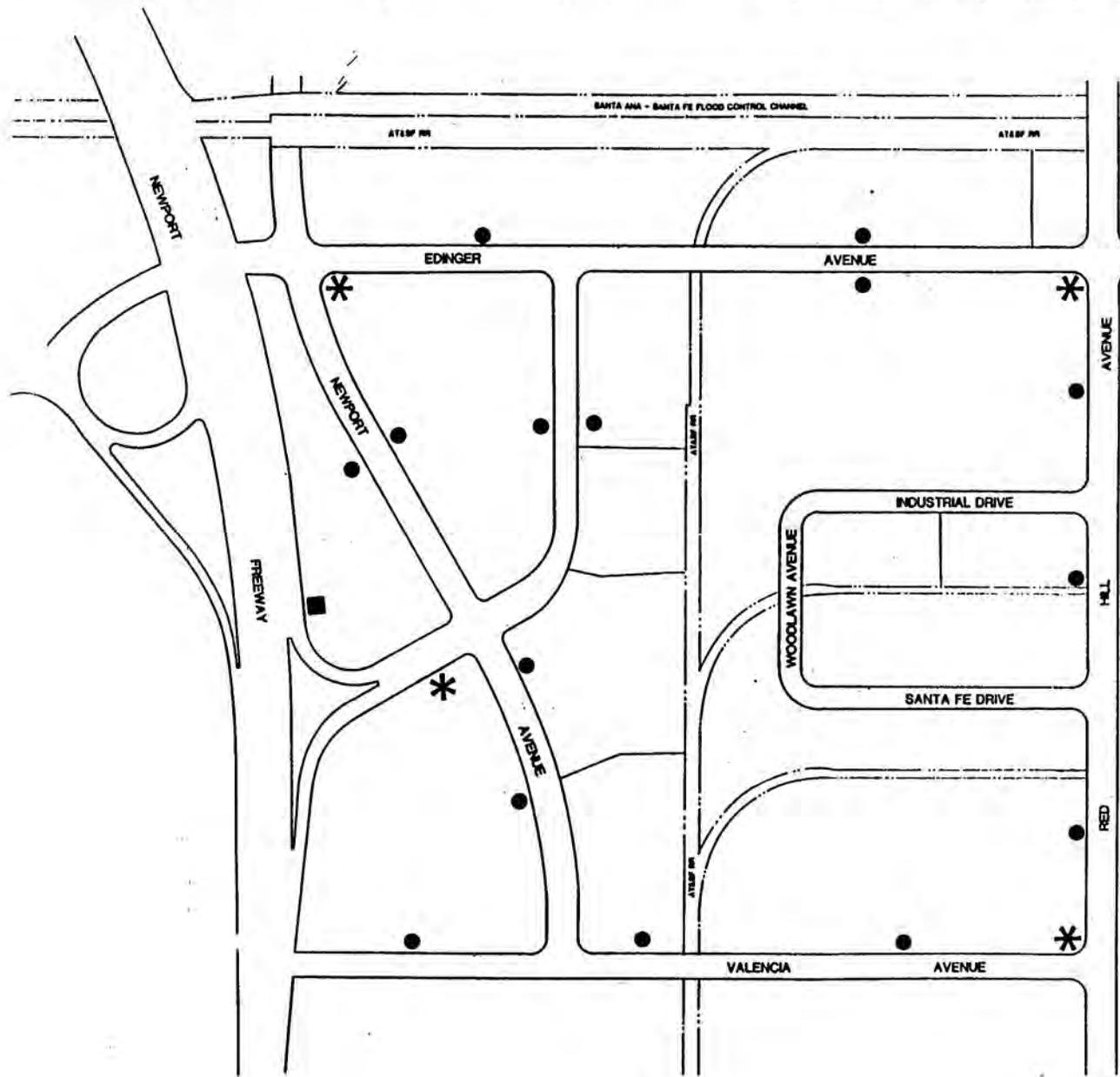
1. Prior to painting, erecting, constructing, altering, rebuilding, replacing or moving any sign, except those exempt from a permit by the Tustin sign code, approval of the Community Development Department is required.
2. Applications shall be accompanied by information as required for a standard sign plan or master sign plan pursuant to the Tustin City Code. A master sign plan is required for all new developments and is subject to design review approval. In addition to requirements for standard sign plans, a master sign plan submittal shall include the following information on the proposed signage program for the site:
 - a. Plan specifications including the type of texture of materials and colors proposed for the signs and the building façade.

- b. A colored elevation of the proposed signs and the building façade.
 - c. Drawings illustrating the lettering styles and sizes proposed and the use of logos, if any.
 - d. Photographs of buildings and signs on adjacent sites.
 - e. Any restrictions on the use of temporary signs.
3. General criteria applicable to all signs – proposed signs and their materials, size, color, lettering, location and arrangement shall conform to the following criteria:
- a. Signs shall be consistent throughout a development site by incorporating common design elements such as quality of materials, letter style, colors (not more than three excluding black and white per individual sign), illumination, sign type or sign shape. All signs and letter colors are subject to the city’s design review procedures.
 - b. Signs shall be compatible with, and bear a harmonious relationship to the visual image and architectural design of the buildings they identify in terms of material, colors, and design motif within the specific plan area and also identify with the primary signage elements shown on Exhibit 16.
 - c. Signs shall relate to a human scale, and shall be directed toward pedestrians as well as motorists.
 - d. Signs shall contain only that information necessary to identify the businesses or uses of the property on which the sign is located.
 - e. Signs shall be appropriately visible, legible, as far as spacing and proportion of letters and details, and shall not dominate the visual quality of the site or obscure from view existing or adjacent signs.
 - f. Signs shall be compatible with the visual characteristics of the development and signs in the surrounding area and shall not detract from, or cause depreciation of the value of adjacent developed properties.
 - g. New signs in existing developments shall be designed in accordance with the established master sign plan for the building or center where the sign is to be located. If no such plan exists, any new signs shall be designed to be harmonious with other existing signs on the property and/or architectural theme or design features of the building(s).

- h. Freestanding signs may be located in required setback areas provided the following criteria are met:
 - 1) Said location is not within a required visual clearance area as identified by the City Engineer.
 - 2) Said location in the event of a lighted sign will not cause negative light and glare impacts on adjacent sensitive land uses.
 - i. All signs shall conform with provisions contained in the Tustin City Code unless otherwise noted in this section.
4. Criteria applicable to master sign plan – In addition to general criteria listed in item 3 above of this section, master sign plans shall be reviewed for conformance with the following criteria:
- a. Signs shall reflect a common theme, incorporating similar design elements in terms of materials, letter styles, colors, illumination, sign type and sign shape.
 - b. Signs shall utilize materials, colors, and a design motif which is compatible and which reflects the special qualities of the architecture of the buildings on the site in both daytime and nighttime situations and also identify with primary signage elements shown in Exhibit 16.
 - c. Directional signage shall identify and direct traffic to SR-55, Edinger Avenue and to the City of Santa Ana.
 - d. Selected signage shall direct users to subareas of the project.
5. A master sign program may deviate from specific Tustin City Code standards for proposed/center identification and business identification signs subject to submittal and approval of a Conditional Use Permit application. Pacific Center East project area signs shall be permitted in addition to the center identification sign standards in the Tustin City Code provided they are generally consistent with locations shown in Exhibit 16.

3.9 Phasing Concept Plan

The phasing concept outlines the amount of permitted development bases on the sequence of circulation improvements anticipated within the specific plan area. Development phasing is also tied to market demand, development opportunities, circulation capacity and financing factors not currently available.



-  Freeway Oriented Signage
-  Center Identification or Development Site Signage
-  Specific Plan Project Area Signage

Signage Concept
PACIFIC CENTER EAST
 CITY OF TUSTIN

The phasing of development is closely tied to available circulation capacity, approval of circulation plans by state agencies and approval of the railroad/flood channel overpass or underpass north of Edinger Avenue at Newport Avenue.

With these constraints, the phasing concept is influenced by these public infrastructure improvements:

1. Acquisition of public right-of-way and westerly railroad spur removal.
2. SR-55 eastbound ramps are constructed/extended to Del Amo Avenue.
3. Newport Avenue is extended from Edinger Avenue to Valencia Avenue and existing SR-55 eastbound ramps are removed.
4. Public utilities and services are extended or relocated concurrently with street extensions or widenings.
5. Newport Avenue underpass is constructed.
6. Development proceeds to buildout.

Based on the above sequencing, the available environmental analysis and financing constraints, the proposed phasing of the project is presented in three phases (Table 3). Each phase defines the parameters necessary for development to occur within the study area at any given time and the magnitude of development which can occur. Following is a brief description of each phase, location and sequencing of public improvements necessary.

Table 3
PHASING SCHEDULE

PHASE 1

Improvement or Action Needed	Building Permits or Certificates of Occupancy May Be Issued For
<p>A. Compliance with all conditions of approval and compliance with original development and Conditional Use Permit (CUP) 84-20, including any necessary infrastructure improvements.</p>	<p>Permits for 224,520 square feet of office and certificate of occupancy for 224,520 square feet of office already entitled by CUP 84-20 to be located in Planning Area 8 only (must be consistent with approved development plan for CUP 84-20).</p>
<p>B. Required Plan Submittals</p>	
<p>1. Project study report to Caltrans for Newport Avenue/SR-55 interchange northbound ramp reconfiguration.</p>	
<p>2. Plans, specifications, agreements for:</p>	
<p>a. SR-55 northbound ramp configuration</p>	
<p>b. Newport Avenue extension south of Edinger Avenue to Valencia Avenue.</p>	
<p>c. Edinger Avenue widening east of SR-55.</p>	
<p>d. Del Amo Avenue extension and widening from reconfigured SR-55 ramp to Edinger Avenue.</p>	
<p>3. Approved plans for additional traffic improvements needed to mitigate impacts of Phase 1 square footage for which certificate of occupancy will be issued, including:</p>	
<p><u>Within City of Tustin</u></p>	
<p>Red Hill Avenue and Edinger Avenue² Add third northbound through lane Add third westbound through lane Add second southbound left-turn lane</p>	
<p><u>Within City of Santa Ana</u></p>	
<p>SR-55 SB ramp and Edinger Avenue² Add second westbound left-turn lane</p>	<p>Permits for 100,000 square feet of office, minimum 250-room hotel and 100,000 square feet of commercial in Phase II.B and permits for 224,520 square feet of additional office in Planning Area 8 in Phase IA (in the event proposed development does not comply with CUP 84-20 entitlement).</p>

Table 3 (cont'd)

Improvement or Action Needed	Building Permits or Certificates of Occupancy May Be Issued For
C. Plan Approvals or Construction Completions	
<ol style="list-style-type: none"> 1. Approval of all submittals, plans shown in IB1 and IB2. 2. Completion of all improvements as shown in IB3. 	<p>Certificate of occupancy for permits authorized to be issued for in Phase IB and IA (in the event proposed development does not comply with CUP 84-20 entitlement).</p>

PHASE II

<u>Improvement or Action Needed</u>	<u>Building Permits or Certificates of Occupancy May Be Issued For</u>
A. Required Plan Submittals	
<ol style="list-style-type: none"> 1. Plans, specifications, and reports for Newport Avenue extension north of Edinger Avenue. 2. Approved plans for all traffic improvements noted in EIR and necessary to mitigate traffic impacts of Phase II square footages for which certificates of occupancy will be issued, including: 	
<u>Within City of Tustin</u>	
<p>Red Hill at Walnut and Sycamore¹ Remove parking and stripe northbound and southbound through lanes</p> <p>Del Amo and Edinger² Add second northbound left-turn lane</p> <p>Red Hill and Valencia² Add second northbound left-turn lane</p>	
<u>Within City of Santa Ana</u>	
<p>SR-55 SB ramp and Edinger^{1, 3} Add third eastbound through lane Add third westbound through lane</p> <p>Edinger intersections in Santa Ana¹ (at Ritchey, Lyon and Grand) Participate in improvements through TSIA fees³</p>	<p>Permits for 500,000 square feet of office, 50,000 square feet of commercial</p>

Table 3 (cont'd)

<u>Improvement or Action Needed</u>	<u>Building Permits or Certificates of Occupancy May Be Issued For</u>
<u>Within Project Area</u>	
B. Plan Approvals or Completion of Construction	
1. Completion of all improvements as shown in IIA.2.	
2. Completion of improvements shown in IB.2.	
	Certificate of occupancy for permits authorized to be issued in Phase II

PHASE III

<u>Improvement or Action Needed</u>	<u>Building Permits or Certificates of Occupancy May Be Issued For</u>
A. Required Plan Submittals or Approvals	
1. Approved plans for additional traffic improvements needed to mitigate impacts of Phase III square footages for which certificates of occupancy will be issued, including:	
<u>Within City of Tustin</u>	
Newport and Walnut ¹	
Add third northbound through lane	
Add third southbound through lane	
Add eastbound right-turn lane	
Add second southbound left-turn lane	
Newport and Sycamore ²	
Add westbound left-turn lane	
Add second eastbound through lane	
Add second eastbound left-turn lane	
Add second northbound left-turn lane	
Add third northbound through lane	
Add third southbound through lane	
Red Hill and Valencia ²	
Add southbound right-turn lane	
Add westbound right-turn lane	
Add eastbound free right-turn lane	

Table 3 (cont'd)

<u>Improvement or Action Needed</u>	<u>Building Permits or Certificates of Occupancy May Be Issued For</u>
<u>Within City of Santa Ana</u>	
Edinger intersections in Santa Ana ^{1, 3} (at Ritchey, Lyon and Grand) Participate in improvements through TSIA fees ³	
<u>Within Cities of Tustin and Santa Ana</u>	
Red Hill and Warner ² Add southbound right-turn lane	
Red Hill and Dyer ¹ Participate in improvements through TSIA fees ³	
2. Approval of plans and issuance of construction permits for Newport Avenue extension north of Edinger Avenue including approvals from Orange County Environmental Management Agency and AT&SF railroad.	Permits for 300,000 square feet of office and 10,000 square feet of commercial
3. Approved plans for all traffic improvements noted in EIR necessary to mitigate traffic impacts of remaining specific plan buildout square footage, as listed in III A.1.	Permits for remaining buildout square footages authorized by the specific plan
B. Completion of Construction	
1. Completion of improvements shown in III A.1 and III A.2.	Certificate of occupancy for permits authorized to be issued in Phase III, including buildout of the specific plan

1 Project contribution to this improvement.
 2 Project mitigation improvement.
 3 Improvements to Edinger Avenue in proximity to the SR-55 overpass are linked to the design of Newport Avenue extension north of Edinger Avenue. Other improvements along Edinger in the City of Santa Ana actual improvement design and timing for completion of these improvements will be subject to approval by the City of Santa Ana. However, since the project is contributing to areawide improvements, any discretion on design or timing as determined by Santa Ana will not affect issuance of entitlements on the Pacific Center East project in Tustin. Since this section of roadway is part of the CMP network, improvements will be designed for this location so that adequate levels of service will be attained to meet CMP eligibility requirements.

4.0 LAND USE AND DEVELOPMENT REGULATIONS

4.1 Introduction

The Land Use and Development Regulation section acts as a principal part of the controlling mechanism for implementation of the Pacific Center East Specific Plan. The standards set forth in this section will ensure that future development within the specific plan area proceeds in a consistent and appropriate manner. Future review of subdivisions and site plans by the City of Tustin will provide assurance that these standards are realized.

Land use and development standards are provided for four separate land use designations. Land use designations and planning areas are shown on Exhibit 17. The square footage allocation and floor area ratios identified in Table 4 for each planning area shall govern the overall maximum intensity and scale of development within each land use designation or planning area without the necessity of additional or new environmental documentation. However, the square footage for each specific use may be exchanged (eg., commercial versus office) within an individual planning area. These estimates reflect the proposed circulation system, urban design concept and the recommended site development standards. However, all acreage data is approximate and is based on concept plans only; no specific entitlement may be based on these acreages and future entitlements will be based on final maps.

4.2 Commercial Center

A. Purpose and Intent

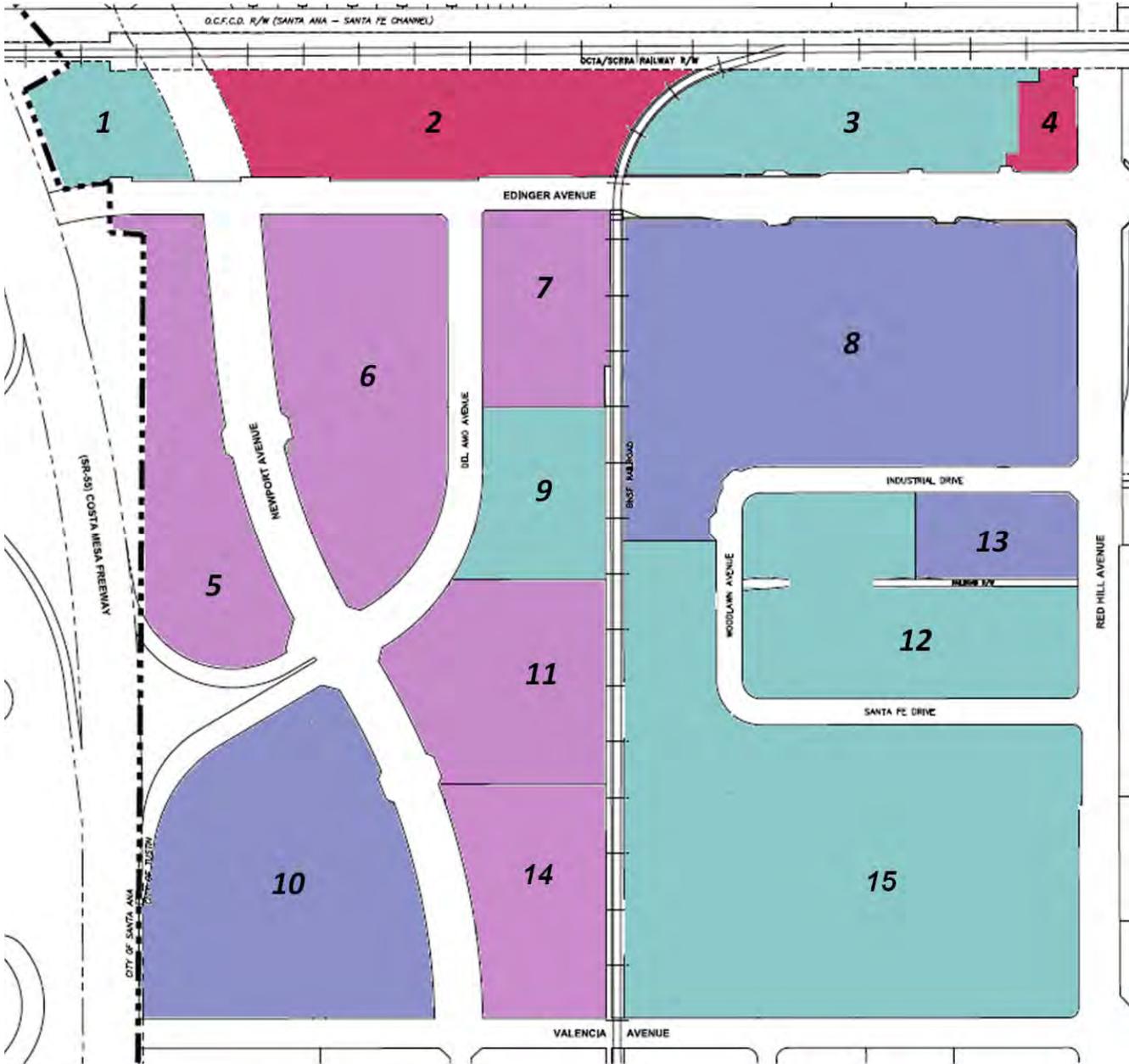
This land use designation is intended for development of commercial uses which complement surrounding, existing and planned land uses. Regulations provide an appropriate amount of flexibility to anticipate future conditions and to integrate properly a mixture of commercial uses.

B. Location

Planning areas designated as Commercial Center in the Land Use Plan are Planning Areas 2 and 4.

C. Permitted Uses and Conditionally Permitted Uses

The following uses shall be permitted by right where the symbol "P" occurs or by conditional use permit where the symbol "C" appears, subject to any identified limitations or uses contained herein or in Section 4.8, Table 6.



*Land Use Plan/
Planning Areas*
Pacific Center East
City of Tustin



Exhibit 17

- Commercial Center
- Regional Center
- Office Center
- Technology Center
- 8 Planning Area

Table 4
MAXIMUM SQUARE FOOTAGE AUTHORIZED IN THE SPECIFIC PLAN
(See Table 6 for Use Limitations for Individual Planning Area)

Center Planning Area*	Net Acres**	Floor Area Ratio	250+ Room Hotel	Commercial	Office	R&D/Light Industrial	TOTAL AUTHORIZED SQUARE FOOTAGE
Commercial Center							
Planning Area 2	7.8	0.35		119,000			119,000
Planning Area 4	1.1	0.13		6,200			6,200
Regional Center							
Planning Area 5	6.2	0.86	195,000	7,000	30,000		232,000
Planning Area 6	12.0	0.80		75,000	326,466		401,466
Planning Area 7	4.0	0.65			113,300		113,300
Planning Area 11	5.8	0.65		8,000	132,000		140,000
Planning Area 14	5.2	0.40			119,929		119,929
Office Center							
Planning Area 8	21.2	0.50		7,000	454,700		461,700
Planning Area 10	13.1	1.50		85,595	770,359		855,954
Planning Area 13	2.4	0.48			50,200		50,200
Technology Center							
Planning Area 1	1.0	0.40				17,400	17,400
Planning Area 3	6.1	0.40		40,920		63,380	106,300
Planning Area 9	3.7	0.40				64,500	64,500
Planning Area 12	8.6	0.40		4,494		145,306	149,800
Planning Area 15	24.8	0.40		12,963		419,137	432,100
TOTAL SQUARE FOOTAGE							
	123.0	0.61	195,000	366,172	1,996,954	711,723	3,269,849

* Footnotes for each planning area listing options available for development included in the specific plan are listed below:

PA 2 – Office use is permitted but shall not exceed 10 percent of the gross floor area for buildout of each parcel and for the planning area. A minimum of 70 percent of the gross floor area of any development within PA 2 shall be retail-generating uses.

PA 5, 6, or 7 – There shall be a minimum of 250 hotel rooms.

PA 5 – 7 – A minimum of 75,000 sf of retail commercial use is required. The general locations shall be delineated during development processing and planning area conceptual reviews.

PA 6 – Office use is limited to 60 percent of the total gross square footage if PA 6 includes the hotel and 85 percent if there is not a hotel in PA 6.

PA 11 – A minimum of 8,000 sf of retail commercial use (e.g. freestanding restaurant) shall be developed.

PA 8 – Retail and service commercial use shall not exceed 3 percent of the gross floor area, as calculated for buildout of each parcel. In any event, the maximum retail and service commercial use in PA 8 shall not exceed 7,000 sf.

PA 10 – Retail and service commercial use within the planning area shall range from a minimum of 5 percent but not exceed a maximum of 10 percent of the gross floor area, as calculated for buildout of the planning area.

Technology Center – Office use for new uses or alterations to existing development which creates new floor area shall not exceed 50 percent of the gross floor area in buildings of each parcel. Accessory commercial use for new uses or alterations to existing development which creates new floor area shall not exceed 3 percent of the gross floor area of each parcel. Existing entitlements within PA 3 for the Resco properly allow 36,000 sf of retail commercial use.

All Planning Areas – Consistent with authorized uses within each planning area by each land use designation, exchanges between permitted uses (e.g. commercial versus office) may be permitted between Planning Areas subject to the Use Limitations contained in Table 6, provided such exchanges are approved by Public Works and Community Development and are subject to review of the total trips generated for the planning area.

** All acreage data is approximate and is based on the land use plan and urban design concept plan. Actual net acreages will be refined during the Planning Area concept Plan and Design Review process.

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Pacific Center East Specific Plan

- 1. Commercial Uses
 - a. Retail businesses conducted within a building including:
 - alcoholic beverage sales subject to the following criteria: C
 - offsite sales establishments shall be located a minimum distance of 100 feet from any residentially zoned or used properties, 300 feet from any other existing offsite sales establishment, 600 feet from any church, public or private school, playground or hospital.
 - antique shops P
 - apparel stores P
 - appliance and hardware stores P
 - automobile parts and supplies C
 - automobile service station C
 - bakeries, retail only P
 - books, gifts and stationary P
 - coffee houses/cafes P
 - convenience markets C
 - delicatessen/cafeterias P
 - department stores P
 - drug stores, pharmacies P
 - florists P
 - furniture stores P
 - general retail stores P
 - hobby stores P

household goods and services	P
jewelry stores	P
office supplies and equipment	P
pet stores, pet supplies, pet grooming	C
photography gallery	P
restaurants, family and specialty	P
restaurants, fast food without drive-through	C
restaurant with drive-through service	C
service stations	C
shoe stores	P
sporting goods	P
supermarkets, grocery stores	P
theaters	C
tire sales and service	C
variety stores	P
 b. Service businesses including retail sales incidental to subject to use limitations contained in Section 4.8, Table 6:	
banks and financial institutions	P
barber, beauty salons	P
coin-operated cleaning or laundromat	C
cocktail lounges and bars (when not an integral part of a restaurant)	C
daycare/nursery schools	C
laundry and dry cleaners	C

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locksmith	P
print shop	P
tailor shop	P
telephone answering service	P
training/technical schools	C
travel agencies	P

2. Office Uses

Office uses are permitted, provided that they shall be limited to ten percent of gross floor area for individual projects developed in this land use category.

a. Professional offices, exemplified by the following, including retail sales incidental thereto:

accountants	P
architects	P
attorneys	P
chiropractors	P
dentists	P
doctors and dental offices, clinics and related laboratory facilities	P
engineers	P
optometrist	P
other similar professional, licensed by the State of California	P

b. General offices, exemplified by the following:

advertising agencies	P
----------------------	---

contractors and building consultants, not including the parking of commercial vehicles or equipment	P
escrow companies	P
insurance companies	P
interior decorator	P
public utility offices	P
real estate sales office	P

D. Temporary Uses

Temporary uses shall be regulated pursuant to the applicable section of the Tustin City Code.

E. Unlisted Uses

Those uses not specifically listed are subject to a determination by the Community Development Director as either permitted, subject to a Conditional Use Permit or prohibited consistent with the purpose of the specific plan and this land use category. Decisions of the Director are appealable to the Planning Commission.

F. Site Development Standards

1. Minimum building site - 5,000 square feet with a minimum lot width of 100 feet.
2. Maximum building height limit with screening - 35 feet pursuant to any limitations contained in Section 4.8, Section A and Section C.
3. Minimum building setbacks¹
 - a. Front and streetside and rear yard setbacks adjacent to a street - 25 feet, which can be reduced to 15 feet for parking only
 - b. Interior side yards between parcels - 5 feet
 - c. Rear yard or interior side yard (adjacent to railroad) - 10 feet

¹ Building setbacks are measured from property lines or future rights-of-way. Landscape setbacks are a combination of parkway, sidewalk and planting areas and are measured from curb.

- d. Minimum space between buildings – 10 feet
4. Landscape setbacks¹
 - a. Newport Avenue – 30 feet
 - b. Edinger Avenue – 20 feet
 - c. Red Hill Avenue – 30 feet
5. Maximum floor area ratio – Planning Area 2 = 0.35, Planning Area 4 = 0.13
6. Landscaping – compliance with City of Tustin landscaping and irrigation requirements and Section 3.5 of this specific plan.
7. Parking – compliance with off-street parking standards contained in Section 4.6.
8. Loading – compliance with loading standards contained in Section 4.7.
9. General regulations – refer to Section 4.8 for other compliance general regulations.

4.3 Regional Center

A. Purpose and Intent

This land use designation is intended for the development of a mixed use commercial and office center, with opportunities for both office and commercial uses whose focus is beyond services for the immediate project area. Regulations integrate properly a mixture of hotel, office and commercial uses. Principal land uses for the Regional Center are limited to office, hotel and commercial uses with certain limitations noted in this Section and in Section 4.8.

B. Location

Planning areas designated in the Land Use Plan as Regional Center are Planning Areas 5, 6, 7, 11 and 14.

C. Permitted Uses and Conditionally Permitted Uses

The following uses shall be permitted by right where the symbol “P” occurs or by conditional use permit where the symbol “C” appears, subject to any identified limitations contained herein or in Section 4.8.

1. Commercial uses

Subject to use limitations contained in Section 4.8, Table 6.

 - a. Retail businesses – all provisions applicable to the Commercial Center designation P/C
2. Service businesses

All use provisions applicable to the Commercial Center designation P/C

 - a. Hotels (a minimum of 250 hotel rooms) P
 - b. Racquetball/health clubs C
 - c. Movie theaters C
3. Office Uses

Subject to use limitations contained in Section 4.8, Table 6.

All office uses permitted in the Commercial Center designation P

D. Temporary Uses

Temporary uses shall be regulated pursuant to the applicable section of the Tustin City Code.

E. Unlisted Uses

Those uses not specifically listed and subject to a determination by the Community Development Director as either permitted, permitted subject to Conditional Use Permit or prohibited consistent with the purpose of the specific plan and this land use category. Decisions of the Community Development Director are appealable to the Planning Commission.

F. Site Development Standards

1. Minimum site building area – 20,000 square feet with a minimum lot width of 100 feet except for retail pad parcels which may have a minimum lot size of 10,000 square feet and no minimum lot width requirement.
2. Maximum building height limit with screening – If located in the southern portion of Planning Area 6, 120 feet or eight stories for a hotel otherwise, the

maximum height for a hotel in other locations is 100 feet; 35 feet for freestanding commercial uses and 65 feet for office uses pursuant to any limitation contained in Section 4.8.

3. Minimum building setbacks²
 - a. Edinger Avenue – 35 feet
 - b. Newport Avenue – 35 feet
 - c. Del Amo Avenue – 20 feet
 - d. All other building setbacks and distance between buildings shall be as required by the City adopted California Building Code.
4. Landscape setbacks²
 - a. Edinger Avenue – 30 feet
 - b. Newport Avenue – 20 feet
 - c. Del Amo Avenue – 25 feet
 - d. All areas not containing approved buildings, hardscape improvements, or parking shall be landscaped.
5. Maximum floor area ratio – Planning Area 5 = 0.45, Planning Area 6 = 1.03, Planning Areas 7 and 11 = 0.65 and Planning area 14 = 0.40.
6. Landscaping – compliance with City of Tustin Landscaping and irrigation requirements and Section 3.5 of this specific plan.

² Building setbacks are measured from property lines of future rights-of-way. Landscape setbacks are a combination of parkway, sidewalk and planting areas and are measured from curb

7. Parking – compliance with off-street parking standards contained in Section 4.6.
8. Loading – compliance with loading standards contained in Section 4.7.
9. General regulations – refer to Section 4.8 for other compliance general regulations.

4.4 Office Center

A. Purpose and Intent

This land use designation is intended for the development of corporate, professional and general offices with limited supporting commercial uses (see Table 6). Regulations provide an appropriate character which complements surrounding, existing and planned uses.

Principal land uses for the office center are primarily corporate and professional offices with certain limitations noted in this Section and in Section 4.8. It is intended that commercial retail and service uses shall be well-integrated components of office uses and serve as complementary support services.

B. Location

Planning areas designated in the Land Use Plan as Office Center are Planning Areas 8, 10, 13.

C. Permitted Uses and Conditionally Permitted Uses

The following uses shall be permitted by right where the symbol “P” occurs or by conditional use permit where the symbol “C” appears, subject to any identified limitations contained herein or in Section 4.8, Table 6.

1. Offices permitted or conditionally permitted in the Commercial Center designation plus corporate offices P
2. Retail service uses permitted or conditionally permitted in the Commercial Center designation P/C
3. Restaurants, family or specialty, only in Planning Areas 8 and 10 P

D. Temporary Uses

Temporary uses shall be regulated pursuant to the applicable section of the Tustin City Code.

E. Unlisted Uses

Those uses not specifically listed and subject to a determination by the Community Development Director as either permitted, permitted subject to Conditional Use Permit or prohibited consistent with the purpose of the specific plan and this land use category. Decisions of the Community Development Director are appealable to the Planning Commission.

F. Site Development Standards

1. Minimum site building area - 20,000 square feet with a minimum lot width of 100 feet.
2. Maximum building height limit with screening - 50 feet or three stories for Planning Area 13, 50 feet or three stories for Planning Area 8, and 9 stories or 140 feet for Planning Area 10 provided that up to two stories or 40 feet of additional height may be permitted if certain additional amenities are provided subject to approval by the Planning Commission in conjunction with Design Review. The maximum height figure includes screening materials. Specific recommendations for the amenity incentive are:
 - a. Inclusion of water elements in public plazas
 - b. Extra monetary contributions toward child care facilities and programs
 - c. Funding for commissioning of art in public places
 - d. Extra monetary contributions toward vanpool and ridesharing programs
3. Minimum building setbacks³
 - a. Front and street side and rear yards - 30 feet, which may be reduced to 15 feet for parking areas along Industrial, Woodlawn or Valencia.
 - b. Interior side yards between parcels - 5 feet
 - c. Rear yard or interior side yard (adjacent to railroad) - 10 feet
 - d. Minimum space between the proposed buildings - 10 feet plus 1 additional foot for each two feet of the proposed building height from the closest adjacent buildings

³ Building setbacks are measured from property lines or future rights-of-way. Landscape setbacks are a combination of parkway, sidewalk and planting areas and are measured from curb.

- e. SR-55 freeway and interchange ramps - 35 feet
 - f. Any portion of a building which exceeds 35 feet in height shall be set back from the lot line no less than the minimum plus one foot for each four feet by which the individual building height exceeds 35 feet. Setbacks are to property lines or rights-of-way.
4. Landscape setbacks³
 - a. Edinger Avenue - 30 feet
 - b. Newport Avenue - 30 feet
 - c. Red Hill Avenue - 30 feet
 - d. SR-55 - 20 feet
 5. Maximum floor area ratio - Planning Area 10 = 1.50, Planning Area 8 and 13 = 0.60
 6. Landscaping - compliance with City of Tustin landscaping and irrigation requirements and Section 3.5 of this specific plan.
 7. Parking - compliance with off-street parking standards contained in Section 4.6.
 8. Loading - compliance with loading standards contained in Section 4.7.
 9. General regulations - refer to Section 4.8 for other compliance general regulations.

4.5 Technology Center

A. Purpose and Intent

This land use regulations section is intended for the development of a general research and development, industrial and high technology center, with opportunities for limited ancillary commercial and office uses. Principal land uses for the technology center are limited primarily to research and development and industrial uses subject to certain limitations noted in this Section and in Section 4.8.

B. Location

Planning areas designated in the Land Use Plan as Technology Center are Planning Areas 1, 3, 9, 12 and 15.

C. Permitted Uses and Conditionally Permitted Uses

The following uses shall be permitted by right where the symbol "P" occurs or by conditional use permit where the symbol "C" appears subject to any identified limitations contained in Section 4.8, Table 6:

- | | | |
|----|---|-----|
| 1. | Research and development (including laboratories, product development and manufacturing) | P |
| 2. | Light industrial (including manufacturing, assembly and distribution) | P |
| 3. | Accessory office uses permitted or conditionally permitted or conditionally permitted in the Commercial Center designation ⁴ | P/C |
| 4. | Accessory commercial uses permitted or conditionally permitted in the Commercial Center designation ⁴ | P/C |
| 5. | Day care/nursey schools | C |

D. Temporary Uses

Temporary uses shall be regulated pursuant to the applicable section of the Tustin City Code.

E. Unlisted Uses

Those uses not specifically listed and subject to a determination by the Community Development Director as either permitted, permitted subject to Conditional Use Permit or prohibited consistent with the purpose of the specific plan and this land use category. Decisions of the Community Development Director are appealable to the Planning Commission.

F. Site Development Standards

1. Minimum site building area - 20,000 square feet with a minimum lot width of 100 feet.
2. Maximum building height limit with screening - 50 feet subject to limitations in Section 4.8.

⁴ Accessory uses are incidental to that of the main use of the facility or structure.

3. Minimum building setbacks⁵
 - a. Front and street side and rear yards - 30 feet
 - b. Interior side yards between parcels - 5 feet
 - c. Rear yard or interior side yard (adjacent to railroad) - 10 feet
 - d. Minimum space between the proposed buildings - 10 feet plus 1 additional foot for each two feet of the proposed building height from the closest adjacent building

4. Landscape setbacks⁵
 - a. Edinger Avenue - 30 feet
 - b. Newport Avenue - 30 feet
 - c. Del Amo Avenue - 30 feet
 - d. Valencia Avenue, Industrial, Woodlawn, Santa Fe - 25 feet
 - e. SR-55 - 20 feet

5. Maximum floor area ratio = 0.40

6. Landscaping - compliance with City of Tustin landscaping and irrigation requirements and Section 3.5 of this specific plan.

7. Parking - compliance with off-street parking standards contained in Section 4.6.

8. Loading - compliance with loading standards contained in Section 4.7.

9. General regulations - refer to Section 4.8 for other compliance general regulations.

4.6 Off-Street Parking Requirements

A. Basic Regulations

1. New Uses - Off-street parking shall be provided for any new building

⁵ Building setbacks are measured from property lines or future rights-of-way. Landscape setbacks are a combination of parkway, sidewalk and planting areas and are measured from curb.

constructed and for any new use established; for any addition or enlargement of an existing building or use; and for any change in the occupancy of any building or the manner in which any use is conducted that would result in additional parking spaces being required.

2. Existing Buildings and Uses - For additions or enlargements of existing buildings or uses, or any change of occupancy or manner of operation that would increase the number of parking spaces required, the additional parking would be required only for the proposed change and not for the previous existing use or building unless as required as a condition of approval of a conditional use permit.
3. Joint Use of Parking Areas/Transportation Management - Parking required by this Section for any building or use shall not be considered as providing parking for any other building or use, except where a joint parking facility has been authorized by the Planning Commission.

a. Shared Parking

Parking facilities may be used jointly for uses with significantly different peak hours of operation. Requests for shared parking must meet the following requirements:

- 1) A parking study shall be submitted by the applicant demonstrating that no substantial conflict will exist in the peak hours of parking demand for the uses for which joint use is proposed. The methodology to be utilized in preparing the study shall be the Urban Land Institute's shared parking study.
- 2) The number of parking stalls which may be credited against the requirements for the structures or uses involved shall not exceed the number of stalls reasonably anticipated to be available during differing hours of operation.
- 3) Parking spaces designated for joint use shall be located so that they will adequately serve the use it is intended.
- 4) A written agreement shall be drawn to the satisfaction of the City Attorney and the Director of Community Development and executed by all parties concerned assuring the continued availability of the number of stalls designated for joint use.

b. Transportation Management Plans

Transportation management plans shall be prepared where required

as environmental mitigation or by the Air Quality Management District. Where required, the plan and monitoring data shall be submitted to the Community Development Department for review and approval.

4. Maintenance - All required off-street parking spaces (Table 5) shall be available permanently and marked and maintained permanently for parking purposes. Off-street parking areas shall not be used for the sale, display or repair of motor vehicles or the storage of materials or other goods and services. Any repair or restriping of parking areas shall not change the configuration, size or location of parking spaces and landscaping unless approved by the Community Development Department.

Table 5
PARKING STANDARDS

Use	Required ⁶ Parking Spaces
Appliance, hardware store	1 space per 500 square feet of GFA
Service stations	2 spaces plus four for each service bay
Banks, savings and loans, etc.	1 space per 250 square feet of GFA with a 6-space minimum
Barber, beauty salons	1 space per 200 square feet of GFA
Business supply and support services	1 space per 250 square feet of GFA
Cafeterias	1 space per 75 square feet of GFA
Cocktail lounges and bars	1 space for each 3 seats
Coffee houses and cafes	1 space for each 3 seats
Commercial, ancillary	1 space per 250 square feet of GFA
Dance floor	1 space per 7 square feet of dance floor area
Daycare center, preschools	1 space per employee and 1 space per five students
Delicatessens	1 space per 200 square feet of GFA
Dry cleaners	1 space per 250 square feet of GFA

⁶ GFA = gross floor area

Table 5 (cont'd)

PARKING STANDARDS

Use	Required ⁷ Parking Spaces
Health clubs and spas	1 space per 100 square feet of GFA (swimming pools shall be included in square footage)
Racquetball/handball	1.5 spaces for each court plus spaces for additional uses onsite and for employees on maximum shift
Home improvement/furnishings	1 space per 250 square feet of GFA
Hotels	1 space per unit plus accommodations for resident manager and space for other uses onsite
Laundromats	1 space per 3 machines or 1 space per 250 square feet of GFA, whichever is greater
Manufacturing, assembly	1 space per 500 square feet of GFA but not less than 2 spaces per 3 employees. If there is more than 1 shift, the number of employees on the largest shift shall be used.
Medical clinics or offices, dental clinics or offices	1 space per 150 square feet of GFA
Offices, professional and corporate other than medical or dental	1 space per 250 square feet of GFA for buildings
Research and development	1 space per 250 square feet of GFA for all office use and 1 space per 500 square feet of GFA for manufacture or assembly (but not less than 2 spaces per 3 employees on maximum shift)
Restaurants, fast food	1 space per 100 square feet of GFA plus minimum stacking area of seven (7) cars where a service window is located
Restaurants with drive-through	1 space per 100 square feet of GFA plus minimum stacking area of seven (7) cars where a service window is located

⁷ GFA = gross floor area

Table 5 (cont'd)

PARKING STANDARDS

Use	Required ⁸ Parking Spaces
Retail businesses except as specified otherwise	1 space per 200 square feet of GFA
Service businesses, general unless otherwise specified	1 space per 250 square feet of GFA
Supermarkets, grocery stores, convenience stores	1 space per 200 square feet of GFA
Theaters, movies	1 space per 3 seats plus 5 spaces for employees
Tire sales and service	1 space per 250 square feet of GFA of sales area and 4 spaces per service bay
Training, technical schools	10 spaces per 1,000 square feet of GFA
Warehouse, storage	1 space per 1,000 square feet of GFA up to 20,000 square feet. 1 space per 2,000 square feet of GFA for second 20,000 square feet. 1 space per 4,000 square feet in excess of 40,000 square feet plus space for other uses onsite.
Wholesale establishments and warehouses not used exclusively for storage	1 space per 1,000 square feet of GFA; less that area devoted to office or sales which shall be calculated at 1 space per 250 square feet of GFA
Other light industrial use	1 space per 500 square feet of GFA

⁸ GFA = gross floor area

B. Parking Spaces Required

1. All uses shall provide off-street parking in accordance with Table 5. The requirement for any use not listed specifically shall be determined by the Community Development Director on the basis of the requirements for similar uses. The number of handicap spaces shall be as required by the Uniform Building Code and state handicap requirements.
2. Fraction of Parking Spaces - Where the application of the off-street parking requirements results in a fractional number of spaces required then the fraction shall be rounded to the next higher whole number.
3. Location of Parking Spaces - All required off-street parking spaces shall be located on the same lot as the use to be served unless otherwise approved by the Director of Community Development Department. If required parking is approved offsite, the following requirements shall be met:
 - a. The offsite parking shall be located so that it will adequately serve the use for which it is intended. In making this determination the following factors shall be considered:
 - 1) Proximity of the offsite parking facilities.
 - 2) Ease of pedestrian access to the offsite parking facilities.
 - 3) The type of use the offsite parking facilities are intended to serve, ie., offsite parking may not be appropriate for high turnover uses such as retail.
 - b. A written agreement shall be drawn to the satisfaction of the City Attorney and the Community Development Director and executed by all parties concerned assuring the continued reservation and availability of the offsite parking facilities for the use they are intended to serve.
4. Restricting the Use of Parking Spaces - All required parking spaces shall be made available permanently for automobile parking not only for employees working at the premises for which parking is required but also for customers and guests having lawful reason to be at the premises for which such parking is required. In the absence of prior approval from the Community Development Department, it is unlawful for any owner, lessee, tenant or any person having control of the operation of any premises for which parking is required to prevent, prohibit or restrict authorized persons from using parking.
5. Gates which limit or control access to parking areas require special consideration as their design can significantly impact the parking demand and usage

and the vehicular circulation on a site. Gates shall require approval through the Design Review process. In instances where gates are being requested for a project that is subject to the review and approval of the Planning Commission, such request for gates shall be heard by the Planning Commission. The applicant shall submit to the city a study addressing the following issues:

- a. Type and operation of the gate.
 - b. Adequacy of vehicle stacking area at gate.
 - c. Effect of gate on parking usage and distribution onsite.
 - d. Effect of gate on parking for surrounding or adjacent areas.
6. All handicapped parking spaces shall be located as close as is practical to the entrance(s) to the use they are intended to serve, and oriented so that a user of the handicapped parking space does not have to go past the rear of automotive parking spaces in order to reach the building's main entrance.
7. Parking area dimensions, location, aisle width, access, etc. shall conform to the city design review criteria on file in the Community Development Department.

4.7 Off-Street Loading

All buildings containing commercial, office, research and development, or industrial use types, shall be provided with loading spaces⁹ as follows:

<u>Total Square Footage/Gross Floor Area</u>	<u>Number of Loading Spaces Required</u>
1. <u>Research and Development/Industrial Uses</u>	
5,000 to 19,999 square feet	1
20,000 to 39,999 square feet	2
40,000 to 79,999 square feet	3
80,000 square feet and over	4 plus 1 space for each additional 50,000 square feet

⁹ An off-street space or berth used for the loading or unloading of commercial vehicles.

Pacific Center East Specific Plan

2. Office Buildings

Less than 50,000 square feet	1
50,000 to 100,000 square feet	2
100,000 square feet and over	3

3. Other Permitted Uses

5,000 to 15,000 square feet	1
15,000 to 45,000 square feet	2
45,000 square feet and over	3

4. Loading spaces shall have safe and adequate means of ingress and egress for trucks to and from a public street or alley.

5. The size, location, and design treatment of loading spaces shall be subject to design review approval.

4.8 General Regulations

A. Additional Building Height Limitations

Building height shall not exceed the current regulations for imaginary surfaces, as defined within the Air Installation Compatibility Use Zone Study for MCAS(H) Santa Ana and the current land use regulations for John Wayne Airport, as defined in the approach and transitional surfaces of the Approach and Clear Zone Plan. If conflicts occur between the stated building/structural height limit in the development standards and MCAS(H) or John Wayne Airport regulations, the controlling regulation shall be the airport/helicopter land use regulations. The maximum height allowed in Planning Area 10 consistent with JWA regulations is approximately 300 feet. Construction shall require review by the Airport Land Use Commission, as applicable.

B. Limitations On Uses

1. All uses shall be conducted within a completely enclosed building, except for off-street parking, loading, temporary uses and any outdoor dining space in conjunction with cafes or restaurants where permitted. In the Technology Center, minor ancillary activities¹⁰ associated with the uses permitted may be located outside a building provided solid screening is provided and there are no negative noise impacts on adjacent properties.

¹⁰ Ancillary activities include parking of service vehicles, temporary material storage for short durations (2-3 days) and temporary assembly activities for short durations (2-3 days).

2. No overnight parking or vehicles other than those used in conjunction with a permitted use.
3. Storage shall be permitted only within an entirely enclosed structure, and shall be limited to accessory storage of commodities sold or utilized in the conduct of a permitted use on the premises.
4. Table 6 lists specific limitations for each land use designation, planning area and use.

C. Mechanical Equipment

Compressors, air conditioning units or similar mechanical equipment, located outside the exterior walls of any building shall comply with the following:

1. All such mechanical equipment shall be screened from view from any abutting street or highway. Location and architectural design of the enclosure shall be subject to design review. The screen shall be considered as an element of the overall design of the project, shall blend with the architectural design of the buildings, and is included in the maximum height limitation for the building.
2. All such mechanical equipment shall be maintained in a clean and proper condition to prevent a collection of litter and filth and to avoid the emission of unnecessary noise, dust or fumes.

D. Fences and Walls

No walls are required, except for those that would be imposed based upon the review of grading plans for a building permit. Fences, hedges and walls, not greater than six (6) feet, eight (8) inches in height shall be permitted on or within all rear and side property lines on interior lots and on or to the rear of all front yard setback lines. No fence, wall or hedge over three (3) feet in height shall be permitted in any required front yard, or in the required side yard on the street side of a front yard or in the required side yard on the street side of a reverse corner lot. All provisions of the Tustin City Code shall also apply to the construction of walls, fences and hedges.

E. Refuse Collection Areas

All outdoor refuse collection shall be screened visually from streets and highways. Collection areas shall be shielded from view within a building or enclosed by a wall not less than six feet in height. Said enclosures shall be designed consistent and to complement main building materials. The city, during design review, shall require accommodation of adequate facilities to encourage refuse recycling, including interior storage or source-separated materials and innovative designs inside and outside of buildings to make recycling more convenient and accessible.

Table 6

USE LIMITATIONS

Land Plan Designation	Planning Area	Designations
Regional Center	5,6,7	There shall be a hotel use of a minimum of 250 rooms located in PA 5, 6 or 7.
	5,6,7	A minimum of 75,000 total square feet of retail commercial use is required within PA 5-7. The general location shall be delineated during development processing and planning area conceptual reviews.
	6	Office use within PA 6 is limited to 60 percent of the total gross square footage if PA 6 includes a hotel and 85 percent if there is no hotel use in PA 6.
	11	A minimum of 8,000 square feet of retail commercial use (e.g., freestanding restaurant) shall be developed in PA 11.
Office Center	8	Retail and service commercial uses shall not exceed 3 percent of the gross floor area as calculated for buildout of each parcel. In any event, the maximum retail and service commercial use in PA 8 shall not exceed 7,000 square feet.
	10	Retail and service commercial use within the planning area shall range from a minimum of five percent but not exceed a maximum of ten percent of the gross floor area, as calculated for buildout of the planning area.
Commercial Center	2	Office uses shall not exceed 10 percent of the gross floor area for buildout of each parcel and for the planning area.
		A minimum of 70 percent of the gross floor area of any development within PA 2 shall be retail-generating uses.

Table 6 (cont'd)

USE LIMITATIONS

Land Plan Designation	Planning Area	Designations
Technology Center ¹	1, 3, 9, 12, 15	Office use for new uses or alterations to existing development which creates new floor area shall not exceed 50 percent of the gross floor area in buildings on each parcel. Accessory commercial use for new uses or alterations to existing development which creates new floor area shall not exceed 3 percent of the gross floor area of each parcel. ²
All Planning Areas		Consistent with the authorized uses within each planning area by Land Use Designations in Table 4, exchanges of square footage between permitted uses (e.g., commercial versus office) may be approved subject to the Use Limitations herein in Table 6, provided that such exchanges are approved by Public Works and Community Development and are subject to review of the total trip generations for the planning area.

¹ In Planning Area 3, existing entitlements on the Resco property permit up to 36,000 square feet of retail commercial use.

² Accessory uses are incidental to that of the main use of the facility or structure.

F. Landscaping and Screening

1. Screening shall be installed along all parking areas abutting streets. Except as otherwise provided below, the screening shall have a total height of not more than thirty-six (36) inches.

Where the finished elevation of the property at the boundary line, or within five (5) feet inside the boundary line, is lower than an abutting property elevation, such change in elevation may be used in lieu of, or in combination with, additional screening to satisfy the screening requirements of this section.

A screen as referred to above shall consist of one or any combination of the following, as reviewed and approved by the Community Development Department during design review.

- a. Walls, including retaining walls: A wall shall consist of concrete stone, brick, tile or similar type of masonry material.
 - b. Berms: A berm shall be constructed of earthen materials and it shall be landscaped.
 - c. Landscaping: Vegetation, consisting of evergreen or deciduous trees or shrubs shall be employed. Appropriate species are listed in Section 3.5 of this specific plan.
2. An opaque screen consisting of plant material and masonry walls not to exceed a maximum of six feet, eight inches in height shall be installed along site boundaries where adjacent to residential areas and railroad (north of Edinger Avenue only).
 3. All landscaping installation shall conform with the City of Tustin landscape guidelines and submittal requirements. Building setbacks are a combination of parkway, sidewalk and planting areas and are measured from curb.

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5.0 IMPLEMENTATION/ADMINISTRATION

5.1 Responsibility

The Department of Community Development shall be responsible for the administration and enforcement of provisions of this specific plan.

5.2 Interpretation

If ambiguity arises concerning the appropriate application of provisions contained in this specific plan, the Director of Community Development shall make the appropriate determination. In making these determinations, the Director shall consider the following, but not by way of limitation:

1. Prior administrative interpretation of similar provisions;
2. General intent and purpose of specific plan and land use designation;
3. Provisions contained in the general plan.

Any decision of the Director may be appealed to the Planning Commission.

5.3 Development Processing

The Land Use Plan (Exhibit 4) shall generally guide location of the land use areas within the Pacific Center East Specific Plan. The following development processes shall be followed in proceeding with implementation of the specific plan.

Planning Areas Concept Plan Review

Prior to or concurrent with the submission of an initial subdivision map or development project within Planning Areas 1, 2, 5, 6, 7, 9, 10, 11, conceptual plans for one or more planning areas shall be submitted for Planning Commission approval. This allows the city to assess the overall concept proposed for each planning area for its conformity with the specific plan and to utilize the approved concept plans in review of subsequent increments of development. Any change in the intent or conditions imposed on a concept plan require resubmission for Planning Commission approval. Information to be submitted shall include:

1. Preliminary geologic and soils report
2. Overall drainage plan
3. Conceptual median and parkway landscaping plan including master signage program

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4. The proposed uses and intensity of square footage and phasing within a planning area, including a conceptual site plan illustrating subparcels with a planning area and the overall external/internal circulation and vehicular access plan.

Development Project Review

Following or concurrently with submittal of a planning area concept plan, an initial subdivision, or in the absence of the requirement for a concept plan, an individual development project will require submittal of an application to the Community Development Department pursuant to its application procedures and adopted fee schedule. Said application shall be reviewed subject to all site plan and design review requirements and submittal information required by the Tustin city code and Department of Community Development.

After acceptance of an application as complete, all development projects shall be reviewed by the Planning Commission as either permitted by right in a land use zone or as a discretionary project (ie., a land use or development project which requires a subdivision, conditional use permit or other special approval). All development projects require design review of the project by the Planning Commission and the Redevelopment Agency. The Planning Commission's action on the design review of a development project is a recommendation to the Tustin Redevelopment Agency who have final authority review for review of development projects within the South Central Redevelopment Project Area. The Planning Commission's action on discretionary approvals other than subdivisions, zone changes, zoning code amendments, and specific plan amendments are final unless appealed to the City Council.

The review process for minor plans not subject to the planning area concept plan process or the Planning Commission and Redevelopment Agency design review process or discretionary approval process, shall be subject to review and approval of the Department of Community Development. Staff is delegated authority to review individual signage, interior improvements, final construction drawings, and other minor improvements not considered a "development project."

Subdivisions

All divisions of land within the Pacific Center East project area shall be processed in accordance with the Tustin city code and state subdivision map act.

Variances, Conditional Permits, Use Determinations, Temporary Uses and Non-Conforming Uses

All applications shall be processed in accordance with the Tustin city code.

5.4 Minor Modifications/Adjustments

The Director of Community Development or designee may take action on requests for minor modifications or adjustments to "site development standards" contained in the specific plan when such requests constitute a reasonable use of property not permissible under a strict literal interpretation of the regulations. Administrative adjustments are intended for special circumstances of hardship and are not intended for uniform application throughout.

Minor modifications/adjustments may allow the following:

- ▶ A decrease of not more than five percent in the minimum required building site area.
- ▶ A decrease of not more than ten percent in the minimum required building or landscaping setbacks.
- ▶ An increase of not more than twenty percent in the maximum permitted height of a fence or wall, subject to city-approved structural design.
- ▶ An increase of not more than ten percent of the permitted projection of steps, stairways, landings, eaves, overhangs, masonry chimneys, and fireplaces into any required building setback.
- ▶ An increase of not more than five percent in the maximum permitted building height.
- ▶ An increase of not more than ten percent in the maximum height or area of allowable signage.
- ▶ A decrease in the number of required off-street parking spaces for uses up to a maximum of ten percent, not to exceed ten spaces. Requests for larger than a ten-space reduction may be granted adjustments commensurate with a Planning Commission approved transportation management plan or shared parking plan prepared by a qualified traffic engineer. The Director and/or the Planning Commission shall include findings in its determination addressing the following:
 - a. The intent of the parking regulations is preserved.
 - b. The parking provided will be sufficient to serve the use intended and potential future uses of the same site.
 - c. The adjusted decrease in the number of parking spaces shall not be detrimental to the public health, safety or general welfare or materially injurious to properties located in the general vicinity.

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Applications

- A. An application for an adjustment shall be filed with the Community Development Department pursuant to its current application procedures and fee schedule.
- B. The Director of Community Development or designee shall conduct a public meeting to review the application, statement and drawings submitted. At the meeting, the Director or designee shall act on the application and may approve, modify or deny said application.
- C. In granting an adjustment, the Director of Community Development or designee shall make findings in fact as prescribed by current law for the granting of a variance by the Planning Commission. In addition, the Director shall find:
 - 1. That there are practical difficulties or benefits of improved design which justify the deviation.
 - 2. The adjustment, where applicable, shall provide equal or greater benefit to adjacent property.
 - 3. The adjustment is not in conflict with objectives or the general intent of the specific plan.
- D. If the Director of Community Development or designee denies an application for an adjustment, or if the applicant disagrees with the conditions imposed on the granting of an adjustment, if any, the applicant may file for a variance in accordance with the procedures prescribed in the Tustin city code.

5.5 Specific Plan Amendment

Any amendment to the Pacific Center East Specific Plan which: 1) changes any property from one land use designation to another, 2) imposes any new regulation upon property not therefore imposed, or, 3) removes or modifies any such regulation upon property not therefore imposed, may be initiated and adopted on a planning area or land use designation basis only.

Amendments to the Pacific Center East Specific Plan shall be initiated and processed in the same manner set forth by the Tustin city code for amending the city zoning ordinance. However, only the planning area for amendment shall be reviewed in the amendment and subject to the public hearing process.

Whenever any portion of the Pacific Center East Specific Plan specifies that a public hearing be held prior to a decision on a particular application, notice of such hearing shall be given as provided by the Tustin city code for "Notices of Hearing" of the zoning ordinance of the City of Tustin.

5.6 Enforcement

This specific plan has been adopted by ordinance and is therefore subject to penalty provisions of the city municipal code. Specifically, violations of land use, development standards or performance standards shall be subject to Penalty Provisions and Citation Procedures of the Tustin city code in addition to the city's authority to seek civil litigation in a court of law.

Subdivision map and development plan conditions imposed pursuant to city code shall also be subject to penalty provisions and citation procedures of the city municipal code.

5.7 General Notes

1. Where required, approval shall be obtained from the Air Quality Management District of any plans, devices or facilities for control of air pollutants and any Transportation Demand Management program(s).
2. Energy conservation provisions shall be considered when building orientation, materials and design are developed.
3. As a condition of any increment of a proposed development, any applicant shall install the necessary infrastructure improvement, as identified by the specific plan.
4. Whenever the regulations contained herein conflict with the regulations of the Tustin city code, the Pacific Center East Specific Plan shall take precedence. Any standard or regulation not covered by the specific plan shall be subject to regulations contained in the Tustin city code.
5. Businesses located within the Pacific Center East project area shall not be operated or maintained in a fashion detrimental to the public health, safety or welfare or materially injurious to properties or improvements in the immediate area.
6. Mitigation measures identified and adopted in the environmental impact report for the specific plan shall be incorporated by reference in the specific plan and as applicable.

5.8 Severability Clause

If any section, subsection, subdivision, sentence, clause, phrase, or portion of this specific plan, or the application thereof to any person or place, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this specific plan or its application to other persons or places. The City Council hereby declares that it would have adopted this

specific plan, and each section, subsection, subdivision, sentence, clause, phrase or portion thereof, irrespective of the fact that any one or more sections, subsections, subdivisions, sentences, clauses, phrases, or portions, or application thereof to any person or place, be declare invalid or unconstitutional.

5.9 CONSISTENCY WITH THE GENERAL PLAN

The Pacific Center East Specific Plan implements each of the elements of the City of Tustin General Plan. As such, the specific plan is supportive of and consistent with the existing goals and policies of the general plan. The following presents a discussion of the relationship of this specific plan to each of the general plan elements.

Land Use Element

The Land Use Element (LUE) has established nine major land use policies for the purpose of guiding the land uses within the City of Tustin. Generally stated, the major goals of the LUE that relate to the Pacific Center East Specific Plan are presented below.

- ▶ Promote an economically balanced community with complementary and buffered land uses to include industrial, commercial, professional, multifamily, and single family development.
- ▶ Establish a city identity as a professional office building area, providing quality structures with design amenities.

This specific plan provides for a development that has an appropriate mix of land uses and provides an important element toward achieving an overall balance of land uses within the city. This project provides new high quality office, commercial, hotel, and industrial development, promotes the city's identity as a professional office building area, and offers an opportunity for community betterment.

The project site is currently designated Industrial in the current Land Use Element. The project will require a general plan amendment to designated the area as Planned Commercial Mixed Use, with a specific plan overlay. Consideration of the general plan amendment will occur simultaneously with review of the specific plan.

Housing Element

The Housing Element contains an assessment of housing needs, descriptions of the city's goals and policies for development and improvement of housing and an implementation program. The Housing Element includes a goal of adding 2,085 units between 1989-1994.

Some proportion of future employees of the project will reside within the city and others will seek housing opportunities within the city. The project is evaluated for its conformity to job/housing policies in the Draft EIR and mitigations are proposed to reduce vehicle trips and miles consistent with state implementation plan/SCAG policies.

Open Space and Conservation Element

The Open Space and Conservation Element recognizes the loss of natural resources and prime agricultural land through the expansion of urbanized areas. Goals with policies are adopted to mitigate against total loss of these resources and to preserve those that are unique and irreplaceable. Goals that relate to the specific plan include:

- ▶ A variety of open space areas will accommodate diverse needs related to recreation, noise attenuation and visual amenities for Tustin residents.
- ▶ A maximum feasible amount and utility of open space area will have minimal fiscal impact upon community resources for acquisition, development, and maintenance of the areas.
- ▶ Accommodation of health, safety, and utility needs of present and future residents of the community.

The Pacific Center East Specific Plan implements the goals of the Open Space and Conservation Element. Open space is provided through landscape setbacks, freeway edge setbacks, and intersection treatments. The project also encourages courtyards and plaza as public gathering areas. These spaces will provide opportunities for city residents and employees to enjoy outdoor areas within an urban setting. The plazas and setback areas will be owned and maintained by landowners, thus avoiding a fiscal impact to the city. The specific plan will result in the development of agricultural land. However, agricultural use is an interim use within the project area.

Noise Element

The City of Tustin Noise Element identifies major noise sources, noise levels, and programs to reduce current levels as well as measures to avoid creation of future noise hazards.

The Pacific Center East Specific Plan is not considered a noise-sensitive land use. Major sources of noise to the project include traffic on surrounding roadways (including the Newport Freeway), the railway line north of Edinger Avenue, helicopters from the adjacent Marine Corps Air Station and aircraft using John Wayne Airport.

According to the Noise Element, portions of the project are located within the 65 dB CNEL. Acoustical design features that satisfy the interior noise standards of the city will be incorporated into the final building plans for the project. Plazas and public gathering plazas will be located outside areas where the CNEL is above 65 dB.

Circulation Element

The Circulation Element identifies traffic deficiencies and presents general guidelines for the improvement of transportation facilities within the jurisdiction of the City of Tustin.

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Road improvements and changes in traffic resulting from the specific plan are consistent with the guidelines set forth in the Circulation Element.

Red Hill Avenue and Edinger Avenue are designated as major arterials on the city arterial highway plan. Valencia Avenue is designated a secondary and the Newport Avenue extension is listed as a secondary up to Edinger Avenue. However, future traffic volumes indicate that a major or modified major arterial designation is appropriate for Newport Avenue through the project site. The city will process a general plan Circulation Element amendment. Del Amo Avenue will receive a secondary arterial designation, both the Newport Avenue extension and Newport Avenue (now a primary) will become major arterials.

The Circulation Element includes general standards for scenic amenities along state freeways. The typical landscape treatment in industrial areas is stated "as set forth for Steelcase Tract and the Fesco Tract." Field observation of the Steelcase facilities indicates the main office is approximately 39 feet from the SR-55 right-of-way and one parking area is 28 feet from the SR-55 right-of-way. Turf, hedges, vines, London plane trees and purple leaf plum comprise the landscape treatment.

The specific plan proposes a 20-foot landscape setback along SR-55, which may be reduced to 6-15 feet in Planning Areas 1, 5. The required landscape setback from SR-55 is 35 feet in Planning Area 10 and 20 feet in Planning Areas 1, 5. The landscape concept for the SR-55 edge is shown in Exhibit 9k. The proposed setbacks are generally consistent with the Steelcase property.

The Master Plan of Tustin Bikeways includes a city trail along Newport Avenue north of Edinger Avenue. The project proposes to include a bikeway on Newport Avenue southerly to Valencia. Red Hill Avenue is currently designated as a bikeway but no improvements are recommended for this street at this time.

Scenic Highway Element

This element provides an official guide for the identification of scenic amenities within the city. Several objectives relevant to the project are:

- ▶ The creation of landscaped major arterials with median islands on Newport, Red Hill and Edinger Avenues.
- ▶ The buffering of freeway frontages by walls, berms, and landscaping.
- ▶ The eradication of billboards and the prohibition of pole signs along freeways.
- ▶ The incorporation of bicycle lanes and landscape areas along Newport Avenue and Red Hill Avenue.

A bikeway is proposed along Newport Avenue. The landscape plan includes landscaping for all project arterials.

Seismic Safety Element

Public safety policies established in the Seismic Safety Element of the general plan address relevant safety measures with regard to potential earthquake hazards, geologic hazard/slope stability, and emergency preparedness.

The Pacific Center East Specific Plan incorporates measures necessary for conformance with the Seismic Safety Element. Measures required by the city to minimize potential seismic hazards by requiring all design and construction of future buildings to comply with the Uniform Building Code. The potential for other geologic and/or slope stability hazards is considered remote; however, full geotechnical investigations were conducted onsite.

Safety Element

The primary objective of the Safety Element is to require that hazard issues, such as geologic hazards, flood hazards, fire, crime and hazardous and toxic materials be accounted for in city planning programs.

Adequate police, fire and flood control protection will be provided to the Pacific Center East project. As presented under the Seismic Safety Element discussion, all buildings shall conform to the Uniform Building Code. The Fire Department will enforce city regulations for the storage and use of toxic materials, which may be used in the Technology Center.

Orange County Master Plan of Arterial Highways

The Orange County Master Plan of Arterial Highways (MPAH) designates roadway alignments and street sections within the county. Since the extension of Newport Avenue and Del Amo Avenue are not included in the MPAH, a circulation MPAH amendment is required for these roadways to be eligible for county roadway funds.